

# Level 2 Categorical Exclusion Evaluation

SR0080 Section 08B

## I-80 Over Lehigh River Bridges Project

*White Haven Borough, Luzerne County*

*East Side Borough and Kidder Township, Carbon County*

October 2022

Prepared by:  
District 5-0  
1002 Hamilton Street  
Allentown, PA 18101



LEVEL 2 CATEGORICAL EXCLUSION EVALUATION  
for the  
SR 0080 SECTION 08B LUZERNE AND CARBON COUNTY  
INTERSTATE 80 LEHIGH RIVER BRIDGES PROJECT

MPMS #99552

Prepared by:  
US Department of Transportation  
Federal Highway Administration  
and  
Pennsylvania Department of Transportation  
Engineering District 5-0

Pursuant to 42 U.S.C. 4332(2)(c) and, as applicable:  
Executive Order 11990, Protection of Wetlands; Executive Order 11988, Floodplain Management;  
Executive Order 12898, Environmental Justice; and 49 U.S.C. Section 303(c), Section 4(f)



## Level 2 CE Approval

As supported by the attached Categorical Exclusion, this project qualifies for a Level 2 Categorical Exclusion in accordance with 23 CFR 771.117(d), Item Number 13. Furthermore, the project will not result in any of the four circumstances cited in 23 CFR 771.117(b).

**County:** Luzerne & Carbon **SR/Sec:** 0080/08B **MPMS:** 99552 **Project:** I-80 Lehigh River Bridges

**Prepared By:** Diane Nulton, HDR

**Title:** Senior Environmental Project Manager **Date:** 10/14/2022

**Approved By:**

JONATHAN P CRUM

Digitally signed by JONATHAN P CRUM  
Date: 2022.10.19 10:51:51 -04'00'

**Title:**

---

The following individuals concurred with the statement above.

**Environmental Manager:** *Drew Ames*

**Date:** 10/14/22

**Bureau Director:** *Christine Norris*

**Date:** 10/14/22

**HDTs: Eastern Region:** *Brian Shunk*

Digitally signed by Brian Shunk  
Date: 2022.10.17 14:10:45 -04'00'

**Date:** 10/17/22

Table of Contents

Level 2 CE Approval..... ii

ACRONYMS AND ABBREVIATIONS..... v

1.0 INTRODUCTION..... 1

2.0 I-80 OVER LEHIGH RIVER BRIDGES PROJECT OVERVIEW ..... 3

2.1 Project Bridges..... 3

2.2 Project Purpose and Needs ..... 3

2.3 Project Setting and Distinct Project Features..... 3

3.0 ALTERNATIVES ..... 6

3.1 No-Build Alternative ..... 6

3.2 Proposed Action ..... 6

3.3 Impact Summary Table..... 8

4.0 AFFECTED ENVIRONMENT AND ENVIRONMENTAL CONSEQUENCES ..... 18

4.1 Aquatic Resources ..... 18

4.2 Land ..... 24

4.3 Wildlife..... 28

4.4 Cultural Resources ..... 29

4.5 Section 4(f) Resources ..... 31

4.6 Air Quality and Noise ..... 34

4.7 Socioeconomic Areas..... 34

4.8 Permits Checklist ..... 37

5.0 PUBLIC INVOLVEMENT ..... 38

6.0 ENVIRONMENTAL JUSTICE..... 43

7.0 ENVIRONMENTAL COMMITMENTS AND MITIGATION..... 45

Figures

Figure 1 – Project Location Map.....5

Tables

Table 1 – Construction Station and Length .....7

Table 2 – Impact Summary Table .....8

Table 3 – Public Outreach Activities for Public Meeting .....40

Table 4 – Public Outreach Activities for the EA .....41

Table 5 – Public Outreach Activities for the CE .....42

Appendices

Appendix A – Engineering Information

Appendix B – Project Design Exhibit

Appendix C – Preliminary Design Plans

Appendix D – Agency Coordination

Appendix E – Threatened and Endangered Species

Appendix F – Section 4(f) Evaluation

Appendix G – Bicycle and Pedestrian Checklist

Appendix H – List of Preparers

Appendix I – References

## ACRONYMS AND ABBREVIATIONS

ALPP	Agricultural Land Preservation Policy
AOC	Areas of Concern
APE	Area of Potential Effect
ATON	Aids to Navigation
BMPs	Best Management Practices
CE	Categorical Exclusion
CFR	Code of Federal Regulations
CRPs	Cultural Resource Professionals
CWF	Cold Water Fishes
DCNR	Department of Conservation & Natural Resources
DEP	Department of Environmental Protection
E&S	Erosion & Sedimentation
EA	Environmental Assessment
EB	Eastbound
ECMTS	Environmental Commitments & Mitigation Tracking System
EDD	Environmental Due Diligence
ESA	Environmental Site Assessment
ESF	Environmental Stewardship Fund Act
EV	Exceptional Value
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FIRM	Flood Insurance Rate Map
FPPA	Farmland Protection Policy Act
GG2	Growing Greener Bond Fund
H&H	Hydrology and Hydraulics
HOV	High Occupancy Vehicle
HQ	High Quality
HQ-CWF	High Quality-Cold Water Fishes
I-80	Interstate 80
I-81	Interstate 81
I-476	Interstate 476

Key 93	Keystone Recreation, Park and Conservation Fund
LOD	Limits of Disturbance
LWCF	Land and Water Conservation Fund
MF	Migratory Fishes
MIT	Massachusetts Institute of Technology
MPO	Metropolitan Planning Organization
MSATs	Mobile Source Air Toxics
NAC	Noise Abatement Criteria
NAAQS	National Ambient Air Quality Standards
NEPA	National Environmental Policy Act
NESHAP	National Emission Standards for Hazardous Air Pollutants
NPDES	National Pollutant Discharge Elimination System
NPS	National Park Service
NRCS	Natural Resources Conservation Service
NRHP	The National Register of Historic Places
OSHA	Occupational Safety and Health Administration
P3	Public Private Partnership
PA	Pennsylvania
PA DCNR	Pennsylvania Department of Conservation and Natural Resources
PADEP	Pennsylvania Department of Environmental Protection
PAGWIS	Pennsylvania Groundwater Information System
PASPGP	Pennsylvania State Programmatic General Permit
PCSM	Post Construction Stormwater Management
PEL	Alternative Funding Planning and Environmental Linkages Study
PEM	Palustrine Emergent
PennDOT	Pennsylvania Department of Transportation
PFBC	Pennsylvania Fish and Boat Commission
PFO	Palustrine Forested
PNDI	Pennsylvania Natural Diversity Inventory
PSA	Project Study Area
PSS	Palustrine Scrub Shrub
RFFAs	Reasonably Foreseeable Future Actions

RIRA	Recreational Improvement and Rehabilitation Act
ROW	Right-of-Way
SMP	Soil Management Plan
SR	State Route
TCE	Temporary Construction Easement
TIP	Transportation Improvement Program
TNM	Traffic Noise Model
TSF	Trout Stocked Fishes
USACE	United States Army Corps of Engineers
USDA	United States Department of Agriculture
USDOT	United States Department of Transportation
USFWS	United States Fish and Wildlife Service
USGS	United States Geological Survey
UST	Underground Storage Tank
UNT	Unnamed Tributary
VMT	Vehicle Miles Traveled
WB	Westbound
WSE	Water Surface Elevation
WWF	Warm Water Fishes

## 1.0 INTRODUCTION

This Level 2 Categorical Exclusion (CE) Evaluation has been prepared to replace the Environmental Assessment (EA) previously made available on May 3, 2022, because PennDOT is no longer going to toll the Interstate 80 (I-80) Lehigh River Bridges. This CE compares the effects of the Build Alternative without tolling to the No Build (or do nothing) Alternative.

### Supporting documentation for Chapter 1 includes:

- *Alternative Funding: Planning and Environmental Linkages Study* (September 2021)

### Project History

In fall of 2020, PennDOT began a statewide Planning and Environmental Linkages (PEL) study to identify potential funding options to fill an \$8.1 billion (and growing) funding gap for maintaining and improving the State's highways and bridges. The *Alternative Funding PEL Study* identified near-term and long-term potential funding solutions that could be implemented. Tolling major bridges and using the toll money to cover the costs of rehabilitating or replacing and maintaining the bridge over a period of time was identified as a near-term solution that could be implemented relatively quickly. In February 2021, PennDOT identified nine candidate bridges for tolling, one of which was the I-80 Lehigh River Bridges project.

Upon identification as a candidate bridge, the effects of tolling the I-80 Lehigh River bridges were evaluated, including: effects on low-income persons using the bridges, effects associated with constructing toll equipment, and effects associated with people choosing to divert onto local roadways to avoid paying the toll. A low-income program was adopted to off-set effects on low-income persons and improvements along diversion routes were incorporated into the project to off-set the effects on local roadways. Diversion route improvements included:

- Prohibit 102" twins and trailers over 28.5 feet along SR 940 from SR 534 to I-80 in White Haven, similar to the existing restriction on SR 437, north of SR 940.
- Review the existing speed limit signage along SR 940 between the I-476 interchange and the Lehigh River, and supplement in appropriate locations, provide paint striping and radar speed signs emphasizing speed limit.
- Improve the signage on eastbound SR 940 approaching East Side Borough to clarify that vehicles continuing east on SR 940 must stay to the left and that trucks are prohibited on Bridge Street.
- Install a flashing beacon for crossing SR 940 at Main Street to improve the visibility of the crosswalk and repaint crosswalk at the existing crossing of the D&L Trail at SR 940.
- Install curb bulb outs along SR 940 at both Main Street and Towanda Street in order to reduce the width of roadway that needs to be crossed by pedestrians and improve the visibility of people waiting to cross SR 940.
- Mill and repave SR 940 (Berwick Street) between Church Street and Towanda Street, including base repairs at the multiple areas of significant pavement shoving/rutting.
- Improve the overall vertical alignment of SR 940 in the immediate area of the railroad tracks in White Haven to minimize some of the steep grade differences.
- Shift the double yellow lane line, on the westbound SR 940 approach to the intersection with SR 437 by approximately 5 feet to the north, to allow the northbound right-turn movement to occur without encroachment into the adjacent travel lane.
- Mill the existing pavement and repave the intersection of SR 940 and SR 437, to reestablish the curb line and update the ADA ramps at the intersection to meet the new pavement elevation.

- In order to calm traffic traveling on SR 940, east of the Lehigh River, reduce the lane width along SR 940, between Springhill Road and Centre Street, from 11 feet to 10 feet through modified paint striping and the addition of 3 center islands to emphasize the cross-section change and install a gateway treatment at the eastern end of this corridor.
- Modify the existing pavement striping on SR 534 at SR 940 to bring left-turning SR 534 vehicles closer to a 90-degree angle at the intersection.
- Repair/replace the gabion retaining wall, adjacent pavement, and guide rail along SR 940 along the curve to the east of the Lehigh River.

An EA comparing the effects of the No Build Alternative and the Build Alternative with bridge tolling was prepared and was made available for official public review and comment on May 3, 2022. A Public Hearing was scheduled to be held on May 18, 2022.

On May 18, 2022, as a result of a lawsuit, the court issued an injunction and all work related to the Major Bridge Public Private Partnership (MBP3) initiative ceased, so the May 18, 2022 public hearing was cancelled. Other litigation resulted in a ruling on the viability of the MBP3 as a Public-Private Transportation Project (P3). Subsequently, Act 84 of 2022 amended the P3 law and revoked PennDOT's ability to implement mandatory tolls such as the proposed bridge tolling under the MBP3, but preserved the contract resulting from the MBP3.

As a result of the lawsuits and the subsequent enactment of Act 84 of 2022, **PennDOT is moving the I-80 Lehigh River Bridges project forward, but without tolling. Since tolling will not be initiated, diversion of traffic onto local roads to avoid the tolls will not occur; therefore, the proposed improvements along the diversion routes will no longer be included in the project.**

The PennDOT MBP3 was established to accelerate the replacement or rehabilitation of major bridges. Under MBP3, PennDOT entered into an agreement with a Development Entity to design, build, finance, and maintain (DBFM) a "package" (or group) of PennDOT bridges – including the I-80 Lehigh River Bridges. PennDOT will repay the amounts financed by the Development Entity through recurring availability payments over 30 years. Act 84 of 2022 authorizes the bridges identified in the MBP3 to be carried out via DBFM by the Development Entity, without mandatory tolling.

Funding to make the availability payments will consist of a blend of federal and state funds that could have been used on other projects. PennDOT will take advantage of additional funding opportunities arising out of the federal Infrastructure Investment Jobs Act ("IIJA"), also known as the Bipartisan Infrastructure Law ("BIL") and potentially supplemented by funds that are currently included in the outer years of the Twelve Year Program (TYP) or by the deferral or elimination of some other (TYP) projects. NOTE: IIJA (BIL) funding was not available at the start of MBP3, but those additional funding sources will provide additional opportunities for PennDOT to pursue the Build Alternative without tolling with less effect to other projects.

**This CE documents and compares the effects associated with the No Build Alternative and the Build Alternative without tolling.** Effects associated with constructing tolling equipment, improving diversion routes, and paying tolls have been removed from the document.

The comments received during the EA comment period (May 3 to June 2, 2022) have been reviewed and considered. The overwhelming majority of comments received during the EA comment period were related to tolling and diversion of traffic and are no longer applicable to the project since tolling is no longer being implemented. Comments received on the EA relevant to the project without tolling were considered and additional information incorporated into the respective sections within this document as appropriate.



## 2.0 I-80 OVER LEHIGH RIVER BRIDGES PROJECT OVERVIEW

### 2.1 Project Bridges

PennDOT, in cooperation with the Federal Highway Administration (FHWA), is advancing the replacement of two bridges on I-80, Section 08B. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and State Route (SR) 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The anticipated project limits are approximately 1.5 miles along I-80 and are bound between the White Haven interchange, Exit 273, to the west and the Hickory Run State Park interchange, Exit 274, to the east (mile marker 272.5 to mile marker 273.3). **Figure 1 – Project Location Map** shows the location of the I-80 Lehigh River bridges and the Project Study Area (PSA).

### 2.2 Project Purpose and Needs

**Purpose:** The purpose of this project is to address the deterioration of the I-80 Lehigh River Bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and local residents.

**Needs:**

- The existing I-80 dual bridges are more than 50 years old. The bridges, originally constructed in 1965, have nonredundant critical elements, show deterioration, and are approaching the end of their service life. The bridges underwent a major rehabilitation in 1981 and 1982 for westbound and eastbound, respectively. Maintenance and repairs were completed on the bridges in 1991 and again in 2014. Due to the age and condition of the bridges, they require maintenance and repairs at increased frequency. The most recent significant rehabilitation of the bridges occurred over 35 years ago in 1981/1982, and the bridges require rehabilitation or replacement in the near future.
- I-80 provides a critical link in the Interstate commerce system, and as such is vital to efficient movement of goods and people. Further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

Based on the 2018 bridge inspections, the superstructures and substructures of both I-80 bridges received condition ratings of “5” on a scale of 0 to 9, which indicates the bridges are in “Fair” condition. The 2014 inspection of the eastbound bridge determined that the substructure was in a condition state of “4” or “Poor.” Subsequent repairs were performed to restore the substructure condition to “5” or “Fair.” A condition state of “4” or “Poor,” if not addressed, would require the use of a Safe Load Capacity Reduction Factor of 0.8 which would reduce the operating load ratings by 20 percent. This reduction in the load rating would require the eastbound bridge to be load posted. Load posting of the interstate bridge would negatively affect commerce and connectivity in the region and is not an acceptable outcome.

### 2.3 Project Setting and Distinct Project Features

The general site topography is forested rural woodlands with rolling terrain. The project is bounded by the White Haven Interchange and the Borough of White Haven to the west, and East Side Borough and the Hickory Run State Park Interchange to the east. The project is surrounded by a mix of residential, commercial, and public use (Sections 6(f) and 4(f)) properties. The public use properties consist of the White Haven Borough owned Lehigh Park in the northwest quadrant and the Commonwealth-owned Lehigh Gorge State Park which encompasses the

Lehigh River and the Lehigh Gorge Trail. The Lehigh River is a State Scenic River and a PA Fish and Boat Commission (PFBC) designated water trail. Near the western extent of the project, I-80 is separated by a grass median. The eastern extent of the project contains a grass median near the existing bridges which transitions to a forested median near the limit of work.

**Describe the involvement with utilities with this project:**

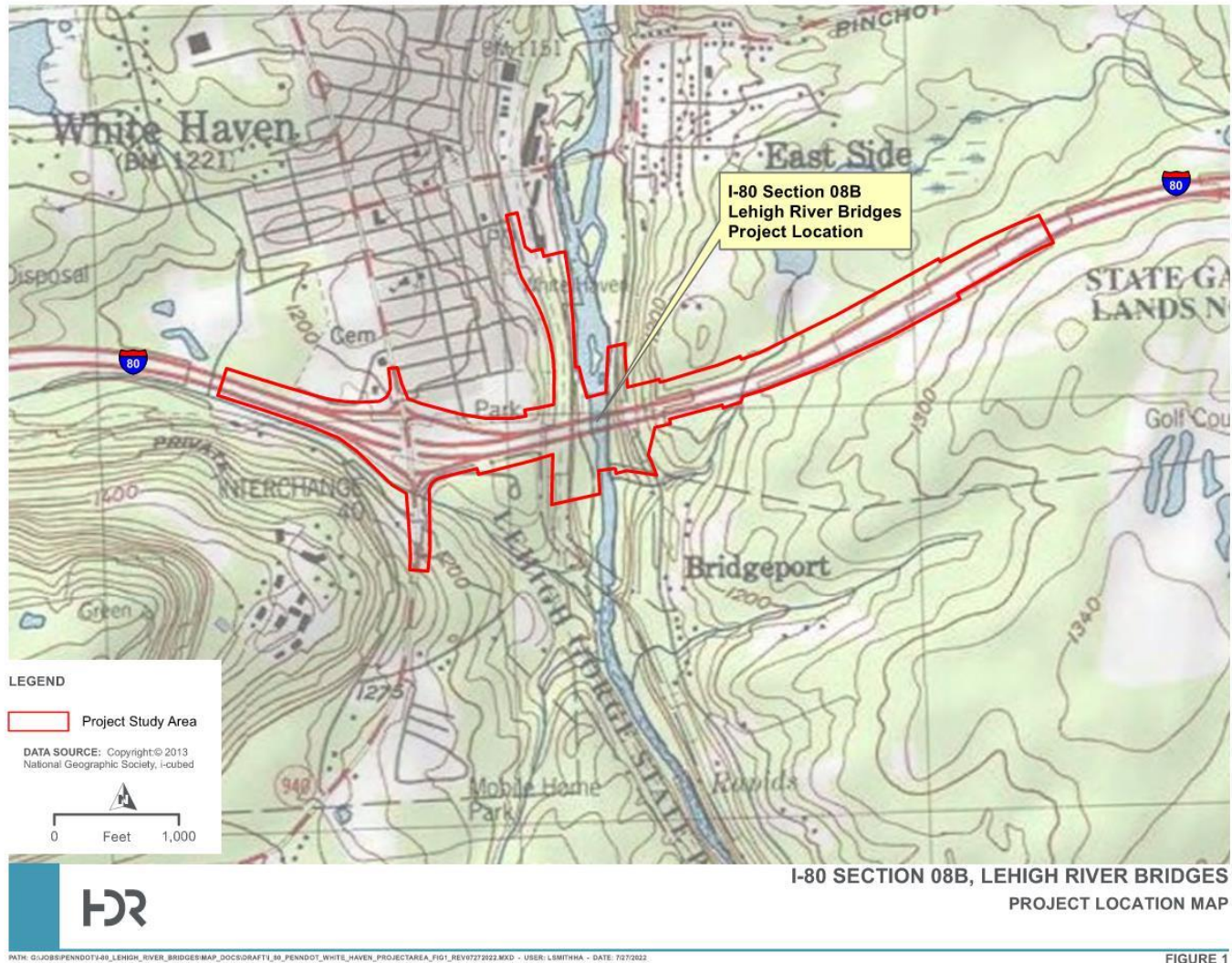
No aerial utilities are present along I-80. Aerial utilities are present along SR 1005 underneath the I-80 structures and along SR 940. Underground utilities have not been identified within the project limits.

**Describe the involvement with any railroad (active or inactive) including all rail lines, crossings, bridges, or signals:**

The bridges carry I-80 over the active Reading Blue Mountain and Northern Railroad line. Coordination with the railroad is required.

**Describe changes to access control:**

No changes to access control are needed.



## 3.0 ALTERNATIVES

### 3.1 No-Build Alternative

Under the no-build alternative, regular maintenance would be assumed to occur. This alternative would fail to address other project needs such as addressing the identified nonredundant critical elements and bridge deterioration. The I-80 Lehigh River Bridges are nearing the end of their useful life. Currently, both the EB and WB bridges are in fair condition and have cracks and delaminated concrete on decks and barriers, rust and section loss in steel superstructure elements, rust, and section loss in floorbeams, and delaminated concrete with exposed reinforcement in abutments, piers, and wingwalls. Without replacement, these bridge structures will need more frequent maintenance and repairs. However, such maintenance can only extend the service life of these bridges for so long before they are at risk of failure.

I-80 is the longest east-west interstate in the Commonwealth of Pennsylvania. Within Pennsylvania, I-80 extends 311 miles across the northern tier of Pennsylvania, providing access to New Jersey, the New York City Metropolitan Area and New England to the east and Ohio and Midwestern states to the west. In the project area, the I-80 corridor is a vital link between two north-south interstates, I-81 to the west and I-476 to the east and is critical for the movement of people and goods through the northern tier of Pennsylvania and beyond. As a critical link in the regional and national highway network, allowing the deterioration of these bridges to reach a level of failure is not reasonable; therefore, due to the project needs, the no-build alternative would not be a reasonable alternative.

The no-build alternative is presented in this CE as a baseline for comparison purposes only.

### 3.2 Proposed Action

The project is located in White Haven Borough, Luzerne County and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The eastbound bridge will be replaced on a new alignment immediately to the south of the existing eastbound bridge. The new eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge. At Exit 273 (SR 940), the eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert.

Two lanes of traffic in both directions will be maintained on I-80 during the majority of construction using temporary lane shifts and temporary cross overs. SR 1005 (River Road), which is also PA Bike Route L, will be closed and detoured during construction. A map of the detour route is included in Appendix A – Engineering Information.

Additional information is provided in Table 1 – Construction Station and Length, Appendix A – Engineering Information, Appendix B – Project Design Exhibit, and Appendix C – Preliminary Design Plans.

**Table 1**  
**Construction Station and Length**

<b>Limits of Work (Segment/Offset)</b>		<b>Construction Stations</b>	
<b>Start:</b>	<b>End:</b>	<b>Start:</b>	<b>End:</b>
2720/1542 EB	2730/1273 EB	1989+00 EB	2039+75 EB
2721/2330 WB	2731/2253 WB	2197+00 WB	2249+70 WB
<b>Total Length:</b>			
4,875 ft (0.92 mile) EB; 5,070 ft (0.96 mile) WB			

The bridge replacement and roadway work will impact 8 parcels with 2 parcels totally taken; 1 parcel with an aerial easement; 3 parcels with only temporary construction easements (TCEs); 1 parcel with an aerial easement, permanent slope easement, and TCEs; and 1 parcel with both partial takes and TCEs.

### 3.3 Impact Summary Table

**Table 2**  
**Impact Summary Table**

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
<b>Aquatic Resources</b>			
<b>Streams, Rivers, &amp; Watercourses</b>	No Impact	Streams: HQ-CWF, MF; stocked trout and wild trout  79 linear feet permanent impact 578 linear feet temporary impact	No work will be permitted in streams from February 15 to June 1 (stocked trout) and October 1 to December 31 (wild trout).  Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application.  Mitigation measures will be entered into the Environmental Commitments & Mitigation Tracking System (ECMTS).
<b>Wild &amp; Scenic Rivers and Streams</b>	No Impact	Pennsylvania Scenic River - 230 square feet permanent impact for piers - 500 linear feet (1.5 ac) temporary impact for causeways	-Vegetative buffers will be provided to the extent practicable. -Piers will not be allowed in the middle portion of the river. -Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting. -Where possible, riprap will be covered with soil and vegetation. -Scenic river signs will be placed at the

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
			leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users
<b>Navigable Waterways</b>	No Impact	<p>US Coast Guard Navigable, PFBC Water Trail, Recreational Boating Waterway</p> <ul style="list-style-type: none"> <li>- 230 square feet permanent impact for piers</li> <li>- 500 linear feet (1.5 ac) temporary impact for causeways</li> </ul> <p>The existing boat launch directly under the bridges will be closed during construction; however, three additional boat launches are being constructed by DCNR south of the project area and will remain open during I-80 bridge construction.</p>	<p>Aids to Navigation (ATON) Plan will be implemented during construction.</p> <p>Signs will be placed at boat launches upstream of the project.</p> <p>Advanced notification to PFBC, DCNR, and the public will be provided.</p>
<b>Groundwater</b>	Not Present	Not Present	None
<b>Wetlands</b>	No Impact	Wetlands: 0.11 acre permanent impact	<p>Through consultation with permitting agencies, mitigation details will be determined in Final Design and incorporated into the waterway permit application.</p> <p>Mitigation measures will be added to ECMTS.</p> <p>Protective fencing will be placed to prohibit contractor access in Wetland 2.</p>



Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
<b>Floodplains</b>	No Impact	No significant floodplain encroachment would occur.	None
<b>Soil Erosion and Sedimentation</b>	No Impact	<p>Erosion and Sediment (E&amp;S) Control Plan and Post Construction Stormwater Management Plan will be prepared.</p> <p>NPDES permit application will need to be submitted prior to construction.</p>	<ul style="list-style-type: none"> <li>- Best Management Practices (BMPs) will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.</li> <li>- The E&amp;S Control Plan will be reviewed by the Pennsylvania Department of Environmental Protection (PADEP) and coordination will be conducted to ensure the selected BMPs are adequate for the project.</li> <li>- The approved E&amp;S Control Plan will be implemented prior to any earth disturbance, during construction.</li> <li>- Installed BMPs will be inspected and maintained throughout the duration of construction.</li> <li>- All areas of earth disturbance will be stabilized immediately following completion of earthwork.</li> <li>- Post Construction Stormwater Management (PCSM) controls will be evaluated in final design and included in the National Pollutant Discharge Elimination System (NPDES) permit application, if required.</li> </ul>



<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
<b>Land Use</b>			
<b>Agricultural Resources</b>	No Impact	No active agricultural land Impacts to farmland soils are exempt from FPPA provisions.	None
<b>Vegetation</b>	No Impact	Forested land, riparian vegetation and roadside vegetation are present and will be impacted.	In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.
<b>Geologic Resources</b>	No Impact	Lehigh Gorge is an Outstanding Scenic Geological Feature of Pennsylvania, no impact.	None
<b>Parks and Recreation Facilities</b>	No Impact	Lehigh Gorge State Park – Section 6(f): approximately 6 acres	To comply with Section 6(f) of the Land and Water Conservation Fund (LWCF) Act, approximately 6 acres of replacement lands will be provided.

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
		<ul style="list-style-type: none"> <li>- 0.4 acre of Proposed Slope Easement</li> <li>- 0.5 acre Proposed Aerial Easements</li> <li>-4.9 acre of TCE within LGSP</li> </ul>	
<b>State Forest and Gamelands</b>	Not Present	Not Present	None
<b>Wilderness, Natural, &amp; Wild Areas</b>	Not Present	Not Present	None
<b>Hazardous or Residual Waste Sites</b>	No Impact	Phase I, II, and III Environmental Site Assessment (ESA) have been completed and identified 2 potential sites.	<p>A Soil Management Plan (SMP) will be prepared in final design, and recommendations will be incorporated into the construction contract, as appropriate.</p> <p>If excavated historic fill material cannot be reused within the project ROW, it will be characterized for offsite disposal in accordance with applicable local, state, and federal regulations.</p>
<b>Wildlife</b>			
<b>Wildlife Refuges &amp; Critical Habitat</b>	Not Present	Not Present	None
<b>Threatened &amp; Endangered Species</b>	No Impact	Potential impact to tri-colored bat	USFWS coordination for tri-colored bat: During final design, the project team will initiate conferencing with USFWS regarding the project's potential effects to the tri-

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			colored bat and measures to avoid and minimize harm.
Cultural Resources			
Archaeological Resources	Not Present	Not Present	None
Historic Resources	No Historic Properties Affected	No Historic Properties Affected	None
Section 4(f) Resources	No Impact	<p><u>Lehigh Gorge State Park – De Minimis Use</u></p> <p>- 0.4 acre of Proposed Slope Easement</p> <p><u>Lehigh River Water Trail / PA Scenic River – De Minimis Use</u></p> <p>- Closure of one of four boat launches during construction</p>	<p><u>Mitigation measures for Lehigh Gorge State Park will include:</u></p> <p>- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:</p> <ul style="list-style-type: none"> <li><i>April through October: Overnight closure of the trail. Short-term closure of the trail during daylight hours Monday through Friday.</i></li> <li><i>November through March: Long-term closure of the trail for certain construction activities.</i></li> <li><i>Advance notice of trail closures will be provided.</i></li> <li><i>The contractor may temporarily realign the trail through the project site. A 10' wide trail will be maintained except as needed for closures.</i></li> </ul> <p>- If necessary due to pier locations, reconfiguration of the driveway, parking, or</p>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			<p>boat launches will be coordinated with DCNR.</p> <p>- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification will be provided.</p> <p>- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. A planting plan will be prepared during Final Design.</p> <p>- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.</p> <p><u>Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:</u></p> <p>- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.</p> <ul style="list-style-type: none"> <li>• <i>Temporary closures of boat traffic will be allowed for certain activities.</i></li> <li>• <i>If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.</i></li> </ul>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			<ul style="list-style-type: none"> <li><i>The contractor will restore disturbed areas.</i></li> </ul> <p>To mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions:</p> <ul style="list-style-type: none"> <li><i>Vegetative buffers for construction staging areas will be provided to the extent practicable.</i></li> <li><i>Piers will not be allowed in the middle portion of the river.</i></li> <li><i>Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.</i></li> <li><i>Where possible, riprap will be covered with soil and vegetation.</i></li> <li><i>Scenic river signs will be placed at the leading ends of each bridge. Signs will be placed on the bridge and/or piers for water trail users.</i></li> </ul>
Air Quality and Noise			
Air Quality	No Impact	Exempt; no impact	None
Noise	No Impact	Type III Project; noise analysis not required	None
Socioeconomic Areas			

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
<b>Regional &amp; Community Growth</b>	No Impact	No Impact	None
<b>Public Facilities &amp; Services</b>	No Impact	<p>Positive Impacts: Access for public facilities and services will be improved due to design improvements resulting from the project.</p> <p>Temporary Impact: SR 1005 (River Road) will be detoured during construction.</p>	The P3 Development Entity will coordinate with Weatherly School District officials and maintain school bus access on SR 1005 (River Road), including turnaround area(s) if needed.
<b>Community Cohesion</b>	No Impact	No impact	None
<b>ROW Acquisitions</b>	No Impact	<p>8 parcels impacted:</p> <ul style="list-style-type: none"> <li>- 2 parcels totally taken;</li> <li>- 1 parcel with an aerial easement;</li> <li>- 3 parcels with only TCEs;</li> <li>- 1 parcel with an aerial easement, permanent slope easement and TCEs; and</li> <li>- 1 parcel with both partial takes and TCEs.</li> </ul>	<p>Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.</p> <ul style="list-style-type: none"> <li>• While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.</li> <li>• Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current</li> </ul>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			design plans, no displacements are anticipated.
Displacements	No Impact	No relocation of people, businesses or farms	None
Aesthetics	No Impact	No impact	None
Environmental Justice	No Impact	No disproportionately high and adverse effects on low-income or minority populations have been identified.	None

**Footnote:**

<sup>1</sup> While the No-Build Alternative would not directly affect resources, should the bridge deteriorate to the point where it would have to be weight-posted, closed, or should it experience a partial collapse, there would be impacts to the resources below the bridge. A full or partial closure would have a profound effect on commerce reliant on I-80 and would detour vehicles onto local roads.

## 4.0 AFFECTED ENVIRONMENT AND ENVIRONMENTAL CONSEQUENCES

### 4.1 Aquatic Resources

	PRESENCE	IMPACTS
<b>STREAMS, RIVERS &amp; WATERCOURSES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Intermittent (streams only)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Perennial	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Wild trout streams	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Stocked trout streams	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

#### Identify all streams and their classifications per Chapter 93 of 25 PA Code (e.g. CWF, WWF, HQ, EV)

Field investigations conducted November 12, 2018 to November 15, 2018, December 19, 2018 to December 20, 2018, January 16, 2019, March 13, 2019, and November 20, 2020 identified ten stream channels totaling 4.946 acres (4,665 linear feet) within the project study area. These streams included the Lehigh River, Linesville Creek, six Unnamed Tributaries (UNTs) to Lehigh River, and two UNTs to Linesville Creek.

The Lehigh River Basin, Tobyhanna Creek to Buck Mountain Creek, is listed in Pennsylvania Code Title 25, Chapter 93 Water Quality Standards, as High Quality-Cold Water Fishes, Migratory Fishes (HQ-CWF, MF). All tributaries within this section are within the basin and, therefore, also HQ-CWF, MF.

**Linear feet of Streams permanently impacted:** 79

#### Describe Any Permanent Impacts

Approximately 79 linear feet of permanent impacts are anticipated due to new pier locations and proposed embankment along the Lehigh River and extension of a culvert carrying a UNT to Lehigh River.

#### Describe Any Temporary Impacts

Approximately 578 linear feet of temporary impacts are anticipated due to causeways in the Lehigh River needed to construct the bridges and along one UNT to Lehigh River for placement of temporary bypass measures.

**Is mitigation incorporated?** ☐ No ☒ Yes

#### Mitigation Remarks

Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application. Mitigation measures will be entered into the Environmental Commitments & Mitigation Tracking System (ECMTS). ECMTS is a computer application for tracking mitigation commitments from inception during preliminary design through construction, to be used by construction inspectors to ensure mitigation measures are completed as intended for protection of environmental resources.

The Lehigh River is listed as a natural reproducing trout and stocked trout stream and Linesville Creek is listed as a natural reproducing trout stream. Construction work within these streams and their tributaries will be prohibited from February 15 to June 1 (stocked trout) and October 1 to December 31 (wild trout).

#### Supporting documentation for Chapter 4.1 includes:

- *I-80 Lehigh River Bridges Final Wetland ID and Delineation Report* (February 2021, revised February 2022)
- *I-80 Lehigh River Bridges H&H Study* (November 2021)
- *I-80 Lehigh River Bridges ATON Plan and PFBC approval* (August 2021)



	PRESENCE	IMPACTS
<b>FEDERAL WILD &amp; SCENIC RIVERS &amp; STREAMS</b>	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

#### Remarks

Review of the USGS Quadrangle and Federal Wild and Scenic Rivers System website has confirmed there are no Federal Wild and Scenic Rivers and Streams within the project area.

	PRESENCE	IMPACTS
<b>STATE SCENIC RIVERS &amp; STREAMS</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

#### Documentation

☒ DCNR Coordination Letter

#### Describe Any Permanent and Temporary Impacts

The stretch of the Lehigh River within the project study area is designated as a Pennsylvania Scenic River by Pennsylvania Department of Conservation and Natural Resources (DCNR), with the scenic designation from the Francis E. Walter Dam north of the project to Jim Thorpe to the south. This designation encompasses a 32-mile upper section of the Lehigh River and is intended to help preserve the primitive qualities, as well as the natural aesthetic values, of our rivers and adjacent land environment. Designated rivers shall be free-flowing and capable of supporting both aquatic life and water-based recreation.

The proposed replacement I-80 Lehigh River Bridges would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). The final determination of the pier locations will be dependent on the contractor's selection of bridge structure type.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years.

**Is mitigation incorporated?** ☐ No ☒ Yes

#### Describe Mitigation

Through coordination with PA DCNR, PennDOT agreed to incorporate the following mitigation measures into the project. The DCNR coordination letter is provided in Appendix D – Agency Coordination.

- Vegetative buffers will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.

**PRESENCE****IMPACTS****NAVIGABLE WATERWAYS**

Coast Guard Navigable

☐ Not Present ☒ Present☐ Not Present ☒ Present☒ No ☐ Yes

PFBC Water Trail

☐ Not Present ☒ Present☐ No ☒ Yes

Recreational Boating Waterway

☐ Not Present ☒ Present☐ No ☒ Yes**Documentation**☒ PFBC Aids to Navigation Plan☒ Coast Guard Coordination**Describe Any Permanent and Temporary Impacts**

The stretch of the Lehigh River within the project study area is a water trail designated by PFBC. Water based recreation, including recreational boating, is conducted along the 72-mile-long Lehigh River Water Trail, which emphasizes low-impact use and promotes stewardship of water resources. The 41-mile Northern section of the Lehigh River Water Trail begins at the White Haven boat access on the west shore of the Lehigh River underneath the I-80 bridges.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years.

The existing boat launch directly under the bridges will be closed during construction; however, three additional boat launches are being constructed by DCNR south of the project area and will remain open during I-80 bridge construction.

**Is mitigation incorporated?** ☐ No ☒ Yes

**Describe Mitigation**

A preliminary ATON Plan was prepared and submitted to PFBC for review. PFBC approved the ATON Plan on August 16, 2021. The ATON Plan and approval letter are in the project technical files. The ATON Plan shows the proposed causeways and staging, and sign locations in the vicinity of the bridges. Signs will also be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided. The P3 Development Entity will be responsible for implementation of the ATON plan during construction.

Detailed information regarding impacts and mitigation for the Lehigh River Water Trail is provided in the attached Section 4(f) Evaluation prepared for the Lehigh River Water Trail / PA Scenic River (see Chapter 4.5 of this CE).

**Remarks**

Based on the Navigable Waters of the US Section 10, the Lehigh River is navigable 72 miles from the confluence with the Delaware River to the downstream side of the PA Route 940 bridge, which includes the I-80 Lehigh Bridges project study area. PennDOT coordinated with the United States Coast Guard (USCG), which determined that a Coast Guard bridge permit will not be required for the proposed bridge replacements over the Lehigh River. The project was placed in the Coast Guard Authorization Act of 1982 exemption category, and this determination is valid for five years from the date of the USCG letter, November 18, 2021. If the construction

project does not commence within this time, USCG must be contacted for reaffirmation of this determination. The USCG Coordination letter is included in Appendix D – Agency Coordination.

---

**PRESENCE**

**OTHER SURFACE WATERS**

☒ Not Present ☐ Present

**Remarks**

Based on secondary source aerial views and field investigations, no other surface waters are present in the project area.

---

**PRESENCE**

**GROUNDWATER RESOURCES**

☒ Not Present ☐ Present

**Remarks**

There are no recorded wells in the project study area. According to PADEP's eMapPA online tool that uses PA Groundwater Information System data, there are 4 wells located within 0.1 miles to 0.3 miles of the project bridge.

---

**PRESENCE**

**IMPACTS**

**WETLANDS**

Open Water

☐ Not Present ☒ Present

☒ No ☐ Yes

Vegetated

☒ Not Present ☐ Present

Emergent

☒ Not Present ☐ Present

☒ No ☐ Yes

Scrub Shrub

☐ Not Present ☒ Present

☐ No ☒ Yes

Forested

☐ Not Present ☒ Present

☐ No ☒ Yes

Exceptional Value

☐ Not Present ☒ Present

☐ No ☒ Yes

**Documentation**

☐ Data Forms

☒ Wetland Identification and Delineation Report

☐ Conceptual Mitigation Plan

☐ 404 (b)(1) Alternative Analysis

☐ Jurisdictional Determination Functional

☐ Assessment Analysis

**Methodology**

Field investigations conducted November 12, 2018 to November 15, 2018, December 19, 2018 to December 20, 2018, January 16, 2019, March 13, 2019, and November 20, 2020 identified eight wetland areas, totaling 0.914 acres, within the project study area. Wetlands were classified as Palustrine Forested (PFO) and Palustrine Scrub Shrub (PSS) wetlands.

Field investigations were conducted in accordance with the methodology described in the United States Army Corps of Engineers (USACE Corp of Engineers Wetland Delineation Manual (Technical Report Y-81-1) and the USACE Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Eastern Mountains and Piedmont Region Version 2.0), PADEP, Chapter 105 regulations, Section 404 of the Federal Clean Water Act, and the Pennsylvania Department of Transportation (PennDOT) Publication No. 325.

The wetlands within the project study area are associated with a naturally reproducing trout water and PA

Scenic River, and therefore are designated as Exceptional Value (EV).

**Number of Wetlands permanently impacted:** 3

**Acreage of Wetlands permanently impacted:** 0.11

**Describe Any Permanent Impacts**

Approximately 0.11 acre of permanent impacts to wetlands are anticipated: 0.075 acre PFO wetlands (Wetland 1 and Wetland 7) and 0.035 PSS (Wetland 6). The wetlands are located in the Lehigh River floodplain and will be impacted due to crane placement and contractor access necessary to construct the bridges.

**Describe Any Temporary Impacts**

Temporary impacts to wetlands are not anticipated.

**Is mitigation incorporated?** ☐ No ☒ Yes

**Mitigation Remarks**

Wetland mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application. Possible mitigation for wetland impacts could include debiting credits from an approved wetland mitigation bank, performing wetland mitigation on-site, or working with the PADEP to pay a fee in lieu of other mitigation options. Mitigation measures will be entered into ECMTS. Protective fencing will be placed to prohibit contractor access in Wetland 2.

**Executive Order 11990 Compliance**

**Compliance requires the determination that there is no practicable alternative to the proposed construction in wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from such use.**

**Options/design modifications were investigated to avoid impacts to wetlands:** ☒ Yes ☐ No ☐ N/A

**There are no practicable alternatives to construction within the wetlands:** ☒ Yes ☐ No ☐ N/A

**Alternative chosen (proposed project) includes all practicable measures to minimize harm to wetlands:**  
☒ Yes ☐ No ☐ N/A

	PRESENCE	IMPACTS
<b>COASTAL ZONE</b>	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
<b>Remarks</b>	There are no coastal zones located within the project area.	

	PRESENCE	IMPACTS
<b>FLOODPLAINS</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
	<input checked="" type="checkbox"/> No significant floodplain encroachment would occur.	

**Describe Any Permanent and Temporary Impacts**

The Lehigh River is the borderline between Carbon and Luzerne counties. Both counties have Federal Emergency Management Agency (FEMA) studies for the Lehigh River. The project is located in a detailed FEMA study area for Luzerne County. The I-80 bridges over Lehigh River are in an area studied by detailed methods (Zone AE with floodway area) with published peak flows and base flood (100-year) elevations. For the Carbon County FEMA

study, the I-80 bridges are in an area studied by approximate methods (Zone A area). The UNT to Lehigh River in Carbon County is in FEMA Zone X, which is an area that was not studied by FEMA.

Based on preliminary Hydrology and Hydraulics (H&H) analysis, the proposed bridge replacement will not increase the risk of flooding on the Lehigh River upstream or downstream of the proposed bridges for the FEMA 100-year flood event. The proposed culvert extension under I-80 also will not increase the risk of flooding upstream or downstream of the culvert for the 100-year event.

The project will have no significant floodplain encroachment, as defined in 23 CFR Part 650, Subpart A, Section 650.105(q), since the project will not: 1. Have a significant potential for interruption or termination of a transportation facility which is needed for emergency vehicles or provides a community's only evacuation route, 2. Have a significant risk, 3. Have a significant adverse impact on natural and beneficial flood plain values.

**Is mitigation incorporated?**      ☒ No ☐ Yes

---

## **SOIL EROSION & SEDIMENTATION**

**Are there activities that could cause erosion or sedimentation and would require E&S Controls?**

☒ Yes ☐ No ☐ N/A

### **Documentation**

- ☒ Coordination w/County Conservation
- ☒ District E&S Control Plan
- ☒ NPDES Stormwater Construction Permit

**Is mitigation incorporated?**      ☐ No ☒ Yes

### **Mitigation**

Preliminary Erosion and Sedimentation (E&S) Control Plan and Post Construction Stormwater Management (PCSM) Plan have been prepared for the project and coordination with Luzerne and Carbon County Conservation Districts, PADEP, and USACE has been initiated. Final plans will be prepared during Final Design and will be incorporated into the construction contract. The P3 Development Entity will prepare and submit the NPDES permit application and secure necessary permit authorizations prior to construction.

- BMPs will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.
  - The E&S Control Plan will be reviewed by the PADEP and coordination will be conducted to ensure the selected BMPs are adequate for the project.
  - The approved E&S Control Plan will be implemented prior to any earth disturbance, during construction.
  - Installed BMPs will be inspected and maintained throughout the duration of construction.
  - All areas of earth disturbance will be stabilized immediately following completion of earthwork.
  - PCSM controls will be evaluated in final design and included in the NPDES permit application, if required.
-

## 4.2 Land

	PRESENCE	IMPACTS
<b>AGRICULTURAL RESOURCES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Productive Agricultural Land	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Security Areas	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Prime Agricultural Land	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Conservation Easement	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Farmland Enrolled in Preferential Tax Assessments	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Zoning	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Soil Capability Classes I, II, III, IV	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Prime or Unique Soil	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Statewide or Locally Important Soils	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

### Describe Any Permanent and Temporary Impacts

No impacts to agricultural resources are anticipated.

Is mitigation incorporated? ☒ No ☐ Yes

### Remarks

Examination of United States Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS) web soil survey mapping for the project area identified one Prime Farmland soil type and three Farmland of Statewide Importance soil types within the project study area. These soil classifications are protected under the Farmland Protection Policy Act (FPPA). However, bridge replacements are exempt from FPPA provisions as per Farmland Protection Policy Manual, 523.11, C. Activities Not Subject to Provisions of FPPA, (10) Restoration, maintenance, renovation, or replacement of existing structures prior to the time of Federal Assistance.

Site visits have confirmed that no active agricultural land is present within the project study area; therefore, there will be no impact to Agricultural Land Preservation Policy (ALPP) Prime Agricultural Land. This project is in conformance with 4 Pa Code Chapter 7, Section 7.301 et seq., ALPP. Additionally, this project is an upgrade of existing transportation facility and is exempt from Acts 43 and 100.

	PRESENCE	IMPACTS
<b>VEGETATION</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Landscaped	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Forest Land	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Rangeland	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

### Describe Any Permanent and Temporary Impacts

Vegetation within the project corridor primarily consists of deciduous forest land, riparian vegetation, and roadside vegetation along I-80. Permanent and Temporary impacts will occur to the project corridor vegetation to construct project improvements. This includes roadside vegetation as well as land below and adjacent to the

I-80 bridges for crane placement and contractor access.

☒ Invasive Non-Native Plants are Present

**Mitigation:**

Are measures being taken to minimize movement of invasive plant parts (roots, tubers, seeds)? ☒ Yes ☐ No

Will native plants be used in project landscaping or mitigation? ☒ Yes ☐ No

Other? ☐ Yes ☒ No

**Describe Mitigation**

In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

	PRESENCE	IMPACTS
<b>GEOLOGIC RESOURCES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
DCNR Heritage Geology Site	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Describe Any Permanent and Temporary Impacts**

None.

Is mitigation incorporated? ☒ No ☐ Yes

**Remarks**

Lehigh Gorge is an Outstanding Scenic Geological Feature of Pennsylvania. Coordination with PA DCNR has determined that the I-80 Lehigh River Bridges project will not impact the Lehigh Gorge geoheritage site (Appendix D).

	PRESENCE	IMPACTS
<b>PARKS &amp; RECREATION FACILITIES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
National	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
State	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Local	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Were any of the impacted properties acquired through the use of :**

- ☒ Land and Water Conservation Fund (Section 6(f))
- ☐ Project 70 Fund
- ☐ Project 500 Fund
- ☐ Recreational Improvement and Rehabilitation Act (RIRA)
- ☐ Growing Greener Bond Fund (GG2)
- ☐ Keystone Recreation, Park and Conservation Fund (Key 93)
- ☐ Environmental Stewardship Fund Act (ESF)

## Documentation

- ☒ Coordination with DCNR as representative for NPS (LWCF)
- ☐ Coordination with DCNR and PA General Assembly (Project 70)
- ☐ Coordination with DCNR (Project 500, GG2, Key93, RIRA or ESF)

## Describe Any Permanent and Temporary Impacts

Within the project area, Lehigh Gorge State Park (LGSP) extends on both the east and west sides of the Lehigh River on land owned by the Commonwealth of Pennsylvania and administered by PA DCNR. According to DCNR's website, the 6,107-acre park follows the Lehigh River from Francis E. Walter Dam north of I-80 to Jim Thorpe to the south. DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for LGSP. The entire state park is afforded protection under Section 6(f). As such, any land necessary to be acquired for construction of the project would be considered a conversion from public outdoor recreation use to transportation use. Temporary non-conforming use of Section 6(f) protected land (i.e., TCEs) lasting longer than 180 days is considered a permanent conversion also requiring replacement of property according to the National Park Service's policy. The temporary impacts for this project will be longer than 180 days. The National Park Service can approve such conversion only if it is found to be in accordance with the comprehensive statewide outdoor recreation plan and with the replacement of property of at least equal fair market value and of reasonably equivalent usefulness and location.

The project will result in the following impacts to LGSP:

### Permanent Impacts:

- 0.4 acre of Proposed Slope Easement along the eastbound on-ramp at SR 940 due to shift to the south (there are no park amenities at this location);

### Temporary Impacts:

- 0.5 acre Proposed Aerial Easements for new eastbound bridge due to shift to the south, which will also include temporary use for construction staging in excess of 180 days.
- 4.9 acre of TCE within LGSP for crane placement, causeway construction, and contractor access

The area of proposed aerial easements and TCEs will impact LGSP facilities. Temporary impacts to LGSP include closure of the Lehigh Gorge Trail during specified times and durations, driveway and parking facility closures, and closure of one boat launch under the I-80 bridges (three boat launches being constructed by DCNR south of the project area will remain open).

Approximately 6 acres of land will be replaced to satisfy Section 6(f). The concurrence letter from DCNR and email from NPS are provided in Appendix D.

Is mitigation incorporated? ☐ No ☒ Yes

## Mitigation:

Approximately 6 acres of land will be replaced for permanent and temporary impacts within LGSP. LGSP is also protected under Section 4(f) of USDOT Act of 1966. Additional mitigation measures are proposed for Section 4(f) uses within LGSP (see Chapter 4.5 of this CE).



## Remarks

Lehigh Park, owned by the Borough of White Haven, is adjacent to the I-80 westbound off-ramp at SR 940 and would be protected under Section 4(f). The proposed project will have no temporary or permanent impacts to Lehigh Park.

### PRESENCE

#### FOREST & GAMELANDS

☒ Not Present ☐ Present

## Remarks

A review of Google Maps, the PA Gazetteer (DeLorme 2012), aerial imagery, PennDOT One Map, and the results of the field reconnaissance did not identify any State Forests or State Gamelands within the project study area.

### PRESENCE

#### WILDERNESS, NATURAL & WILD AREAS

☒ Not Present ☐ Present

## Remarks

Review of USGS mapping, PADEP eMap, and site investigations confirmed there are no Federal and/or State Wilderness, Natural or Wild Areas within the project study area.

### PRESENCE

#### NATIONAL NATURAL LANDMARKS

☒ Not Present ☐ Present

### IMPACTS

☒ No ☐ Yes

## Remarks

No National Natural Landmarks are present within project limits.

### PRESENCE

#### HAZARDOUS OR RESIDUAL WASTE SITES

☐ Not Present ☒ Present

### IMPACTS

☐ No ☒ Yes

## Documentation

- ☒ Phase I
- ☒ Phase II
- ☒ Phase III
- ☐ Other
- ☐ No Documentation Required

### Supporting documentation for Chapter 4.2 includes:

- *I-80 Lehigh River Bridges Phase I ESA report* (April 2021)
- *I-80 Lehigh River Bridges Phase II ESA* (January 2022)
- *I-80 Lehigh River Bridges Phase III ESA* (April 2022)

## Describe Any Permanent and Temporary Impacts

A Phase I Environmental Site Assessment (ESA) was conducted in accordance with PennDOT Publication 281, "Waste Site Evaluation Procedures for the Highway Development Process" to determine if hazardous, residual, or municipal waste sites exist within the study area.

The Phase I ESA included site reconnaissance on November 19, 2020, environmental database review, historical data review, and personal interviews. Three potential sites were identified. The Phase I ESA findings and conclusions resulted in the following recommendations:

### RECOMMENDATION 1 – ACTION REQUIRED

The abandoned municipal White Haven Landfill was identified as a Recognized Environmental Condition (REC). Conducting a Phase II geophysical survey in the area of proposed excavation that intercepts the historic landfill is

recommended. The geophysical survey would be helpful to determine the location of bedrock and determine the extent of landfill material if significant trash or debris exists. If the geophysical survey or geotechnical borings being performed for the project identify trash, municipal waste or inappropriate material, a Phase III investigation is recommended to understand the nature of the material and provide verification of its vertical and horizontal extents for the purpose of excavation on the project. Municipal waste found within excavation areas will need to be managed or disposed of properly during construction.

#### RECOMMENDATION 2 – ACTION REQUIRED

A Phase III investigation to characterize historic fill material in the areas of pier replacement along the Lehigh Gorge Trail/D&L Trail (former railroad) is recommended. The characterization data would be used to prepare a Soil Management Plan (SMP) that would be required to dispose of waste that cannot be managed on site.

#### RECOMMENDATION 3 – ACTION REQUIRED

An SMP will be developed and will be included in the bid specification. The SMP will provide the contractor with instruction and methods to identify unsuitable fill material from an environmental perspective and instruction on how to address impacted historic fill material that may be identified in the area of pier replacement.

Phase II and Phase III ESAs were completed in October 2021. In the Phase II ESA geophysical survey was conducted to identify where waste was potentially buried during the former White Haven Landfill operation. The geophysical survey identified potential non-native fill and other potential buried municipal waste, and Phase III ESA was recommended. The purpose of the Phase III ESA was to characterize areas of potential buried fill material in the former White Haven Landfill and historic fill material in the areas of pier replacement along the Lehigh Gorge Trail/D&L Trail (former railroad). Soil samples were collected from soil borings that were advanced within the anomalies identified during the geophysical survey. Soil borings were also advanced within the former railroad where piers to support the bridge replacement are proposed for construction. None of the samples collected in the former railroad exceeded clean fill concentration limits. Three samples from the former landfill area exceeded clean fill concentration limits for lead and vanadium.

Is remediation/mitigation incorporated? ☐ No ☒ Yes ☐ Unknown at this time

#### **Describe Remediation/Mitigation**

In Final Design, a Soil Management Plan (SMP) will be prepared to manage, reuse, and cap the historic fill from the former landfill area and former railroad within the project ROW. The SMP will include details of historic fill capping, including areas to be capped and cap design, and will be incorporated into the construction contract, as appropriate. If excavated historic fill material cannot be reused within the project ROW, it will be characterized for offsite disposal in accordance with applicable local, state, and federal regulations.

---

### 4.3 Wildlife

#### **PRESENCE**

##### **WILDLIFE & HABITAT**

☒ Not Present ☐ Present

#### **Remarks**

The results of the field reconnaissance and review of the Pennsylvania Gazetteer (DeLorme 2015), US Fish and Wildlife Service and Nature Conservancy Map Portals did not identify any wildlife sanctuaries, wildlife refuges, unique or critical habitat, or wildlife preserves in the vicinity of the project study area.

**THREATENED & ENDANGERED  
PLANTS & ANIMALS**

**PRESENCE**

- ☐ Not Present  
☒ Present  
☐ No Coordination  
Needed

**IMPACTS**

- ☒ No Potential Impacts  
☐ Potential Impacts with Avoidance Measures  
☐ Potential Impacts with Conservation Measures  
☐ Potential Impacts

**Documentation**

- ☒ PNDI ER Receipt

**Agency Documentation**

- ☒ PFBC Correspondence  
☐ PGC Correspondence  
☐ DCNR Correspondence  
☐ USFWS Correspondence

**Remarks**

The Pennsylvania Natural Diversity Inventory (PNDI) review was conducted in October 2021 for the project study area. The PNDI returned a potential impact with a species of special concern, the cricket frog, under the jurisdiction of PFBC. Further consultation with PFBC determined that given the nature of the proposed project, the immediate location or current status of the species occurrence, no adverse impacts are expected as a result of the project. This determination is valid for two years. The PNDI receipt and PFBC letter are provided in Appendix E.

PNDI 739744, 10/13/2021. I-80 Lehigh River Bridges Project

*Although not addressed in the PNDI review, a decision is expected in 2023 to list the tri-colored bat as Endangered. A mitigation commitment is added to Chapter 7.0: During final design, the project team will initiate conferencing with USFWS regarding the project's potential effects to the tri-colored bat and measures to avoid and minimize harm.*

---

## 4.4 Cultural Resources

Were Cultural Resource Professionals (CRPs) needed for project scoping? ☒ Yes ☐ No

CRP Scoping Field View Date: 03/29/18

CRP Architectural Historian in Attendance: Kristina Thompson (field viewed 5/10/18)

CRP Archaeologist in Attendance: Kevin Mock

Was a Project Early Notification / Scoping Results Form completed? ☒ Yes ☐ No

---

Is the project exempted from review by the District Designee or CRP as per Appendix C of the Statewide Section 106 Programmatic Agreement?

☐ Yes ☒ No

---

Is the project exempted from review by the District Designee or CRP as per Stipulation III of the Emergency Relief Projects Programmatic Agreement (2005)? ☐ Yes ☒ No

	PRESENCE				LEVEL OF EFFECTS		
	Not Present	Potentially Eligible Resource Present	Eligible Resource Present	Listed Resource Present	No Historic Properties Affected	No Adverse Effect	Adverse Effect
CULTURAL RESOURCES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Archaeology</u>							
Pre-Contact:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Contact Native American:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Above-Ground Historic Properties</u>							
Structure/Building:	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
District:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

#### Documentation

For projects *not having a known adverse effect*, one from *each* column:

##### Above-Ground Historic Properties

- ☒ Above-Ground Historic Properties Field Assessment and Finding
- ☐ Above-Ground Historic Properties Finding Letter
- ☐ Section 106 (Above-Ground Historic Properties) Effect Concurrence Letter
- ☐ TE Project Field Assessment and Finding Checklist

##### Archaeology

- ☒ Archaeology Field Assessment and Finding
- ☐ Archaeology Finding Letter
- ☐ Section 106 (Archaeology) Effect Concurrence Letter
- ☐ TE Project Field Assessment and Finding Checklist
- ☐ Deferred Archaeological Testing Form
- ☐ Project Specific Programmatic Agreement

#### Supplemental documentation should be completed as warranted:

- ☐ Historic Structures Survey / Determination of Eligibility Report
- ☒ Phase Ia Archaeological Sensitivity Report
- ☐ Geomorphological Survey Report
- ☒ Archaeological Disturbance Report
- ☒ Archaeology Identification (Phase I) Report
- ☐ Archaeology Negative Survey Form
- ☐ Archaeology Evaluation (Phase II) Report
- ☐ Combined Archaeology Identification/Evaluation Report
- ☐ Determination of Effects Report
- ☐ (Bridge) Feasibility Report
- ☐ Other

#### Describe Any Permanent and Temporary Impacts

None.

Are mitigation and/or standard treatments required? ☒ No ☐ Yes

#### Remarks

PennDOT tested the single area within the Limits of Disturbance (LOD) featuring intact soils. No archaeological sites were identified. The following two historic districts are located in the vicinity of the project study area: National Register eligible Lehigh Valley Railroad and the potentially eligible Central Railroad of New Jersey. The Lehigh Valley Railroad does not have any contributing resources within the Area of Potential Effect (APE), nor does the Central Railroad of New Jersey. No other historic properties are in the APE. PennDOT posted a finding of No Historic Properties Affected on PATH [August 16, 2021](#).

After addition of diversion route traffic improvements to the project, a revised finding was posted to PATH March 15, 2022. A subsequent posting on May 10, 2022 was made to remove the tolling facility and toll diversion route traffic improvements, with the same result of No Historic Properties Affected for above and below ground resources. Section 106 cultural resource documentation is located in PATH at <https://path.penndot.gov/ProjectDetails.aspx?ProjectID=55546>.

## 4.5 Section 4(f) Resources

### PRESENCE

### USE

#### SECTION 4(f) RESOURCES

☐ Not Present ☒ Present

☐ No ☒ Yes

#### Documentation

- ☐ Individual Section 4(f) Evaluation
- ☐ Programmatic Section 4(f) Evaluation
- ☐ Section 2002 Evaluation
- ☒ De Minimis Use/No Adverse Use Checklist
- ☐ Non-Applicability/No Use Checklist
- ☐ Temporary Use Checklist
- ☐ FHWA Coordination Documents

Will temporary easements during construction be necessary from Section 4(f) resources? ☐ No ☒ Yes

#### Describe Any Permanent and Temporary Impacts

The project bridge spans the Lehigh River, Lehigh Gorge State Park, and the Lehigh Gorge Trail (a section of the Delaware and Lehigh National Heritage Corridor Trail). The portion of the Lehigh River within the project area is part of the PFBC designated Lehigh River Water Trail with boating access near the project bridges. The Lehigh River is also a state designated Scenic River.

Permanent and temporary impacts to Lehigh Gorge State Park (LGSP) and the Lehigh River Water Trail / PA Scenic River are described in detail in the Section 4(f) Evaluation documents provided in Appendix F. A separate *De Minimis Use/No Adverse Use* checklist is provided for LGSP and the Lehigh River Water Trail / PA Scenic River to satisfy Section 4(f) and Section 2002 of Pa Act 120.

- Lehigh Gorge State Park: The proposed bridge replacement project will acquire 0.4 acre of Proposed Slope Easement, 0.5 acre Proposed Aerial Easements, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within

DCNR Easement. Proposed Slope Easement is necessary to replace the eastbound bridge on an alignment south of the existing bridge. Proposed Aerial Easement (which is not a Section 4(f) use) is needed for future access, maintenance, and construction. TCE is necessary to provide contractor access and staging. The 4.9 acres and 0.3 acres of temporary impacts are not a Section 4(f) use because the scope of the work in these areas is minor, the areas will be restored with no permanent impacts and DCNR agrees that the impacts are temporary. Additional temporary impacts to LGSP include closure of the Lehigh Gorge Trail during specified times and durations, driveway and parking facility closures, and closure of one boat launch under the I-80 bridges (three boat launches being constructed by DCNR south of the project area will remain open).

- Lehigh River Water Trail / PA Scenic River: The proposed replacement I-80 Lehigh River Bridges would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). A Section 4(f) use would not occur due to pier placement since there is no additional land being converted to transportation use; in fact, there would be a reduction. The final determination of the pier locations will be dependent on the contractor's selection of bridge structure type.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the I-80 bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction over a four-year period. Again, the temporary impacts to the water trail are not a Section 4(f) use. Boat traffic through the bridge construction site will be maintained for most of construction; however, temporary closures of boat traffic will be allowed for certain activities such as bridge demolition, construction of piers, and setting of beams. For the safety of boaters traveling through the construction site, an ATON Plan will be implemented.

The existing boat launch under the I-80 bridges on the west shore of the river will be closed during construction. Three boat launches being constructed by DCNR south of the project area will remain open. Because the boat launch will be closed for the duration of construction, this was considered a de minimis use rather than temporary occupancy under Section 4(f).

### **Summary of Section 4(f) Uses**

Within Lehigh Gorge State Park, the proposed bridge replacement project will acquire approximately 0.4 acre of Proposed Slope Easement to replace the eastbound bridge on an alignment south of the existing bridge, 0.5 acre Proposed Aerial Easements (which is not a Section 4(f) use) necessary for bridge construction, contractor access and future access, maintenance, and construction. The 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement and temporary impacts to the Lehigh Gorge Trail, PA Scenic River and PFBC Water Trail, are not considered Section 4(f) uses because the uses are temporary and minor, there is no anticipated permanent impacts to these areas, the areas will be restored upon completion of construction, and DCNR and PFBC agreed that these impacts to LGSP, the Scenic River and Water Trail will be temporary.

**Is mitigation incorporated?**      ☐ No ☒ Yes

### **Describe Mitigation**

Mitigation measures for Lehigh Gorge State Park will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion within LGSP totaling approximately 6 acres. Although not required for Section 4(f) mitigation, this replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area.

Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:

- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas.

In addition, to mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers for construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users. PA Scenic Rivers coordination letters are provided in Appendix D.

## Remarks

A local park and baseball field, Lehigh Park, is located adjacent to the I-80 westbound off-ramp to SR 940 in White Haven Borough. No Section 4(f) use of Lehigh Park is anticipated.

The project bridges span the National Register eligible Lehigh Valley Railroad (Allentown to Wilkes-Barre) and Central Railroad of New Jersey/Lehigh Valley Railroad. Through Section 106 consultation, it was determined that there are no contributing resources for either historic district within the APE, therefore no Section 4(f) use of these historic railroads.

---

## 4.6 Air Quality and Noise

### AIR QUALITY

Is the project exempt from regional ozone conformity analysis and a CO, PM10 & PM2.5 Hot-Spot analysis? ☒ Yes ☐ No

Is the project exempt from an analysis for MSATs based on Pub #321? ☒ Yes ☐ No

## Remarks

Per the PennDOT "Project Level Air Quality Handbook" Publication 321, this project is classified as exempt from air quality analysis because it is a bridge replacement project. The project will not add travel lanes and will not result in changes in traffic volumes, vehicle mix, location of existing facility or other factor that would cause an increase in emissions relative to existing conditions.

---

### NOISE

Is the project a:

A. Type I Project? ☐ Yes ☒ No

B. Type II Project? ☐ Yes ☒ No

C. Type III Project? ☒ Yes ☐ No

The project meets the criteria for a Type III project established in 23 CFR 772. Therefore, the project requires no analysis for highway traffic noise impacts. Type III projects do not involve added capacity, construction of new through lanes or auxiliary lanes, changes in the horizontal or vertical alignment of the roadway or exposure of noise sensitive land uses to a new or existing highway noise source. PennDOT acknowledges that a noise analysis is required if changes to the proposed project result in reclassification to a Type I project.

---

## 4.7 Socioeconomic Areas

### REGIONAL & COMMUNITY GROWTH

Will the project induce impacts (positive and negative) on planned growth, land use, or development patterns for the area? ☐ Yes ☒ No



**Is the project consistent with planned growth?**

☒ Yes ☐ No

**Basis of this determination:**

The project is programmed on the 2023-2026 Interstate Transportation Improvement Program (TIP). The project replaces existing infrastructure and is not anticipated to induce growth.

**Will the project induce secondary growth?**

☐ Yes ☒ No

---

**PUBLIC FACILITIES & SERVICES**

**Will the project induce negative impacts on health and educational facilities; public utilities; fire, police, and emergency services; civil defense; religious institutions; or public transportation?**

☐ Yes ☒ No

**Does the project incorporate bicycle or pedestrian facilities into the overall design or operations (including construction)?**

☒ Yes ☐ No

PA Bike Route L, which is SR 1005 through the project area, will be detoured during construction. The detour will follow SR 940 east to SR 534 south. A bicycle/pedestrian checklist is included in Appendix G.

**Will the project have a positive impact to the public facilities and services listed above?**

☒ Yes ☐ No

The proposed bridge replacement project will maintain a reliable crossing over the Lehigh River and SR 1005, while also widening bridge curb-to-curb width and roadway shoulders, having a positive impact to public facilities and services.

To address temporary impacts during construction, the P3 Development Entity will coordinate with Weatherly School District officials and maintain school bus access on SR 1005 (River Road), including turnaround area(s) if needed.

---

**COMMUNITY COHESION**

**Will the project induce impacts to community cohesion?**

☐ Yes ☒ No

**Will the project induce impacts to the local tax base or property values?**

☐ Yes ☒ No

## ENVIRONMENTAL JUSTICE (see Chapter 6.0 of this CE)

---

### RIGHT-OF-WAY ACQUISITIONS OR DISPLACEMENTS OF PEOPLE, BUSINESSES OR FARMS

#### How many parcels require right-of-way acquisition, either partial or total?

8 parcels impacted with 2 parcels totally taken, 1 parcel with an aerial easement, 3 parcels with only TCEs, 1 parcel with an aerial easement, permanent slope easement and TCEs, and 1 parcel with both partial takes and TCEs.

#### Describe the extent and locations of acquisitions. Indicate for each acquisition whether it is temporary or permanent.

- White Haven Borough: Total take of existing Service Road ROW along I-80 EB ramp
- RBMN RR: Aerial Easement along south side of I-80 EB bridge
- Commonwealth of Pennsylvania (Lehigh Gorge State Park): Slope Easement along I-80 EB on ramp, Aerial Easement along south side of I-80 EB bridge, TCEs at all four quadrants of bridges
- Residential property at South East quadrant of I-80 EB bridge: Partial Take and TCEs
- Unknown Owner at South East quadrant of I-80 EB bridge: Total Take, vacant forested land
- Commercial property north of bridge: TCE
- Utility property north of bridge: TCE
- Commercial property north of bridge: TCE

Will the project require the relocation of people, businesses, or farms? ☐ Yes ☒ No

Will the project induce impacts to economic activity, including employment gains and losses? ☐ Yes ☒ No

### Mitigation

Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

- While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
- Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.

---

### MAINTENANCE AND OPERATING COSTS OF THE PROJECT AND RELATED FACILITIES

Will the project induce increases of operating or maintenance costs? ☐ Yes ☒ No

---

### AESTHETIC AND OTHER VALUES

Will the project be visually intrusive to the surrounding environment? ☐ Yes ☒ No

Will the project include "multiple use" opportunities? ☐ Yes ☒ No

Will the project involve "joint development" activities? ☐ Yes ☒ No

---

## 4.8 Permits Checklist

☐ **No Permits Required**

☒ **United States Army Corps of Engineers Section 404 and/or Section 10 Permit**

☐ Individual ☐ Nationwide ☒ PASPGP

☒ **DEP Waterway Encroachment (105) Permit**

☒ Standard ☐ Small Project ☐ General ☐ Other

☒ **DEP 401 Water Quality Certification**

☐ **Coast Guard Permit**

☒ **NPDES Permit**

☐ General ☒ Individual ☐ Exempt

☐ **Other Permits**

### Remarks

Although it is anticipated that the permits indicated above will be required for the project, a final determination of their applicability will be determined during Final Design. The P3 Development Entity will prepare and submit the permit applications and secure necessary permit authorizations prior to construction. Permit conditions will be added to ECMTS as mitigation commitments.

## 5.0 PUBLIC INVOLVEMENT

	#	Comments
<input checked="" type="checkbox"/> Plans Display	1	See Remarks
<input checked="" type="checkbox"/> Public Officials Meetings	1	See Remarks
<input checked="" type="checkbox"/> Public Meetings	1	See Remarks
<input type="checkbox"/> Public Hearing		See Remarks
<input checked="" type="checkbox"/> Special Purpose Meetings (specify)	3	Diversion Route Workshop 8/9/2021 and follow-up briefing 11/17/2021. Two additional meetings with White Haven and East Side Borough. See Remarks
<input type="checkbox"/> Section 106 Public Involvement / Consulting Parties		
<input checked="" type="checkbox"/> Section 106 Tribal Consultation		Absentee-Shawnee Tribe of Indians of Oklahoma; Delaware Nation, Oklahoma; Delaware Tribe of Indians; Eastern Shawnee Tribe of Oklahoma; Oneida Indian Nation; Onondaga Nation; Seneca-Cayuga Nation; Shawnee Tribe; Stockbridge-Munsee Community, Wisconsin; Tuscarora Nation
<input checked="" type="checkbox"/> Environmental Justice Community Involvement		Knowledgeable Parties emails and flyers, See Remarks
<input type="checkbox"/> Other information dissemination activities		
<input checked="" type="checkbox"/> Commitment for Further Public Involvement		The contractor will continue to coordinate with local municipalities and the public.

## Remarks

Public outreach activities were conducted beginning in November 2020 for the PennDOT Pathways program under an Alternative Funding PEL Study. The project was identified as a candidate for bridge tolling through PennDOT Pathways Program: The Major Bridge P3 Initiative in February 2021. Additional public outreach effort was conducted for the I-80 Lehigh River Bridges.

- Project information was posted on a project-specific website in February 2021 at <https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-White-Haven.aspx>
- A diversion route workshop was conducted on August 9, 2021 to gather additional information regarding potential issues along the diversion routes. The focus of the workshop, conducted at the Split Rock Resort in Lake Harmony, PA, was to collect input and identify concerns related to potential routes travelers may use to avoid paying a toll on the bridge.
- The diversion route workshop attendees were invited to attend a follow-up briefing on November 17, 2021 to review the proposed diversion route improvements included in the public meeting materials.
- Additional coordination was held with White Haven and East Side Borough officials on December 14, 2021 and February 10, 2022 to discuss potential traffic improvements to enhance safety along the diversion route.
- A project-specific virtual public meeting was held from November 17, 2021, to December 17, 2021. The online meeting was comprised of text, graphics and videos that provided a project overview and explained the project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, comment process and next steps. The online meeting website provided a comment form that allowed individuals to submit their comments directly within the virtual public meeting. The website also noted other ways in which comments could be submitted, including the comment form on the general project website, project phone number, project email and a physical mailing address.
- An in-person public open house was held on Wednesday December 8, 2021 at Split Rock Resort in Lake Harmony, PA. At the in-person public open house display boards were provided for project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, and schedule. Comment forms were provided for individuals to submit their comment while in attendance or at their convenience. While the comment period for the public meeting has closed, the online meeting materials are available for reference via the project website. In-person meeting materials were printed versions of the online content. Public involvement documentation is located in the project's technical file.

### Supporting documentation for Chapter 5 includes:

- *I-80 Over Lehigh River Bridges Project Virtual Public Meeting Summary* (November 17 to December 17, 2021)
- *I-80 Over Lehigh River Bridges Public Meeting Summary* (January 2022)

Prior to and during the public comment period for the public meeting, the project team executed several outreach strategies to maximize public participation at the public meeting or online consultation of the Virtual Public Meeting on project website. The outreach activities are listed in **Table 3**.

**Table 3**  
**Public Outreach Activities for Public Meeting**

<b>Outreach Type</b>	<b>Number of Recipients</b>	<b>Type of Recipients</b>	<b>Date Sent</b>
Virtual Public Meeting Website	N/A	-General Public via <a href="https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-Lehigh-VPM.aspx">https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-Lehigh-VPM.aspx</a>	Launched 11/1/2021
Postcard	5,567	-General Public -Mailed via Every Door Direct Mail Service -Sent to all postal routes within the direct project area and along the diversion route.	Mailed Week of 11/8/2021
Legal Ad	Circulation approximately ~1,000	-General public Placed in <i>The Journal-Herald</i>	Ran 11/11 /2021
Stakeholder & Public Mailing List Email	118	-Key stakeholders, legislators and those who requested to be put on the project's mailing list. -Email with information about the virtual public meeting and in-person open house.	11/17/2021
Knowledgeable Parties Email & Flyer	13	-Knowledgeable parties identified in environmental justice analysis -Email with information about virtual and in-person meetings, along with a flyer to be distributed in the community and copies of social media art for sharing	11/17/2021
News Release	N/A	-Sent to area media to distribute via news stories and calendars of events for the general public.	11/17/2021
Public Officials Briefing	N/A	-Invited public officials to a pre-launch briefing to get a first look at the materials to launch in the virtual public meeting	11/17/2021
Social Media Posts	30,733 people reached total from three posts	-Social media posts on PennDOT social media regarding how to participate in the public meeting and comment period -104 engagements across three posts	11/17/2021, 12/8/2021, 12/16/2021

An EA comparing the effects of the No Build Alternative and the Build Alternative with bridge tolling was prepared and was made available for official public review and comment on May 3, 2022. A Public Hearing was scheduled to be held on May 18, 2022, but was cancelled when all work related to the MBP3 initiative ceased May 18, 2022 due to a court ordered injunction. The comments received during the EA comment period (May 3 to June 2, 2022) have been reviewed, considered, and where appropriate, additional information was incorporated into this CE. During the public comment period for the EA, the project team executed several outreach strategies to maximize public participation as listed in **Table 4**.

**Table 4**  
**Public Outreach Activities for the EA**

<b>Outreach Type</b>	<b>Number of Recipients</b>	<b>Type of Recipients</b>	<b>Date Sent</b>
Virtual Public Hearing Website	N/A	<ul style="list-style-type: none"> <li>General Public via <a href="https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-White-Haven.aspx">https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-White-Haven.aspx</a></li> </ul>	5/3/2022
Postcard	5,567	<ul style="list-style-type: none"> <li>General Public</li> <li>Mailed via Every Door Direct Mail Service</li> <li>Sent to all postal routes within the direct project area and along the diversion route.</li> </ul>	Mailed week of 5/2/2022
Legal Ad	Print circulation approx. 8,500	<ul style="list-style-type: none"> <li>General public</li> <li>Placed in the <i>Standard Speaker</i></li> </ul>	Ran 5/3/2022
Stakeholder & Public Mailing List Email	188	<ul style="list-style-type: none"> <li>Key stakeholders, legislators and those who requested to be put on the project's mailing list.</li> <li>Email with information about the Virtual Open House and Public Hearing.</li> </ul>	5/3/2022
Knowledgeable Parties Email & Flyer	8	<ul style="list-style-type: none"> <li>Knowledgeable parties identified in environmental justice analysis</li> <li>Email with information about virtual open house and in-person hearing, along with a flyer to be distributed in the community and copies of social media art for sharing</li> </ul>	5/3/2022
News Release	N/A	<ul style="list-style-type: none"> <li>Sent to area media to distribute via news stories and calendars of events for the general public.</li> </ul>	5/3/2022
Social Media Posts	1,894	<ul style="list-style-type: none"> <li>Social media posts on PennDOT social media regarding how to participate in the public hearing and comment period</li> </ul>	5/11/2022
Elected Official Notification	Key Elected Officials List	<ul style="list-style-type: none"> <li>Elected officials (State and Local)</li> <li>Direct reach out by District 5</li> </ul>	5/2/2022

On May 18, 2022, as a result of a lawsuit, the court issued an injunction and all work related to the MBP3 initiative ceased. Subsequently, Act 84 of 2022 amended the P3 law and revoked PennDOT's ability to implement mandatory tolls such as the proposed bridge tolling under the MBP3 initiative. As a result of the lawsuits and the subsequent enactment of Act 84 of 2022, PennDOT is moving the I-80 Lehigh River Bridges project forward, but without tolling.

As the project is reverting back to a bridge replacement with associated approach roadway work, this CE was prepared to document the current effects of the Build Alternative without tolling. The project team completed outreach in September 2022 to educate and inform the public about the CE with the removal of tolling. Outreach activities are summarized in **Table 5**.

**Table 5**  
**Public Outreach Activities for the CE**

<b>Outreach</b>	<b>Audience &amp; Subject</b>
News Releases	Two news releases to media: <ol style="list-style-type: none"> <li>1. Statewide release regarding removal of tolling from MBP3 program.</li> <li>2. I-80 Lehigh River Bridges Project specific release with information on the CE.</li> </ol>
Email Blasts	Two email blasts to mailing list sign-ups: <ol style="list-style-type: none"> <li>1. Statewide email blast regarding removal of tolling from MBP3 program.</li> <li>2. I-80 Lehigh River Bridges Project specific email blast with information on the CE.</li> </ol>
Social Posts	Two social posts on Facebook and Twitter. <ol style="list-style-type: none"> <li>1. Statewide social post regarding removal of tolling from MBP3 program.</li> <li>2. I-80 Lehigh River Bridges Project specific social post with information on the CE.</li> </ol>
Bridge Website Update & Online Educational Resource	The bridge project website was updated to include information on the project's current status, description and history. An online educational resource about the CE and Potential Impacts was also developed to provide information to the public on what is presented in the CE. A comment form was available on the website for those who wished to provide feedback on the project. Comments were considered as the CE was finalized.

Public involvement documentation covering the NEPA process for the project is located in the project technical files.



## 6.0 ENVIRONMENTAL JUSTICE

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations* (February 11, 1994), directs federal agencies to identify and address, as appropriate, disproportionately high and adverse human health or environmental effects of programs, policies, and activities on minority and low-income populations. To achieve effective and equitable decision-making, the U.S. Department of Transportation (USDOT) identifies three fundamental principles of environmental justice to consider in all USDOT programs, policies, and activities:

- To avoid, minimize, or mitigate disproportionately high and adverse human health and environmental effects, including social and economic effects, on environmental justice communities of concern.
- To ensure the opportunity for full and fair participation by all potentially affected communities in the transportation decision-making process.
- To prevent the denial of, reduction in, or substantial delay in the receipt of benefits by any environmental justice community of concern.

The *I-80 Lehigh River Environmental Justice Analysis*, March 2022, was prepared to address the effects of bridge tolling and associated traffic diversion to avoid tolls on low-income and minority populations; a copy is included in the project technical files. While bridge tolling is no longer under consideration, the report contains relevant background information describing low-income and minority populations in the vicinity of the proposed project.

The Environmental Justice analysis for the project was performed by completing the following process:

- **Step 1: Define the Study Area.** Consistent with NEPA practices, identify the reasonable and logical boundaries by considering the potential for direct and indirect impacts related to the project.

The project study area includes approximately one mile of I-80 bounded by the SR 940 Exit 273 White Haven interchange and the SR 543 Exit 274 Hickory Run State Park interchange. The project will replace two bridges carrying I-80 over the Lehigh River, Reading Blue Mountain & Northern Railroad, and SR 1005 (River Road). The project also includes extension and repair of an existing concrete box culvert that carries an unnamed tributary to Lehigh River under I-80 east of the river.

- **Step 2: Identify Low-income and Minority Populations.** Collect recent data on race, color, national origin, income, tribal governments, and seasonal and migrant workers in the study area, and apply FHWA and PennDOT methodology to identify low-income and minority populations.

Low-income and minority populations are identified in the *I-80 Lehigh River Environmental Justice Analysis*, March 2022.

- **Step 3: Solicit Input from Low-income and Minority Populations.** Using PennDOT's *Public Involvement Handbook* and other environmental justice outreach guidance, identify appropriate outreach techniques. Through targeted outreach to potentially affected low-income and minority populations, identify transportation needs and concerns about the project to inform Steps 4, 5, and 6.

Public outreach was conducted throughout the development of the project including plans display/public meetings and additional stakeholder outreach targeted to parties knowledgeable about environmental justice issues (see Chapter 5.0 of this CE).

### Supporting documentation for Chapter 6 includes:

- *I-80 Lehigh River Environmental Justice Analysis* (March 2022)

- **Step 4: Evaluate Adverse and Beneficial Effects.** Analyze whether the project would create impacts to communities or populations in the near, medium, or long term. Then, with input from the community, assess whether the impacts are adverse, beneficial, or both.

Since the project involves on-location reconstruction of existing roadway and replacement of an existing bridge in a rural setting, the effects on the local community are minimal. During construction, some diversion through the community may occur as some travelers may opt to avoid the construction zone even with two lanes largely being retained in each direction; however, once the project is completed the reconstructed roadway and replaced bridges would provide improved service along the I-80 corridor.

- **Step 5: Identify Disproportionately High and Adverse Effects.** Determine whether adverse effects are predominately borne by low-income persons and minorities, and if these effects are more or greater than those effects borne by the general population.

As a result of this analysis and associated outreach effort, no disproportionately high and adverse effects on low-income or minority populations have been identified for the I-80 Lehigh River Bridges project since adverse effects to these populations are not anticipated as a result of the project

- **Step 6. Evaluate Mitigation Measures.** If adverse effects would be predominately borne by low-income and minority populations and are more or greater in magnitude than the adverse effect that would be suffered by the general population, consult with the community to identify measures to avoid, minimize, or mitigate the impacts. Determine whether the mitigation measures are practical. Practical mitigation measures are those that are: effective and do not create other adverse effects that are more severe; feasible in terms of implementation and operation; and cost effective, while maintaining the financial viability of the project.

**As no disproportionately high and adverse effects on low-income or minority populations are anticipated to occur, evaluation of mitigation measures was not necessary.**

- **Step 7: Re-evaluate Disproportionately High and Adverse Effects and Document Decision.** If practical mitigation measures have been identified, re-evaluate whether adverse effects borne by low-income and minority populations are appreciably more severe or greater than those effects borne by non-environmental justice populations.

Re-evaluation of effects on low-income and minority populations was not necessary.

## 7.0 ENVIRONMENTAL COMMITMENTS AND MITIGATION

The mitigation measures summarized in this section shall be incorporated into the project's design documents. In order to track and transfer mitigation commitments through the project development process, Environmental Commitments & Mitigation Tracking System (ECMTS) documentation shall be prepared and submitted through appropriate channels, as the project moves through Final Design and Construction.

Impacts and mitigation commitments are based on Preliminary Design and may change as the project moves through Final Design and Construction. Final design information and final mitigation commitments will be included in the ECMTS documentation.

---

### STREAMS

**Permanent Stream Impacts:** 79 linear feet

**Mitigation Remarks:** Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application. Mitigation measures will be entered into ECMTS.

No work will be permitted in streams from February 15 to June 1 (stocked trout) and October 1 to December 31 (wild trout).

### WETLANDS

**Permanent Wetland Impacts:** 0.11 acres

**Mitigation Remarks:** Replacement wetlands will be provided to compensate for the wetlands impacted by the project. Through consultation with permitting agencies, mitigation details will be determined in Final Design and incorporated into the waterway permit application.

Mitigation measures will be entered into ECMTS.

Protective fencing will be placed to prohibit contractor access in Wetland 2.

### COMMITMENTS FOR FURTHER PUBLIC INVOLVEMENT

The contractor will continue to coordinate with local municipalities and the public.

### STATE SCENIC RIVERS & STREAMS

Through coordination with PA DCNR, PennDOT agreed to incorporate the following mitigation measures into the project:

- Vegetative buffers around construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users

### NAVIGABLE WATERWAYS

A preliminary ATON Plan was prepared and submitted to PFBC for review. PFBC approved the ATON Plan on August 16, 2021. The ATON Plan and approval letter are in the project technical files. The ATON Plan shows the proposed causeways and staging, and sign locations in the vicinity of the bridges. Signs will also be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and

the public will be provided. The P3 Development Entity will be responsible for implementation of the ATON plan during construction.

## **SOIL EROSION & SEDIMENTATION**

The P3 Development Entity will complete Final Design, update plans, prepare permit applications for submission, and secure necessary permit authorizations prior to construction.

- BMPs will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.
- The E&S Control Plan will be reviewed by the PADEP and coordination will be conducted to ensure the selected BMPs are adequate for the project.
- The approved E&S Control Plan will be implemented prior to any earth disturbance, during construction.
- Installed BMPs will be inspected and maintained throughout the duration of construction.
- All areas of earth disturbance will be stabilized immediately following completion of earthwork.
- PCSM controls will be evaluated in final design and included in the NPDES permit application, if required.

## **VEGETATION**

In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

## **PARKS & RECREATIONAL FACILITIES**

Approximately 6 acres of land will be replaced for permanent and temporary impacts within LGSP to satisfy Section 6(f) and NPS's policies.

LGSP is also protected under Section 4(f) of USDOT Act of 1966. Additional mitigation measures are proposed as mitigation for Section 4(f) uses within LGSP (see Section 4(f) below).

## **HAZARDOUS OR RESIDUAL WASTE SITES**

In Final Design, a Soil Management Plan (SMP) will be prepared to manage, reuse, and cap the historic fill from the former landfill area and former railroad within the project ROW. The SMP will include details of historic fill capping, including areas to be capped and cap design, and will be incorporated into the construction contract, as appropriate. If excavated historic fill material cannot be reused within the project ROW, it will be characterized for offsite disposal in accordance with applicable local, state, and federal regulations.

## **THREATENED & ENDANGERED PLANTS & ANIMALS**

### USFWS coordination for tri-colored bat:

During final design, the project team will initiate conferencing with USFWS regarding the project's potential effects to the tri-colored bat and measures to avoid and minimize harm.

## **SECTION 4(F) RESOURCES**

Mitigation measures for Lehigh Gorge State Park will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area

Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.

In addition, to mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers around construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.

#### **NON-RESOURCE SPECIFIC MITIGATION COMMITMENTS**

- The P3 Development Entity will coordinate with Weatherly School District officials and maintain school bus access on SR 1005 (River Road), including turnaround area(s) if needed.
- The P3 Development Entity will complete Final Design, update plans, prepare permit applications for submission, and secure necessary permit authorizations prior to construction.
- The NPDES and waterway permit conditions will be added to ECMTS as mitigation commitments.
- Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.
  - While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
  - Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.
- If the P3 Development Entity requires area outside of the PSA delineated in this CE, the P3 Development Entity is required to coordinate with PennDOT to determine necessary NEPA Reevaluation studies and documentation.

This NEPA Reevaluation may include but not be limited to:

- Delineation of aquatic resources in accordance with USACE protocol;
- Phase I ESA or Environmental Due Diligence (EDD) statement;
- PNDI review and coordination with resource protection agencies;
- Section 106 Consultation; and
- Public outreach.

## **Appendix A**

### **Engineering Information**

## **Project Identification**

**Originating Office:** 05, 04

**Date:** 03/30/21

**Federal Project Number:** TBD

**Township/Municipality:** White Haven Borough, East Side Borough, Kidder Township

**Local Name:** I-80 White Haven Bridges

**Limits of Work (Segment/Offset)**

<b>Start:</b>	<b>End:</b>
2720/1097	2743/1236
2721/2330	2731/2253

**Construction Stations**

<b>Start:</b>	<b>End:</b>
2720/0997	2743/1136
2721/2430	2731/2153

**Total Length:** 8143 EB, 7993 WB ft

**Date of First Federal Authorization for Preliminary Engineering:**

Not Applicable

**Date of Federal Authorization Time Extension(s) for Preliminary Engineering(if applicable):**

Not Applicable



## **Design Criteria**

**Roadway Description:** SR 0080-08B

**Functional Classification:** Freeways/Interstates

☐ Urban ☒ Rural

**Current ADT:** WB/EB 15805/16797

**Design Year No-Build ADT:** WB/EB 26050/27686

**Current LOS:** N/A

**Design Year Build ADT:** WB/EB 26050/27686

**Design Year Build LOS:** N/A

**DHV:** WB/EB 1824/1938

**Truck %:** 34

**D (Directional Distribution) %:** 52

**Design Speed:** 70 mi/h

**Posted Speed:** 65 mi/h

### **Required Minimum Widths**

**Lane Width:** 12 ft

**Shoulder Width:** 12 Right 8 Left ft

**Bridge Curb-to-Curb:** 44 ft

**Design Exception Required?**

☒ Yes ☐ No

The existing stopping sight distance of I-80 Westbound through the SR 940 overpass at the western end of the project is substandard. The proposed I-80 Westbound alignment will approximately match existing, and the stopping sight distance will remain substandard.

**Typology:** Limited Access Freeway – Rural Interstate

**Topography:** ☐ Level ☒ Rolling ☐ Mountainous

**Proposed Design Criteria:** New and Reconstruction

---

## **Traffic Control Measures**

**The following traffic control measures will be implemented:**

- ☐ Temporary Bridge(s)
- ☒ Temporary Roadway
- ☒ Detour
- ☐ Ramp Closure
- ☒ Other (specify)
- ☐ None

**Other Description:** Temporary ramp realignment due to traffic staging. Phased construction on I-80 will utilize temporary roadway consisting of temporary lane shifts and temporary cross overs.

**Provisions for access by local traffic will be made and so posted.**

☒ True ☐ False

**Through-traffic dependent business will not be adversely affected.**

☒ True ☐ False

**There will be no interference with any local special event or festival.**

☒ True ☐ False

**There will be no substantial environmental consequences associated with the traffic control measure(s).**

☒ True ☐ False

**There is no substantial controversy associated with the traffic control measure(s).**

☒ True ☐ False

**There are no substantial impacts to bicycle or pedestrian routes.**

☒ True ☐ False

**An alternate, ADA compliant bicycle/pedestrian access route is available.**

☒ True ☐ False

---

**Approximate length of planned detour:** 5.3 miles

☒ Detour Map

**Make the selection that best describes the planned detour:**

- ☐ Detour will use local roads with no improvements.
- ☐ Detour will involve improvements to local roads with no resulting impacts on safety or the environment.
- ☐ Detour will involve improvements to local roads and will impact safety and/or the environment.
- ☒ Detour will use only state owned roads.

**Describe impacts**

SR 1005 (River Road) will be closed and detoured during construction. The detour route is attached. The P3 Development Entity will coordinate with Weatherly School District officials and maintain school bus access, including turnaround area(s) if needed.

PA Bike Route L travels on SR 1005 (River Road) through the project area. Bike Route L will be detoured during construction. The detour will use SR 940 east to SR 534 south.

---

## **Estimated Costs**

**Engineering:** \$ 4,900,000

**Right-of-Way:** \$ 1,600,000

**Construction:** \$ 67,867,000

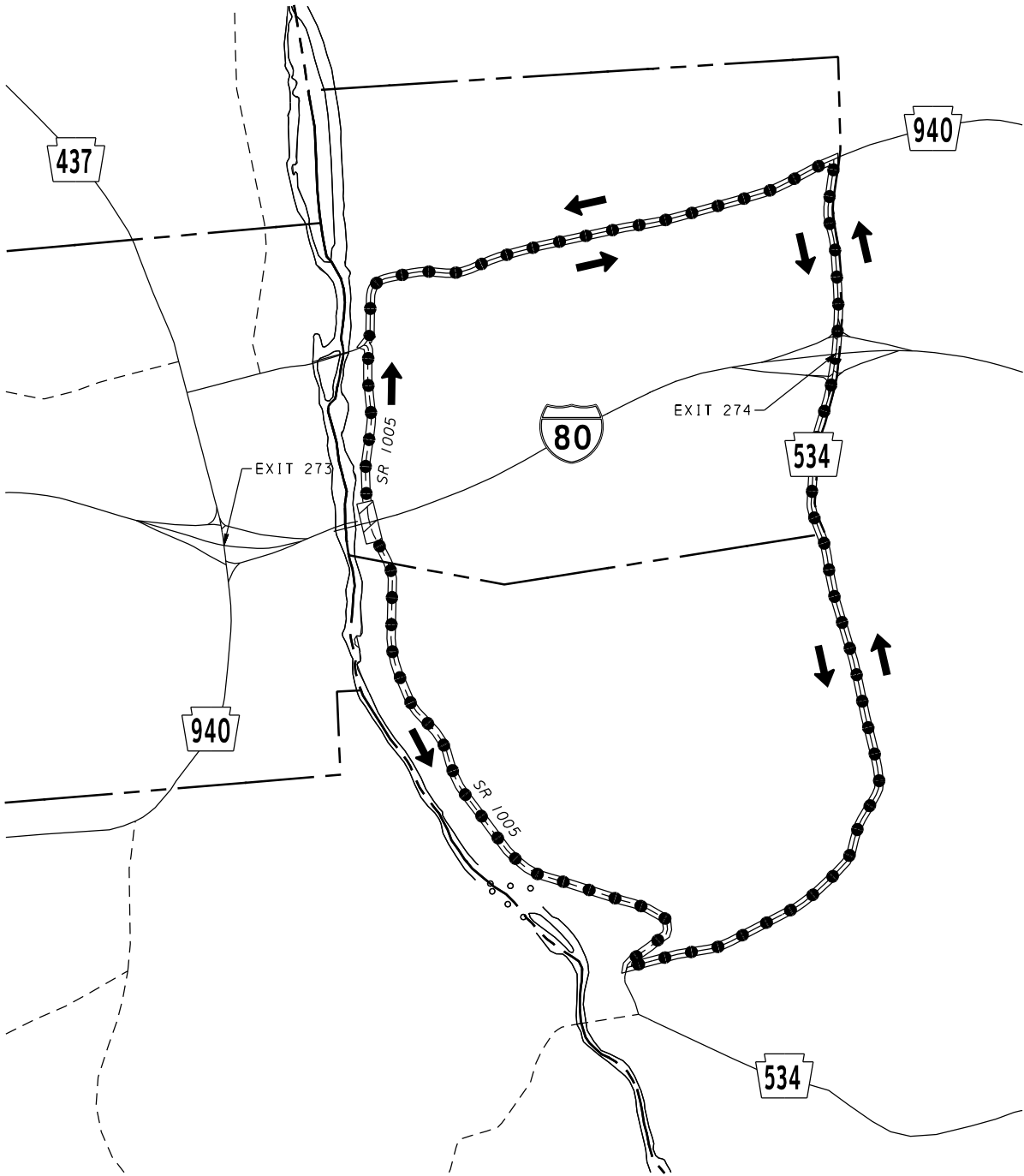
**Utilities:** \$ 200,000

---

USER: AHOLTZAP | PLOT DRIVER: PennDOT.PDF\_Memo.plt | PLOT DATE: 12-01-2021 | 12:29:16 PM  
PATH: c:\pwworking\esri\gdb\42835\ | FILE: 1008008B-TC-DT04.dgn | MODEL: traffic plan references

\*  
LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	59 OF 59	
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

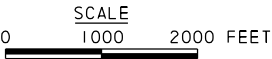


LEGEND

- - DETOUR ROUTE
- ➔ - DIRECTION OF DETOUR
- ▨▨▨ - CONSTRUCTION
- - HIGHWAY
- - - - STATE ROUTE
- - - - MUNICIPAL BOUNDARY

NOTE:  
THIS DETOUR TO BE IN EFFECT FOR THE  
ENTIRE DURATION OF CONSTRUCTION ACTIVITIES.

TRAFFIC CONTROL PLAN  
SR 1005 & BICYCLE ROUTE L CLOSED



## Roadway

### Roadway Description

SR 0080-08B

	Existing	Proposed
<b>Number of Lanes:</b>	4	4
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	4 ft	Right/Left 12/8 ft
<b>Median Width:</b>	Varies ft	Varies ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

### Roadway Description

SR 8006 (Ramp B) I-80 WB Exit Ramp to SR 940

	Existing	Proposed
<b>Number of Lanes:</b>	1	1
<b>Lane Width:</b>	14 ft	14 ft
<b>Shoulder Width:</b>	10 outside / 4 inside ft	10 outside / 4 inside ft
<b>Median Width:</b>	N/A ft	N/A ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

### Roadway Description

SR 8006 (Ramp C) - I-80 EB On Ramp from SR 940

	Existing	Proposed
<b>Number of Lanes:</b>	1	1
<b>Lane Width:</b>	14 ft	14 ft
<b>Shoulder Width:</b>	10 outside / 4 inside ft	10 outside / 4 inside ft
<b>Median Width:</b>	N/A ft	N/A ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

### Roadway Description

SR 940 at I-80

	Existing	Proposed
<b>Number of Lanes:</b>	Varies, see Remarks	Varies, see Remarks
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	10 ft	10 ft
<b>Median Width:</b>	14 ft	14 ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	14 ft	14 ft

### Remarks

Number of lanes is 4 (2 in each direction) on bridge over I-80 and to the south. Number of lanes north of I-80 into White Haven is 2 lanes (1 in each direction). Outer shoulder on bridge over I-80 is 5'-6".

## Structure

**BMS Number:** 13-0080-2728-0000

**BRKEY:** 8972

**Description:** I-80 Eastbound over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 1005 (River Road)

	Existing	Proposed
<b>Structure Type:</b>	Girder-floorbeam-stringer	Multi Steel Plate Girder
<b>Weight Restrictions:</b>	None ton	None ton
<b>Height Restrictions:</b>	None ft	None ft
<b>Curb to Curb Width:</b>	33 & varies ft	56 ft
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	4.5 ft	12 outside / 8 inside ft
<b>Sidewalk Width:</b>	None ft	None ft
<b>Total Bridge Width*:</b>	36.5 and varies ft	59.375 ft

**\*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.**

<b>Under Clearance:</b>	26.9 ft	29.1 ft
<b>Lateral Clearance:</b>	2 ft	53.1 ft
<b>Sufficiency Rating:</b>	62	
<b>Structure Length:</b>	836 ft	855 ft

### Remarks

Auxiliary lane for on-ramp is carried across the bridge.

**BMS Number:** 13-0080-2729-0000

**BRKEY:** 8974

**Description:** I-80 Eastbound over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 1005 (River Road)

	Existing	Proposed
<b>Structure Type:</b>	Girder-floorbeam-stringer	TBD
<b>Weight Restrictions:</b>	None ton	None ton
<b>Height Restrictions:</b>	None ft	None ft
<b>Curb to Curb Width:</b>	33 and varies ft	44 ft
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	4.5 ft	12 outside / 8 inside ft
<b>Sidewalk Width:</b>	None ft	None ft
<b>Total Bridge Width*:</b>	36.5 and varies ft	47.375 ft

**\*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.**

<b>Under Clearance:</b>	24.5 ft	24.7 ft
<b>Lateral Clearance:</b>	2 ft	36.25 ft
<b>Sufficiency Rating:</b>	63	
<b>Structure Length:</b>	757 ft	770 ft

## Structure

**BMS Number:** 13-0080-2730-0300

**BRKEY:** 8973

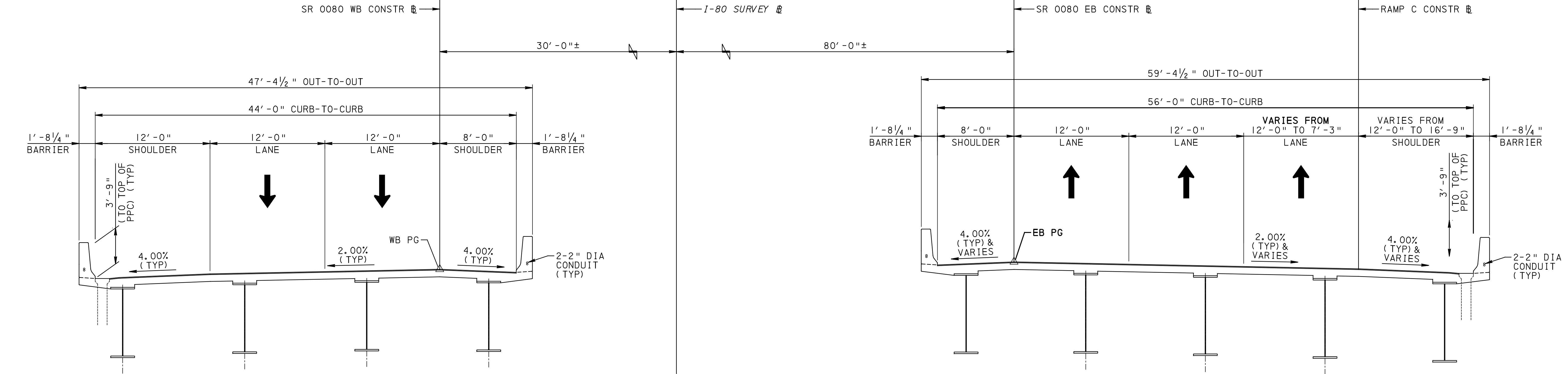
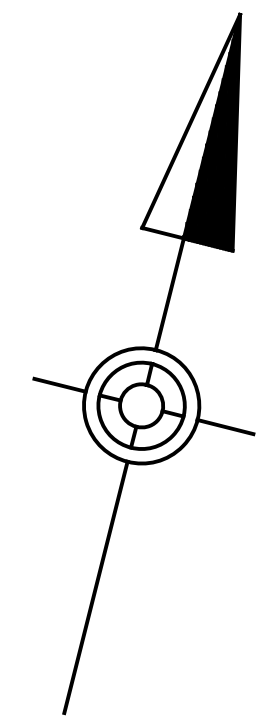
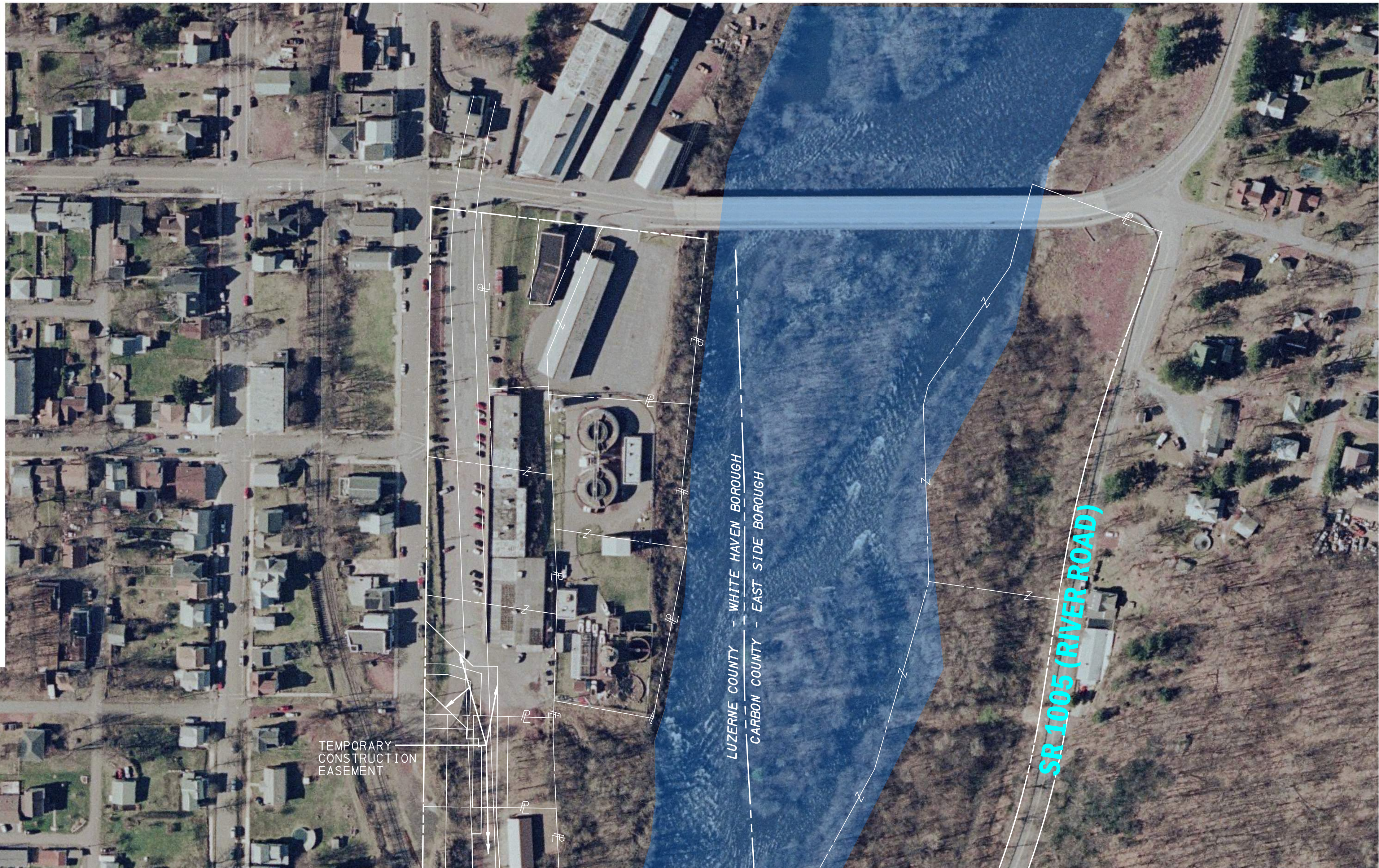
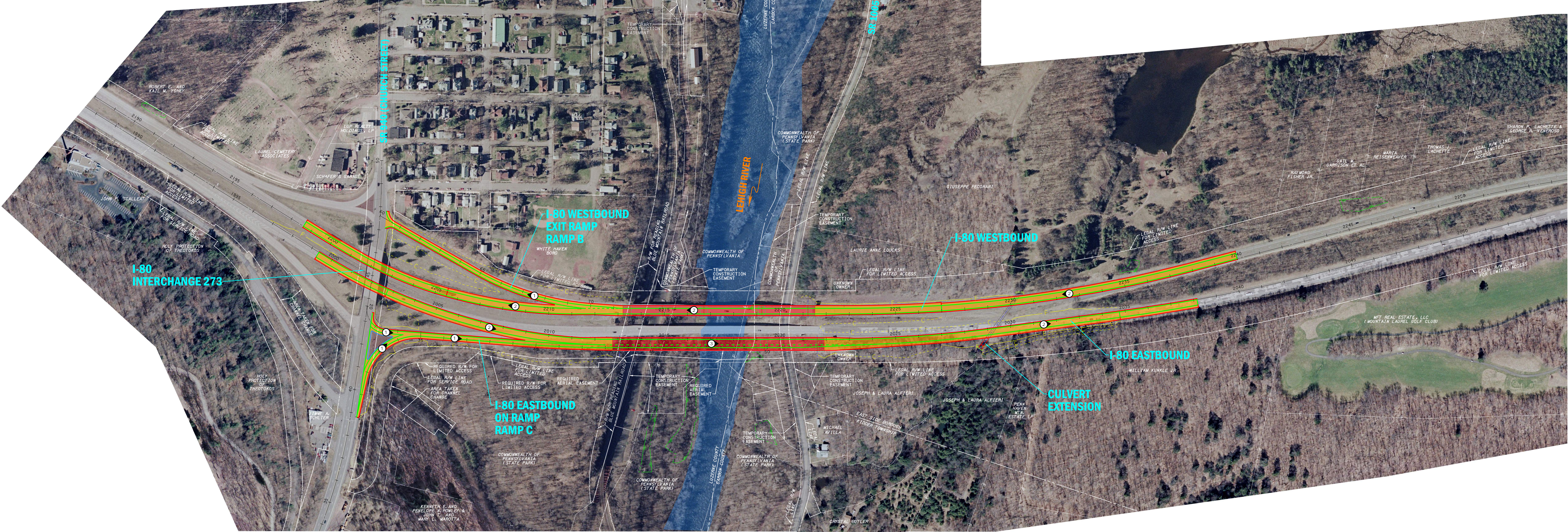
**Description:** I-80 over Tributary to Lehigh River

	Existing	Proposed
<b>Structure Type:</b>	Concrete Box Culvert	Concrete Box Culvert
<b>Weight Restrictions:</b>	None ton	None ton
<b>Height Restrictions:</b>	None ft	None ft
<b>Curb to Curb Width:</b>	N/A under fill ft	N/A under fill ft
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	12 outside / 8 inside ft	12 outside / 8 inside ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Total Bridge Width*:</b>	243 ft	274.5 ft
<b>*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.</b>		
<b>Under Clearance:</b>	N/A ft	N/A ft
<b>Lateral Clearance:</b>	N/A ft	N/A ft
<b>Sufficiency Rating:</b>	70	
<b>Structure Length:</b>	16 ft	16 ft

**Appendix B**  
**Project Design Exhibit**



# I-80 Over Lehigh River Bridges Eastbound and Westbound Replacement



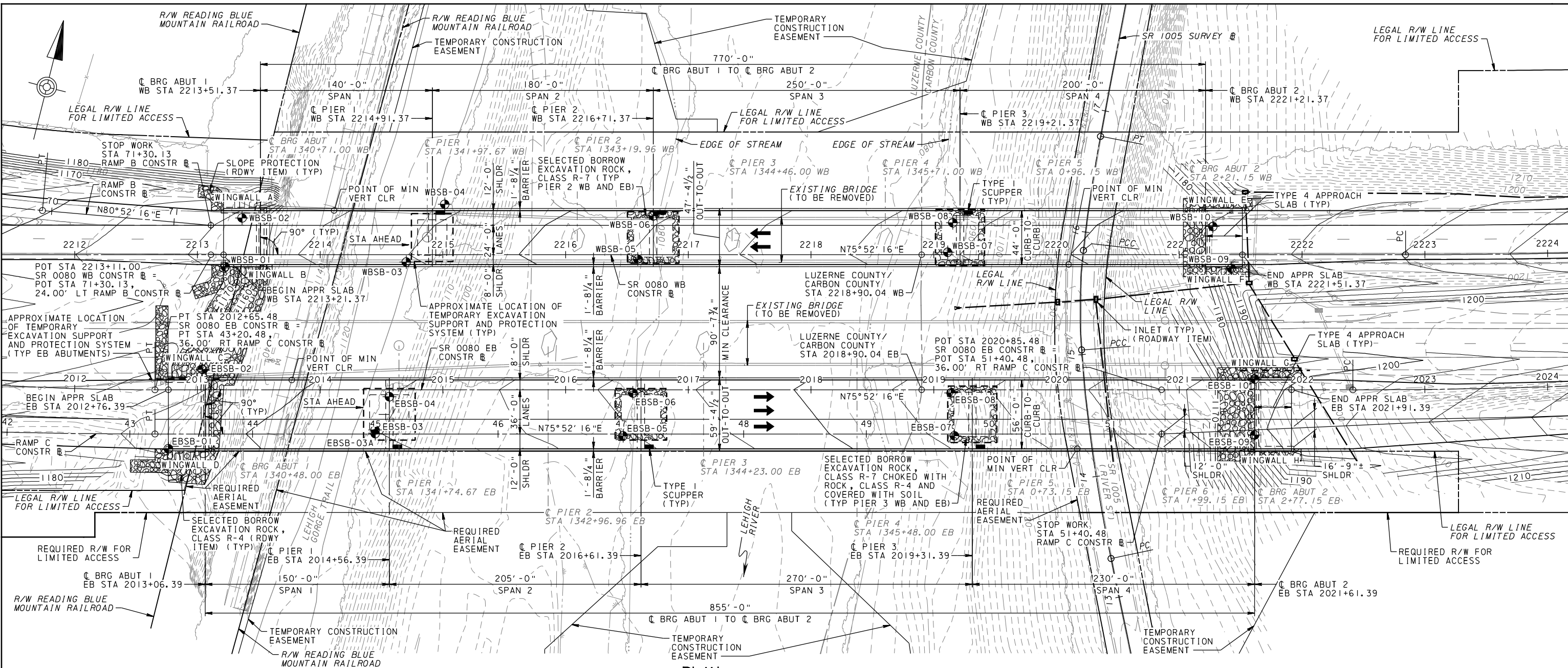
SR 0080 TYPICAL BRIDGE SECTION

## Legend

Proposed Pavement Edge	
Proposed Shoulder Edge	
Property Line	
Cut Line	
Fill Line	
Wetland Boundary	
Proposed Structure	
Proposed Pavement	
Number/Direction Lanes	



**Appendix C**  
**Preliminary Design Plans**



## PLAN

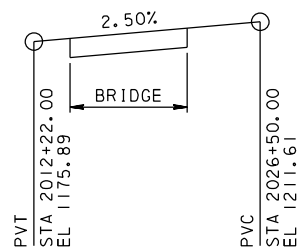
20 0 20 40 FEET

## LEGEND:

- AS-DRILLED TEST BORINGS
- EXISTING CONTOUR
- PROPOSED CONTOUR
- EDGE OF STREAM
- WETLAND LINES
- DIRECTION OF TRAVEL
- APPROXIMATE LOCATION OF TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM

## NOTES:

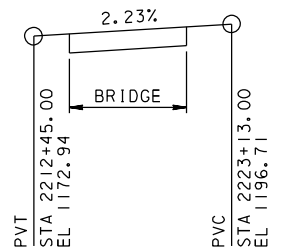
- FOR GENERAL NOTES AND LIST OF SUPPLEMENTAL DRAWINGS, SEE SHEET 3.
- FOR TYPICAL SECTION, SEE SHEET 4.
- FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
- THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING B.
- FOR SR 1005 AND RAMPS B AND C VERTICAL GEOMETRY, SEE SHEET 4.
- FOR SR 0080 HORIZONTAL GEOMETRY, SEE SHEET 4.

SR 0080 EB CONSTR B  
VERTICAL GEOMETRY

## HYDRAULIC DATA

DRAINAGE AREA= 311 SQ MI

EXISTING SR 0080 WB STRUCTURE	PROPOSED SR 0080 WB STRUCTURE
DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 6.7 FPS WS EL= 1086.64	DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 7.2 FPS WS EL= 1086.14
100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 6.9 FPS WS EL= 1087.30	100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 7.4 FPS WS EL= 1086.79

SR 0080 WB CONSTR B  
VERTICAL GEOMETRY

## AS-DRILLED BORING LOCATIONS (WB)

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
WBSB-01	2213+22.7	9.7 RT	1174.1
WBSB-02	2213+36.4	30.0 LT	1174.4
WBSB-03	2214+70.2	6.0 RT	1120.0
WBSB-04	2215+01.5	41.1 LT	1119.1
WBSB-05	2216+58.8	3.5 RT	1083.1
WBSB-06	2216+71.4	31.6 LT	1081.8
WBSB-07	2219+11.3	2.0 LT	1084.8
WBSB-08	2219+15.5	26.1 LT	1084.4
WBSB-09	2221+42.4	11.1 RT	1190.8
WBSB-10	2221+27.3	23.0 LT	1192.0

## AS-DRILLED BORING LOCATIONS (EB)

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EBSB-01	2012+76.4	48.0 RT	1166.3
EBSB-02	2013+04.1	16.6 LT	1175.7
EBSB-03	2014+44.8	32.7 RT	1118.4
EBSB-03A	2014+44.8	35.7 RT	1118.4
EBSB-04	2014+56.4	5.6 RT	1118.9
EBSB-05	2016+46.8	37.5 RT	1080.4
EBSB-06	2016+54.8	2.5 RT	1080.8
EBSB-07	2019+16.8	37.5 RT	1089.9
EBSB-08	2019+14.0	2.5 RT	1088.7
EBSB-09	2021+61.4	36.8 RT	1178.3
EBSB-10	2021+61.4	8.8 LT	1185.0

PREPARED BY:



HDR ENGINEERING, INC.  
3025 CHEMICAL RD  
SUITE 110  
PLYMOUTH MEETING, PA 19462

SIGNATURE &amp; DATE

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009. MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATIONLUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
CONCEPTUAL TYPE, SIZE, & LOCATION - PLAN

RECOMMENDED

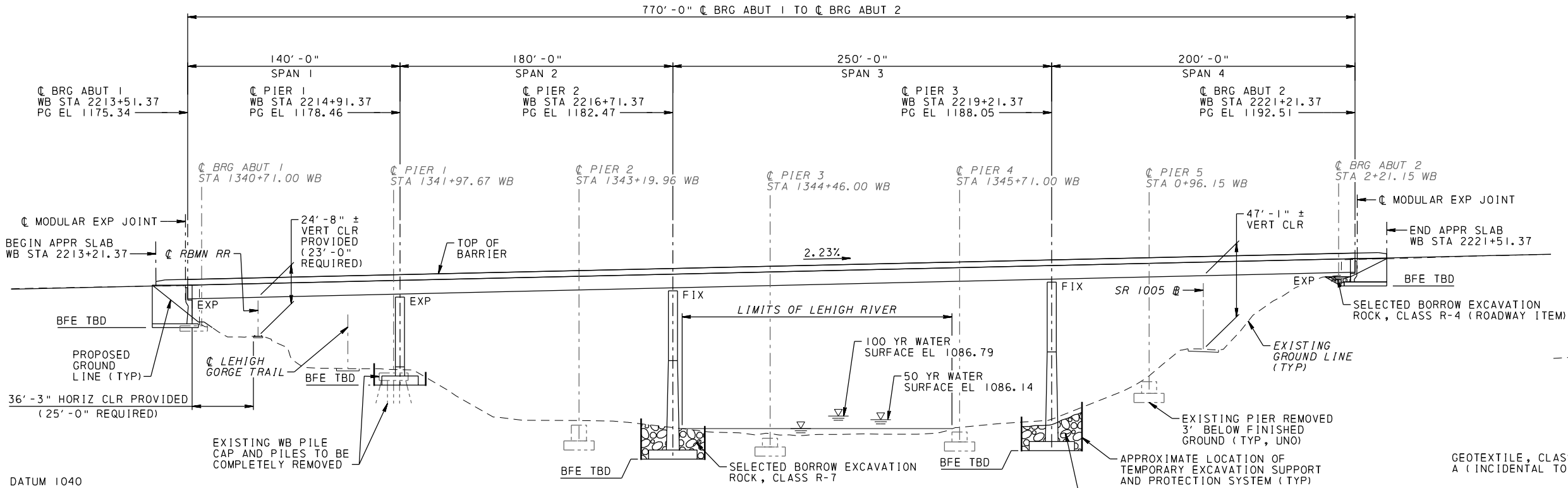
DISTRICT BRIDGE ENGINEER

SHEET 1 OF 7  
+SUPPLEMENTAL  
DRAWINGS

S-XXXX

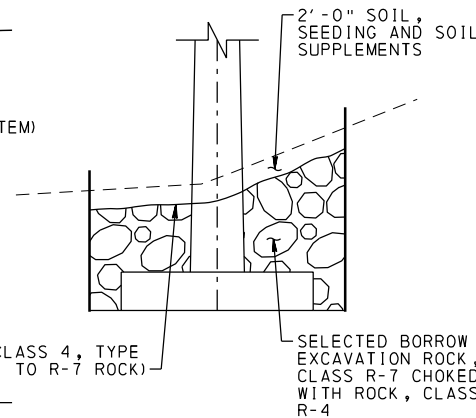
DES: PJC DWG: ARC CKD: KRS

USER: AGRIES  
PATH: C:\pwworking\hdr\projects\194488  
FILE: 000008B-BR-TSL-PLN.dgn  
PLOT: DRIVER: PenPlot.PDF  
PLOT DATE: 08-24-2021  
8:22:47 AM  
MODEL: Dwg (ou) 1



WB BRIDGE - ELEVATION

20 0 20 40 FEET

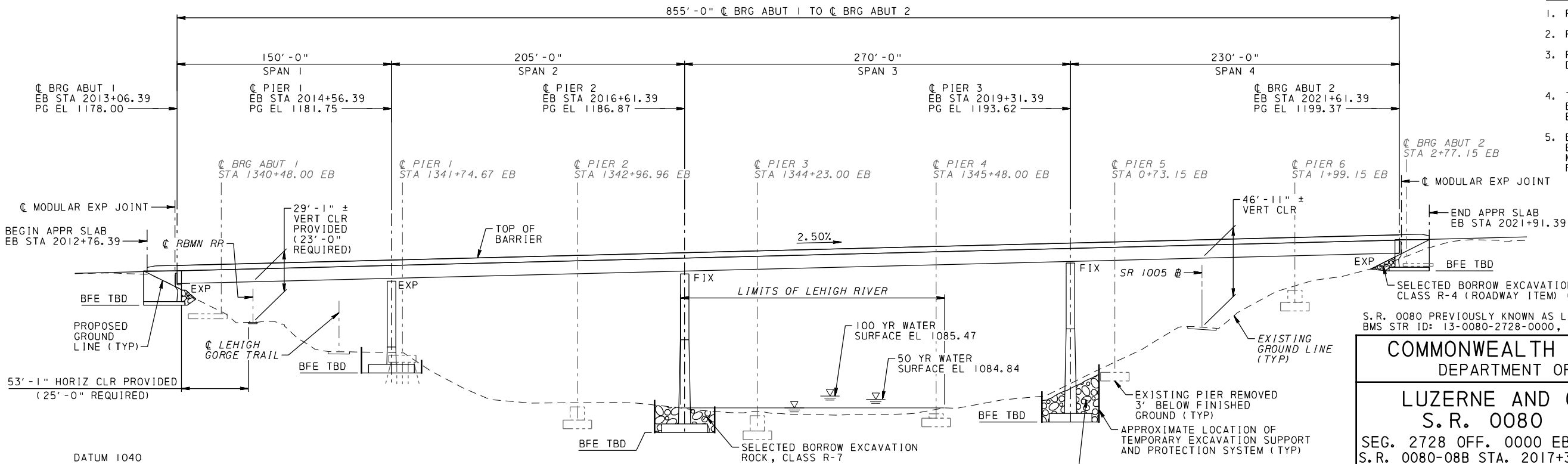


PIER 3  
SCOUR PROTECTION DETAIL

NOT TO SCALE

NOTES:

1. FOR GENERAL NOTES, SEE SHEET 3.
2. FOR TYPICAL SECTION, SEE SHEET 4.
3. FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
4. THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING  $\phi$ .
5. BEARING FIXITY IS CONCEPTUAL. BEARING TYPE, FIXITY AND JOINT MOVEMENT TO BE DETERMINED DURING FINAL DESIGN.



EB BRIDGE - ELEVATION

20 0 20 40 FEET

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
CONCEPTUAL TYPE, SIZE, & LOCATION - ELEVATIONS

RECOMMENDED \_\_\_\_\_

SHEET 2 OF 7

S-XXXXX

USER: AGRIES PLOT: DRIVER: PenDOT.PDF: Memo: p11c1g PLOT DATE: 08-23-2021 8:08:57 AM  
PATH: c:\pwworking\101\1984488\ FILE: 000008B-BR-TSL-ELEV.dgn MODEL: D061011

GENERAL NOTES:

DESIGN SPECIFICATIONS

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION, 2017, AND AS SUPPLEMENTED BY DESIGN MANUAL, PART 4, DECEMBER 2019.

LIVE LOAD DISTRIBUTION TO BEAMS IS BASED UPON DESIGN MANUAL, PART 4 DISTRIBUTION FACTOR METHOD.

DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.

DESIGN LIVE LOADS

PHL-93, P-82, AND P2016-13

FATIGUE DESIGN IS BASED ON THE FOLLOWING:

STEEL STRUCTURES: ADTT 9,413 (2046) 1-80 EB (ONE-DIRECTIONAL)  
ADTT 8,857 (2046) 1-80 WB (ONE-DIRECTIONAL)

DEAD LOADS

INCLUDES A SURFACE AREA DENSITY OF 15 PSF FOR THE POLYESTER POLYMER CONCRETE (PPC) OVERLAY ON THE DECK SLAB.

INCLUDES A SURFACE AREA DENSITY OF 30 PSF FOR FUTURE WEARING SURFACE ON THE DECK SLAB, WITH THE POLYESTER POLYMER CONCRETE (PPC) OVERLAY REMOVED.

INCLUDES A SURFACE AREA DENSITY OF 0.015 KSF FOR PERMANENT METAL DECK FORMS, WHICH TAKES INTO ACCOUNT THE WEIGHT OF THE FORM PLUS THE WEIGHT OF THE CONCRETE IN THE VALLEYS OF THE FORMS.

GENERAL

PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH SPECIFICATIONS, PUBLICATION 408/2020, AASHTO/AWS D1.5M/D1.5:2008 BRIDGE WELDING CODE, AND THE CONTRACT SPECIAL PROVISIONS. (USE AASHTO/AWS D1.1/D1.1M:2008 FOR WELDING NOT COVERED IN AASHTO/AWS D1.5M/D1.5:2008).

PROVIDE STRUCTURAL STEEL CONFORMING TO AASHTO M 270/M 270M, GRADE 50W (ASTM A709/A 709M, GRADE 50W) DESIGNATION, EXCEPT WHEN NOTED OTHERWISE.

PROVIDE 2" CONCRETE COVER ON REINFORCEMENT BARS, EXCEPT AS NOTED.

USE CLASS AAP CEMENT CONCRETE IN DECK SLAB.

USE CLASS AA CEMENT CONCRETE IN BARRIERS, TYPE 4 APPROACH SLABS, DRAIN TROUGHS, ABUTMENT BACKWALLS, CHEEKWALLS, AND SLEEPER SLABS.

USE CLASS A CEMENT CONCRETE IN PILE CAPS, ABUTMENTS BELOW BRIDGE SEAT, PEDESTALS, WINGWALLS, FOOTINGS AND PIERS.

USE CLASS C CEMENT CONCRETE BELOW THE BOTTOMS OF FOOTINGS WHEN SPECIFIED.

A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO THE DEPARTMENT, IF APPROVED BY THE DISTRICT BRIDGE ENGINEER.

PROVIDE GRADE 60 REINFORCING STEEL BARS THAT MEET THE REQUIREMENTS OF ASTM A615/A 615M, A996/A996 M, OR A706/A 706 M. DO NOT WELD GRADE 60 REINFORCING STEEL BARS UNLESS SPECIFIED. GRADE 40 REINFORCING STEEL BARS MAY BE SUBSTITUTED WITH A PROPORTIONAL INCREASE IN CROSS-SECTIONAL AREA, IF APPROVED BY THE CHIEF BRIDGE ENGINEER. DO NOT USE RAIL STEEL A996 REINFORCEMENT BARS IN BRIDGE PIERS, ABUTMENTS, FOOTINGS, BARRIERS OR WHERE BENDING OR WELDING OF THE REINFORCEMENT BARS IS INDICATED.

USE EPOXY-COATED REINFORCEMENT BARS IN THE DECK SLAB, BARRIERS, ABUTMENT BACKWALLS, U-WINGS ABOVE THE CONSTRUCTION JOINT, AND ABUTMENT SEAT BARS. EPOXY-COAT J-BAR REINFORCEMENT IN SUBSTRUCTURE UNITS AND EPOXY-COAT OTHER SUBSTRUCTURE REINFORCEMENT BARS AS INDICATED.

GALVANIZED REINFORCING STEEL BARS MAY BE SUBSTITUTED FOR EPOXY-COATED REINFORCING STEEL BARS AT NO ADDITIONAL COST TO THE DEPARTMENT.

RAKE-FINISH ALL HORIZONTAL CONSTRUCTION JOINTS, EXCEPT AS INDICATED.

SITE CLASS IS NOT CLASS E.

VERIFY ALL DIMENSIONS AND GEOMETRY OF THE EXISTING STRUCTURE IN THE FIELD AS NECESSARY FOR PROPER FIT OF THE PROPOSED CONSTRUCTION.

CONSTRUCT DECK SLAB TRANSVERSE CONSTRUCTION JOINTS PARALLEL TO BRIDGE CENTERLINE OF BEARINGS.

CHAMFER EXPOSED CONCRETE EDGES ¾ IN BY ¾ IN, EXCEPT AS NOTED.

ALL DIMENSIONS SHOWN ARE HORIZONTAL, EXCEPT AS NOTED.

USE EITHER PERMANENT METAL FORMS OR REMOVABLE FORMS TO CONSTRUCT THE DECK SLAB.

DECK SLAB THICKNESS INCLUDES A ½ " INTEGRAL WEARING SURFACE.

SUPERSTRUCTURE DIMENSIONS SHOWN ARE FOR A NORMAL TEMPERATURE OF 68° F.

PROVIDE MINIMUM EMBEDMENT AND SPLICE LENGTHS IN ACCORDANCE WITH STANDARD DRAWING BC-736M, UNLESS OTHERWISE INDICATED.

PREPARE BEARING AREAS AS SPECIFIED IN PUBLICATION 408, SECTION 1001.3(K)9.

PROTECTIVE COATING FOR REINFORCED CONCRETE SURFACES (PENETRATING SEALERS, BRIDGE SUPERSTRUCTURE): APPLY PENETRATING SEALER TO THE TOP OF THE BRIDGE DECK, AND TOPS AND INSIDE FACES OF BARRIERS.

WELDING OF REINFORCEMENT BARS DURING FABRICATION OR CONSTRUCTION IS NOT PERMITTED UNLESS SPECIFIED.

GENERAL (CONTINUED)

NOTIFY THE REGIONAL HEADQUARTERS OF THE FISH COMMISSION PRIOR TO CONSTRUCTION AND COOPERATE WITH FISH COMMISSION DURING CONSTRUCTION.

WATERWAY CONSERVATION OFFICER  
5566 MAIN ROAD  
SWEET VALLEY, PA 18656  
(570) 477-5717

IN-STREAM RESTRICTIONS FROM OCTOBER 1 THROUGH DECEMBER 31 (WILD TROUT) AND MARCH 1 THROUGH JUNE 15 (STOCKED TROUT) WILL APPLY TO THIS PROJECT.

UTILITY NOTES

COORDINATE, LOCATE, AND CONDUCT ALL WORK RELATED TO PUBLIC AND PRIVATE UTILITIES IN ACCORDANCE WITH PUBLICATION 408 SECTIONS 105.06 AND 107.12.

STEEL GIRDERS

IF GIRDERS CANNOT BE SHIPPED IN THE LENGTHS SHOWN ON THE PLANS, FIELD SPLICE(S) WILL BE PERMITTED AT THE REQUEST OF THE CONTRACTOR, BUT NO COMPENSATION WILL BE ALLOWED FOR THE SPLICES.

IF GIRDERS CAN BE FABRICATED IN LENGTHS LONGER THAN THE SECTIONS SHOWN ON THE PLANS BY ELIMINATING FIELD SPLICES, FIELD SPLICE(S) MAY BE OMITTED AT THE REQUEST OF THE CONTRACTOR. THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR SECURING A HAULING PERMIT. APPROVAL FOR ELIMINATION OF A FIELD SPLICE AT THE SHOP DRAWING STAGE DOES NOT OBLIGATE THE DEPARTMENT TO ISSUE A HAULING PERMIT.

DO NOT USE FORM SUPPORT SYSTEMS THAT WILL CAUSE UNACCEPTABLE OVERSTRESS OR DEFORMATION TO PERMANENT BRIDGE MEMBERS.

ALL FASTENERS ARE ASTM F3125 GRADE A325, TYPE 3 HIGH STRENGTH BOLTS, UNO.

REAM SUBDRILLED OR SUBPUNCHED HOLES FOR FIELD SPLICES IN THE FABRICATION SHOP.

INSTALL SHEAR CONNECTORS IN THE FIELD AFTER INSTALLING THE DECK FORMS.

STABILITY OF PARTIAL GIRDERS AND COMPLETE GIRDERS IS TO BE MAINTAINED BY THE CONTRACTOR DURING ERECTION, UNTIL ALL GIRDERS AND DIAPHRAGMS ARE IN-PLACE AND ALL BOLTS ARE PROPERLY INSTALLED. ERECTION LOADS INCLUDING SELF WEIGHT OF THE STEEL MEMBERS, WIND LOADING AND CONSTRUCTION LIVE LOAD EFFECTS ARE TO BE EVALUATED BY THE CONTRACTOR FOR STABILITY, STRESSES, AND DEFLECTIONS ON THE STEEL MEMBERS DURING ANY STAGE OF ERECTION.

DO NOT MAKE WELDS BY MANUAL SHIELDED METAL ARC PROCESS FOR PRIMARY GIRDER WELDS, SUCH AS FLANGE-TO-WEB WELDS OR FOR SHOP SPLICES OF WEBS AND FLANGES.

DO NOT WELD PERMANENT METAL DECK FORMS OR OTHER ATTACHMENTS TO GIRDER TOP FLANGES IN TENSION AREAS. THREADED STUDS FOR THE SUPPORT OF THE OVERHANG DECK FORMING BRACKET IS PERMITTED PROVIDED THE THREADED STUD IS ATTACHED WITH THE SAME WELDING PROCESSING AS THE SHEAR STUDS.

PROVIDE WELDED STUD SHEAR CONNECTORS MANUFACTURED FROM STEEL CONFORMING TO ASTM A108.

THE STEEL SUPERSTRUCTURE SHALL BE DETAILED AND FABRICATED FOR TOTAL DEAD LOAD FIT (TDLF). GIRDER WEBS SHALL BE PLUMB UNDER THE FULL DEAD LOAD EXISTING AT THE END OF CONSTRUCTION.

SET ANCHOR BOLTS TO TEMPLATE OR IN PREFORMED HOLES. DO NOT DRILL UNLESS SPECIFICALLY INDICATED ON PLANS. FILL THE PREFORMED HOLES WITH NON-SHRINK GROUT. FILL THE CLEARANCE BETWEEN ANCHOR BOLTS AND HOLES IN MASONRY PLATES WITH APPROVED NON-HARDENING CAULKING COMPOUND CONFORMING TO PUBLICATION 408, SECTION 705.8.

DO NOT FIELD-WELD ON ANY PART OF THE EXISTING BRIDGE, EXCEPT WHERE SHOWN ON THE DRAWINGS, WITHOUT PRIOR APPROVAL OF THE REPRESENTATIVE.

BRACE STEEL FRAMEWORK IN LONGITUDINAL AND LATERAL DIRECTIONS UNTIL MEMBERS ARE IN STABLE (FINAL BRACED) CONDITION.

PROVIDE ADDITIONAL TEMPORARY BRACING BETWEEN THE FASCIA AND THE FIRST INTERIOR BEAM IF THE FASCIA BEAM COULD ROTATE, IN THE OPINION OF THE REPRESENTATIVE, WHEN PLACING DECK CONCRETE.

PERFORM CHARPY V-NOTCH TESTS AS SPECIFIED AS PER PUBLICATION 408, SECTION 1105.02(A)5.

PAINT ALL STEEL WITHIN 12 FEET OF THE CENTER LINE OF BEARING AT EACH ABUTMENT IN ACCORDANCE WITH PUBLICATION 408, SECTION 1060.

NOTES FOR EXISTING PLANS

DO NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE BY THE DEPARTMENT OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT WILL BE ENCOUNTERED IN THE FIELD.

THE INFORMATION SHOWN ON THE PLANS FOR THE EXISTING BRIDGES IS NOT PART OF THE PLANS, PROPOSAL, OR CONTRACT AND IS NOT TO BE CONSIDERED AS A BASIS FOR COMPUTATION OF THE UNIT PRICES USED FOR BIDDING PURPOSES. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT INFORMATION IS CORRECTLY SHOWN. THE BIDDER IS NOT TO RELY ON THIS INFORMATION, BUT IS TO ASSUME THE POSSIBILITY THAT CONDITIONS AFFECTING THE COST AND/OR QUANTITIES OF WORK TO BE PERFORMED MAY DIFFER FROM THOSE INDICATED. (ORIGINAL PLANS S-5849 - 1964 AND REHABILITATION PLANS S-14076 - 1991).

THE EXISTING BRIDGE STRUCTURAL MEMBERS ARE ASSUMED TO CONTAIN LEAD PAINT AND OTHER TOXIC MATERIALS.

USER: AGRIES  
PATH: c:\pwworking\ags\01\1984488\  
FILE: 008008B-BR-NOT1.dgn  
PLOT DRIVER: PenPlot\_PDF\_Memo.plt  
PLOT DATE: 08-17-2021 4:12:37 PM  
MODEL: 08.fou1.t

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

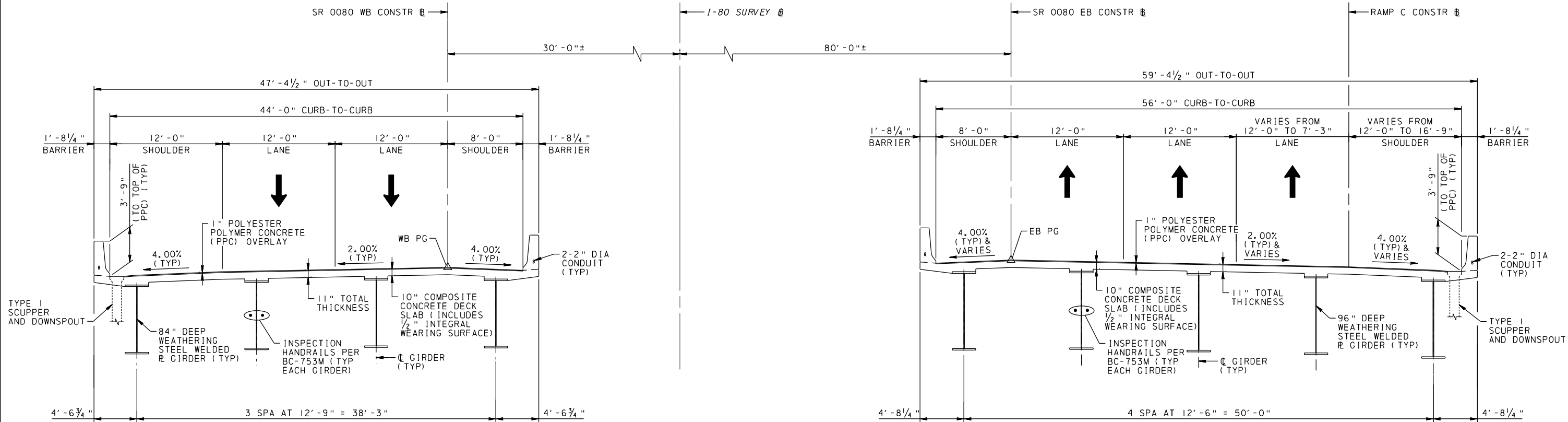
GENERAL NOTES

RECOMMENDED \_\_\_\_\_

SHEET 3 OF 7

S-XXXXX

CLASSIFICATION OF EARTHWORK FOR STRUCTURES	RC-11M	6/1/2010
BACKFILL AT STRUCTURES	RC-12M	2/8/2019
SUBSURFACE DRAINS	RC-30M	12/17/2019
ENDWALLS	RC-31M	6/1/2010
ELECTRICAL DETAILS	BC-721M	2/19/2021
PERMANENT METAL DECK FORMS	BC-732M	1/31/2019
ANCHOR SYSTEMS	BC-734M	2/19/2021
WALL CONSTRUCTION AND EXPANSION JOINT DETAILS	BC-735M	9/30/2016
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	1/31/2019
BRIDGE DRAINAGE	BC-751M	1/31/2019
CONCRETE DECK SLAB DETAILS	BC-752M	2/19/2021
STEEL GIRDER DETAILS	BC-753M	1/31/2019
STEEL DIAPHRAGMS FOR STEEL BEAM/GIRDER STRUCTURES (STRAIGHT GIRDERS ONLY)	BC-754M	1/31/2019
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	1/31/2019
DESCRIPTION	DWG. NO.	APP. DATE
SUPPLEMENTAL DRAWINGS		



PROPOSED TYPICAL SECTION

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

SR 0080 WB  
HORIZONTAL GEOMETRY  
WEST APPROACH

PI STA 2207+12.22  
Δ = 23°19'06" LT  
T = 397.22'  
L = 783.44'  
R = 1925.00'  
E = 40.56'  
PCC STA 2203+15.00  
PT STA 2210+98.44  
SUPERELEVATE 8.0%  
DESIGN SPEED 70 MPH

SR 0080 WB  
HORIZONTAL GEOMETRY  
EAST APPROACH

PI STA 2224+58.96  
Δ = 3°05'48" LT  
T = 174.61'  
L = 349.13'  
R = 6460.00'  
E = 2.36'  
PC STA 2222+84.35  
PCC STA 2226+33.48  
SUPERELEVATE 3.2%  
DESIGN SPEED 70 MPH

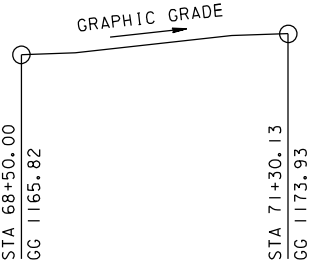
SR 0080 EB  
HORIZONTAL GEOMETRY  
WEST APPROACH

PI STA 2010+35.80  
Δ = 12°38'39" LT  
T = 231.55'  
L = 461.23'  
R = 2090.00'  
E = 12.79'  
PCC STA 2008+04.25  
PT STA 2012+65.48  
SUPERELEVATE 7.8%  
DESIGN SPEED 70 MPH

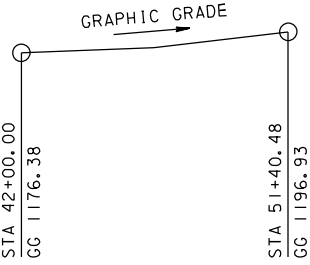
SR 0080 EB  
HORIZONTAL GEOMETRY  
EAST APPROACH

PI STA 2025+76.65  
Δ = 7°35'47" LT  
T = 335.26'  
L = 669.53'  
R = 5050.00'  
E = 11.12'  
PC STA 2022+41.39  
PT STA 2029+10.92  
SUPERELEVATE 4.0%  
DESIGN SPEED 70 MPH

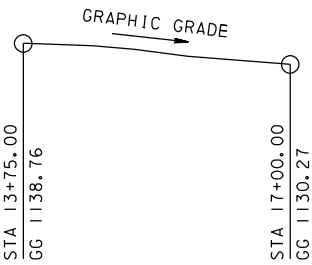
SR 0080 RAMP B  
VERTICAL GEOMETRY



SR 0080 RAMP C  
VERTICAL GEOMETRY



SR 1005  
VERTICAL GEOMETRY



NOTE:

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

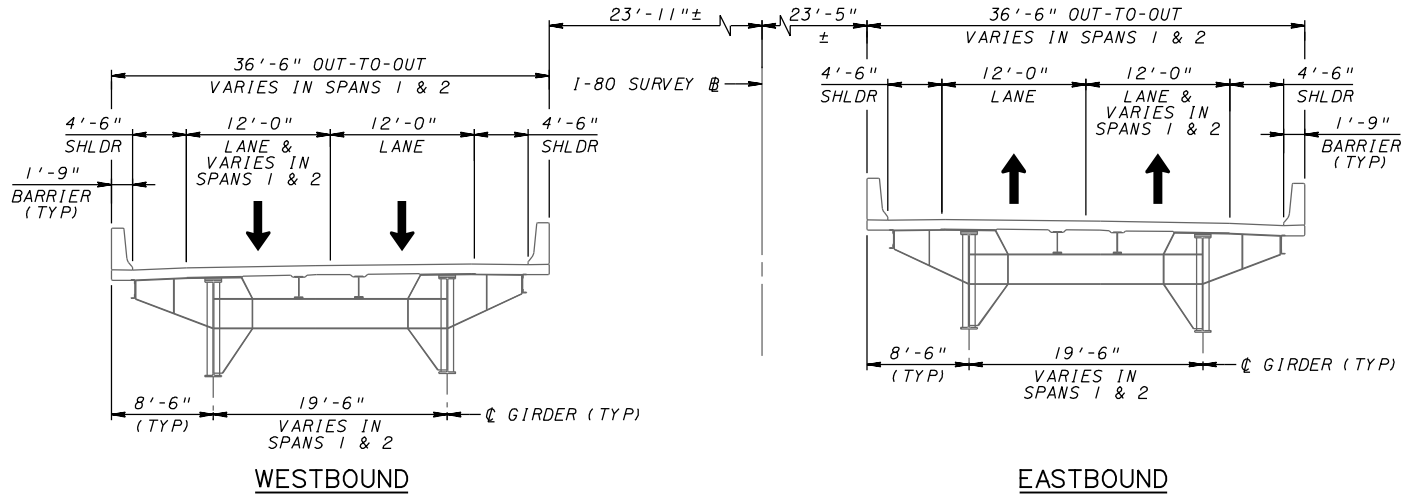
SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

TYPICAL SECTION

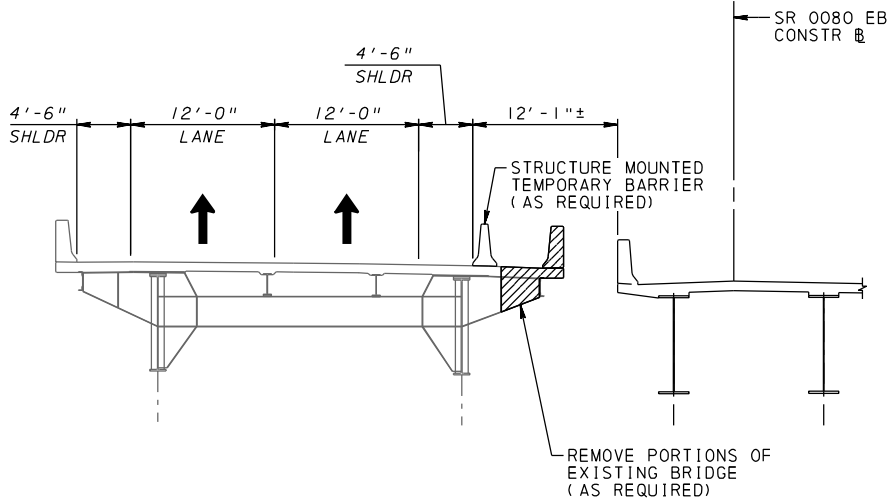
RECOMMENDED \_\_\_\_\_

SHEET 4 OF 7

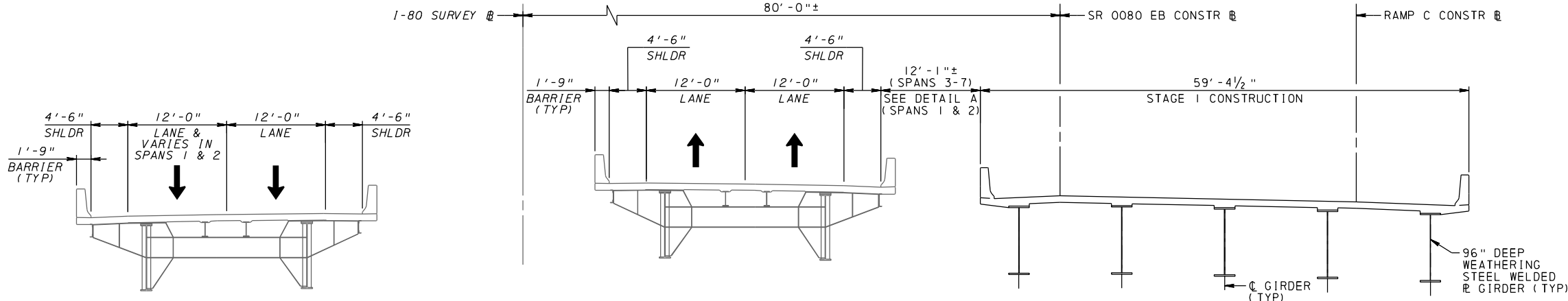
S-XXXXX



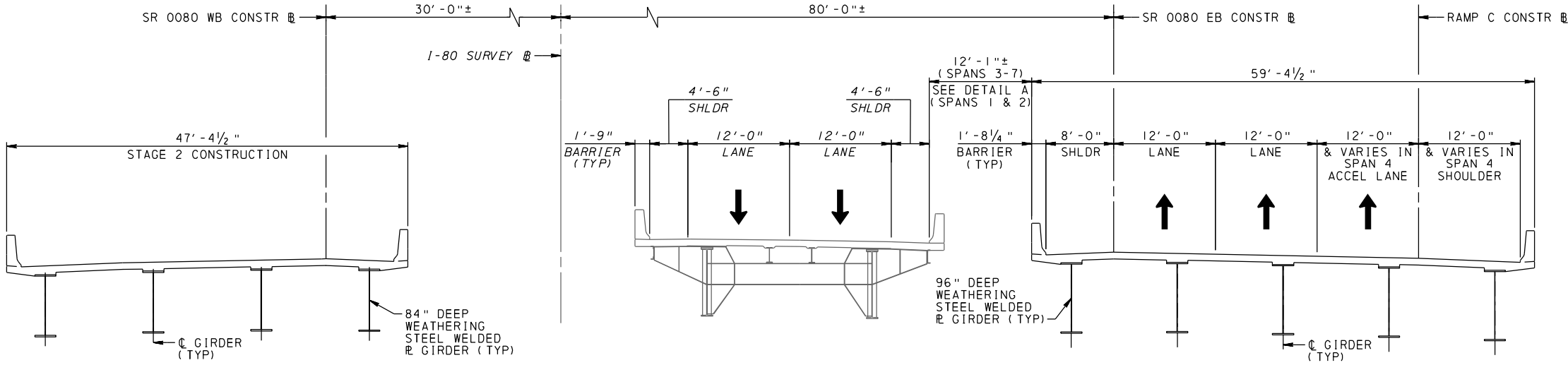
**EXISTING**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**DETAIL A**  
(SPANS 1 & 2 ONLY)  
2 0 4 8 FEET



**STAGE 1 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**STAGE 2 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

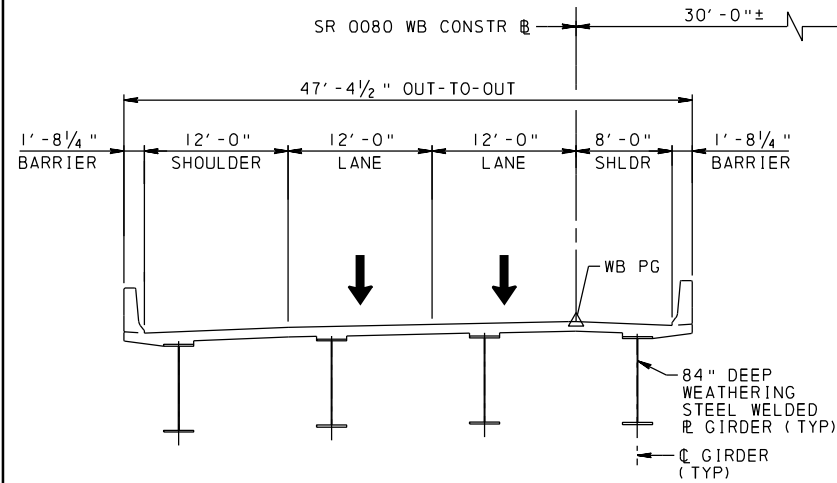
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

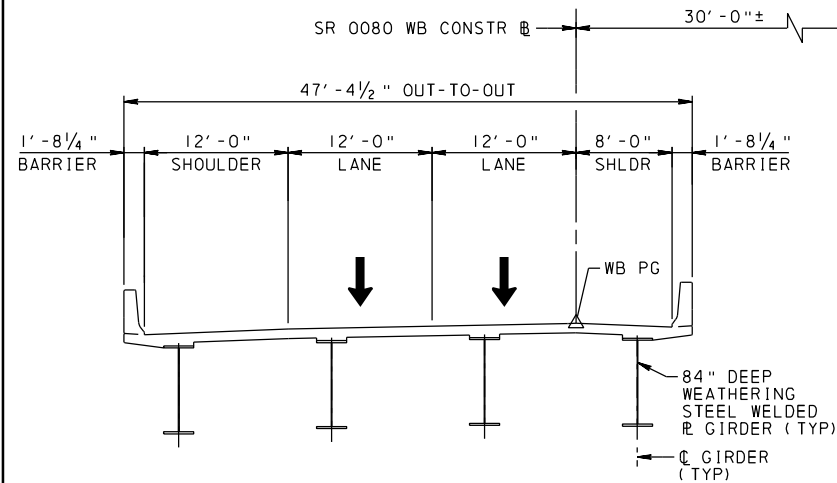
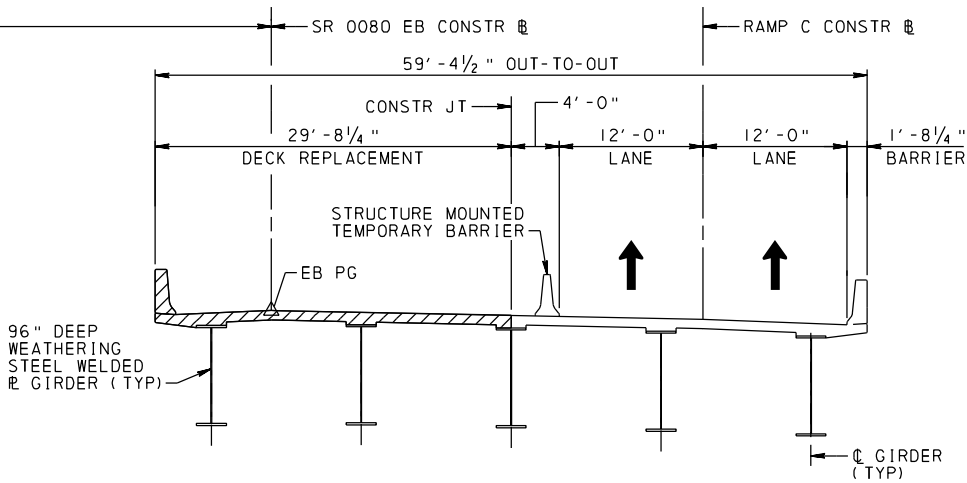
**CONSTRUCTION STAGING**

RECOMMENDED _____	SHEET <u>5</u> OF <u>7</u>
	S-XXXXX

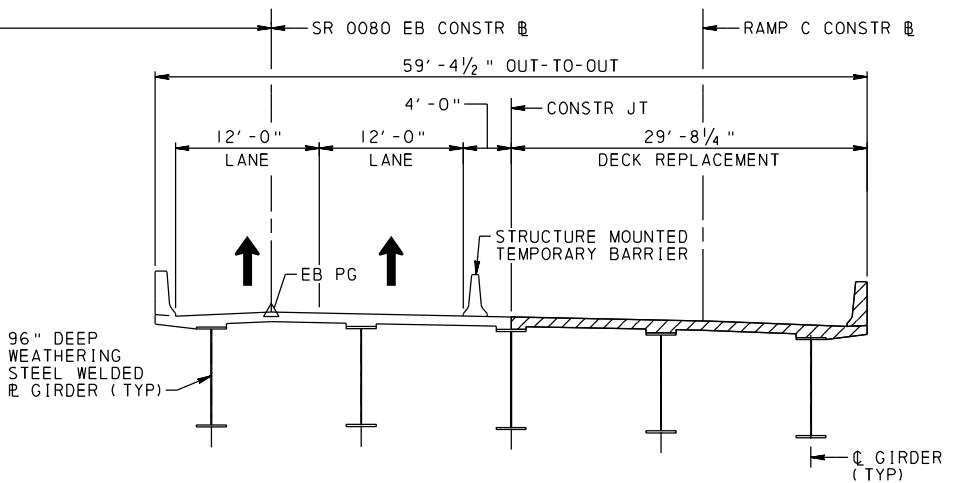
USER: AGRIES PLOT: DRIVER: PenDOT\_PDF\_Memo.plt: 4/12/24 4:45 PM  
PATH: c:\pwworking\101\1984488\ FILE: 1008008B-BR-CONSTR-STG.dgn  
MODEL: D:\p\out



PHASE 1



PHASE 2



NOTE:

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

FUTURE DECK REPLACEMENT

RECOMMENDED

SHEET 6 OF 7

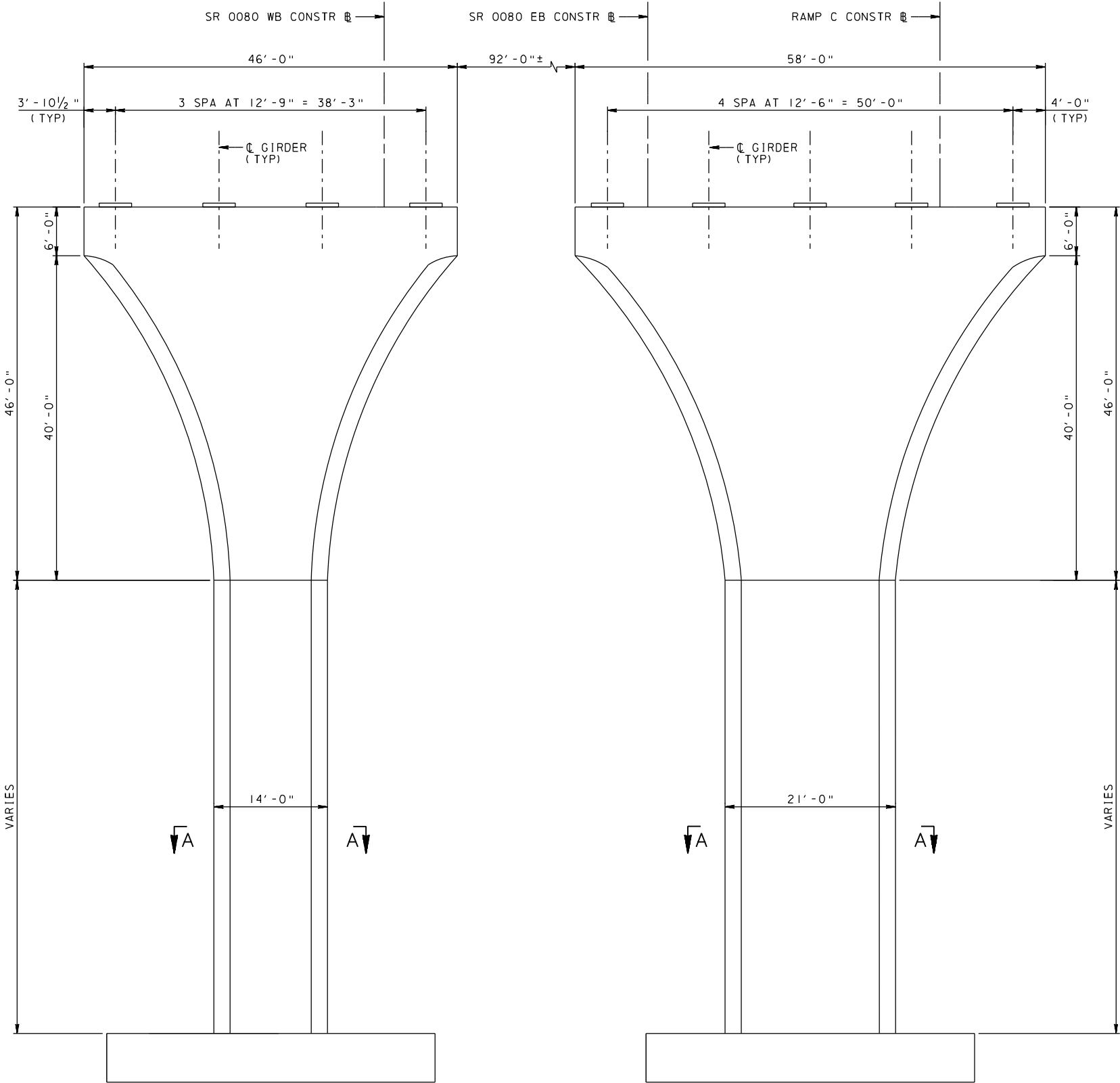
S-XXXXX

FUTURE DECK REPLACEMENT

(LOOKING STATIONS AHEAD)

20 4 8 FEET

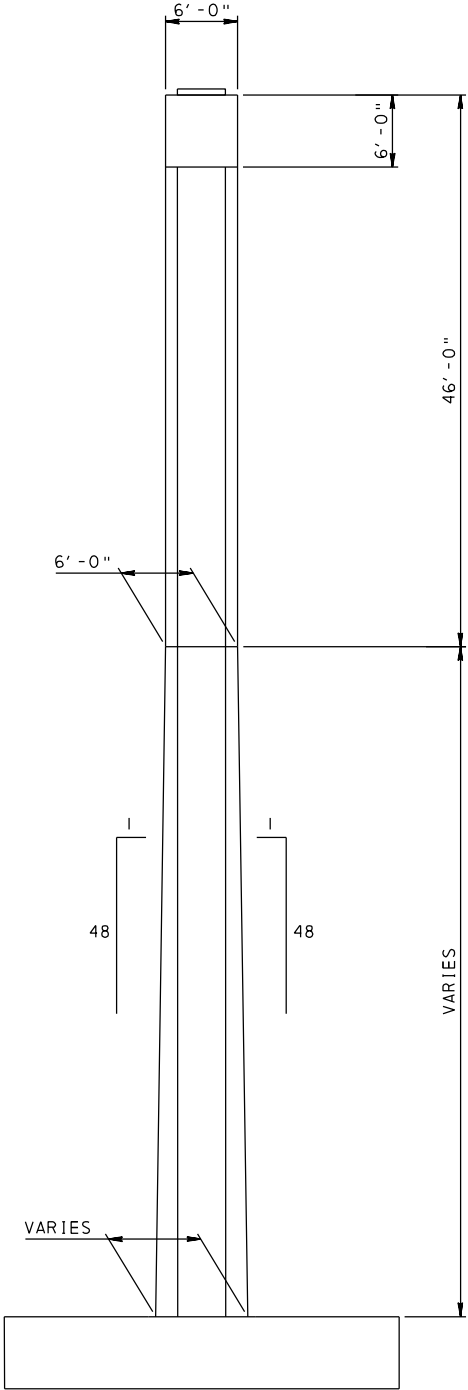
USER: AGRIES  
PATH: c:\pwworking\agst01\civil\984488\  
FILE: 008008B-BR-PIER-DET.dgn  
PLOT: DRIVER: PennDOT.PDF- Mono.plt  
PLOT DATE: 08-17-2021 4:12:52 PM  
MODEL: Defoult



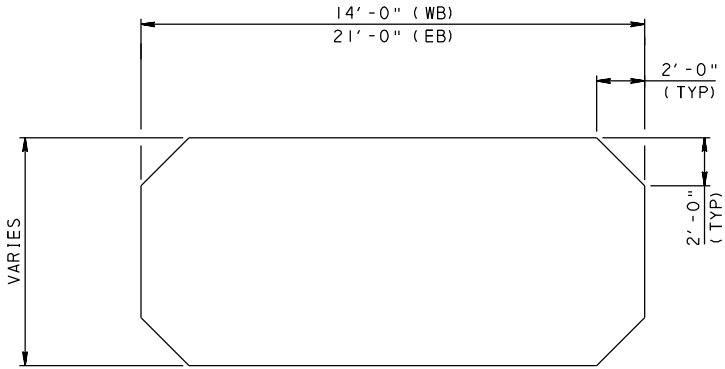
WB PIER - ELEVATION



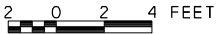
EB PIER - ELEVATION



SIDE VIEW



SECTION A-A



NOTE:

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

PIER DETAILS

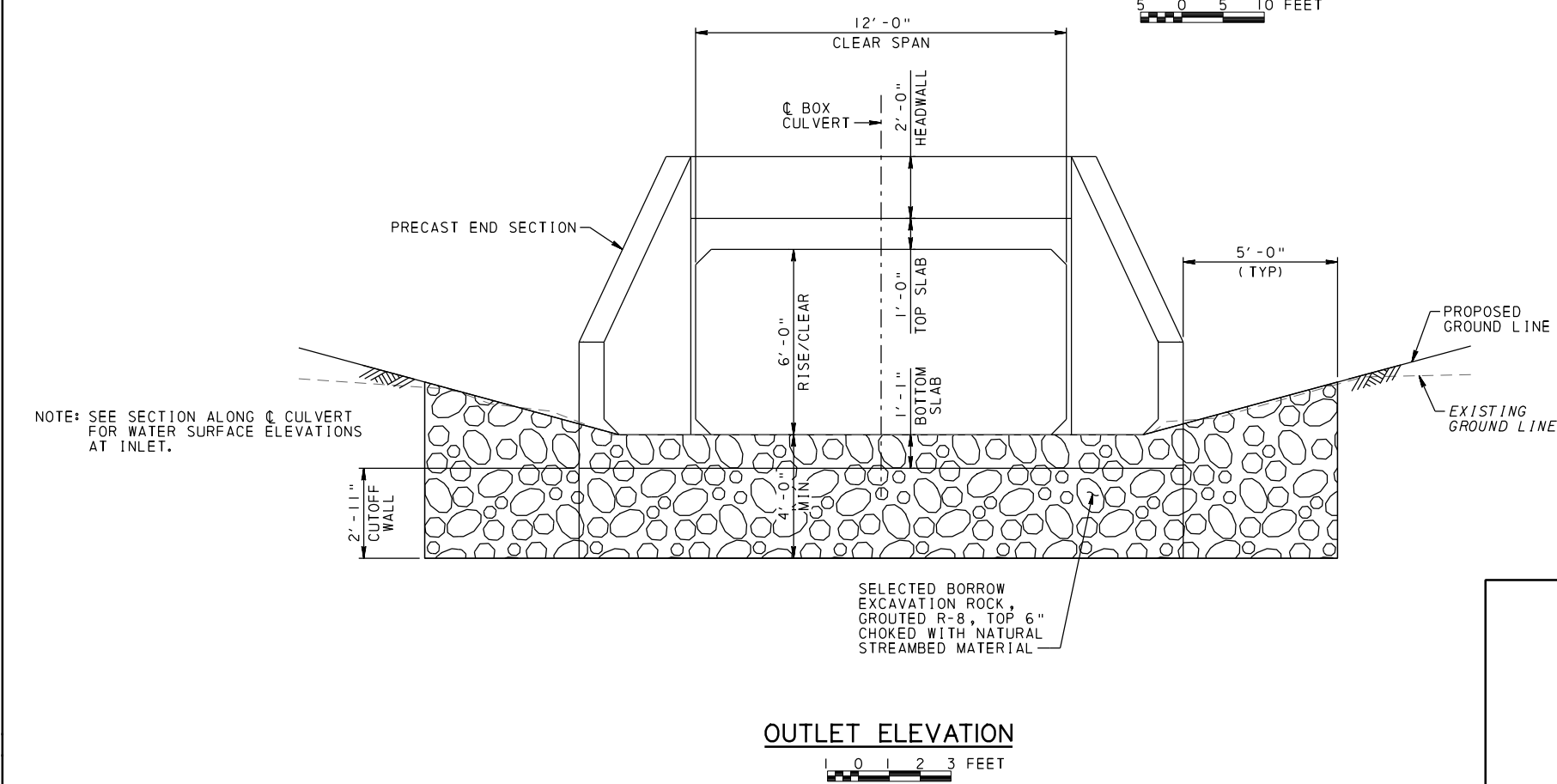
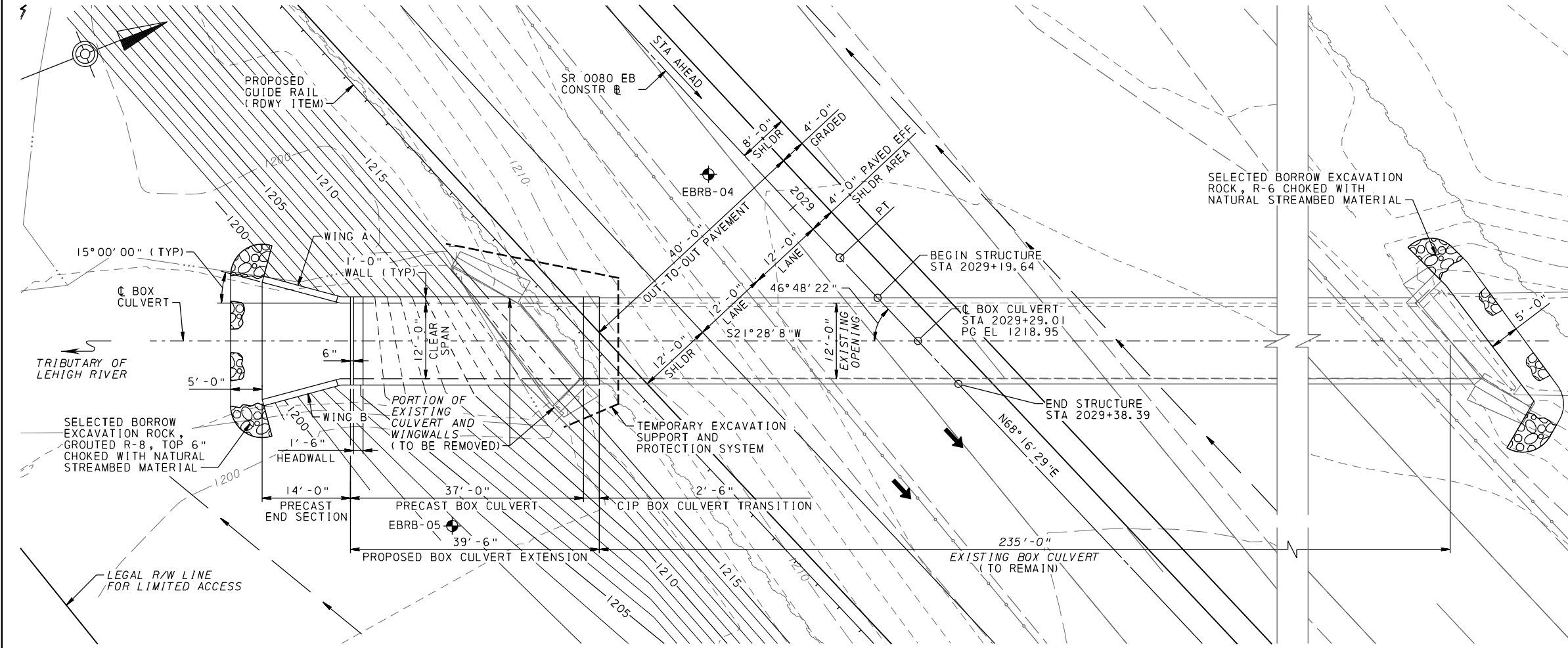
RECOMMENDED \_\_\_\_\_

SHEET 7 OF 7

S-XXXXX



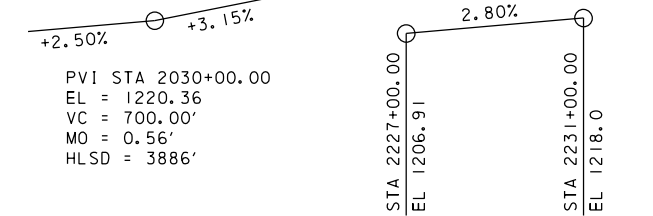
USER: AGRIES  
PATH: c:\pwworking\agries\01\1994974\  
FILE: 008008B-BR-CULV-TSL.dgn  
PLOT: DRIVER: PennDOT.PDF, Memo: p11c1c1g  
PLOT DATE: 11-03-2021 2:03:01 PM  
MODEL: Defoult



AS-DRILLED BORING LOCATION			
BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EBRB-04	2028+87.00	6.22 RT	1218
EBRB-05	2029+00.00	74.00 RT	1202

VERTICAL GEOMETRY  
SR 0080 EB CONSTR B

VERTICAL GEOMETRY  
SR 0080 WB CONSTR B



HORIZONTAL GEOMETRY  
SR 0080 EB CONSTR B

HORIZONTAL GEOMETRY  
SR 0080 WB CONSTR B

PI STA 2025+76.65 Δ = 7°35'47" LT T = 335.26' L = 669.53' R = 5050.00' E = 11.12' PC STA 2022+41.39 PT STA 2029+10.92 SUPERELEVATE 4.0% DESIGN SPEED 70 MPH	PI STA 2230+17.69 Δ = 7°34'41" LT T = 384.21 L = 767.30' R = 5801.43' E = 12.71' PCC STA 2226+33.48 PT STA 2234+00.79 SUPERELEVATE N/A DESIGN SPEED 70 MPH
--	---

HYDRAULIC DATA

DRAINAGE AREA= 1.7 SQ MI

DESIGN FLOOD (50 YEAR):  
Q = 511 CFS  
VELOCITY= 4.7 FPS  
WS EL= 1211.42

100 YEAR FLOOD:  
Q = 635 CFS  
VELOCITY= 4.8 FPS  
WS EL= 1212.43

FLOOD OF RECORD: N/A

- LEGEND:
- PROPOSED TEST BORINGS
  - EXISTING CONTOUR
  - PROPOSED CONTOUR
  - EDGE OF NORMAL HIGH WATER
  - DIRECTION OF TRAFFIC

PROPOSED REPAIRS TO EXISTING CULVERT

THE EXISTING CULVERT PRESERVATION CONSISTS OF, BUT IS NOT LIMITED TO, THE FOLLOWING ITEMS OF WORK:

- PERFORM TYPE I CONCRETE REPAIRS TO DETERIORATED AREAS OF EXISTING CULVERT (APPROX 425 SF).
- PERFORM TYPE II CONCRETE REPAIRS TO DETERIORATED AREAS OF EXISTING CULVERT (APPROX 6 SF).
- EPOXY INJECTION SEAL CRACKS IN THE EXISTING PORTION OF THE CULVERT (APPROX 275 LF).
- PLACE SELECTED BORROW EXCAVATION ROCK, R-6 CHOKED WITH NATURAL STREAMBED MATERIAL AT INLET (APPROX 19 CY).

- NOTES:
- FOR GENERAL NOTES, SEE SHEET 2.
  - FOR TYPICAL SECTIONS, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

CARBON COUNTY  
S.R. 0080 SECTION 08B  
SEG. 2730 OFF. 0300  
S.R. 0080 EASTBOUND STA. 2029+29.01  
OVER TRIBUTARY OF LEHIGH RIVER  
12'-0" x 6'-0" BOX CULVERT EXTENSION  
CONCEPTUAL TYPE, SIZE, & LOCATION

RECOMMENDED	SHEET 1 OF 3 +SUPPLEMENTAL DRAWINGS
DISTRICT 5-0 BRIDGE ENGINEER	S-XXXXX

PREPARED BY:

**HDR**

HDR ENGINEERING, INC.  
3025 CHEMICAL RD  
SUITE 110  
PLYMOUTH MEETING, PA 19462

SIGNATURE & DATE

GENERAL NOTES:

CULVERT EXTENSION DESIGN SPECIFICATIONS

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION, 2017 AND AS SUPPLEMENTED BY DESIGN MANUAL PART 4 DECEMBER 2019.

DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.

DESIGN LIVE LOADS

PHL-93, P-82, AND P2016-13

LIVE LOAD TO THE CULVERT IS BASED UPON DM-4 SECTION 4.6.2.10.

DEAD LOADS

A UNIT WEIGHT OF 0.140 kcf WAS USED FOR EARTH LOADS FOR CULVERT EXTENSION DESIGN.

GENERAL

PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH SPECIFICATIONS, PUBLICATION 408, 2020, AASHTO/AWS BRIDGE WELDING CODE D1.5-2008 (USE AASHTO/AWS D1.1-2008 FOR WELDING NOT COVERED IN AASHTO/AWS D1.5- 008), AND CONTRACT SPECIAL PROVISIONS.

VERIFY ALL DIMENSIONS AND GEOMETRY OF THE EXISTING STRUCTURE IN THE FIELD AS NECESSARY FOR PROPER FIT OF THE PROPOSED CONSTRUCTION.

USE CARE WHEN REMOVING PORTIONS OF THE EXISTING STRUCTURE SO AS TO NOT DAMAGE REMAINING PARTS OF THE STRUCTURE, OR REMAINING REINFORCEMENT BARS. REPLACE ALL PARTS WHICH ARE DESIGNATED TO REMAIN AND ARE DAMAGED DURING THE REMOVAL OPERATIONS AT NO COST TO THE DEPARTMENT.

DO NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE BY THE DEPARTMENT OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT YOU WILL ENCOUNTER IN THE FIELD.

THE INFORMATION SHOWN ON THE PLANS FOR THE EXISTING STRUCTURE IS NOT PART OF THE PLANS, PROPOSAL OR CONTRACT AND IS NOT TO BE CONSIDERED A BASIS FOR COMPUTATION OF THE UNIT PRICES USED FOR BIDDING PURPOSES. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT INFORMATION IS CORRECTLY SHOWN. THE BIDDER IS NOT TO RELY ON THIS INFORMATION, BUT IS TO ASSUME THE POSSIBILITY THAT CONDITIONS AFFECTING THE COST AND/OR QUANTITIES OF WORK TO BE PERFORMED MAY DIFFER FROM THOSE INDICATED (ORIGINAL PLANS, S-5856 - OCT, 1962).

CONCRETE NOTES

PROVIDE 2 INCH CONCRETE COVER ON REINFORCEMENT BARS EXCEPT AS NOTED.

USE CEMENT CONCRETE WITH A 28 DAY COMPRESSIVE STRENGTH OF 5,000 PSI WHEN CONSTRUCTING PRECAST BOX CULVERTS SEGMENTS AND PRECAST END SECTIONS.

USE CLASS AA CEMENT CONCRETE IN ALL CONCRETE REPAIRS, HEADWALL, AND CUTOFF WALL.

USE CLASS A CEMENT CONCRETE IN CAST-IN-PLACE TRANSITION.

A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO THE DEPARTMENT, IF APPROVED BY THE DISTRICT BRIDGE ENGINEER.

PROVIDE GRADE 60 REINFORCING BARS THAT MEET THE REQUIREMENTS OF ASTM A615/A615M, A996/A996M, OR A706/A706M. DO NOT WELD GRADE 60 REINFORCING STEEL BARS UNLESS SPECIFIED. GRADE 40 REINFORCING STEEL BARS MAY BE SUBSTITUTED WITH A PROPORTIONAL INCREASE IN CROSS-SECTIONAL AREA, IF APPROVED BY THE CHIEF BRIDGE ENGINEER. DO NOT USE RAIL STEEL A996/A996M REINFORCEMENT BARS.

USE EPOXY-COATED REINFORCEMENT BARS.

PROVIDE MINIMUM EMBEDMENT AND SPLICE LENGTHS IN ACCORDANCE WITH STANDARD DRAWING BC-736M, UNLESS OTHERWISE INDICATED.

GALVANIZED REINFORCING STEEL BARS MAY BE SUBSTITUTED FOR EPOXY-COATED REINFORCING STEEL BARS AT NO ADDITIONAL COST TO THE DEPARTMENT.

RAKE-FINISH ALL HORIZONTAL CONSTRUCTION JOINTS, EXCEPT AS INDICATED.

CHAMFER EXPOSED CONCRETE EDGES 1"x1" AND PRECAST CONCRETE EDGES ¾ "x¾ ".

ALL DIMENSIONS SHOWN ARE HORIZONTAL, EXCEPT AS NOTED.

WELDING OF REINFORCEMENT BARS DURING FABRICATION OR CONSTRUCTION IS NOT PERMITTED UNLESS SPECIFIED.

BOX CULVERT NOTES

DO NOT EXCEED A 2'-0" DIFFERENCE IN FILL ELEVATION ON THE SIDES OF THE BOX CULVERT DURING PLACEMENT OF BACKFILL. DO NOT ALLOW THE WHEELS OR ROLLERS TO COME CLOSER THAN 1'-0" TO THE FACE OF THE STRUCTURE DURING COMPACTION OF THE BACKFILL.

CAST ALL (EPOXY COATED OR GALVANIZED) THREADED INSERTS INTO THE PRECAST BOX SEGMENTS. ALL INSERTS ARE INCIDENTAL TO THE BOX CULVERT SEGMENTS.

PREPARE EXISTING CONCRETE SURFACES WHICH WILL BE AGAINST NEWLY PLACED CONCRETE IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1001.3(m). APPLY AN EPOXY BONDING COMPOUND BETWEEN EXISTING AND NEW CAST-IN-PLACE CONCRETE. MATERIAL, SURFACE PREPARATION, AND INSTALLATION IS INCIDENTAL TO CLASS A CEMENT CONCRETE.

USE EPOXY BONDING COMPOUND WHEREVER CAST-IN-PLACE CEMENT CONCRETE COMES IN CONTACT WITH PRECAST CEMENT CONCRETE. THE EPOXY COMPOUND IS TYPE 2, GRADE 2, AS DESCRIBED IN ASTM-C881-90 AND IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1001.2(k). CLEAN SURFACES OF ALL MATERIAL OR CONTAMINATE DETRIMENTAL TO PROPER BONDING. MATERIAL, SURFACE PREPARATION, AND INSTALLATION IS INCIDENTAL TO THE CLASS OF CONCRETE FOR WHICH IT IS APPLIED.

REMOVE THE EXISTING CULVERT ONLY AS CALLED FOR BY PLAN OR AS PERMITTED BY THE DEPARTMENT'S REPRESENTATIVE.

BOX CULVERT NOTES (CONTINUED)

PROVIDE 2'-0" MINIMUM WIDTH OF WATERPROOFING MEMBRANE AS PER PUBLICATION 408/2020, SECTION 680.2(a) OR 680.2(b) ALONG THE TOP AND SIDE OF JOINTS. FOR ADDITIONAL DETAILS, REFER TO BC-788M.

PROVIDE 4'-0" MINIMUM SEGMENT LENGTHS.

SITE CLASS IS NOT CLASS E.

SUBMIT ERECTION DRAWINGS FOR REVIEW AND APPROVAL PRIOR TO SETTING BOX SECTIONS AND PRECAST END SECTIONS.

DETAIL ON SHOP DRAWINGS THE PRECAST BOX SECTION AND PRECAST END SECTION LENGTHS THAT MEET ALL THE HAULING GUIDELINES AND RESTRICTIONS, HAULING PERMITS ARE THE RESPONSIBILITY OF THE CONTRACTOR. SHOW METHOD OF BRACING AND MOVING SECTIONS TO AVOID CRACKING DURING TRANSPORTATION AND ERECTION.

SHIP PRECAST BOX SECTIONS AND PRECAST END SECTIONS FROM FABRICATOR AFTER ATTAINING REQUIRED 28 DAY COMPRESSIVE STRENGTHS.

USE GALVANIZED MECHANICAL STRAP CONNECTIONS WITH PRECAST END SECTION CONSTRUCTION.

PROPOSED CULVERT IS NOT WEIGHT RESTRICTED. SEE PUBLICATION 408 SECTION 105.17 FOR CONSTRUCTION LOADING LIMITS.

UTILITY NOTES

COORDINATE, LOCATE AND CONDUCT ALL WORK RELATED TO PUBLIC AND PRIVATE UTILITIES IN ACCORDANCE WITH PUBLICATION 408, 2020, SECTIONS 105.06 AND 107.12.

COORDINATE THE REQUIREMENTS FOR PROTECTION AND/OR RELOCATION OF UTILITIES WITH THE UTILITY OWNER PRIOR TO STARTING WORK.

VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO STARTING WORK. CONDUCT OPERATIONS IN A MANNER WHICH ENSURES THAT THE UTILITIES WILL NOT BE DISTURBED OR ENDANGERED AND ASSUME FULL RESPONSIBILITY FOR ANY DAMAGE TO UTILITIES DURING CONSTRUCTION. PENNDOT DOES NOT ASSUME RESPONSIBILITY FOR REIMBURSEMENT, PARTICIPATION IN DESIGN AND/OR REVISIONS OR LIABILITY FOR ACCURACY OF TYPE, SIZE AND LOCATION OF ANY UTILITY.

GALVANIZING NOTES

GALVANIZE ALL FABRICATED STRUCTURAL STEEL AFTER FABRICATION.

HOT DIP GALVANIZE THE FOLLOWING:  
BOX CULVERT MECHANICAL STRAP CONNECTION

MECHANICALLY GALVANIZE THE FOLLOWING:  
ASTM 325 BOLTS, ASTM A563 NUTS, AND ASTM F436 WASHERS

REPAIR ALL DAMAGED GALVANIZED SURFACES IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1105.2(s).

POST TENSIONING NOTES

EXTEND BOTTOM SLAB POST-TENSIONING STRANDS THROUGH THE BOTTOM SLAB OF THE PRECAST CONCRETE END SECTIONS.

POST-TENSION THE PRECAST BOX SEGMENTS AND END SECTIONS PER BC-798M AND PUBLICATION 408 SECTION 1108.

SUBMIT THE POST-TENSION CONNECTION DESIGN COMPUTATIONS FOR THE BOX CULVERT AND END SECTIONS WITH THE SHOP DRAWINGS IN ACCORDANCE WITH BC-798M. INCLUDE STRAND PATTERN, STRAND LOADING, AND STRESSING DETAILS ON THE SHOP DRAWINGS.

POST-TENSIONING DUCTS MAY BE PLACED AT EITHER CORNER OF HAUNCH TO SATISFY DESIGN AND CONSTRUCTION REQUIREMENTS. PROVIDE A 3" CONCRETE CLEARANCE. HAUNCH DUCT MUST BE TIED TO INSIDE REINFORCEMENT. LOCATION MUST MEET FABRICATION REQUIREMENTS.

IF TIGHT JOINTS ARE NOT ATTAINED IN ANY LOCATION AFTER POST-TENSIONING, RELEASE STRANDS AND REPEAT UNTIL JOINTS ARE TIGHT AND APPROVED BY THE DEPARTMENT REPRESENTATIVE.

BOX SEGMENTS AND END SECTIONS ARE POST-TENSIONED IN STAGES. THE CONTRACTOR IS REQUIRED TO SUBMIT A PLAN FOR POST-TENSIONING SEQUENCE TO THE DEPARTMENT FOR APPROVAL PRIOR TO SETTING ANY SEGMENTS.

POST-TENSION BOX SEGMENTS FIRST, THEN PROVIDE:  
•MECHANICAL SPLICERS ON BOTTOM STRANDS TO CONNECT WITH THE INLET/OUTLET END SECTIONS AND POST-TENSION BOTTOM STRANDS THROUGH THE END SECTIONS.  
•STRANDS ON SIDES OF END SECTION AS SHOWN ON BC-798M.

STAGING, SPACING, AND POST-TENSIONING FORCE TO BE SHOWN ON FABRICATOR'S SHOP DRAWINGS.

CLASSIFICATION OF EARTHWORK FOR STRUCTURES	RC-11M	6/01/2010
BACKFILL AT STRUCTURES	RC-12M	2/08/2019
ANCHOR SYSTEMS	BC-734M	2/19/2021
WALL CONSTRUCTION AND EXPANSION JOINT DETAILS	BC-735M	9/30/2016
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	1/31/2019
BRIDGE DRAINAGE	BC-751M	1/31/2019
REINFORCED CONCRETE REPAIRS	BC-783M	1/31/2019
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	1/31/2019
MECHANICAL CONNECTION DETAILS	BC-798M	1/31/2019
DESCRIPTION	DWG. NO.	APP. DATE
SUPPLEMENTAL DRAWINGS		

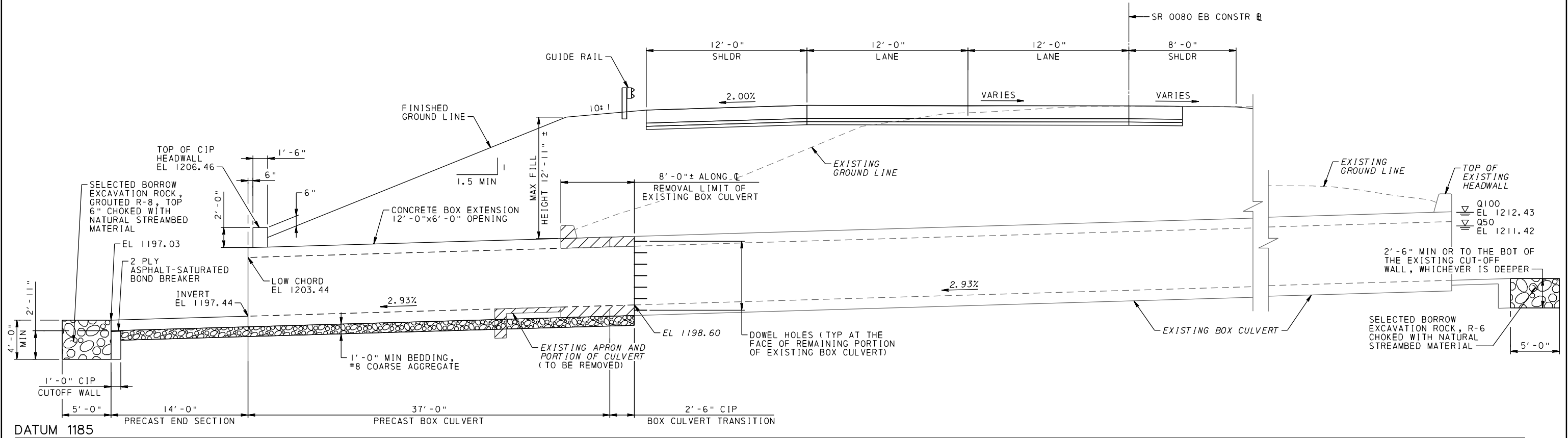
Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

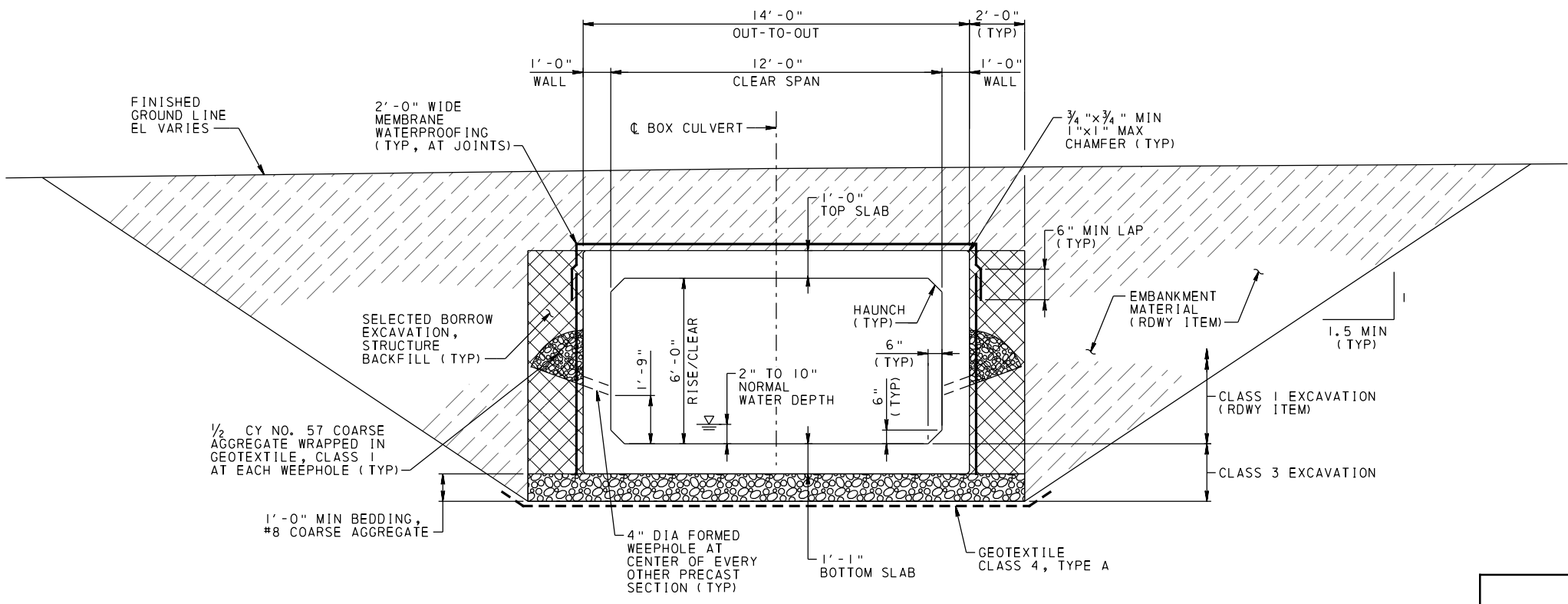
COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

CARBON COUNTY  
S.R. 0080 SECTION 08B  
SEG. 2730 OFF. 0300  
S.R. 0080 EASTBOUND STA. 2029+29.01  
OVER TRIBUTARY OF LEHIGH RIVER  
12'-0" x 6'-0" BOX CULVERT EXTENSION  
GENERAL NOTES

RECOMMENDED _____	SHEET <u>2</u> OF <u>3</u>
S-XXXXX	

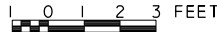


SECTION ALONG  $\phi$  BOX CULVERT



BOX CULVERT EXTENSION TYPICAL SECTION

PERPENDICULAR TO  $\phi$  BOX CULVERT



NOTES:  
1. FOR GENERAL NOTES, SEE SHEET 2.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

CARBON COUNTY  
S.R. 0080 SECTION 08B  
SEG. 2730 OFF. 0300  
S.R. 0080 EASTBOUND STA. 2029+29.01  
OVER TRIBUTARY OF LEHIGH RIVER  
12'-0" x 6'-0" BOX CULVERT EXTENSION  
TYPICAL SECTIONS

RECOMMENDED \_\_\_\_\_

SHEET 3 OF 3

S-XXXXX

DISTRICT	COUNTY	TOWNSHIP	BOROUGH	ROUTE	SECTION	TOTAL SHEETS
5-0	LUZERNE		WHITE HAVEN	0080	08B	26
	CARBON		EAST SIDE			
	CARBON	KIDDER				

MPMS/ECMS NO. 99552

COMMONWEALTH OF PENNSYLVANIA



DEPARTMENT OF TRANSPORTATION

DESIGN DRAWINGS

FOR

CONSTRUCTION

OF

STATE ROUTE 0080 SECTION 08B

IN LUZERNE COUNTY

FROM STA. 1984+55.00 EB TO STA. 2018+90.04 EB LENGTH 3385.04 FT. 0.64 MI.

FROM SEG. 2720 OFFSET 1097 TO SEG. 2728 OFFSET 0558

FROM STA. 2197+00.00 WB TO STA. 2218+90.04 WB LENGTH 2090.04 FT. 0.40 MI.

FROM SEG. 2721 OFFSET 2330 TO SEG. 2729 OFFSET 0533

AND

IN CARBON COUNTY

FROM STA. 2018+90.04 EB TO STA. 2049+06.00 EB LENGTH 2915.96 FT. 0.55 MI.

FROM SEG. 2728 OFFSET 0558 TO SEG. 2730 OFFSET 2204

FROM STA. 2218+90.04 WB TO STA. 2249+70.00 WB LENGTH 2979.96 FT. 0.56 MI.

FROM SEG. 2729 OFFSET 0533 TO SEG. 2731 OFFSET 2253

ALSO

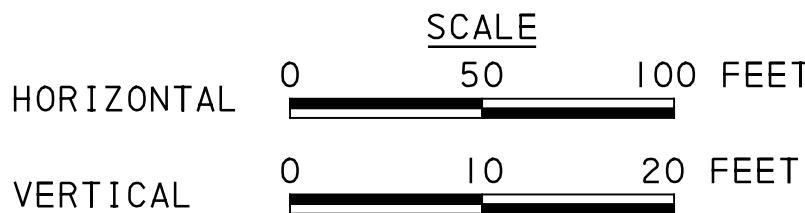
STATE ROUTE 8006 SECTION 08B

IN LUZERNE COUNTY

ALSO INCLUDED:

TRAFFIC CONTROL PLAN	59 SHEETS
SIGNING AND PAVEMENT MARKING PLAN	8 SHEETS
EROSION AND SEDIMENT POLLUTION CONTROL PLAN	6 SHEETS
POST CONSTRUCTION STORMWATER MANAGEMENT PLAN	11 SHEETS
STRUCTURE PLAN	
S-	7 SHEETS
S-	3 SHEETS
CROSS SECTIONS	101 SHEETS
OPEN ROAD CASHLESS TOLLING SITE DEVELOPMENT PLAN	6 SHEETS
OPEN ROAD CASHLESS TOLLING SIGNING AND PAVEMENT MARKING PLAN	27 SHEETS
OPEN ROAD CASHLESS TOLLING ELECTRICAL AND COMMUNICATIONS SERVICE PLAN	5 SHEETS
OPEN ROAD CASHLESS TOLLING STRUCTURE PLAN	3 SHEETS
OPEN ROAD CASHLESS TOLLING UTILITY BUILDING PLAN	9 SHEETS

ESTABLISHED AS A LIMITED ACCESS HIGHWAY FROM STATION 1989+00 EB TO STATION 2039+75 EB AND STATION 2197+00 WB TO STATION 2249+70 WB LEGISLATIVE ROUTE 1009 SECTION 95 R/W APPROVED OCTOBER 8, 1963.



DESIGN DESIGNATION

HIGHWAY CLASSIFICATION	- INTERSTATE/FREEWAY
ROADWAY TYPOLOGY	- LIMITED ACCESS FREEWAY, RURAL INTERSTATE
DESIGN SPEED	- 70 MPH (65 MPH POSTED)
PAVEMENT WIDTH	- 24' (2-12' LANES)
SHOULDER WIDTH	- 20' (12' OUTSIDE, 8' INSIDE)
MEDIAN WIDTH, MAXIMUM	- 110'
MINIMUM	- 60'

TRAFFIC DATA

CURRENT A. D. T.	- 32,602 (2026)
DESIGN YEAR A. D. T.	- 53,736 (2046)
D. H. V. -	3,762
D	- 52%
T	- 34%

DESIGN FIELD VIEW SUBMISSION  
HDR ENGINEERING INC.  
DECEMBER 2021

PREPARED BY:  
**HDR**  
HDR ENGINEERING, INC.  
3025 CHEMICAL ROAD  
SUITE 110  
PLYMOUTH MEETING, PA 19462

REGISTERED PROFESSIONAL  
ENGINEER  
DATE:

RECOMMENDED DATE:

DISTRICT EXECUTIVE

RECOMMENDED DATE:

DEPUTY SECRETARY

APPROVED DATE:

SECRETARY OF TRANSPORTATION  
(ON BEHALF OF THE GOVERNOR  
AS WELL AS THE SECRETARY)





TABULATION OF OVERALL LENGTH

SR 0080 EB STA 1984+55.00 TO STA 2049+06.00 = 6451.00 FT = 1.22 MILES  
SR 0080 WB STA 2197+00.00 TO STA 2249+70.00 = 5270.00 FT = 0.99 MILES  
TOTAL = 11,721.00 FT = 2.22 MILES

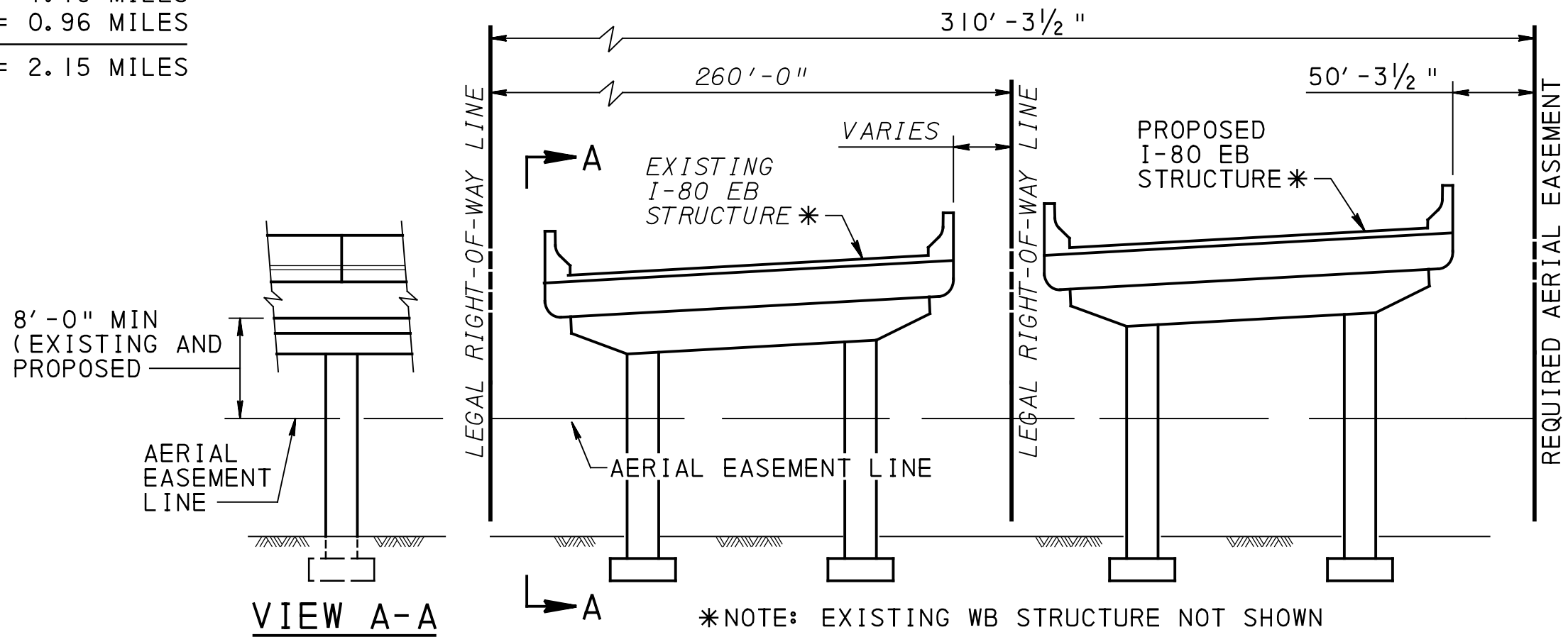
TABULATION OF CONSTRUCTION LENGTH

SR 0080 EB STA 1985+05.00 TO STA 2048+06.00 = 6301.00 FT = 1.19 MILES  
SR 0080 WB STA 2198+00.00 TO STA 2248+70.00 = 5070.00 FT = 0.96 MILES  
TOTAL = 11,371.00 FT = 2.15 MILES

LIST OF EQUALITIES

SR 0940 STA 38+82.93 BK = STA 38+82.89 AHD  
SR 0940 STA 48+65.03 BK = STA 48+65.00 AHD

AERIAL EASEMENT SKETCH



SKETCH SHOWING ESTATE TO BE ACQUIRED FOR A LIMITED AERIAL EASEMENT  
FROM STA 2012+85.49 TO STA 2020+18.88 (SR 0080 EB CONSTR B)

USE OF AIRSPACE BENEATH THE ESTABLISHED GRADELINE OF THE HIGHWAY SHALL PROVIDE SUFFICIENT VERTICAL AND HORIZONTAL CLEARANCES FOR THE CONSTRUCTION, OPERATION, MAINTENANCE, VENTILATION AND SAFETY OF THE HIGHWAY FACILITY. THE ESTATE ACQUIRED ABOVE THE AERIAL EASEMENT LINE MAY BE ENTERED ON BY MOVING VEHICLES SUCH AS TRUCKS OR RAILROAD ROLLING STOCK.

GENERAL NOTES ( CONT)

DETAILS, OTHER THAN THOSE INDICATED, ARE ON THE FOLLOWING STANDARD DRAWINGS:

RC-10M (1) JUNE 1, 2010	TC-8600 (13) JUNE 13, 2013
RC-11M (2) JUNE 1, 2010	TC-8602 (4) JUNE 13, 2013
RC-12M (2) FEB. 8, 2019	TC-8604 (4) JUNE 13, 2013
RC-13M (1) JUNE 1, 2010	TC-8700C (12) JUNE 13, 2013
RC-20M (13) DEC. 17, 2019	TC-8701A (13) JUNE 13, 2013
RC-22M (6) FEB. 8, 2019	TC-8701D (13) JUNE 13, 2013
RC-24M (3) FEB. 19, 2021	TC-8702A (8) JUNE 13, 2013
RC-25M (4) FEB. 8, 2019	TC-8702B (9) JUNE 13, 2013
RC-27M (2) JUNE 1, 2010	TC-8702C (2) JUNE 13, 2013
RC-28M (2) FEB. 8, 2019	TC-8702E (5) JUNE 13, 2013
RC-30M (5) DEC. 17, 2019	TC-8710 (1) JUNE 13, 2013
RC-31M (2) JUNE 1, 2010	TC-8716 (1) JUNE 13, 2013
RC-32M (1) JUN. 1, 2010	TC-8717 (1) JUNE 13, 2013
RC-33M (2) JUNE 1, 2010	
RC-39M (30) FEB. 19, 2021	BC-721M (2) FEB. 19, 2021
RC-40M (1) FEB. 8, 2019	BC-732M (3) JAN. 31, 2019
RC-45M (24) FEB. 19, 2021	BC-734M (2) FEB. 19, 2021
RC-46M (34) FEB. 19, 2021	BC-735M (1) SEPT. 30, 2016
RC-50M (12) FEB. 19, 2021	BC-736M (3) JAN. 31, 2019
RC-51M (15) FEB. 19, 2021	BC-751M (7) JAN. 31, 2019
RC-54M (12) DEC. 17, 2019	BC-752M (2) FEB. 19, 2021
RC-58M (4) AUG. 4, 2017	BC-753M (3) JAN. 31, 2019
RC-60M (3) JUNE 1, 2010	BC-754M (2) JAN. 31, 2019
RC-65M (1) DEC. 17, 2019	BC-783M (4) JAN. 31, 2019
RC-70M (3) FEB. 8, 2019	BC-788M (12) JAN. 31, 2019
RC-71M (4) AUG. 4, 2017	BC-798M (3) JAN. 31, 2019
RC-72M (7) FEB. 8, 2019	
RC-73M (4) FEB. 8, 2019	
RC-75M (1) JUNE 1, 2010	
RC-77M (1) DEC. 17, 2019	

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	3 OF 26
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE

GENERAL NOTES

THE LEGAL RIGHT-OF-WAY ON SR 0080, FORMERLY LR 1009, FROM STATION 1984+55.00 EB TO STATION 2018+90.04 EB AND FROM STATION 2197+00.00 WB TO STATION 2218+90.04 WB VARIES BASED ON THE PLAN OF LR 1009, SECTION 95 R/W, SIGNED ON OCTOBER 8, 1963 AND RECORDED IN THE LUZERNE COUNTY RECORDER'S OFFICE IN MAP BOOK 23, PAGE 5.

THE LEGAL RIGHT-OF-WAY ON SR 0080, FORMERLY LR 1009, FROM STATION 2018+90.04 EB TO STATION 2049+06.00 EB AND FROM STATION 2218+90.04 WB TO STATION 2249+70.00 WB VARIES BASED ON THE PLAN OF LR 1009, SECTION 96 R/W, SIGNED ON OCTOBER 24, 1963 AND RECORDED IN THE CARBON COUNTY RECORDER'S OFFICE IN MAP BOOK 1.

THE LEGAL RIGHT-OF-WAY ON SR 0940, FORMERLY LR 40118, FROM STATION 27+11.75 TO STATION 50+00.00 VARIES BASED ON THE PLAN OF LR 1009, SECTION 95 R/W, SIGNED ON OCTOBER 8, 1963 AND RECORDED IN THE LUZERNE COUNTY RECORDER'S OFFICE IN MAP BOOK 23, PAGE 5.

THE LEGAL RIGHT-OF-WAY ON SR 1005, FORMERLY LR 13018, FROM STATION 5+00.00 TO STATION 26+47.73 IS THIRTY-TWO (32') FEET AS NOTED IN THE PLAN OF LR 1009, SECTION 96 R/W, SIGNED ON OCTOBER 24, 1963 AND RECORDED IN THE CARBON COUNTY RECORDER'S OFFICE IN MAP BOOK 1.

THIS IS A FEDERAL-AID PROJECT AND AS SUCH IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE FEDERAL HIGHWAY ADMINISTRATION AND THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION.

THE PROJECT SURVEY IS BASED UPON THE NATIONAL GEODETIC REFERENCE SYSTEM (FORMERLY USC&GS).

THE HORIZONTAL SURVEY INFORMATION IS BASED UPON THE STATE PLANE COORDINATE SYSTEM NORTH ZONE (NAD83).

THE VERTICAL CONTROL IS BASED UPON THE NORTH AMERICAN VERTICAL DATUM (NADV88).

COMBINED SCALE FACTOR = 1.000072908

ALL CURVE DATA IS BASED ON THE ARC DEFINITION UNLESS OTHERWISE INDICATED.

THREE WORKING DAYS PRIOR TO EXCAVATION, THE CONTRACTOR MUST CONTACT THE PA ONE CALL SYSTEM, INC., PHONE 1-800-242-1776, SERIAL NO. \_\_\_\_\_ FOR WHITE HAVEN BOROUGH SERIAL NO. \_\_\_\_\_ FOR EAST SIDE BOROUGH

TEMPORARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OF THE PROJECT. THE EASEMENT IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE DEPARTMENT.

CHANNEL EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF THE COURSE OF THE CHANNEL. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY FLOW OF WATER.

WHERE AN AERIAL EASEMENT IS ACQUIRED, IT SHALL INCLUDE AN EASEMENT IN THE AIR FOR THE ACCOMMODATION OF THE ELEVATED HIGHWAY STRUCTURE UNLIMITED IN VERTICAL DIMENSION ABOVE THE STRUCTURE, A SURFACE EASEMENT UNLIMITED IN VERTICAL DIMENSION FOR THE ACCOMMODATION OF PIERS AND OTHER APPURTENANCES AND A TEMPORARY EASEMENT FOR CONSTRUCTION PURPOSES INCLUDING THE STORAGE OF MATERIALS DURING CONSTRUCTION FOR THE ENTIRE AREA. THE FOLLOWING LIMITATIONS SHALL BE IMPOSED ON THE PROPERTY BENEATH THE AREA AFFECTED BY THE AERIAL EASEMENT.

SLOPE EASEMENT. AN EASEMENT FOR THE SUPPORT AND PROTECTION OF THE HIGHWAY, INCLUDING THE RIGHT TO CONSTRUCT, INSPECT, MAINTAIN, REPAIR, RECONSTRUCT AND ALTER DRAINAGE FACILITIES AND THE CONTOUR OF THE LAND. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY SUPPORT AND PROTECTION OF THE HIGHWAY RIGHT-OF-WAY AND THE SAFETY OF THE TRAVELING PUBLIC.

LIST OF PUBLIC UTILITIES

PUBLIC UTILITIES					
LEGEND	UTILITY COMPANY	TELEPHONE	MAILING ADDRESS	COMPANY REPRESENTATIVE	E-MAIL
—S—	AQUA PENNSYLVANIA WASTEWATER, INC (SANITARY SEWER)	(570) 443 7099 X55502	1 AQUA WAY WHITE HAVEN, PA 18661	MR. JOSHUA SHOFF	JPSHOFF@AQUAAMERICA.COM
—W—	AQUA PENNSYLVANIA WASTEWATER, INC (WATER)	(570) 443 7099 X55502	1 AQUA WAY WHITE HAVEN, PA 18661	MR. JOSHUA SHOFF	JPSHOFF@AQUAAMERICA.COM
—CTV— —CTVU—	ATLANTIC BROADBAND	(570) 802 5642 X1174	911 N MARKET ST BERWICK, PA 18603	MR. DAVID RICHARDS	DRICHARDS@ATLANTICBB.COM
—E— —EU—	PPL ELECTRIC UTILITIES CORPORATION	(610) 774 6287	2 N 9TH ST ALLENTOWN, PA 18101-1179	MS. CHARLOTTE KRUPA	CAKRUPA@PPLWEB.COM
—G—	UGI UTILITIES INC.	(610) 807 3157 (570) 450 2612	2121 CITY LINE RD BETHLEHEM, PA 18017	MR. BRANDON HAYDT	BHAYDT@UGI.COM
—T— —TU—	VERIZON PENNSYLVANIA LLC	(570) 424 0902	20 S 7TH ST STROUDSBURG, PA 18360	MR. JOE SNYDER	J.ANTHONY.SNYDER@VERIZON.COM
PENNSYLVANIA ONE CALL		TOLL FREE TELEPHONE NO.: 1-800-242-1776		DESIGNER SERIAL NO.: 20183300986, 1013, 1016, 1017	

EARTHWORK SUMMARY ENTIRE PROJECT

THE INFORMATION ON ESTIMATED AMOUNTS OF EARTHWORK HAS BEEN USED IN THE PRELIMINARY ESTIMATE. DO NOT USE AS A WAIVER OF ANY PROVISIONS OF THE SPECIFICATIONS AND CONTRACTS.						
CU. YDS. OF EXCAVATION						CUBIC YDS. OF COMPLETED EMBANKMENT *
CLASS 1	CLASS 1A	CLASS 1B	CLASS 2	CLASS 3	CLASS 4	CUBIC YDS. OF BORROW EXCAVATION
						CUBIC YARDS OF WASTE

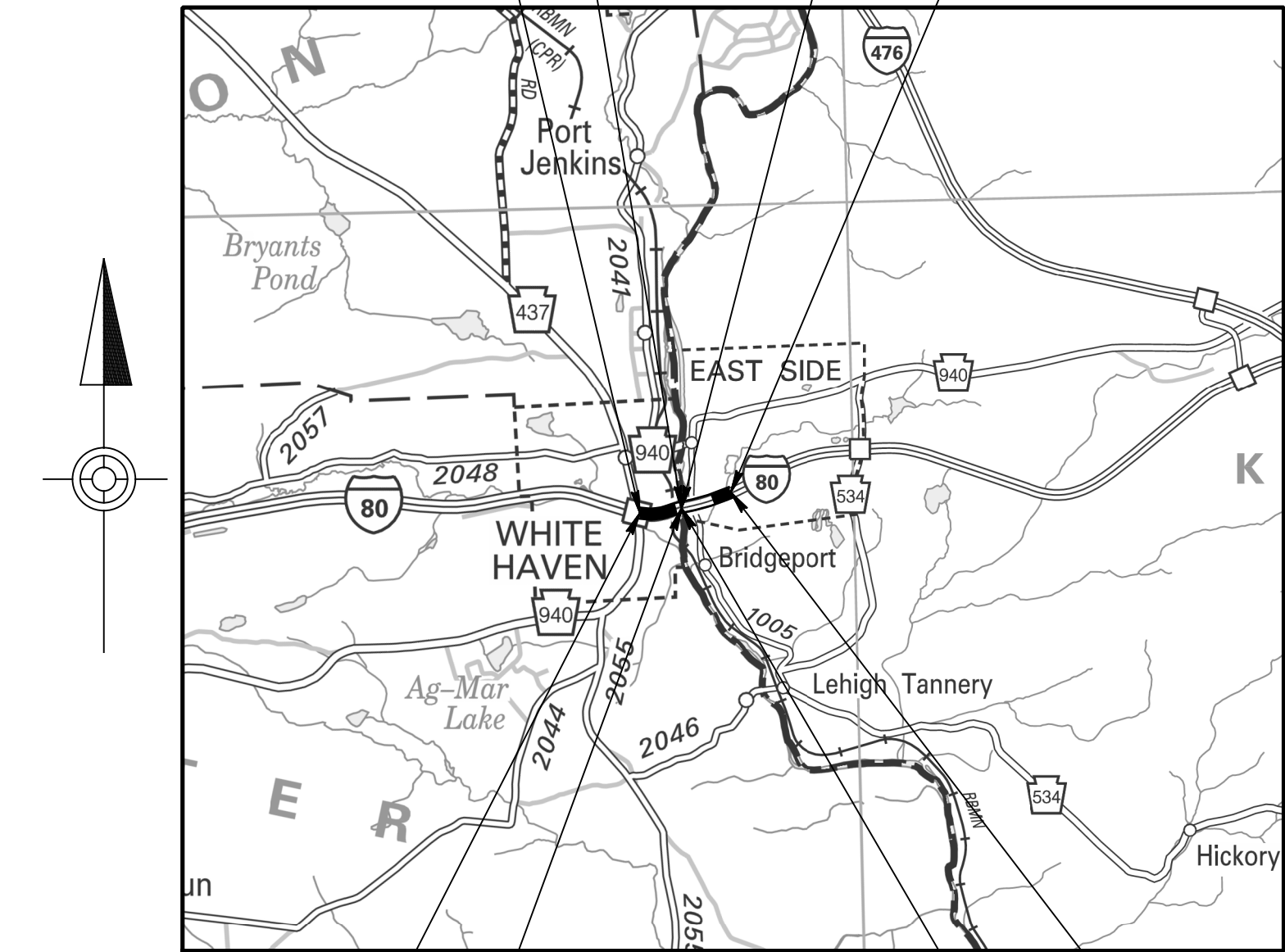
\* INCLUDES ALL BORROW ITEMS.

LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

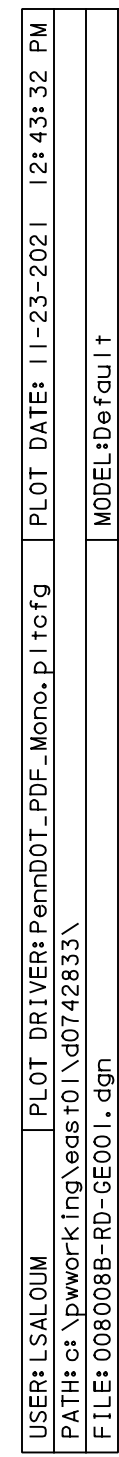
LIMIT OF WORK  
STA 2197+00.00 WB  
SEG 2721 OFFSET 2330  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

LIMIT OF WORK  
STA 2249+70.00 WB  
SEG 2731 OFFSET 2253  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY













USER: LSAJLJUM

PATH: g:\pwworking\1040742833\

FILE: 008008B-RD-SUP01.dgn

PLOT: DRIVER: PenPlot\_PDF\_Memo.plt:cf6g

FILE: 008008B-RD-SUP01.dgn

PLOT DATE: 11-23-2021 12:43:37 PM

MODEL: default

SR 0080 EB SUPERELEVATION TABLE						
STATION	REMARKS	LEFT (SHOULDER)		SR 0080 EB	RIGHT (SHOULDER)	RAMP C GORE
		GRADED	PAVED			
2002+75.00	MATCH EXISTING BEGIN SE TRANSITION	-7.71%	-7.71%	+7.07%	-0.26%	N/A
2002+79.74	PCC	-6.97%	-6.97%	+6.97%	-0.60%	
2002+88.00	END SE TRANSITION BEGIN FULL SE	-6.80%	-6.80%	+6.80%		
2003+00.00	END RIGHT SHOULDER SE TRANSITION				-2.00%	
2007+59.00	BEGIN SE TRANSITION	-6.80%	-6.80%	+6.80%		
2008+04.00	END SE TRANSITION PCC	-7.80%	-7.80%	+7.80%	-2.00%	
2008+50.00	BEGIN RIGHT SHOULDER SE TRANSITION				-2.00%	
2009+00.00	END RIGHT SHOULDER SE TRANSITION				+6.00%	
2009+01.03	BEGIN RAMP C GORE					
2009+18.15	BEGIN GORE TRANSITION					
2009+50.33	END GORE TRANSITION					N/A*
2011+49.00	BEGIN SE TRANSITION - END FULL SE	-7.80%	-7.80%	+7.80%		
2012+44.26	END RAMP C GORE					
2012+65.48	PT	-5.21%	-5.21%	+5.21%		
2013+20.00	END LEFT SHOULDER SE TRANSITION	-4.00%	-4.00%	+4.00%		
2015+90.00	END SE TRANSITION - BEGIN NORMAL CROWN			-2.00%		
2020+31.00	BEGIN SE TRANSITION - END NORMAL CROWN			-2.00%		
2020+85.48	END RAMP C BL				-4.00%	
2021+21.00	BEGIN RIGHT SHOULDER SE TRANSITION			0.00%	-4.00%	
2022+11.00	END RIGHT SHOULDER SE TRANSITION			+2.00%	-2.00%	N/A
2022+39.73	PC	-4.00%	-4.00%	+2.66%	-2.00%	
2023+01.00	END SE TRANSITION - BEGIN FULL SE			+4.00%	-2.00%	
2028+51.00	BEGIN SE TRANSITION - END FULL SE			+4.00%		
2029+10.92	PT	-4.00%	-4.00%	+2.86%	-2.00%	
2029+41.00	END SE TRANSITION			+2.00%	-2.00%	
2034+50.00	BEGIN SHOULDER SE TRANSITION (BOTH)	-4.00%	-4.00%	+2.00%	-2.00%	
2034+65.00	BEGIN SE TRANSITION			+2.00%		
2034+75.00	END SE TRANSITION - MATCH EXISTING	-5.89%	-5.89%	+2.06%	-5.33%	

\*SEE RAMP C SUPERELVATION TABLE FOR RIGHT SHOULDER CROSS SLOPE

SR 0080 WB SUPERELEVATION TABLE						
STATION	REMARKS	SR 0080 WB - RAMP B GORE	LEFT (SHOULDER)	SR 0080 WB	RIGHT (SHOULDER)	
					PAVED	GRADED
2205+50.00	MATCH EXISTING - BEGIN SE TRANSITION BEGIN RIGHT PAVED SHOULDER SE TRANSITION BEGIN LEFT SHOULDER SE TRANSITION	N/A	-8.89%	-6.53%	+0.47%	-2.00%
2205+75.00	END LEFT SHOULDER SE TRANSITION		-8.00%		+2.00%	
2206+17.00	END SE TRANSITION BEGIN FULL SE		-8.00%	-8.00%	+2.00%	-2.00%
2209+73.00	BEGIN LEFT SHOULDER SE TRANSITION		-8.00%			
2210+23.00	END LEFT SHOULDER SE TRANSITION		-2.00%			
2210+23.29	BEGIN RAMP B GORE	-2.00%	-2.00%			
2210+26.00	END FULL SE BEGIN SE TRANSITION			-8.00%		
2210+98.44	PT	-2.00%		-6.00%	+2.00%	-2.00%
2211+16.00	BEGIN RIGHT PAVED SHOULDER SE TRANSITION		N/A	-6.00%	+2.00%	-2.00%
2211+52.33	END RAMP B GORE	-2.00%				
2212+06.00	END RIGHT PAVED SHOULDER SE TRANSITION			-4.00%	-2.00%	-2.00%
2212+96.00	END SE TRANSITION - BEGIN NORMAL CROWN BEGIN RIGHT SHOULDER SE TRANSITION			-2.00%	-2.00%	-2.00%
2213+11.00	END RAMP B RESUME LEFT SHOULDER		-4.00%			
2213+21.00	END RIGHT SHOULDER TRANSITION				-4.00%	-4.00%
2222+08.00	BEGIN RIGHT SHOULDER TRANSITION				-4.00%	-4.00%
2222+48.00	END NORMAL CROWN - BEGIN SE TRANSITION END RIGHT SHOULDER TRANSITION	N/A		-2.00%	-2.00%	-2.00%
2222+84.39	PC		-4.00%	-2.81%	-2.00%	-2.00%
2223+02.00	END SE TRANSITION - BEGIN FULL SE			-3.20%		
2226+33.48	PCC		-4.00%	-3.20%	-2.00%	-2.00%
2226+63.00	BEGIN SE TRANSITION - END FULL SE		-4.00%	-3.20%	-2.00%	-2.00%
2227+00.00	END SE TRANSITION MATCH EXISTING		-4.43%	-2.38%	+0.86%	-1.69%

(+) FOR A POSITIVE GRADE AND (-) FOR A NEGATIVE GRADE WITH RESPECT TO HORIZONTAL AT EACH PIVOT POINT.

SR 8006 ( RAMP B ) SUPERELEVATION TABLE					
STATION	REMARKS	LEFT (SHOULDER)	RAMP B	RIGHT (SHOULDER)	
				PAVED	GRADED
60+45.47	BEGIN SE TRANSITION - MATCH EXISTING	-4.00%	+0.06%	N/A	N/A
60+96.00	END SE TRANSITION - BEGIN NORMAL CROWN	-4.00%	+2.00%	-4.00%	-4.00%
63+70.00	BEGIN RIGHT SHOULDER TRANSITION			-4.00%	-4.00%
63+95.00	BEGIN SE TRANSITION - END NORMAL CROWN AND RIGHT PAVED SHOULDER TRANSITION		+2.00%	-2.00%	-2.00%
64+43.33	BEGIN LEFT & RIGHT PAVED SHOULDER TRANSITION	-4.00%	+4.00%	-2.00%	-2.00%
64+75.18	PC	-5.32%	+5.32%		
64+91.66	END RIGHT PAVED SHOULDER TRANSITION	-6.00%	+6.00%	+2.00%	-2.00%
65+40.00	END SE TRANSITION AND LEFT SHOULDER TRANSITION	-8.00%	+8.00%	+2.00%	-2.00%
68+49.49	END RIGHT SHOULDER - BEGIN RAMP B GORE	-8.00%	+8.00%	+2.00% ( GORE )	
69+10.00	END FULL SE - BEGIN SE TRANSITION	-8.00%	+8.00%		
69+72.30	END RAMP B GORE				
69+92.45	PT	-5.32%	+5.32%	SR 0080 WB	
70+34.67	END LEFT SHOULDER SE TRANSITION	-4.00%	+4.00%		
70+97.00	END SE TRANSITION		+2.00%		
71+30.13	END RAMP B	-4.00%	+2.00%		

SR 8006 ( RAMP C ) SUPERELEVATION TABLE					
STATION	REMARKS	LEFT (SHOULDER)		RAMP C	RIGHT (SHOULDER)
		GRADED	PAVED		
30+00.00	MATCH EXISTING SR 0940 BEGIN SE TRANSITION	N/A	N/A	+0.50%	-5.51%
30+25.00	GRAPHIC GRADE POINTS - MATCH SR 0940			+0.54%	-4.00%
30+50.00	GRAPHIC GRADE POINTS - MATCH SR 0940			+1.85%	
30+75.00	GRAPHIC GRADE POINTS - MATCH SR 0940			+2.07%	
31+00.00	GRAPHIC GRADE POINTS - MATCH SR 0940			+2.08%	
31+25.00	GRAPHIC GRADE POINTS - MATCH SR 0940			+2.67%	
31+50.00	GRAPHIC GRADE POINTS - END MATCH SR 0940			+3.67%	
31+56.40				+4.00%	-4.00%
31+74.42	PC			+4.74%	
31+75.00	BEGIN SE TRANSITION			+4.74%	-4.74%
32+21.00	END SE TRANSITION - BEGIN FULL SE			+7.00%	-7.00%
32+52.56	BEGIN LEFT PAVED SHOULDER SE TRANSITION			+3.40%	
33+02.56	END LEFT PAVED SHOULDER SE TRANSITION			-2.00%	
34+58.00	BEGIN SE TRANSITION - END FULL SE			N/A	+7.00%
35+04.61	PT	-2.00%	-2.00%	+4.67%	-4.67%
35+18.00	END RIGHT SHOULDER TRANSITION			+4.00%	-4.00%
35+58.00	BEGIN LEFT SHOULDER TRANSITION	-2.00%	-2.00%		
35+98.00	END LEFT SHOULDER TRANSITION	-4.00%	-4.00%		
36+38.00	END SE TRANSITION - BEGIN NORMAL CROWN	-4.00%	-4.00%	-2.00%	-4.00%
38+99.00	BEGIN LEFT SHOULDER TRANSITION	-4.00%	-4.00%		
39+25.00	BEGIN RIGHT SHOULDER SE TRANSITION				-4.00%
39+42.94	PC	-5.76%	-5.76%	-2.00%	-2.56%
39+49.00	END LEFT SHOULDER TRANSITION - BEGIN RAMP C GORE	-6.00%	-6.00%		
39+50.00	END RIGHT SHOULDER SE TRANSITION END NORMAL CROWN	N/A		-2.00%	-2.00%
39+67.00	END SE TRANSITION			-2.60%	-2.00%
43+20.48	PT			-2.60%	-2.00%
44+38.00	BEGIN SE TRANSITION - MATCH SR 0080 EB			-2.60%	-2.00%
44+65.00	BEGIN RIGHT SHOULDER TRANSITION			-2.00%	-2.00%
45+55.00	END RIGHT SHOULDER TRANSITION			0.00%	-4.00%
46+45.00	END SE TRANSITION BEGIN NORMAL CROWN			-2.00%	-4.00%
50+86.00	END NORMAL CROWN BEGIN SE TRANSITION			-2.00%	-4.00%
51+40.48	END RAMP C			-0.79%	-4.00%

RAMP C SPUR SUPERELEVATION TABLE					
STATION	REMARKS	LEFT (SHOULDER)		RAMP C SPUR	RIGHT (SHOULDER)
		GRADED	PAVED		
20+73.61	BEGIN RAMP C SPUR - MATCH EXISTING				
20+84.75	TIE TO SR 0940 EOP	-4.00%	-4.00%	+3.29%	N/A
21+00.00				+3.26%	
21+07.27	BEGIN RIGHT SHOULDER			+3.26%	+3.26%
21+10.00	BEGIN RIGHT SHOULDER SE TRANSITION				+3.26%
21+17.00	BEGIN SE TRANSITION	-4.00%	-4.00%	+3.26%	
21+60.00	END RIGHT SHOULDER SE TRANSITION				-2.00%
21+75.20	END RIGHT SHOULDER				
21+83.10	BEGIN LEFT SHOULDER SE TRANSITION	-4.00%	-4.00%	0.00%	
22+23.65	END RAMP C SPUR GORE END LEFT SHOULDER SE TRANSITION	-2.00%	-2.00%	-2.00%	N/A ( RAMP C - RAMP C SPUR GORE )
22+25.00	XS CONTROLLED BY RAMP C EOP			-2.09%	
22+50.00	XS CONTROLLED BY RAMP C EOP			-2.85%	
22+75.00	XS CONTROLLED BY RAMP C EOP			-3.52%	
23+00.00	XS CONTROLLED BY RAMP C EOP			-4.34%	
23+05.28	END SE TRANSITION - MATCH RAMP C SE	-2.00%	-2.00%	-4.67%	

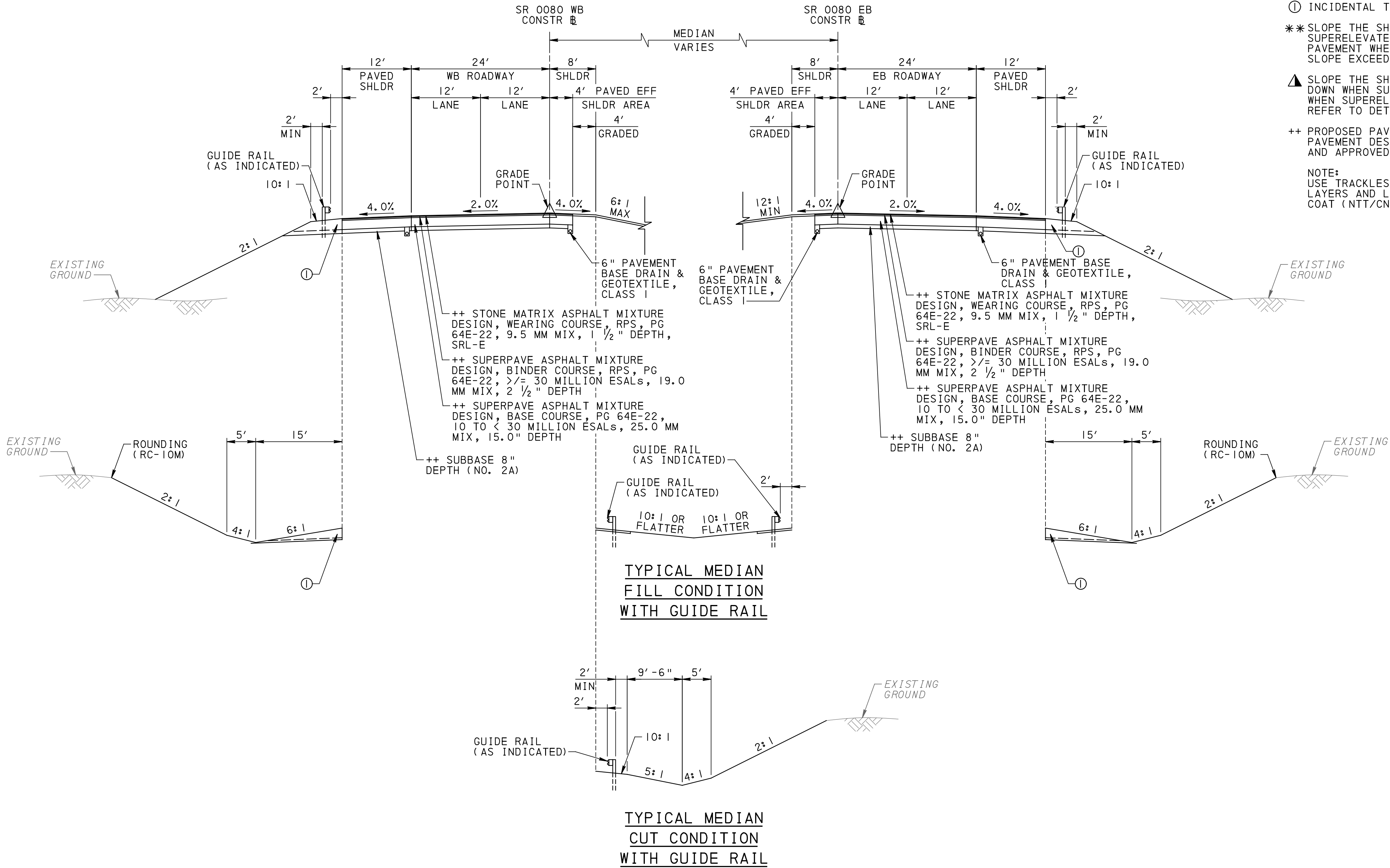
\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	7 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHs AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

USER: LSALOWM PLOT DRIVER: PennDOT - PDF - Mono - Plotter Path: \\pwwork\eng\des\101\10742833\ FILE: 008008B-RD-TYP01.dgn PLOT DATE: 11-23-2021 12:43:41 PM MODEL: Default

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	8 OF 26
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE



① INCIDENTAL TO SUBBASE (NO. 2A)

\*\* SLOPE THE SHOULDER ON THE LOW SIDE OF A SUPERELEVATED SECTION THE SAME RATE AS THE PAVEMENT WHEN THE RATE OF PAVEMENT CROSS SLOPE EXCEEDS 4.0%

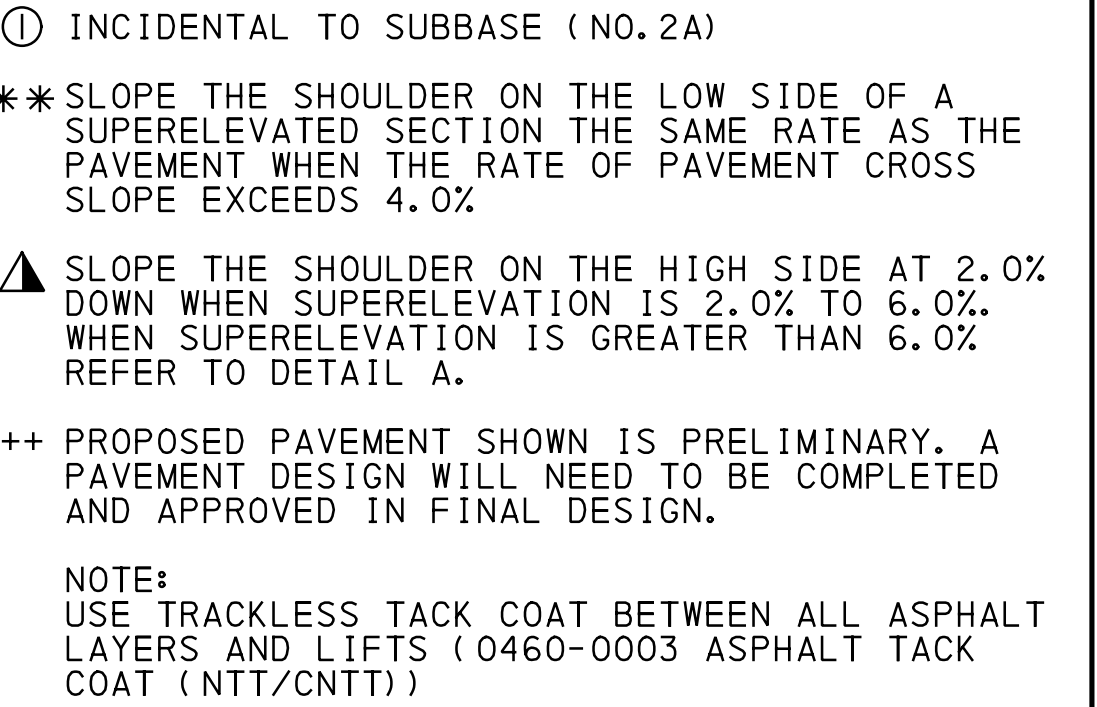
▲ SLOPE THE SHOULDER ON THE HIGH SIDE AT 2.0% DOWN WHEN SUPERELEVATION IS 2.0% TO 6.0%. WHEN SUPERELEVATION IS GREATER THAN 6.0% REFER TO DETAIL A.

++ PROPOSED PAVEMENT SHOWN IS PRELIMINARY. A PAVEMENT DESIGN WILL NEED TO BE COMPLETED AND APPROVED IN FINAL DESIGN.

NOTE:  
USE TRACKLESS TACK COAT BETWEEN ALL ASPHALT LAYERS AND LIFTS (0460-0003 ASPHALT TACK COAT (NTT/CNTT))

SR 0080 TYPICAL TANGENT SECTION

NOT TO SCALE  
SR 0080 STA 2212+96.00 TO STA 2213+21.37 WB  
SR 0080 STA 2221+51.37 TO STA 2222+48.00 WB



		NOT TO SCALE			
SR 0080	STA 2002+75.00	T	TO STA 2012+76.39	EB	
SR 0080	STA 2021+91.39	T	TO STA 2034+75.00	EB	
SR 0080	STA 2205+50.00	T	TO STA 2212+96.00	WB	
SR 0080	STA 2222+48.00	T	TO STA 2227+00.00	WB	



USER: LSALOW  
PATH: \\pwworking\eng\101\10742833\  
FILE: 000008B-RD-TYP03.dgn  
PLOT DATE: 11-23-2021 12:43:46 PM  
MODEL: default

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	10 OF 26	
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

\*\* EXISTING PAVEMENT (LUZERNE COUNTY)  
STA 1999+35.00 EB TO STA 2002+75.00 EB AND  
STA 2198+60.00 WB TO STA 2203+15.00 WB

STONE MATRIX ASPHALT MIXTURE DESIGN, HMA WEARING  
COURSE, RPS, PG 76-22, >= 30 MILLION ESALS, 9.5  
MM MIX, 1 1/2" DEPTH, SRL-E

ULTRA-THIN BONDED WEARING COURSE, GRADATION TYPE  
C, PG 76-22, SRL-E, 1" DEPTH

PLAIN CEMENT CONCRETE PAVEMENT, 13" DEPTH

OGS SUBBASE, 4" DEPTH

SUBBASE, NO 2A, 8" DEPTH

\*\* EXISTING PAVEMENT (CARBON COUNTY)  
STA 2034+75.00 EB TO STA 2038+15.00 EB AND  
STA 2227+00.00 WB TO STA 2240+00.00 WB

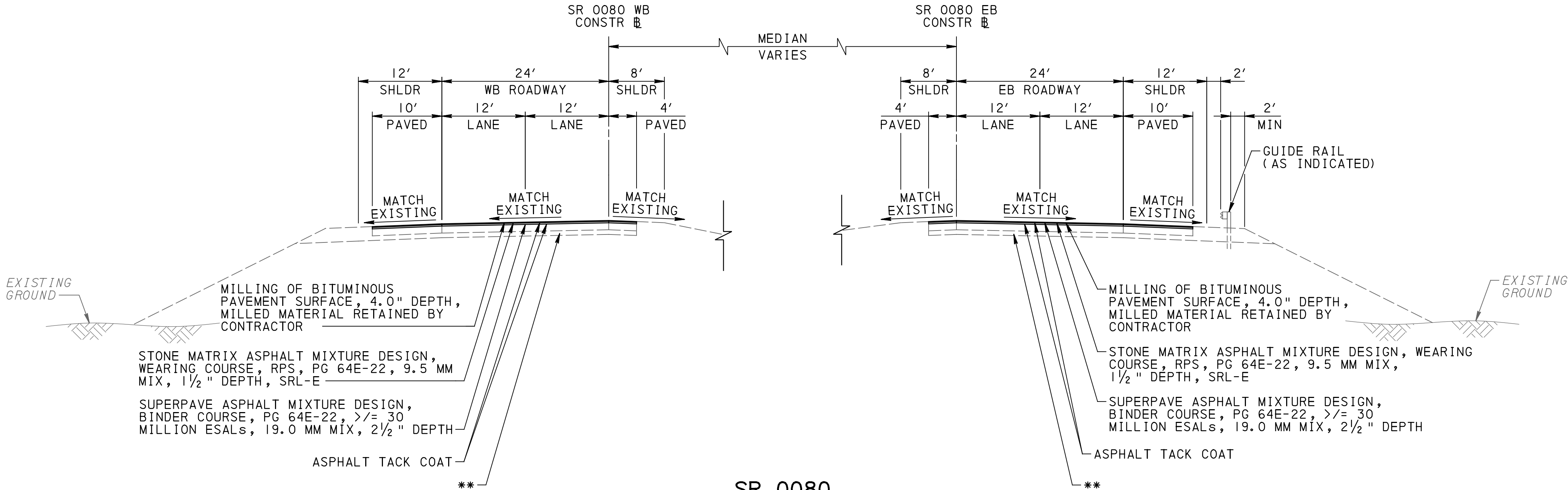
STONE MATRIX ASPHALT MIXTURE DESIGN, WMA WEARING  
COURSE, RPS, PG 76-22, 9.5 MM MIX, 1 1/2" DEPTH,  
SRL-E

SUPERPAVE ASPHALT MIXTURE DESIGN, WMA BINDER  
COURSE, RPS, PG 76-22, >= 30 MILLION ESALS, 19.0  
MM MIX, 2 1/2" DEPTH

SUPERPAVE ASPHALT MIXTURE DESIGN, WMA BASE  
COURSE, PG 64+22, >= 30 MILLION ESALS, 25.0 MM  
MIX, 5" DEPTH

REINFORCED CONCRETE CEMENT PAVEMENT, 13" DEPTH,  
CRACKED AND SEATED

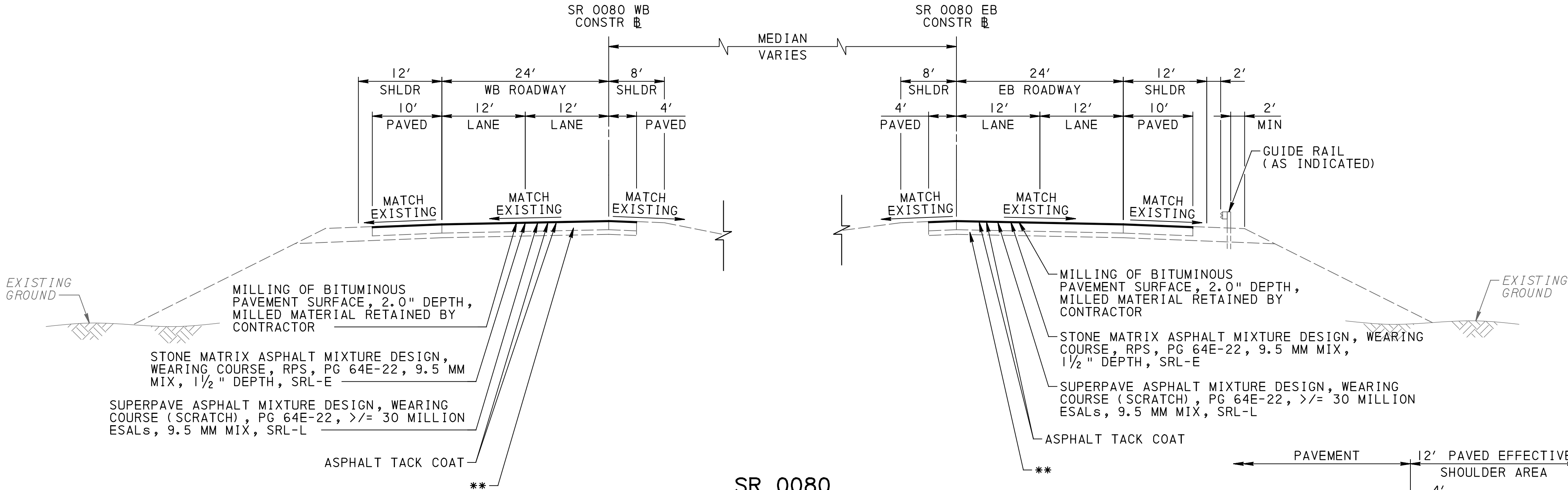
OGS SUBBASE, 12" DEPTH



**SR 0080**  
**TYPICAL MILL AND OVERLAY SECTION (LUZERNE COUNTY)**  
NOT TO SCALE

SR 0080 STA 1999+35.00 EB TO STA 2002+75.00 EB

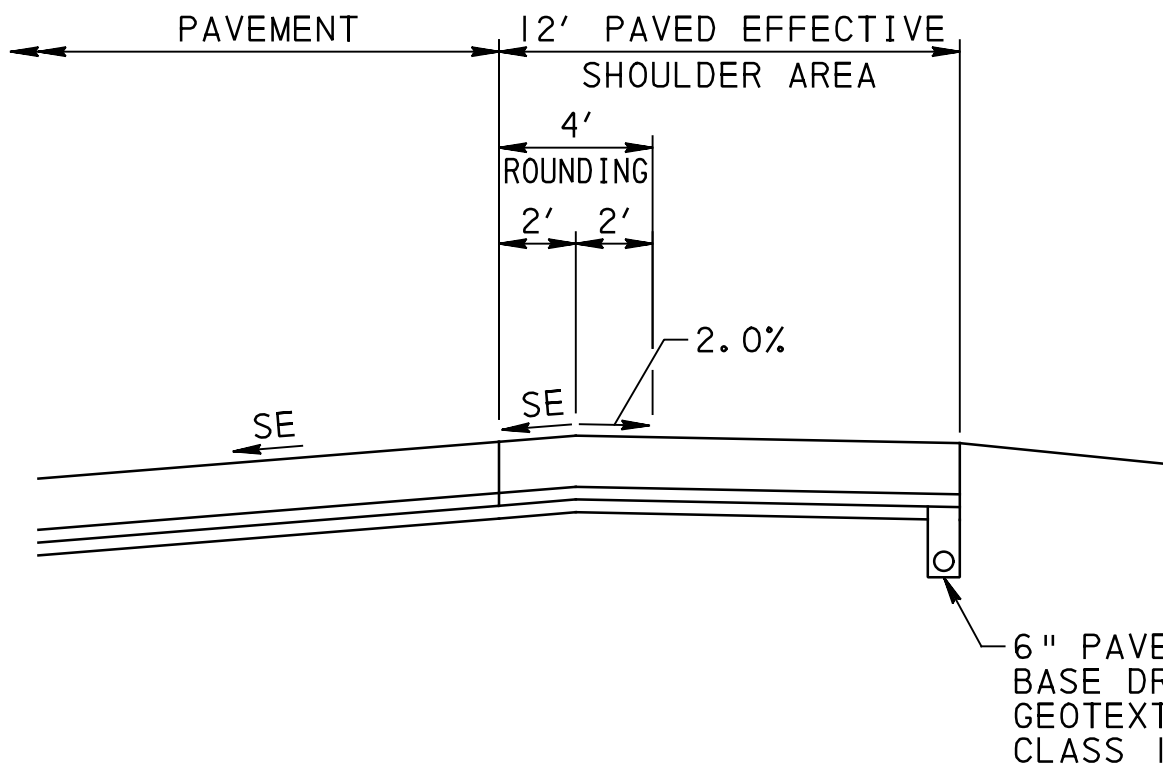
SR 0080 STA 2198+60.00 WB TO STA 2205+50.00 WB



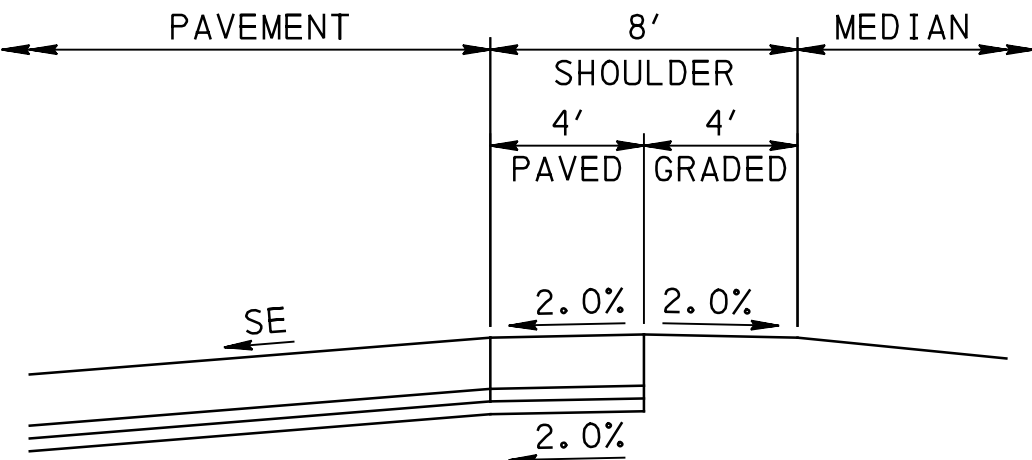
**SR 0080**  
**TYPICAL MILL AND OVERLAY SECTION (CARBON COUNTY)**  
NOT TO SCALE

SR 0080 STA 2034+75.00 EB TO STA 2038+15.00 EB

SR 0080 STA 2227+00.00 WB TO STA 2240+00.00 WB



**OUTSIDE SHOULDER**



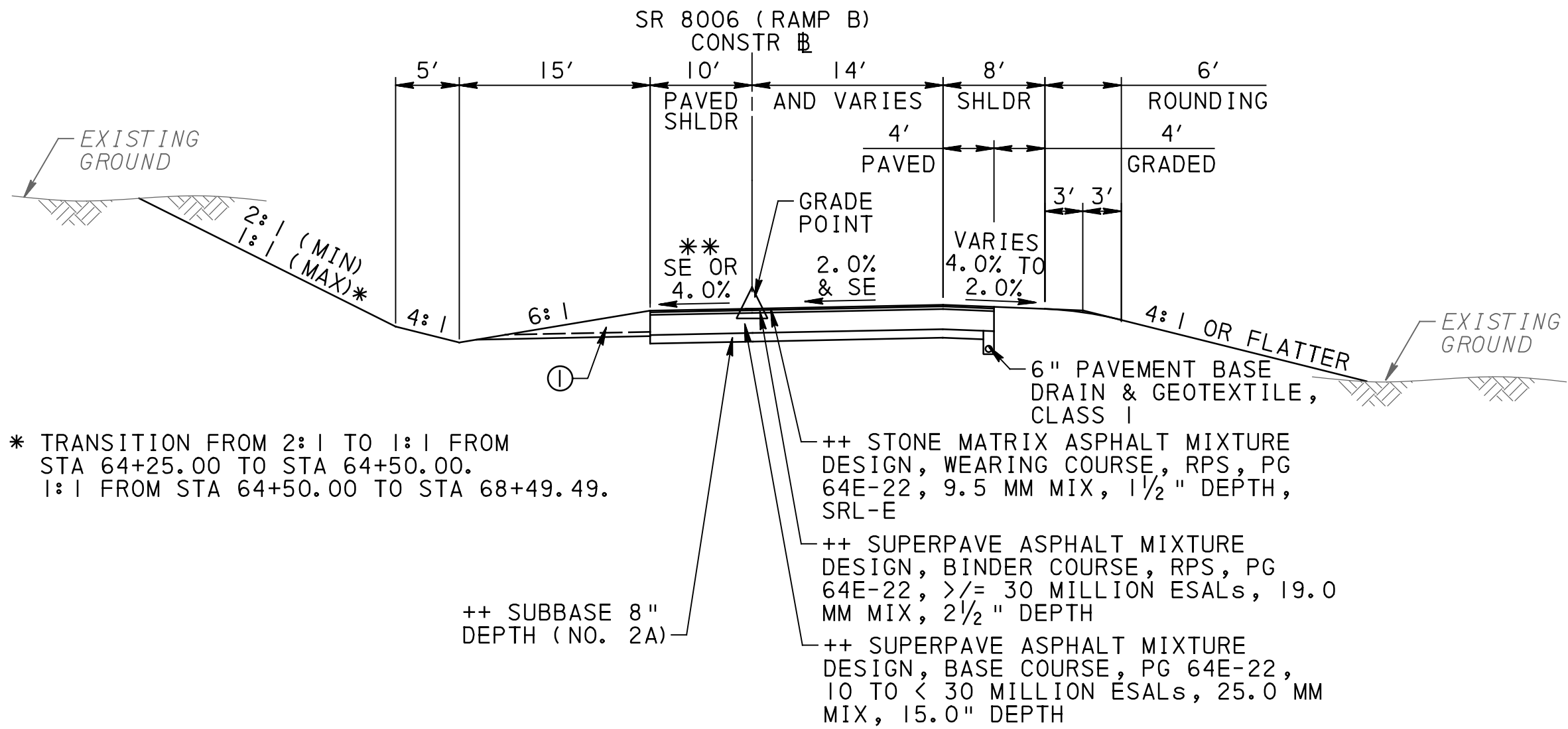
**MEDIAN SHOULDER**

**DETAIL A**  
**SE > 6.0%**  
NOT TO SCALE

USER: LSAJUM PLOT: DRIVER: FernDOT - PDF - Mono - Plotterf9 PLOT DATE: 11-23-2021 12:43:49 PM  
PATH: c:\pwworking\louis\10742833\ FILE: 008008B-RD-TYP04.dgn MODEL: default

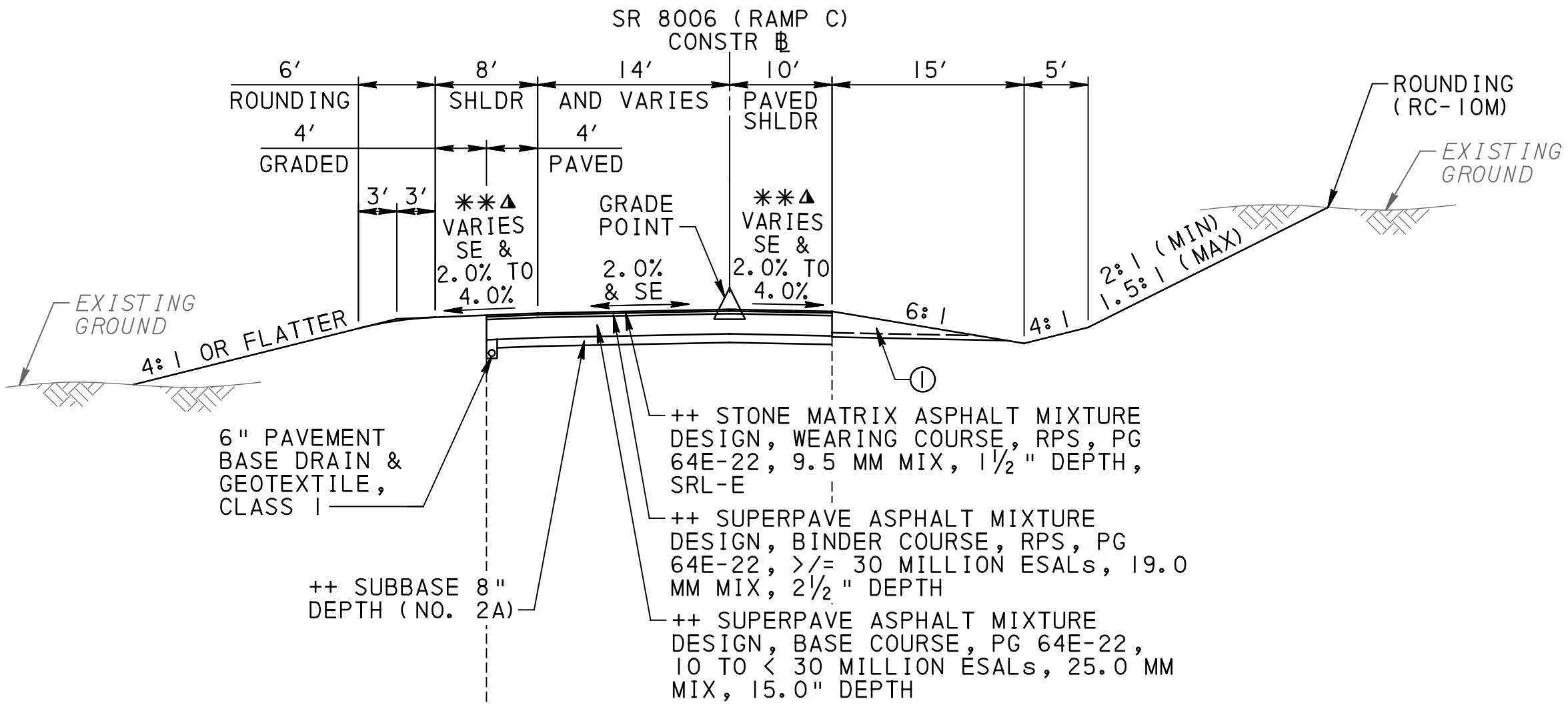
LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	11 OF 26	
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



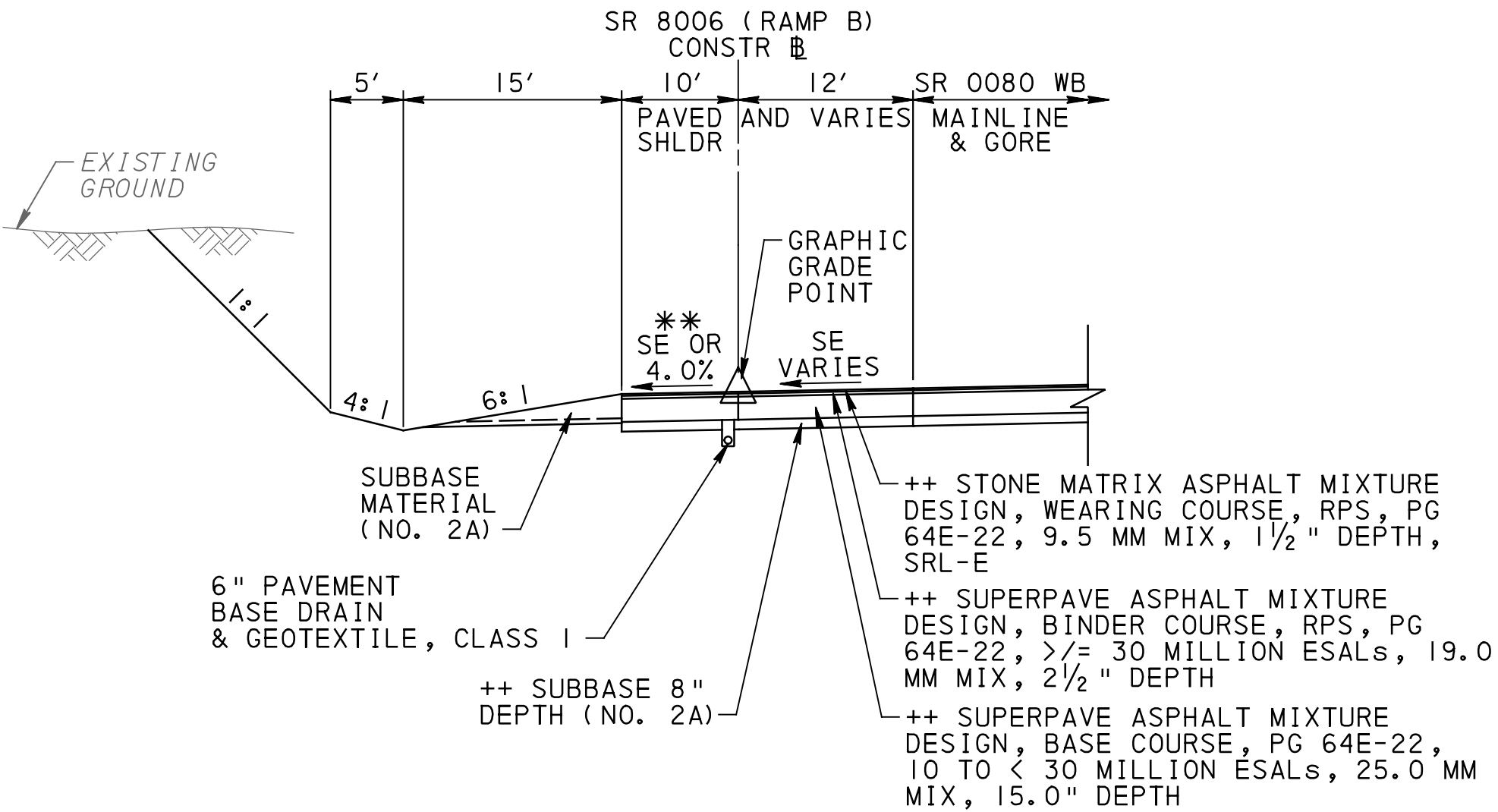
### SR 8006 (RAMP B) TYPICAL SECTION

NOT TO SCALE  
STA 60+14.23 TO STA 68+49.49



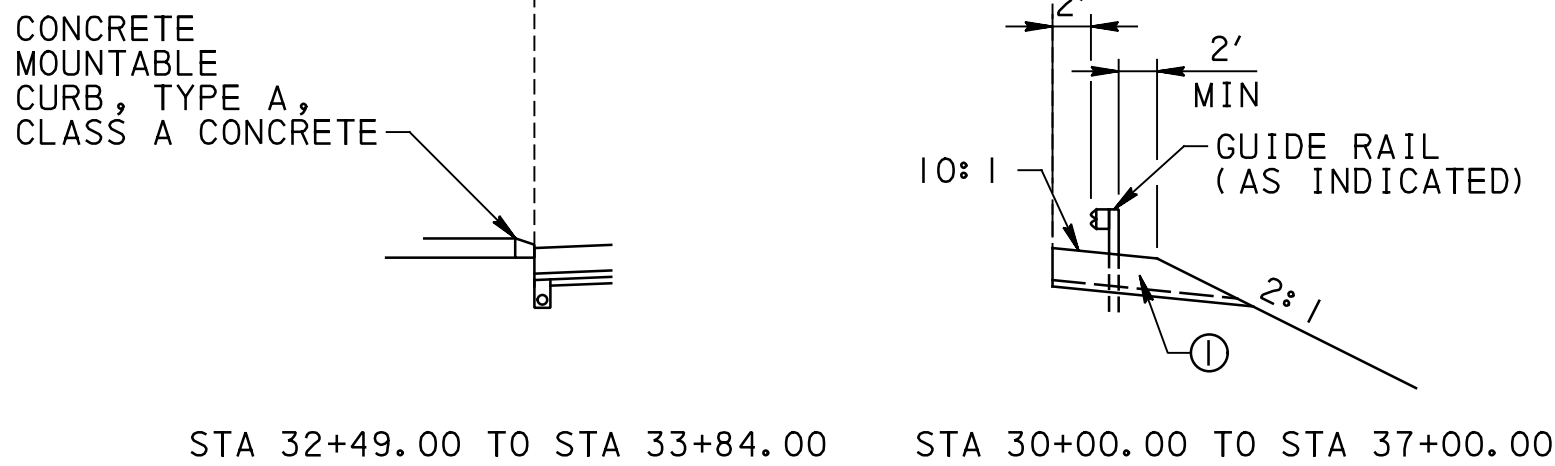
### SR 8006 (RAMP C) TYPICAL SECTION

NOT TO SCALE  
STA 30+00.00 TO STA 39+49.87



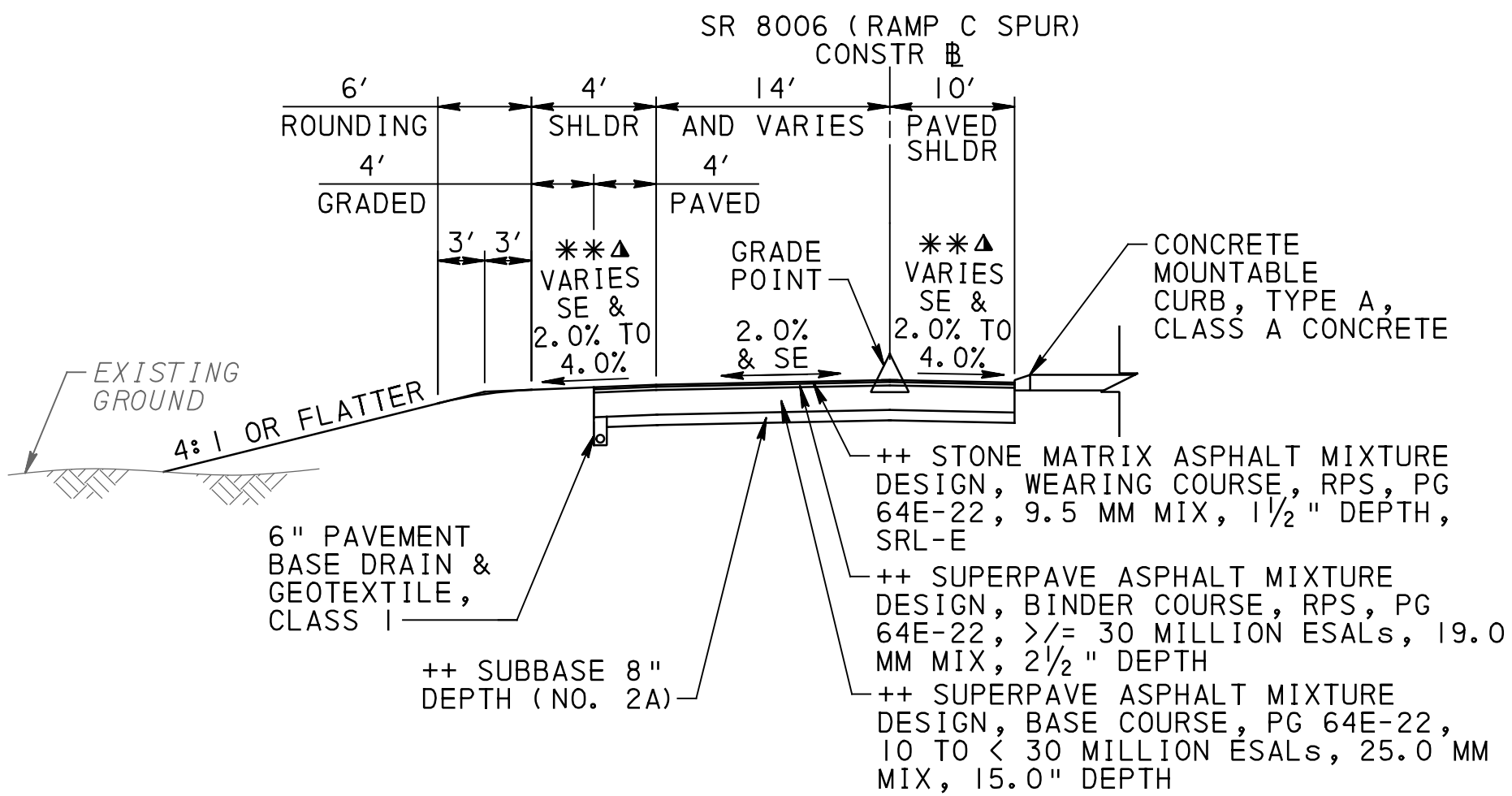
### SR 8006 (RAMP B) TYPICAL SUPERELEVATED SECTION ADJACENT TO MAINLINE PAVEMENT

NOT TO SCALE  
STA 68+49.49 TO STA 71+30.13



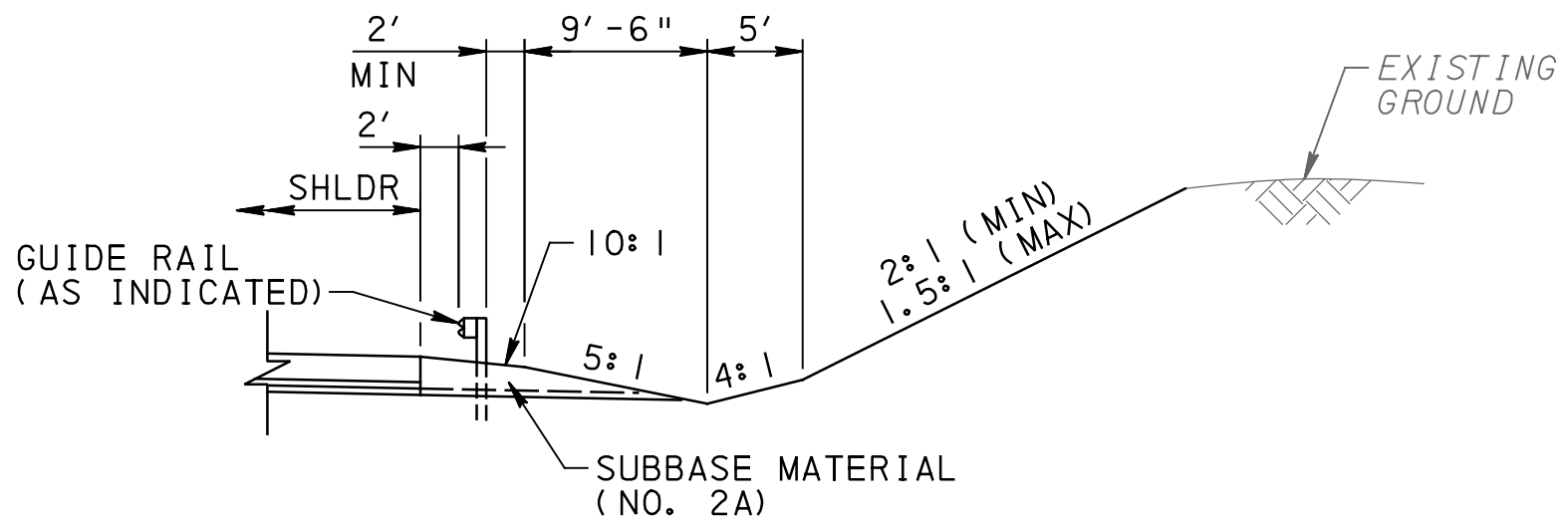
### SR 8006 (RAMP C) TYPICAL SUPERELEVATED SECTION ADJACENT TO MAINLINE PAVEMENT

NOT TO SCALE  
STA 39+49.87 TO STA 43+31.39



### SR 8006 (RAMP C) SPUR TYPICAL SECTION

NOT TO SCALE  
STA 20+73.61 TO STA 23+05.28



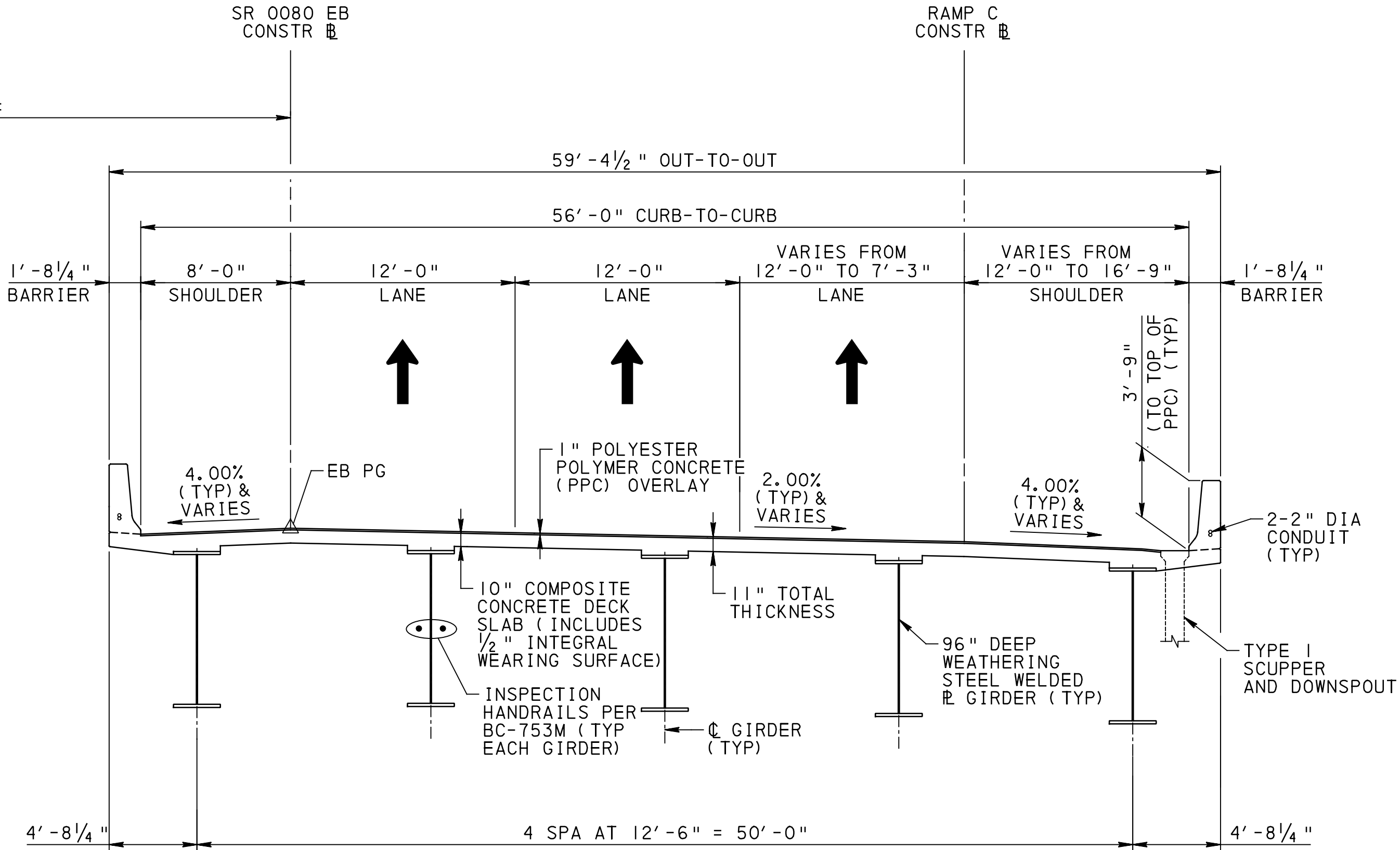
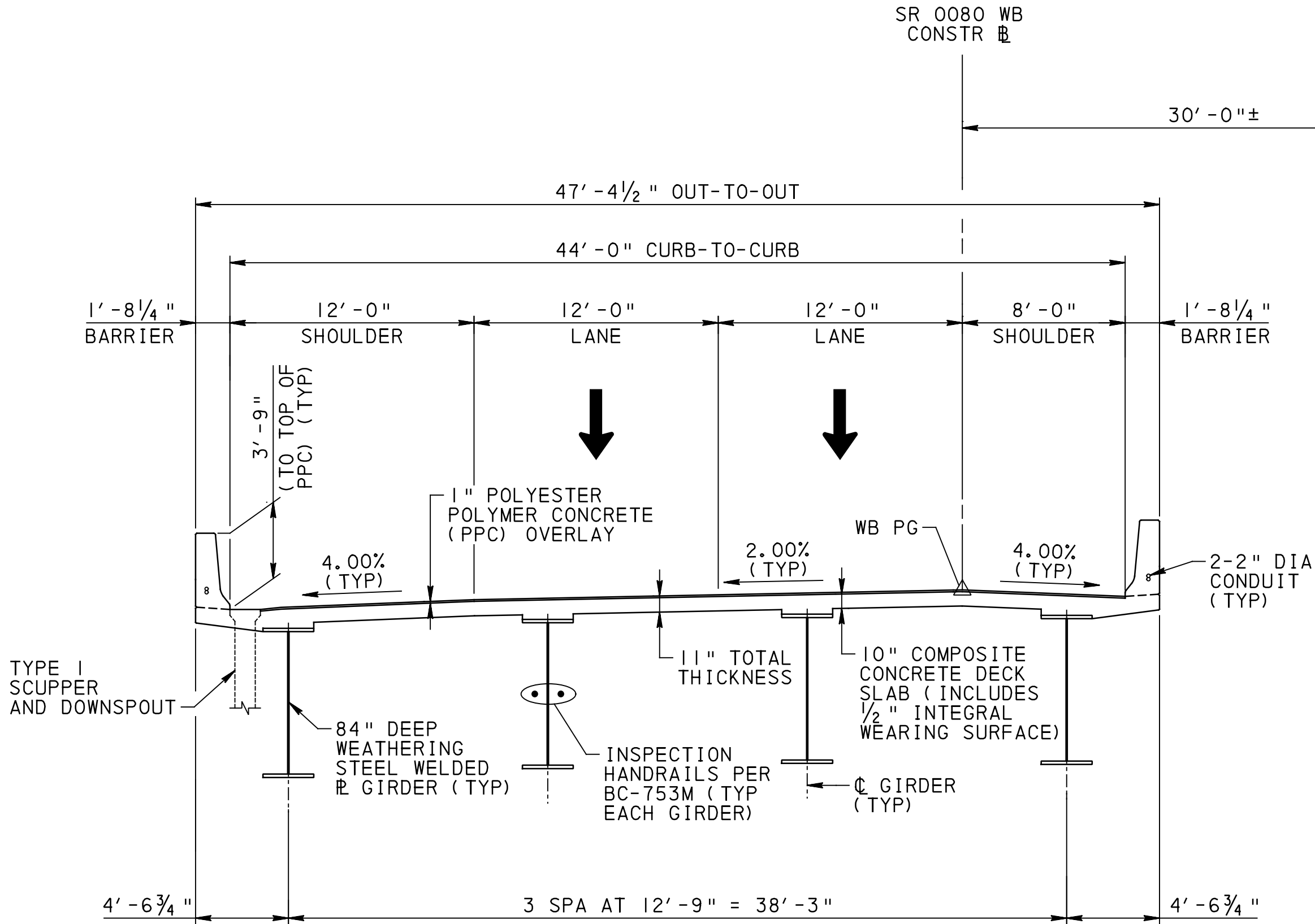
### TYPICAL RAMP CUT CONDITION WITH GUIDE RAIL

NOT TO SCALE

USER: LSALOW    PLOT DRIVER: PenDOT - PDF - Mono.plt    PLOT DATE: 11-23-2021    12:43:52 PM  
PATH: c:\pwork\eng\res10\1\40742833\    MODEL: Default  
FILE: 008008B-RD-TYP05.dgn

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	12 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHs AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



## BRIDGE TYPICAL SECTION

NOT TO SCALE

STA 2012+76.39 EB TO STA 2021+91.39 EB

STA 2213+21.37 WB TO STA 2221+51.37 WB







USER: RUBUCKLE | PLOT: DRIVER: PennDOT | PDF: Mono-pl1016g | PLOT DATE: 11-23-2021 | 4:23:28 PM  
PATH: \\spwork\eng\res\10\1\40742833\ | FILE: 008008B-RD-PL101.dgn | MODEL: road plan 2

HYDRAULIC DATA

DRAINAGE AREA= 311 SQ MI

DESIGN FLOOD (50 YEAR):  
Q = 13,502 CFS  
VELOCITY= 7.2 FPS  
WS EL= 1086.14

100 YEAR FLOOD:  
Q = 15,373 CFS  
VELOCITY= 7.4 FPS  
WS EL= 1086.79

SELECTED BORROW  
EXCAVATION ROCK,  
CLASS R-4 AND  
GEOTEXTILE,  
CLASS 4, TYPE A

STA 2213+21.37 WB  
END FULL DEPTH  
RECONSTRUCTION  
BEGIN BRIDGE  
APPROACH SLAB

5  
WHITE HAVEN  
BORO  
STOP WORK  
STA 71+30.13  
SR 8006  
(RAMP B)

STONE  
RET. WALL  
TO REMAIN

6  
READING &  
NORTHERN R.R.  
(WIDTH VARIES)

7  
COMMONWEALTH OF  
PENNSYLVANIA  
(STATE PARK)

STOP WORK/  
LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

LEGAL R/W LINE  
FOR LIMITED  
ACCESS

TEMPORARY  
CONSTRUCTION  
EASEMENT

7  
COMMONWEALTH OF  
PENNSYLVANIA

UTILITY POLE  
TO BE REPLACED  
(BY OTHERS)

NOTES:

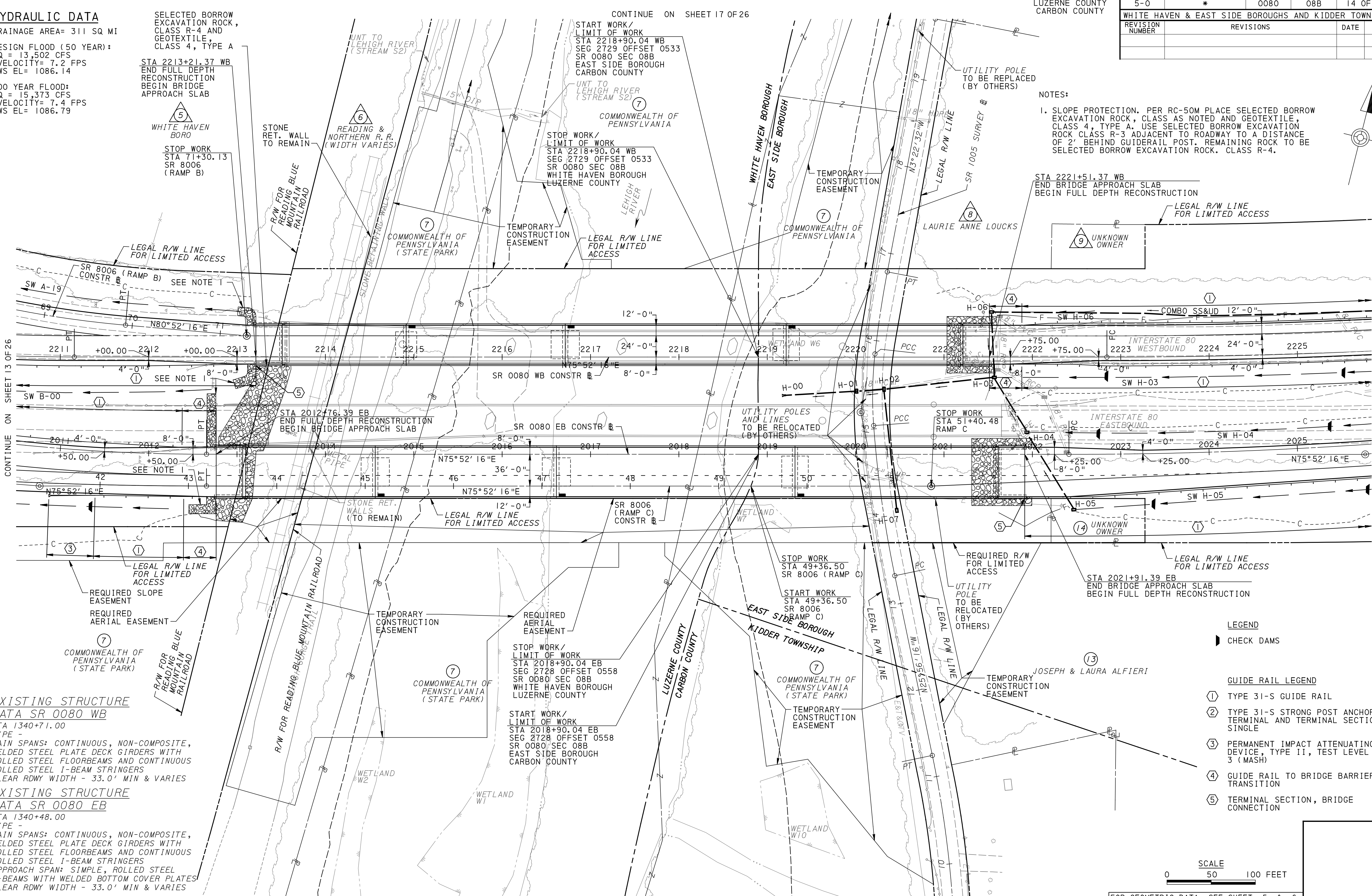
1. SLOPE PROTECTION. PER RC-50M PLACE SELECTED BORROW  
EXCAVATION ROCK, CLASS AS NOTED AND GEOTEXTILE,  
CLASS 4, TYPE A. USE SELECTED BORROW EXCAVATION  
ROCK CLASS R-3 ADJACENT TO ROADWAY TO A DISTANCE  
OF 2' BEHIND GUIDERAIL POST. REMAINING ROCK TO BE  
SELECTED BORROW EXCAVATION ROCK. CLASS R-4.

STA 2221+51.37 WB  
END BRIDGE APPROACH SLAB  
BEGIN FULL DEPTH RECONSTRUCTION

LEGAL R/W LINE  
FOR LIMITED ACCESS

9  
UNKNOWN  
OWNER

LAURIE ANNE LOUCKS



EXISTING STRUCTURE

DATA SR 0080 WB

STA 1340+71.00  
TYPE -  
MAIN SPANS: CONTINUOUS, NON-COMPOSITE,  
WELDED STEEL PLATE DECK GIRDERS WITH  
ROLLED STEEL FLOORBEAMS AND CONTINUOUS  
ROLLED STEEL I-BEAM STRINGERS  
CLEAR ROWY WIDTH - 33.0' MIN & VARIES

EXISTING STRUCTURE

DATA SR 0080 EB

STA 1340+48.00  
TYPE -  
MAIN SPANS: CONTINUOUS, NON-COMPOSITE,  
WELDED STEEL PLATE DECK GIRDERS WITH  
ROLLED STEEL FLOORBEAMS AND CONTINUOUS  
ROLLED STEEL I-BEAM STRINGERS  
APPROACH SPAN: SIMPLE, ROLLED STEEL  
I-BEAMS WITH WELDED BOTTOM COVER PLATES  
CLEAR ROWY WIDTH - 33.0' MIN & VARIES

LEGEND

1 CHECK DAMS

GUIDE RAIL LEGEND

- 1 TYPE 31-S GUIDE RAIL
- 2 TYPE 31-S STRONG POST ANCHOR  
TERMINAL AND TERMINAL SECTION,  
SINGLE
- 3 PERMANENT IMPACT ATTENUATING  
DEVICE, TYPE 11, TEST LEVEL  
3 (MASH)
- 4 GUIDE RAIL TO BRIDGE BARRIER  
TRANSITION
- 5 TERMINAL SECTION, BRIDGE  
CONNECTION

SCALE

0 50 100 FEET

FOR GEOMETRIC DATA, SEE SHEET 5 & 6

DES: RPB

DWG: DMY

CKD: JEB

FOR SR 0080 EB PROFILE, SEE SHEET 19 OF 26

FOR SR 0080 WB PROFILE, SEE SHEET 22 OF 26

FOR RAMP B PROFILE, SEE SHEET 24 OF 26

FOR RAMP C PROFILE, SEE SHEET 26 OF 26

SURVEY BOOK NO. 25003

USER: LSALDUM PLOT: DRIVER: PLOT DATE: 11-23-2021 12:43:59 PM  
PATH: \\pwwork\eng\res\10\100742833\ FILE: 008008B-RD-PLAN1.dgn  
MODEL: road plan 3

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	15 OF 26	
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

10  
GIUSEPPE PECORARI

LIMIT OF WORK  
STA 2249+70.00 WB  
SEG 2731 OFFSET 2253  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

LEGAL R/W LINE  
FOR LIMITED ACCESS

STOP WORK  
STA 2248+70.00 WB  
SEG 2731 OFFSET 2153  
SR 0080 SEC 08B

STA 2240+00.00  
SR 0080 WB  
END MILL AND OVERLAY

STOP WORK  
STA 2048+06.00 EB  
SEG 2730 OFFSET 2104  
SR 0080 SEC 08B

STA 2038+15.00  
SR 0080 EB  
END MILL AND OVERLAY

STA 2034+75.00  
SR 0080 EB  
END FULL DEPTH RECONSTRUCTION  
BEGIN MILL AND OVERLAY

LIMIT OF WORK  
STA 2049+06.00 EB  
SEG 2730 OFFSET 2204  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

LEGEND

1 CHECK DAMS

GUIDE RAIL LEGEND

- 1 TYPE 31-S GUIDE RAIL
- 2 TYPE 31-S STRONG POST ANCHOR, TERMINAL AND TERMINAL SECTION, SINGLE
- 3 PERMANENT IMPACT ATTENUATING DEVICE, TYPE 11, TEST LEVEL 3 (MASH)
- 4 GUIDE RAIL TO BRIDGE BARRIER TRANSITION
- 5 TERMINAL SECTION, BRIDGE CONNECTION

SCALE  
0 50 100 FEET

FOR GEOMETRIC DATA, SEE SHEET 6

HYDRAULIC DATA

DRAINAGE AREA= 1.7 SQ MI  
DESIGN FLOOD (50 YEAR):  
Q = 511 CFS  
VELOCITY= 4.7 FPS  
WS EL= 1211.42  
100 YEAR FLOOD:  
Q = 635 CFS  
VELOCITY= 4.8 FPS  
WS EL= 1212.43

EXISTING CULVERT DATA SR 0080

STA 10+75.00 EB & STA 11+13.30 WB  
TYPE - REINFORCED CONCRETE BOX CULVERT  
LENGTH - 12'-0"  
UNDER CLEAR - 6'-0"  
CLEAR RDWY WIDTH - N/A

DES: RPB DWG: DMY CKD: JEB

FOR SR 0080 EB PROFILE, SEE SHEET 20 OF 26 FOR SR 0080 WB PROFILE, SEE SHEET 23 OF 26

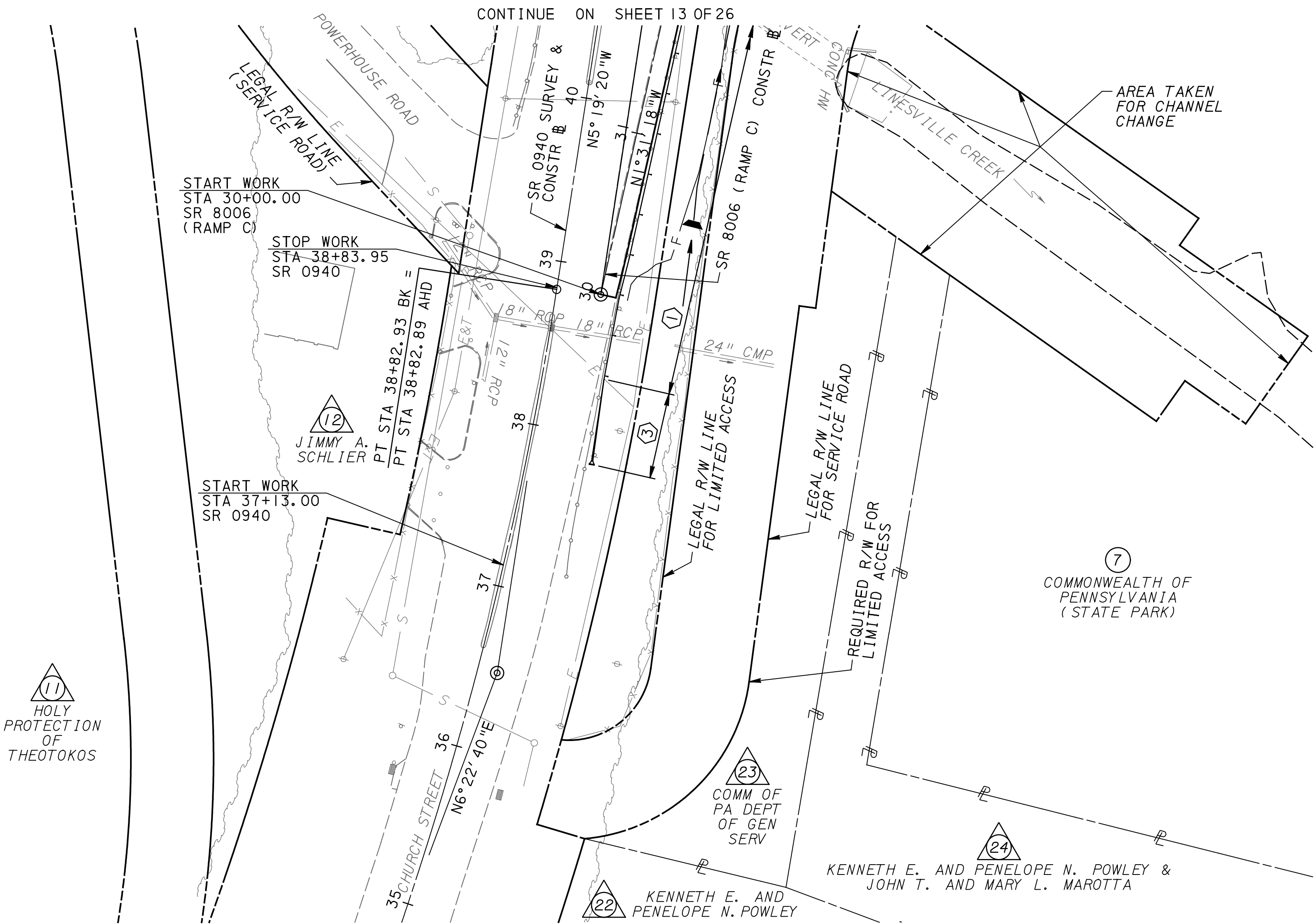
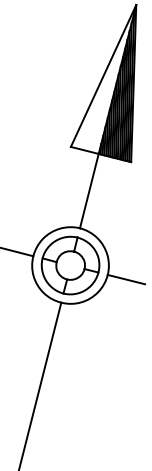
SURVEY BOOK NO. 25003



USER: LSALOW PLOT: DRIVER+PamDOT\_PDF\_Mono.pltcfp PLOT DATE: 11-23-2021 12:44:00 PM  
PATH: g:\pwork\eng\res\11\00742833\ FILE: 008008B-RD-PLAN1.dgn MODEL: road plan 4

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	16 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



LEGEND

CHECK DAMS

GUIDE RAIL LEGEND

- ① TYPE 31-S GUIDE RAIL
- ② TYPE 31-S STRONG POST ANCHOR  
TERMINAL AND TERMINAL SECTION,  
SINGLE
- ③ PERMANENT IMPACT ATTENUATING  
DEVICE, TYPE II, TEST LEVEL  
3 (MASH)
- ④ GUIDE RAIL TO BRIDGE BARRIER  
TRANSITION
- ⑤ TERMINAL SECTION, BRIDGE  
CONNECTION



FOR GEOMETRIC DATA, SEE SHEET 5  
SURVEY BOOK NO. 25003

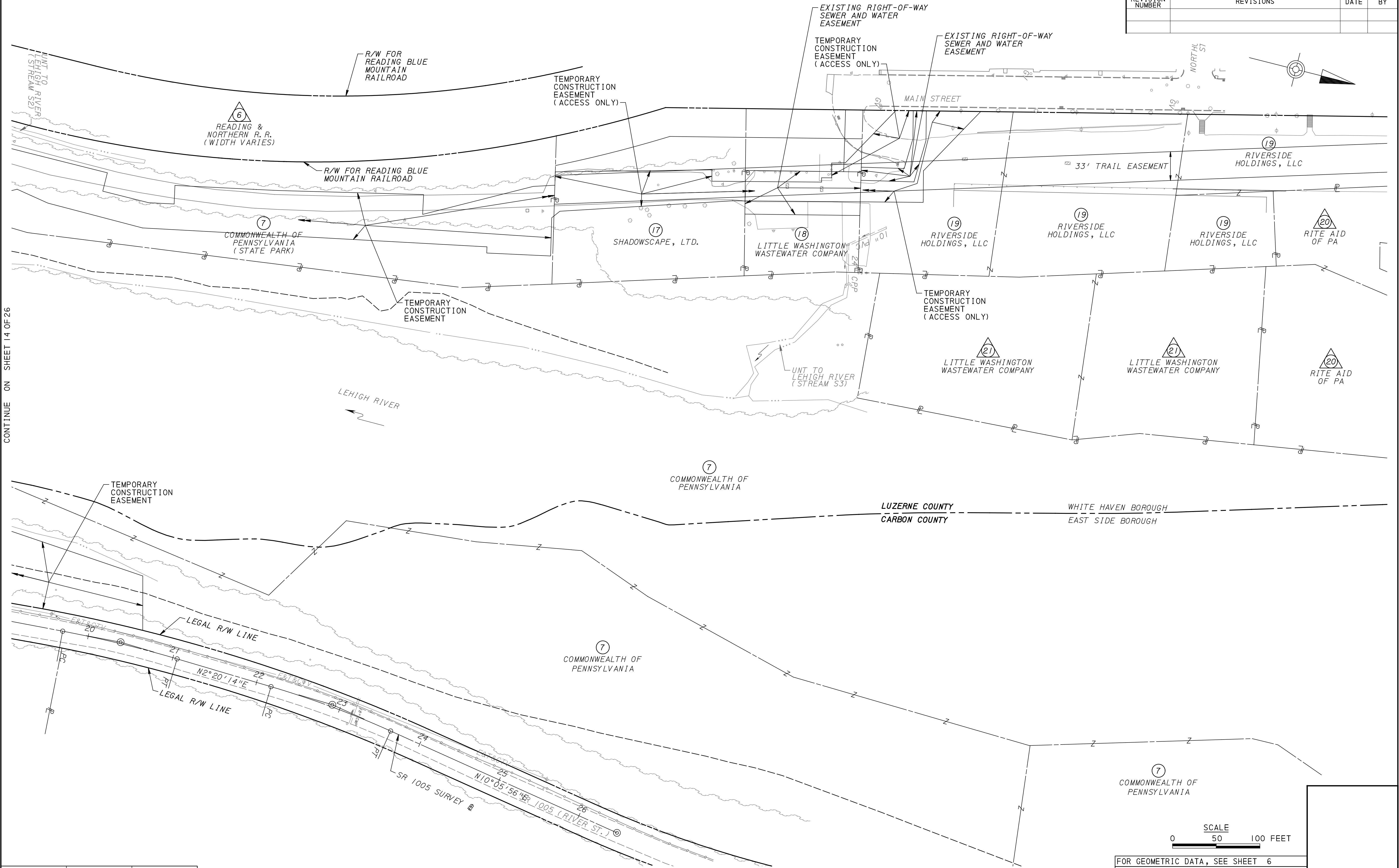
DES: RPB DWG: DMY CKD: JEB

USER: LSALOW PLOT: DR1VER+PennDOT\_PDF\_Memo.pltcfp PLOT DATE: 11-23-2021 12:44:01 PM  
PATH: c:\pwork\eng\res10\1\40742833\ FILE: 000008B-RD-PLAN1.dgn MODEL: road plan 5

CONTINUE ON SHEET 14 OF 26

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	17 OF 26	
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



USER: LSALDUM    PLOT DRIVER: PemdOT\_PDF\_Mono.pltcf    PLOT DATE: 11-23-2021    12:44:04 PM  
PATH: c:\pwork\eng\res10\1\0742833\    MODEL: SHEET 1  
FILE: 00808B-RD-PR01.dgn

DES: RPB    DWG: DMY    CKD: JEB

FOR PLAN, SEE SHEET 13 OF 26

SCALE

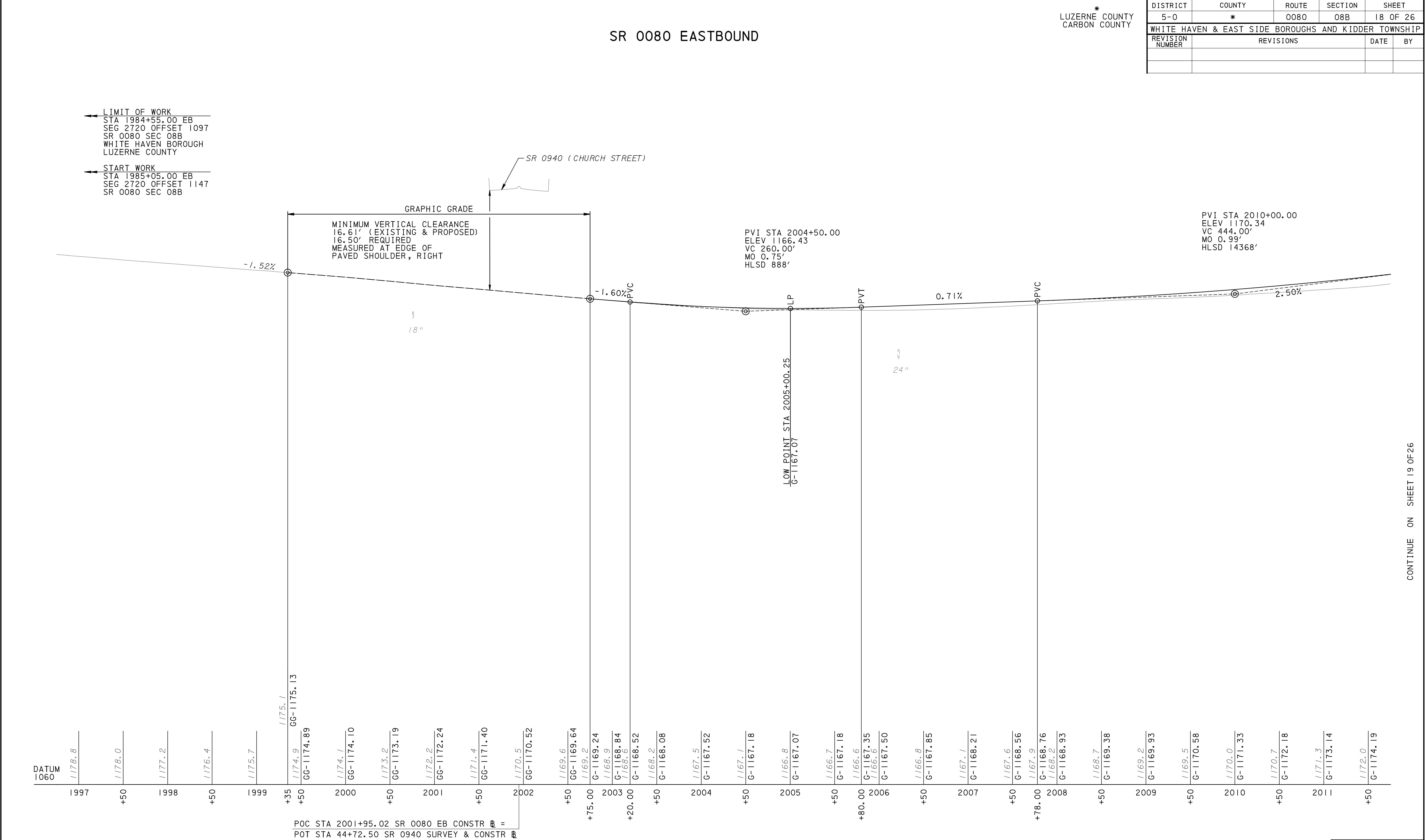
HORIZONTAL

0 50 100 FEET

VERTICAL

0 10 20 FEET

SURVEY BOOK NO. 25003



LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	18 OF 26	
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

CONTINUE ON SHEET 19 OF 26

USER: LSALDUM PLOT DRIVER: PLOTDOT.PDF Mono: plotcfg PLOT DATE: 11-23-2021 12:44:04 PM  
PATH: c:\pwork\eng\res10\140742833\ FILE: 00808B-RD-PR01.dgn MODEL: SHEET 2

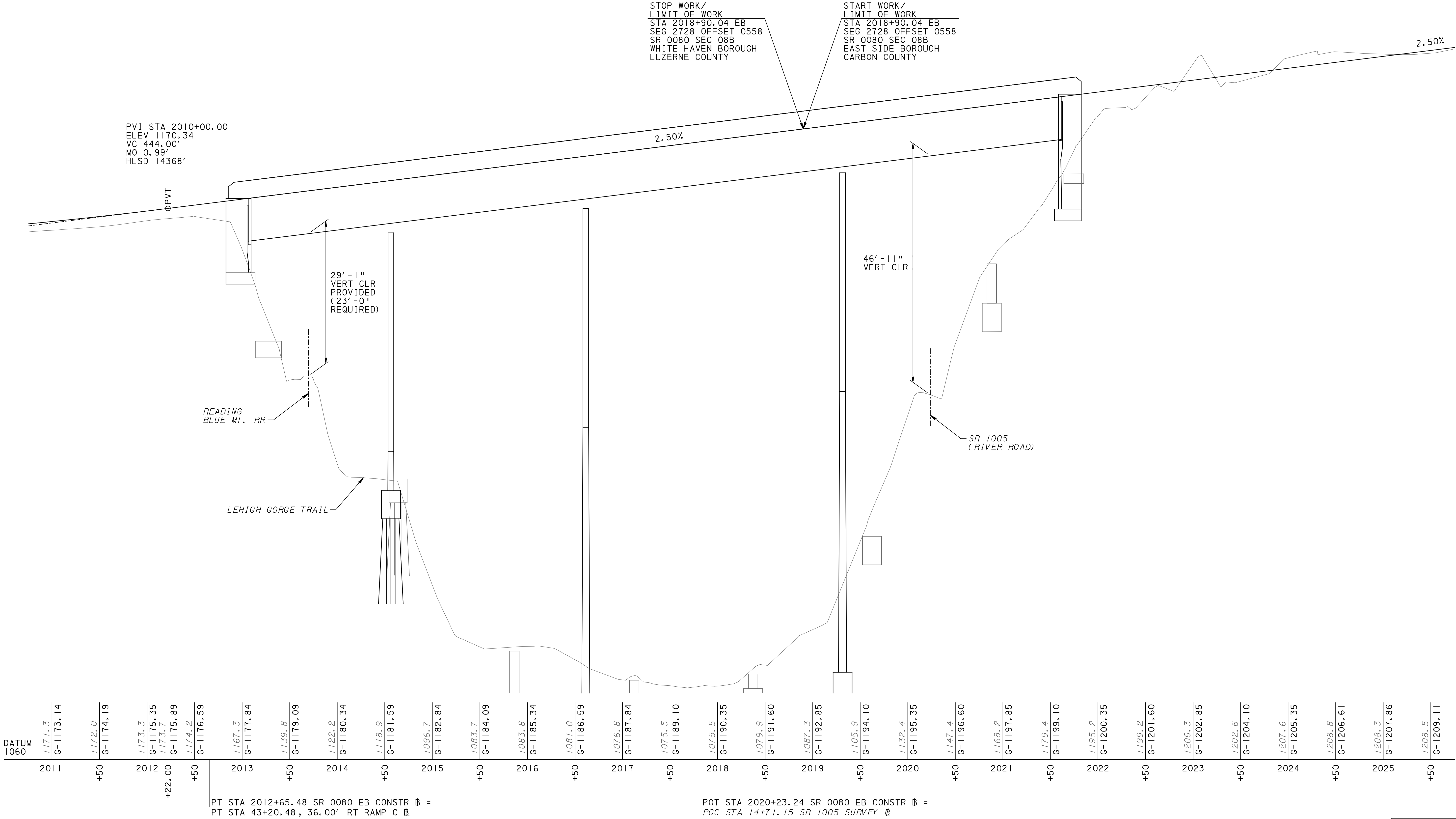
CONTINUE ON SHEET 18 OF 26

DES: RPB DWG: DMY CKD: JEB

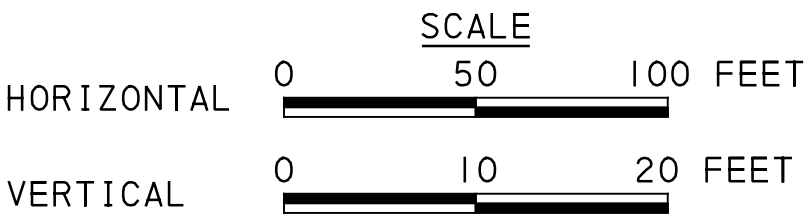
SR 0080 EASTBOUND

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	19 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



CONTINUE ON SHEET 20 OF 26

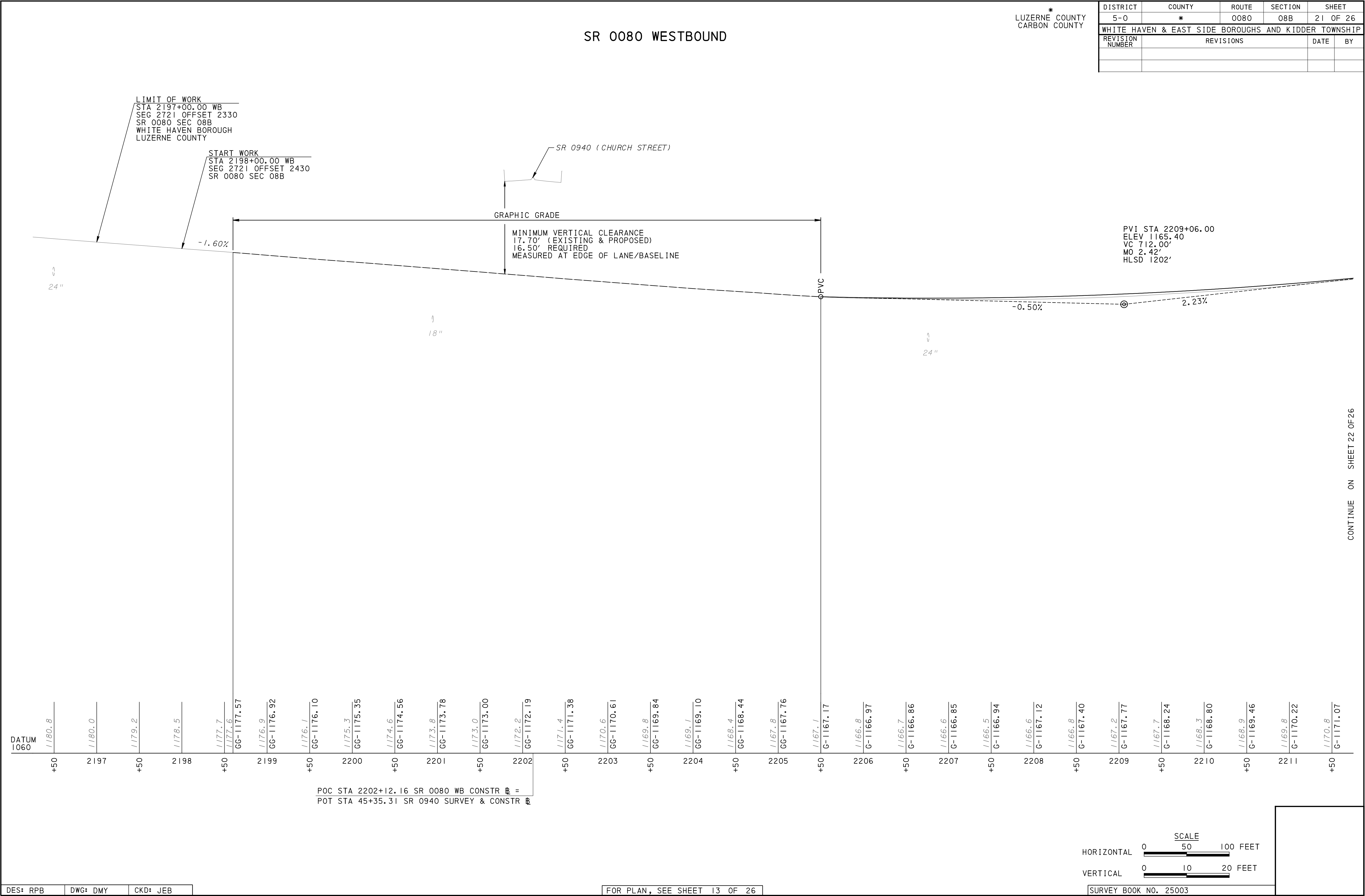


SURVEY BOOK NO. 25003

FOR PLAN, SEE SHEET 14 OF 26



USER: LSALDUM    PLOT DRIVER: PcmdOT\_PDF\_Mono.pltcfgr    PLOT DATE: 11-23-2021 12:44:09 PM  
PATH: c:\pwork\eng\res10\1\0742833\    MODEL: SHEET 1  
FILE: 008008B-RD-PRC02.dgn





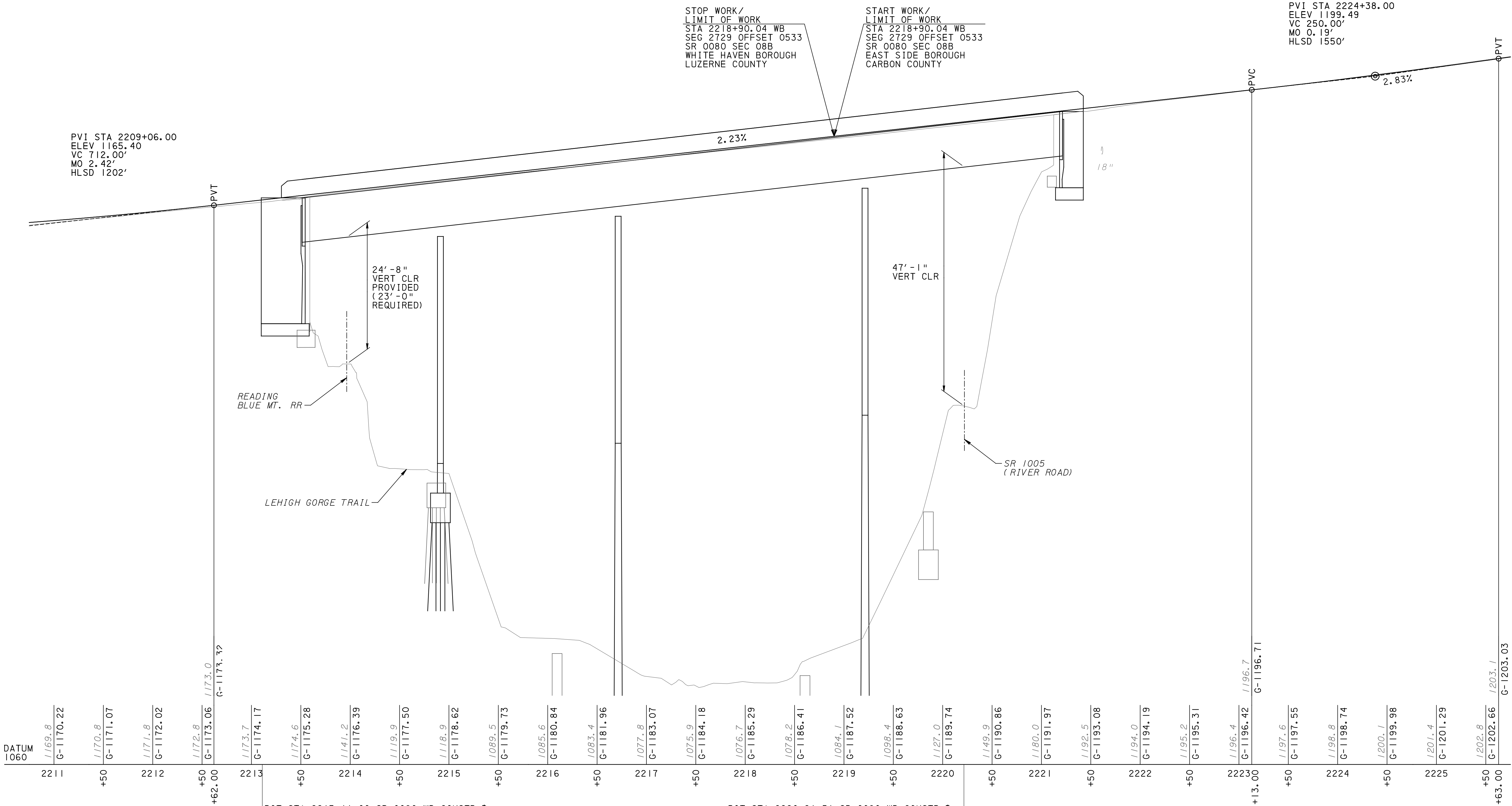
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	22 OF 26
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY

PVI STA 2209+06.00  
ELEV 1165.40  
VC 712.00'  
MO 2.42'  
HLSD 1202'

STOP WORK/  
LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

START WORK/  
LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 7279 OFFSET 0533  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

PVI STA 2224+38.00  
ELEV 1199.49  
VC 250.00'  
MO 0.19'  
HLSD 1550'



POT STA 2213+11.00 SR 0080 WB CONSTR B =  
POT STA 71+30.13 RAMP B CONSTR B, 24.00' LT

POT STA 2220+21.51 SR 0080 WB CONSTR @ =  
POC STA 15+81.41 SR 1005 SURVEY @

SCALE

HORIZONTAL 0 50 100 FEET

VERTICAL 0 10 20 FEET

DES: RPB	DWG: DMY	CKD: JEB
----------	----------	----------

FOR PLAN, SEE SHEET 14 OF 26

SURVEY BOOK NO. 25003

USER: LSALDUM

PATH: c:\pwworking\new10\1\0742833\

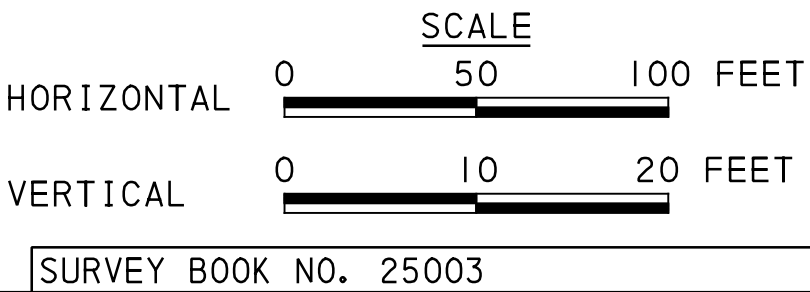
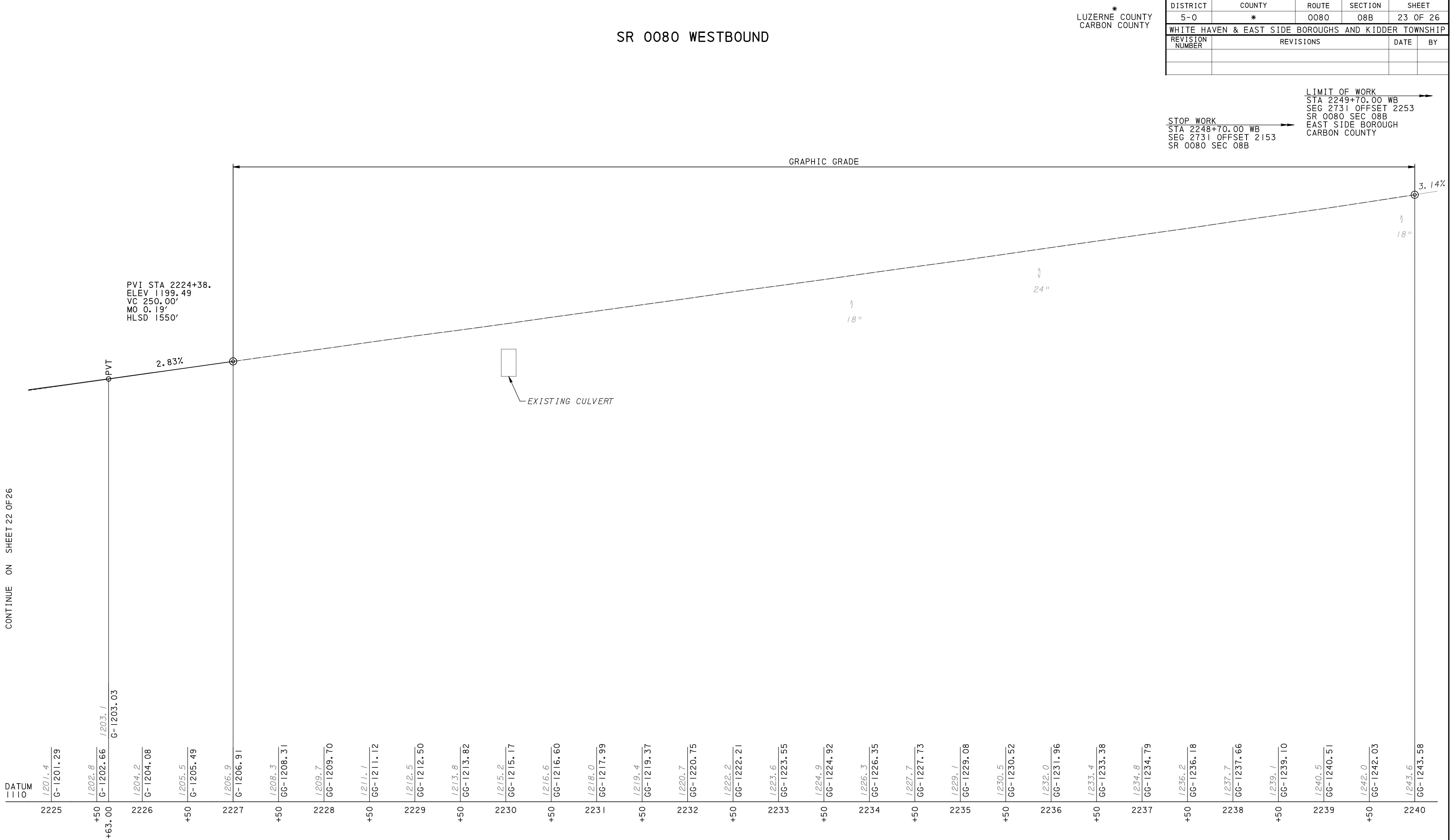
FILE: 00808B-RD-PR02.dgn

PLOT DRIVER: PnD0T\_PDF\_Mono.plt6fg

PLOT DATE: 11-23-2021 12:44:10 PM

MODEL SHEET 3

CONTINUE ON SHEET 22 OF 26



DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	24 OF 26
WHITE HAVEN & EAST SIDE BOROUGH AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY

START WORK  
STA 60+14.23  
RAMP B

PVI STA 60+83.00  
ELEV 1194.55  
VC 100.00'  
MO -0.24'  
SSD 622'

-5.10%

PVI STA 66+98.00  
ELEV 1163.17  
VC 304.00'  
MO 2.60'  
HLSD 232'

1.73%

STOP WORK  
STA 71+30.13  
RAMP B

GRAPHIC GRADE

DATUM  
1130

0  
+28.00  
+33.00  
+50

1196.3  
G-1196.31  
1196.1  
G-1196.15  
1195.3  
G-1195.58

61  
+50

1193.7  
G-1193.58  
1192.3  
G-1191.99  
1191.5  
G-1191.13

62  
+50

1189.1  
G-1188.58  
1186.6  
G-1186.03

63  
+50

1184.2  
G-1183.47  
1181.8  
G-1180.92  
1179.3  
G-1178.37

64  
+50

1176.8  
G-1175.82  
1174.6  
G-1173.27  
1172.3  
G-1170.72  
1170.1  
G-1168.50

65  
+50

1168.5  
G-1166.83  
1167.3  
G-1165.73  
1166.4  
G-1165.19  
1166.0  
G-1165.22

66  
+50

1166.6  
G-1165.80  
1167.8  
G-1166.95  
1169.2  
G-1168.36  
1170.5  
G-1169.95

67  
+50

1171.9  
G-1171.57  
1173.1  
G-1173.13  
1173.8  
G-1173.93

68  
+50

1173.8  
G-1173.93

POT STA 60+00.00 RAMP B CONSTR B =  
POC STA 47+60.67 SR 0940 SURVEY & CONSTR B

POT STA 71+30.13 LT RAMP B B =  
POT STA 2213+11.00, 24.00' RT  
SR 0080 WB CONSTR B

PVI STA 21+00.00  
 ELEV 1191.13  
 VC 50.00'  
 MO 0.07'  
 HLSD UNRESTRICTED

PVI STA 21+97.00  
 ELEV 1189.91  
 VC 52.00'  
 MO -0.25'  
 SSD 303'

START WORK  
 STA 20+73.61  
 RAMP C SPUR

STOP WORK  
 STA 23+05.28  
 RAMP C SPUR

GRAPHIC  
 GRADE

-2.35%  
 -1.26%  
 -5.16%  
 -1.6%

PVC  
 PVT  
 POT

STA 20+73.61  
 STA 21+75.00  
 STA 21+97.00  
 STA 22+23.00  
 STA 23+05.28

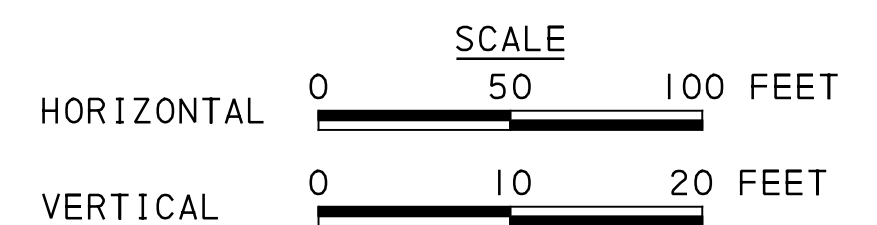
G-1191.75  
 G-1191.72  
 G-1191.20  
 G-1190.81  
 G-1190.50  
 G-1190.23  
 G-1188.9  
 G-1187.9  
 G-1186.4  
 G-1184.05  
 G-1183.76

DATUM  
 1150

20  
 +50  
 +73.61  
 +75.00  
 +25.00  
 +71.00  
 +23.00  
 +50  
 23  
 +05.28

POT STA 23+05.28 RAMP C SPUR CONSTR 14.00' =  
 PT STA 35+04.61 RAMP C CONSTR 14.00' RT

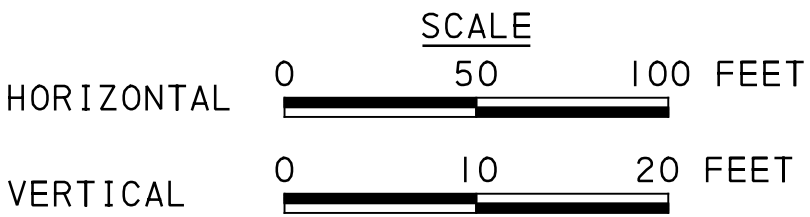
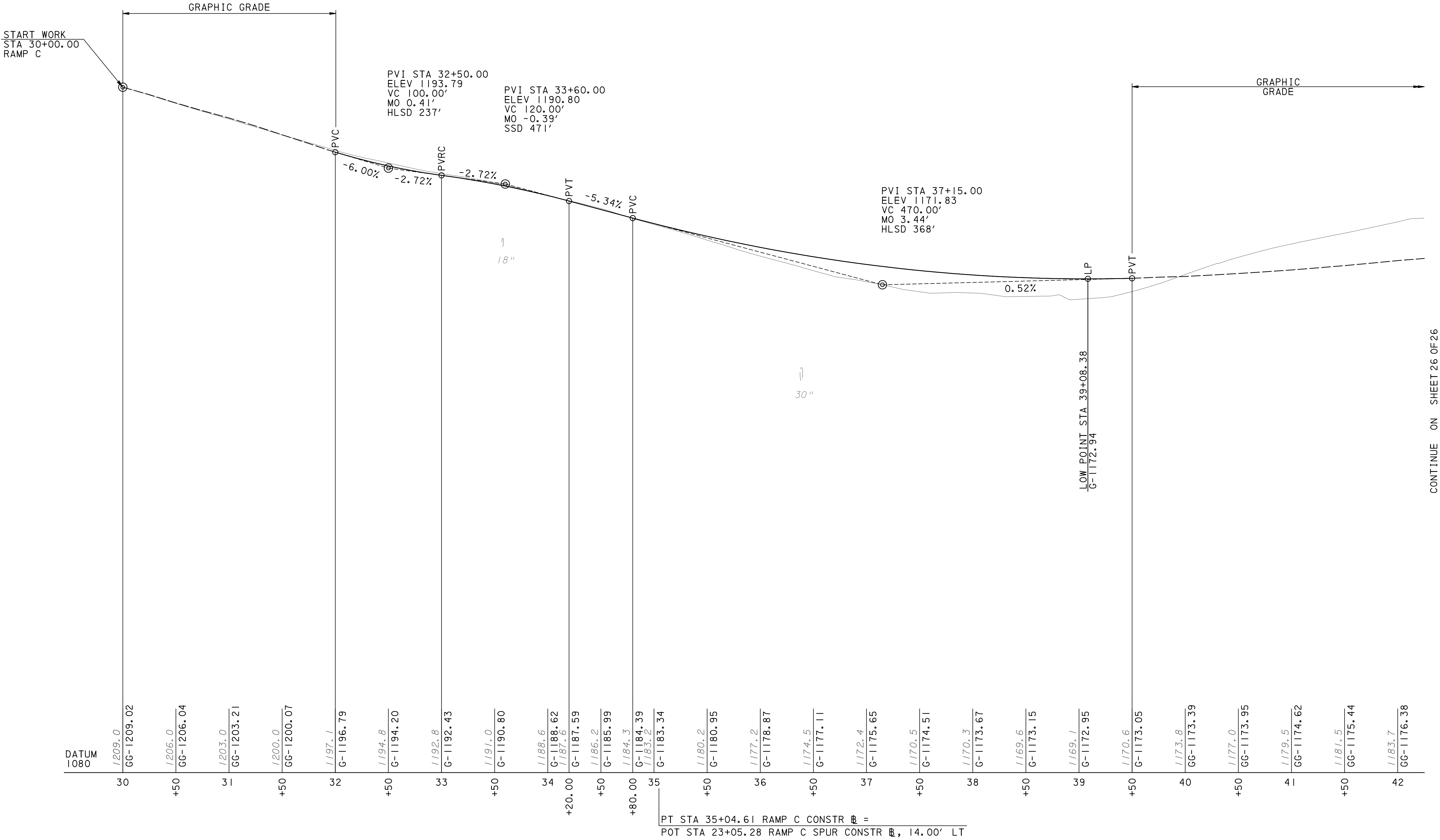
POT STA 23+05.28 RAMP C SPUR CONSTR @ =  
PT STA 35+04.61 RAMP C CONSTR @, 14.00' RT



USER: LSALDUM    PLOT DRIVER: PemdDOT\_PDF\_Mono.pltcfgr    PLOT DATE: 11-23-2021 12:44:17 PM  
PATH: c:\pwork\eng\res10\1\40742833\    FILE: 008008B-RD-PRC04.dgn    MODEL: one sheet

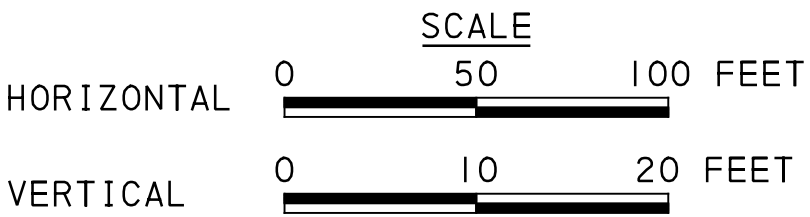
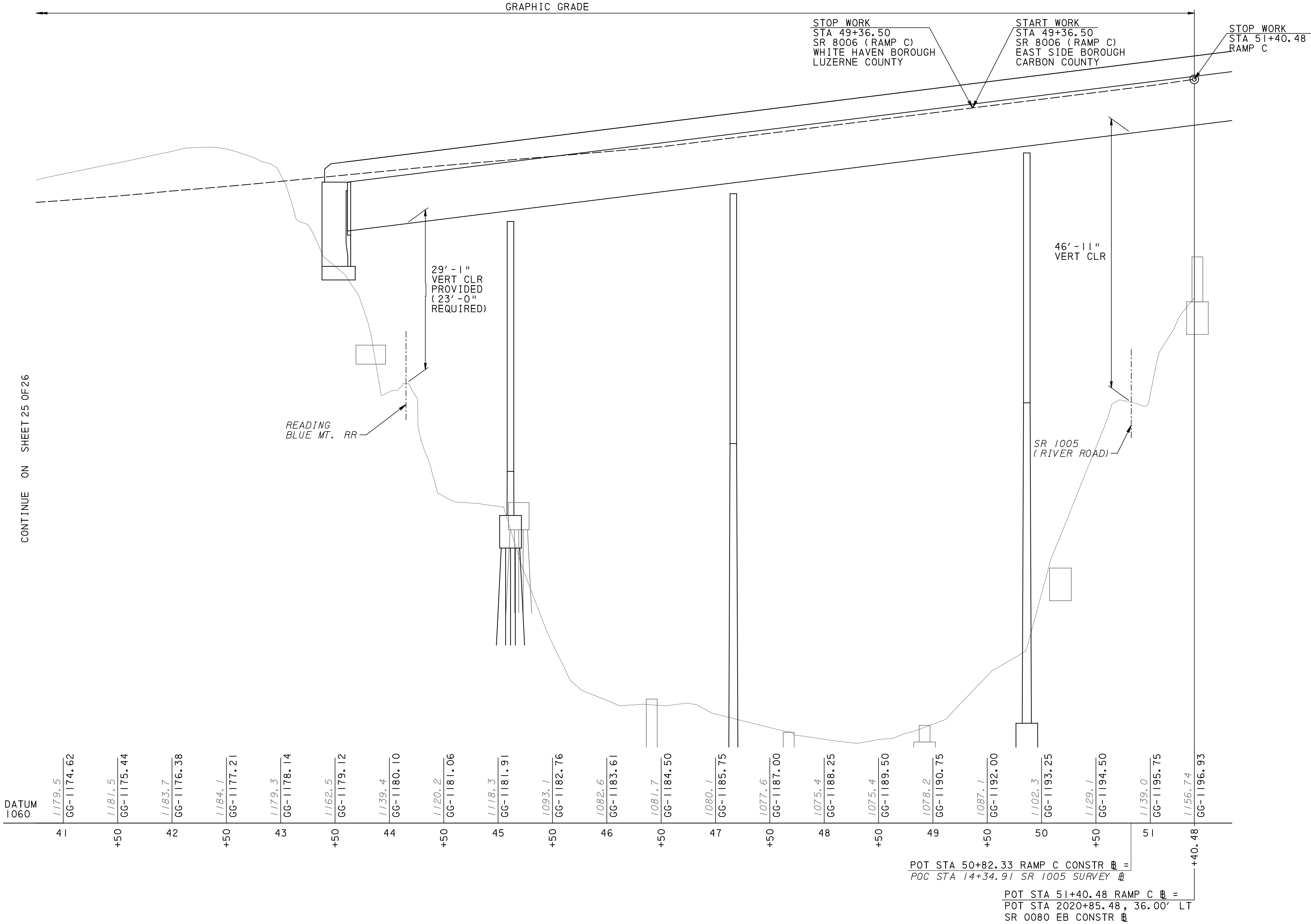
LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	25 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHs AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



USER: LSALOW PLOT DRIVER: PLOT DATE: 11-23-2021 12:44:17 PM  
PATH: \\spwork\eng\res10\1\0742833\ FILE: 008008B-RD-PRC04.dgn  
MODEL SHEET 2

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	26 OF 26
WHITE HAVEN & EAST SIDE BOROUGHs AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY



**Appendix D**  
**Agency Coordination**

## Krommes, Kathy

---

**From:** Rossiter, Kelly <krossiter@pa.gov>  
**Sent:** Thursday, September 2, 2021 12:16 PM  
**To:** Markowitz, Katherine  
**Cc:** Krommes, Kathy; Baumgardner, John  
**Subject:** RE: [External] RE: I-80 White Haven Bridges - Scenic River Review

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you Katie,  
I'll file this with the other project materials. I appreciate the point by point feedback and responses. We typically like to see the concrete on bridges tinted because the standard stark white is a bit jarring in a scenic setting. However, if your construction contract cannot be amended to include such a alteration to standard practice, that is still acceptable.

Regards,

**Kelly Rossiter, AICP** | Rivers Program Specialist  
Department of Conservation and Natural Resources  
Bureau of Recreation and Conservation  
400 Market St, 5<sup>th</sup> Floor | Harrisburg, PA 17101-2301  
Phone: 717.772.3319 | Fax: 717.787.9577  
<http://www.dcnr.state.pa.us/brc> | [www.ExplorePAtrails.com](http://www.ExplorePAtrails.com)

---

**From:** Markowitz, Katherine <Katherine.Markowitz@hdrinc.com>  
**Sent:** Thursday, August 5, 2021 11:27 AM  
**To:** Rossiter, Kelly <krossiter@pa.gov>  
**Cc:** Krommes, Kathy <Kathleen.Krommes@hdrinc.com>; Baumgardner, John <John.Baumgardner@hdrinc.com>  
**Subject:** [External] RE: I-80 White Haven Bridges - Scenic River Review

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Hello Kelly,

HDR, on behalf of PennDOT, has reviewed the DCNR Scenic Rivers Program review letter dated January 5, 2021, for the I-80 Section 08B Bridge Project. Please see the attached response letter dated August 3, 2021.

Thank you,

**Katie Markowitz**  
D 717-516-3155 M 201-258-0508

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)

---

**From:** Rossiter, Kelly <[krossiter@pa.gov](mailto:krossiter@pa.gov)>  
**Sent:** Tuesday, January 5, 2021 1:53 PM  
**To:** Markowitz, Katherine <[Katherine.Markowitz@hdrinc.com](mailto:Katherine.Markowitz@hdrinc.com)>  
**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>

**Subject:** RE: I-80 White Haven Bridges - Scenic River Review

**Importance:** High

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

Attached is the DCNR Scenic Rivers Program review letter related to the subject project. If you have any questions or concerns about the requested mitigations, please email me back or let me know a few times you can talk on the phone and we'll set up a call.

Take care. Best wishes for a healthy 2021.

**Kelly Rossiter, AICP** | Rivers Program Specialist  
Department of Conservation and Natural Resources  
Bureau of Recreation and Conservation  
400 Market St, 5<sup>th</sup> Floor | Harrisburg, PA 17101-2301  
Phone: 717.772.3319 | Fax: 717.787.9577  
<http://www.dcnr.state.pa.us/brc> | [www.ExplorePAtrails.com](http://www.ExplorePAtrails.com)

---

**From:** Markowitz, Katherine <[Katherine.Markowitz@hdrinc.com](mailto:Katherine.Markowitz@hdrinc.com)>  
**Sent:** Friday, December 11, 2020 4:33 PM  
**To:** Rossiter, Kelly <[krossiter@pa.gov](mailto:krossiter@pa.gov)>  
**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>  
**Subject:** [External] I-80 White Haven Bridges - Scenic River Review

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Hello Kelly,

HDR, on behalf of PennDOT, is providing environmental services for the I-80 Bridge Replacement Project near White Haven Borough, Luzerne County, PA. We are pursuing a Scenic River review for the Lehigh River, which our project area traverses. Please find attached a letter requesting review by the DCNR along the project description, location, and accompanying attachments.

Thank you,

**Katie Markowitz**  
Environmental Scientist

**HDR**  
4900 Ritter Road, Suite 101  
Mechanicsburg, PA 17055  
D 717-516-3155 M 201-258-0508  
[katherine.markowitz@hdrinc.com](mailto:katherine.markowitz@hdrinc.com)  
[hdrinc.com/follow-us](http://hdrinc.com/follow-us)



BUREAU OF RECREATION AND CONSERVATION

January 5, 2021

Katherine Markowitz  
HDR Engineering Inc  
4900 Ritter Road, Suite 101  
Mechanicsburg, PA 17055

**RE: Scenic River Reviews – 2021**

**Project Name:** I-80 Section 08B Bridge

**Description:** Bridge Replacement Project

**Location:** White Haven Borough, Luzerne County and East Side Borough, Carbon County

**Applicant Code:** 946

**Request Date:** December 11, 2020

Ms. Markowitz:

Information submitted regarding the subject application has been reviewed. The project lies within the **Lehigh River Pennsylvania Scenic Rivers Corridor System**. Because of Scenic River involvement compliance with Sec 2. of the “Pennsylvania Scenic Rivers Act” of 1972, P.L. 1277, No. 283 as amended May 7, 1982, P.L. 379, No. 110, is necessary:

*“Many of the rivers of Pennsylvania or sections thereof and related adjacent land areas, possess outstanding aesthetic and recreational values of present and potential benefit to the citizens of Pennsylvania. It shall be the policy of the Commonwealth to protect these values and to practice sound conservation policies and practices within this scenic rivers system.”*

The project information provided addressed several typical concerns with bridge projects such as utilizing robust E&S plans and developing an approved Aids to Navigation plan to accommodate recreational use of the waterway during construction. In addition to the plans already presented, DCNR requests that the following actions be integrated into the project to further protect river values that elevate this waterway to inclusion in the scenic rivers program.

- The construction and demolition staging areas should be screened by a vegetative buffer and set back as far as possible from the river’s edge.
- Bridge materials and design should reflect the character of the surrounding environment as well as any local architectural style, normally creating a “natural” or “rustic” appearance. DCNR would encourage clean, simple, lines that complement rather than dominate the landscape. Additionally, since the river segment is a water trail, the design needs to allow safe recreational use / watercraft passage.

- Consider using darker sand and aggregate native to the area or add a color/tint to any concrete mix to soften the appearance of any new concrete. Concrete structures could also be masonry faced with stone native to the area or the concrete surface could be rough faced/textured and tinted a “natural” color (green, brown, tan, grey, etc).
- Exposed metal structures should be painted a “natural” color. Painting procedures should have minimal impact on the creek and its environs and a containment system that prevents paint and paint chips from entering the creek must be used if any part of the bridge is being painted.
- Native or local stone should be used in areas where riprap or stone is needed.
- Native vegetation should be incorporated in the disturbed riverbank areas to maintain the natural character and the scenic qualities of the waterway.
- The Scenic Rivers Program recommends that two types of signs be placed on or in close proximity to the bridge. The first type of sign is to be placed at each end of the bridge to let the public know that they are crossing a designated state scenic river. A copy of this sign design is included as Appendix A.

The second type of sign will aid water trail users and safety responders in navigation. It is to be placed on the upstream side of the bridge and centered on the outside face of the parapet. The sign should identify the bridge name so that users can identify their location along the Lehigh River. The design for this is included as Appendix B.

- DCNR is a proponent of increased waterway access along designated water trails such as the Lehigh River Water Trail. Therefore, this bridge project should include basic accommodations for a non-motorized boat launch as part of the project design, if at all possible.
- Additionally, DCNR encourages that bridge replacements include pedestrian/bike access along the bridge to contribute to a more complete transportation system.

**If you concur with implementation of these actions, please sign off on the line provided below** and return a copy to me (contact information below). Once signed, you may include this letter with your required permit documentation submission to DEP to verify that the subject plans are consistent with the requirements of the Pennsylvania Scenic Rivers Act.

See response letter attached.

---

(Name)

(Position)

Katherine Markowitz  
I-80 Section 08B Bridge Replacement  
January 5, 2021  
Page #3

Should you have any questions regarding our review, process or recommendations for this or future applications, please contact me.

Sincerely,

A handwritten signature in blue ink that reads "Kelly Rossiter". The signature is written in a cursive, flowing style.

Kelly Rossiter, AICP  
Rivers Program Specialist  
Partnerships Division

## APPENDIX A

### I-80 BRIDGE\* SIGN SPECIFICATIONS

#### **Description:**

(2) 48-inch x 24-inch .080 gauge aluminum signs, highway blue background with white border and 4 lines of white lettering. The upper left-hand corner contains a 12-inch x 8-inch logo designed to identify the Pennsylvania Scenic Rivers System.

#### **Printing:**

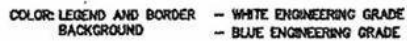
The first line will consist of the Scenic Rivers logo and the name **LEHIGH**. The name on the second line will be RIVER. The logo is to be placed on the left half of the sign area. The name **LEHIGH RIVER** is to be placed on the right half of the sign area and will have lettering 4 inches high. The third and fourth lines will have lettering 3 inches high and will be put on blanks as follows:

PENNSYLVANIA SCENIC  
RIVER SYSTEM

#### **Specific Instructions:**

Signs can be placed upon completion and as directed by the Engineer. The two signs that read **LEHIGH RIVER PENNSYLVANIA SCENIC RIVERS SYSTEM** are to be placed at each end of the bridge.

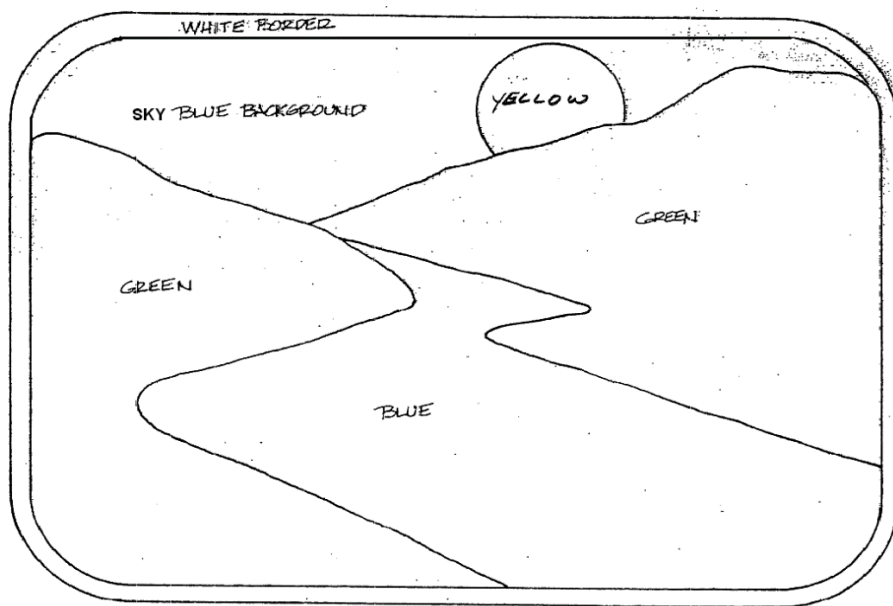
*\*I-80 Bridge is a placeholder: bridge name can be changed, based on how the bridge is known locally, appears on maps, is referenced for emergency management purposes, etc.*



## LOGO SPECIFICATIONS

**Color:** River - Peacock Blue – Ad-vance  
Land - Emerald Green – Colonial  
Sun - Chrome yellow – Colonial  
Background – Sky-blue  
Border - White

**Size:** Size of logo should be in proportion to size of sign with lettering (*see attached sample sign*)



## APPENDIX B

### I-80 BRIDGE SIGN SPECIFICATIONS

#### **Description:**

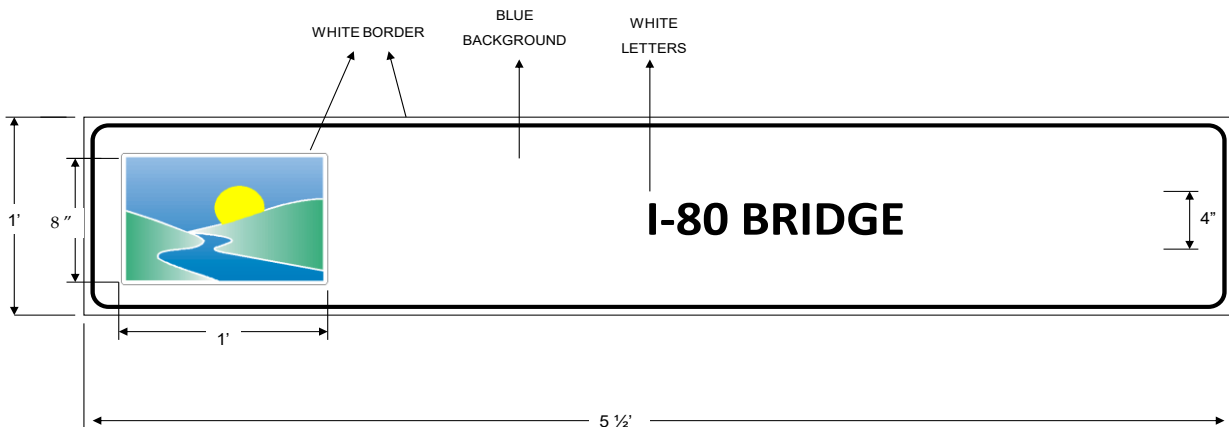
(1) 66-inch x 12 inch .080 gauge aluminum sign, highway blue background with white border and one line of white lettering. The left hand side contains a 12-inch x 8-inch logo designed to identify the Pennsylvania Scenic Rivers System.

#### **Printing:**

The sign will consist of the Scenic Rivers logo and the name **I-80 BRIDGE**. The logo is to be placed on the left hand side of the sign area. The name is to be placed to the right of the logo and will have lettering 4 inches high.

#### **Specific Instructions:**

The sign is to be placed on the outside of the parapet, as directed by the Engineer.





## **Appendix C**

### **Way Finding Signs on Bridges over Water Trails**

Water trails are recreational waterways on lakes, rivers, or streams, suitable for canoes, kayaks and small motorized watercraft. Like conventional trails, water trails are recreational corridors between specific locations, containing access points and day-use and/or camping sites for the boating public. In Pennsylvania, water trails are officially designated by the PA Fish and Boat Commission (PFBC).

For all bridge replacement/rehabilitation projects identification of PFBC designated water trails should be made during the scoping field view. A list of designated water trails can be found on the PFBC website at <http://fishandboat.com/watertrails/trailindex.htm>.

Projects involving these water trails are to include the installation of way finding signs that identify the route/road name of the structure crossing the water trail. The purpose of the sign is to provide aid to boaters in identifying their location on the water trail. The sign(s) should be placed on both the upstream and downstream sides of the bridge and be visible to boaters but should not detract from boaters' experience in the natural setting. If the waterway is 150 feet wide or more, multiple signs should be placed no more than 75 feet apart. On waters less than 150 feet, one sign, placed near mid waterway, is sufficient as long as terrain allows it to be visible from either shoreline.

The project manager should include the sign installation in the Environmental Commitment Mitigation Tracking System.

The Overhead Roadway Identification Sign (I18-1) will be used for this purpose. A non-reflective blue/white sign will be used. A manual SAP order should be placed for an Overhead Roadway Identification (I18-1), blue/ white non-reflective sign and include the specific Route/Road Name needed. The sign standard can be found in [Pub 236, Handbook of Approved Signs](#).

The Route number should be listed first followed by the Road Name. Examples follow:

I-79 Raymond P. Shafer Highway  
US 22 William Penn Highway  
PA 51 Morgantown Street  
PA 2 Lackawanna Trail  
PA 501 Lititz Pike  
SR 0504 Appalachian Thruway



August 3, 2021

Kelly Rossiter, AICP  
Rivers Program Specialist  
Partnerships Division

Re: **Scenic River Reviews – 2021**  
**Project Name:** I-80 Section 08B Bridges  
**Location:** White Haven Borough, Luzerne County and East Side Borough, Carbon County  
**Application Code:** 946

Thank you for providing Scenic River Review for the subject project. In response to the letter from DCNR dated January 5, 2021, the PennDOT bridge design team provides the following response to comments.

DCNR Comment: The project information provided addressed several typical concerns with bridge projects such as utilizing robust E&S plans and developing an approved Aids to Navigation plan to accommodate recreational use of the waterway during construction. In addition to the plans already presented, DCNR requests that the following actions be integrated into the project to further protect river values that elevate this waterway to inclusion in the scenic rivers program.

PennDOT Response: The project will include Erosion and Sedimentation Control Plans, in addition to a NPDES permit. The project Aids to Navigation Plan will be reviewed by PAFBC.

DCNR Comment: The construction and demolition staging areas should be screened by a vegetative buffer and set back as far as possible from the river's edge.

PennDOT Response: Vegetative buffers will be provided to extent practicable.

DCNR Comment: Bridge materials and design should reflect the character of the surrounding environment as well as any local architectural style, normally creating a "natural" or "rustic" appearance. DCNR would encourage clean, simple, lines that complement rather than dominate the landscape. Additionally, since the river segment is a water trail, the design needs to allow safe recreational use / watercraft passage.

PennDOT Response: The anticipated bridge type is steel or concrete multi-girder, supported by concrete piers and abutments. The piers as shown in the Conceptual Type, Size, and Location are "Tulip" shaped. The pier shape may be changed in Final Design by the Development Entity if approved by PennDOT. Piers will not be allowed within the middle portion of the Lehigh River, defined as Station 2017+10 to Station 2018+20 for the Eastbound bridge and Station 2217+25 to Station 2218+25 for the Westbound bridge (see attached).

DCNR Comment: Consider using darker sand and aggregate native to the area or add a color/tint to any concrete mix to soften the appearance of any new concrete. Concrete structures could also be masonry faced with stone native to the area or the concrete surface could be rough faced/textured and tinted a “natural” color (green, brown, tan, grey, etc).

PennDOT Response: PennDOT will utilize its standard concrete mix specifications, without requirements of specific sand or aggregate color. The concrete will not be tinted, stone faced or textured.

DCNR Comment: Exposed metal structures should be painted a “natural” color. Painting procedures should have minimal impact on the creek and its environs and a containment system that prevents paint and paint chips from entering the creek must be used if any part of the bridge is being painted.

PennDOT Response: The existing steel bridges are being removed. Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.

DCNR Comment: Native or local stone should be used in areas where riprap or stone is needed.

PennDOT Response: The riprap will be required to meet PennDOT’s material specifications, and the contract will not require native or local stone. Where possible, the contract will require riprap to be covered with soil and vegetation.

DCNR Comment: Native vegetation should be incorporated in the disturbed riverbank areas to maintain the natural character and the scenic qualities of the waterway.

PennDOT Response: The contract will require native vegetation to be used in the disturbed riverbank areas. PennDOT will continue to coordinate with DCNR regarding the project plantings.

DCNR Comment: The Scenic Rivers Program recommends that two types of signs be placed on or in close proximity to the bridge. The first type of sign is to be placed at each end of the bridge to let the public know that they are crossing a designated state scenic river. A copy of this sign design is included as Appendix A.

The second type of sign will aid water trail users and safety responders in navigation. It is to be placed on the upstream side of the bridge and centered on the outside face of the parapet. The sign should identify the bridge name so that users can identify their location along the Lehigh River. The design for this is included as Appendix B.

PennDOT Response: The contract will require the scenic river signs at the leading ends of each bridge. The contract will require signage on the bridge for water trail users. The Development Entity will coordinate with PennDOT during final design regarding the location of the sign(s), as the bridge is approximately 100 feet above the river. The sign(s) may be placed on piers in the vicinity of the river.

DCNR Comment: DCNR is a proponent of increased waterway access along designated water trails such as the Lehigh River Water Trail. Therefore, this bridge project should include basic accommodations for a non-motorized boat launch as part of the project design, if at all possible.

PennDOT Response: DCNR is building a new park entrance just south of the bridge, with construction anticipated to be completed in 2021. The new park facilities will include four new boat ramps. One of these boat ramps will be disturbed by the bridge project and will be reconstructed after the bridge project is complete. The other three boat ramps will not be disturbed by the bridge project and will remain open during construction. Coordination with DNCR regarding impacts to the park facilities is ongoing.

DCNR Comment: Additionally, DCNR encourages that bridge replacements include pedestrian/bike access along the bridge to contribute to a more complete transportation system.

PennDOT Response: The interstate bridges will not accommodate pedestrian/bike traffic. Temporary impacts to the Lehigh Gorge Trail under the bridge are being coordinated with DCNR.

Please contact me if you should have any questions or require further information.

Sincerely,

A handwritten signature in black ink, appearing to read 'JB' with a stylized flourish extending to the right.

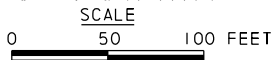
John Baumgardner, PE  
*Project Manager*



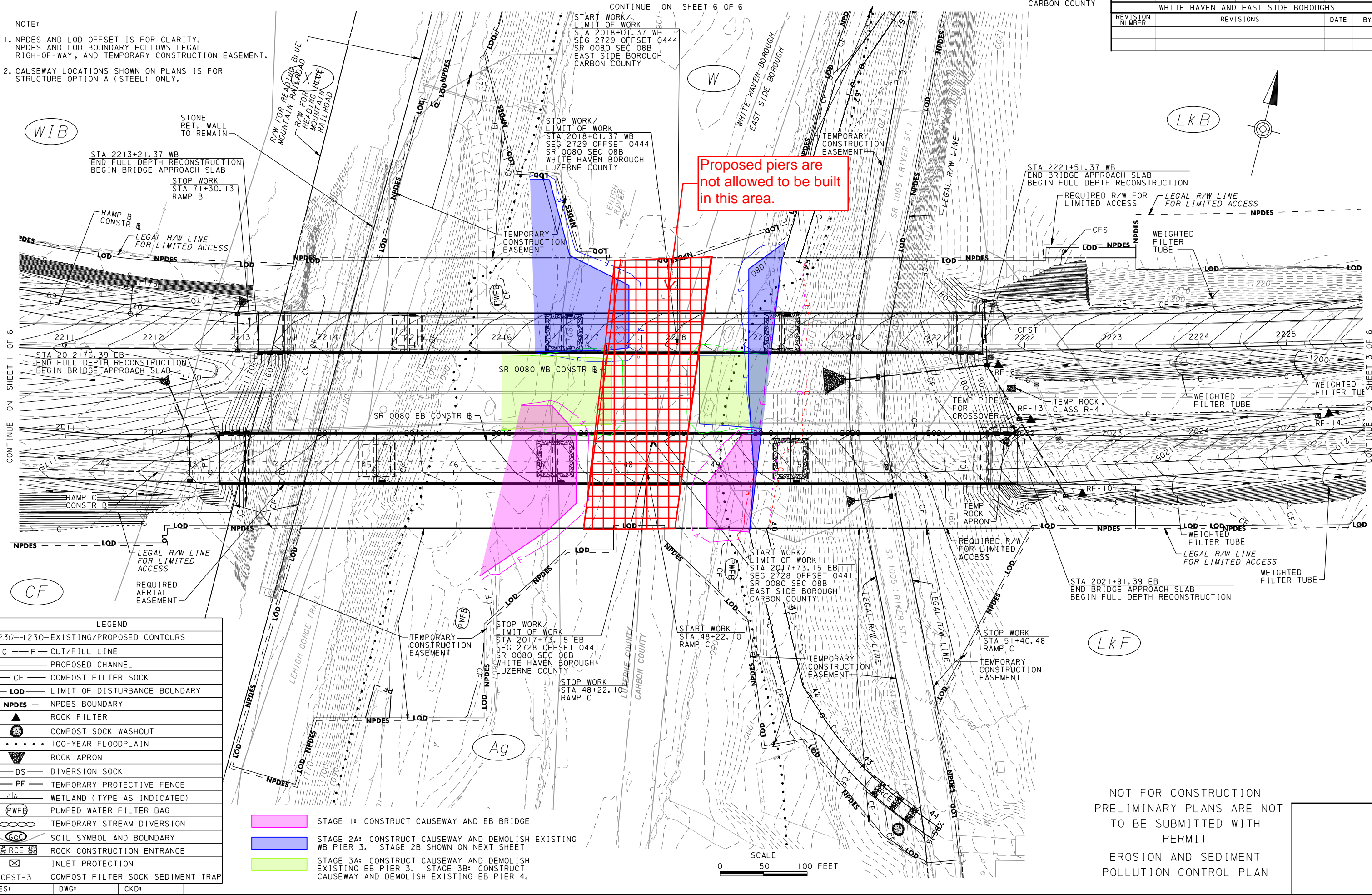
USER:LSALQW PLOT DRIVER:PenDOT\_PDF\_Color.plt;g PLOT DATE:05-07-2021 3:25:08 PM  
PATH:ct:\pwworking\les101\0742841\ FILE:008008E-ES-PLAN1.dgn MODEL:road plan 2

LEGEND	
-1230-1230-	EXISTING/PROPOSED CONTOURS
-C -F-	CUT/FILL LINE
- - -	PROPOSED CHANNEL
CF	COMPOST FILTER SOCK
LOD	LIMIT OF DISTURBANCE BOUNDARY
NPDES	NPDES BOUNDARY
▲	ROCK FILTER
●	COMPOST SOCK WASHOUT
.....	100-YEAR FLOODPLAIN
▼	ROCK APRON
DS	DIVERSION SOCK
PF	TEMPORARY PROTECTIVE FENCE
W	WETLAND (TYPE AS INDICATED)
(PWFB)	PUMPED WATER FILTER BAG
○	TEMPORARY STREAM DIVERSION
Co	SOIL SYMBOL AND BOUNDARY
RCE	ROCK CONSTRUCTION ENTRANCE
⊠	INLET PROTECTION
CFST-3	COMPOST FILTER SOCK SEDIMENT TRAP
DES:	DWG:
	CKD:

- STAGE 1: CONSTRUCT CAUSEWAY AND EB BRIDGE
- STAGE 2A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 3. STAGE 2B SHOWN ON NEXT SHEET
- STAGE 3A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 3. STAGE 3B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 4.



NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
EROSION AND SEDIMENT  
POLLUTION CONTROL PLAN



- NOTE:
- NPDES AND LOD OFFSET IS FOR CLARITY. NPDES AND LOD BOUNDARY FOLLOWS LEGAL RIGHT-OF-WAY, AND TEMPORARY CONSTRUCTION EASEMENT.
  - CAUSEWAY LOCATIONS SHOWN ON PLANS IS FOR STRUCTURE OPTION A (STEEL) ONLY.

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	2 OF 6
WHITE HAVEN AND EAST SIDE BOROUGH				
REVISION NUMBER	REVISIONS			DATE BY

## Krommes, Kathy

---

**From:** Krommes, Kathy  
**Sent:** Monday, December 6, 2021 10:10 AM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] PA Outstanding Geologic Features  
**Attachments:** Lehigh Gorge.docx; I-80\_OutstandingGeologicFeaturesLetter\_20210119.pdf

---

**From:** Shaulis, James <jshaulis@pa.gov>  
**Sent:** Thursday, April 15, 2021 8:34 AM  
**To:** Markowitz, Katherine <Katherine.Markowitz@hdrinc.com>  
**Cc:** Reese, Stuart <streese@pa.gov>; Hand, Kristen <khand@pa.gov>; Ebersole, Craig <craebersol@pa.gov>; Schmid, Katherine <kschmid@pa.gov>  
**Subject:** RE: [External] PA Outstanding Geologic Features

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

I thought I sent this but I didn't. Sorry for the oversight and delay. The bridge replacement project involving the I-80 Section 08B near White Haven will not have any impact on the Leigh Gorge geoheritage site you were alerted to in your PNDI search. Thank you for contacting us and providing us the opportunity to comment on this project.

Sincerely,  
Jim Shaulis

---

**From:** Markowitz, Katherine [mailto:Katherine.Markowitz@hdrinc.com]  
**Sent:** Wednesday, April 14, 2021 11:15 AM  
**To:** Shaulis, James <jshaulis@pa.gov>  
**Subject:** RE: [External] PA Outstanding Geologic Features

Hello Jim,

I wanted to follow up on this request for the I-80 bridge replacement project. Were you able to review the project information and attachments?

Thank you,

**Katie Markowitz**  
D 717-516-3155 M 201-258-0508

[hdrinc.com/follow-us](https://hdrinc.com/follow-us)

---

**From:** Markowitz, Katherine  
**Sent:** Tuesday, January 19, 2021 3:40 PM  
**To:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>  
**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>  
**Subject:** RE: [External] PA Outstanding Geologic Features

Hello Jim,

Please find attached a letter requesting review by the DCNR for the I-80 bridge replacement project. The letter contains project information and attachments including the project description, location, photos, and the PNDI receipt.

Thank you,

**Katie Markowitz**

**D** 717-516-3155 **M** 201-258-0508

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)

---

**From:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>

**Sent:** Thursday, January 7, 2021 1:20 PM

**To:** Markowitz, Katherine <[Katherine.Markowitz@hdrinc.com](mailto:Katherine.Markowitz@hdrinc.com)>

**Subject:** RE: [External] PA Outstanding Geologic Features

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

If could you please send me a pdf with the PNDI receipt information, location, and outline of the project I can get back to you with our comments.

Thanks,

jim

---

**From:** Markowitz, Katherine [<mailto:Katherine.Markowitz@hdrinc.com>]

**Sent:** Thursday, January 7, 2021 12:31 PM

**To:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>

**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>

**Subject:** [External] PA Outstanding Geologic Features

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good afternoon Mr. Shaulis,

I received your name and contact information from Kelly Rossiter to request environmental review in regards to PA Outstanding Geologic Features. HDR, on behalf of PennDOT, is providing environmental services for the I-80 bridge replacement project near White Haven Borough, Luzerne County, PA, where the project traverses over the Lehigh River, Lehigh Gorge Trail, and Lehigh Gorge State Park. Please let me know what project information is needed for this request of environmental review for the project NEPA document. In this time of the pandemic, may I assume an emailed request would be appropriate?

Thank you,

**Katie Markowitz**

*Environmental Scientist*

**HDR**

4900 Ritter Road, Suite 101

Mechanicsburg, PA 17055

**D** 717-516-3155 **M** 201-258-0508

[katherine.markowitz@hdrinc.com](mailto:katherine.markowitz@hdrinc.com)

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)





BUREAU OF RECREATION AND CONSERVATION

February 22, 2022

Nicole L. Auker  
Environmental Planning Supervisor  
PA Department of Transportation  
400 North Street, 7<sup>th</sup> Floor  
Harrisburg, PA 17120

RE: I-80 WHITE HAVEN BRIDGE REPLACEMENT & GABION WALL REPLACEMENT  
SR 0080 & SR 0940 River Street  
Luzerne County, Carbon County  
LWCF 42-00033, 42-00081, Lehigh Gorge State Park

Dear Ms. Auker:

This letter is being written based on PennDOT's need to proceed with the environmental process for the above referenced bridge replacement project. The Federal Highway Administration requires acknowledgement of coordination between agencies to allow the project to continue through the environmental process, to begin the Right-Of-Way process, engage with the landowner for potential acquisition, and complete the National Park Service (NPS) Compliance and Stewardship Form (C&S Form).

The replacement of the I-80 bridge and Gabion wall along SR 0940 in East Side Borough will require permanent/TCE of 6.2 acres of impact to the Land and Water Conservation Fund protected Lehigh Gorge State Park. The impact area breakdown is as follows; 0.1 acre of Required Right-of-way, 0.4 acres of proposed Slope Easement, 0.5 acres of proposed Aerial Easement, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement.

The proposed replacement property acreage is approximately 6.45 acres, Parcel ID (65A-51-C402) in Penn Forest Township. DCNR received NPS concurrence on the proposed conversion and replacement property on 11/22/2021. DCNR Bureau of State Parks, DCNR Bureau of Recreation and Conservation, and PennDOT agree on the proposed replacement property to satisfy the value, location and recreational usefulness criteria under the Land and Water Conservation Fund Act as mitigation for both the I-80 bridge replacement and the SR 0940 gabion wall replacement.

DCNR recognizes PennDOT's need to proceed through the project planning and design phases and agrees with the proposed conversion footprint and replacement property while we await NPS concurrence. Please understand NPS has ultimate approval authority of the conversion package including replacement property.

If you have questions or concerns, please contact me at (717) 783-0318 or Ashley Rebert at (717) 772-3322.

Sincerely,

*Emma Harrison*

Emma Harrison  
Land Conversion Specialist  
Bureau of Recreation and Conservation

cc:, Ashley Rebert

## Krommes, Kathy

---

**From:** Krommes, Kathy  
**Sent:** Monday, March 28, 2022 3:06 PM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

---

**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Monday, November 22, 2021 3:26 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Cc:** Imgrund, Lauren <[limgrund@pa.gov](mailto:limgrund@pa.gov)>  
**Subject:** [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good Afternoon Ashley-

The NPS, LWCF program has reviewed the proposed conversion footprint and replacement property for the SR0080 Section 550 conversion at LWCF 42-00033 Walter Dick Memorial Park and the I-80 White Haven Bridge, conversion replacement property materials (Impacts Lehigh Gorge State Park, Luzerne County).

### Walter Dick Mem Park

The NPS, LWCF can concur that the proposed replacement property adjacent to Walter Dick Memorial Park satisfies the LWCF Act and Post Completion Compliance Regulations (36 CFR 59.3) for appropriate replacement property.

However, please confirm the final conversion footprint acreage: I had to go back and check through Jack's emails and a February email with attachment from you indicates the conversion footprint impact to this park is 7.6 acres; within the PennDot letter of August 31st 2021, PennDOT indicates a 5.5 acre footprint and approximately 6 acres replacement property.

An email from you with the updated/explanation of what the actual conversion footprint is meant to be will suffice. Also keep in mind that when the conversion package is submitted we will need maps indicating the correct acreage.

### Lehigh Gorge SP conversion - I-80 White Haven Bridge, Impacts Lehigh Gorge State Park, Luzerne County

The NPS LWCF also concurs with the proposed replacement property at Unionville Road for the replacement of 7 acres of land converted from Recreation use at Lehigh Gorge SP meets the LWCF Act and regulations.

I sincerely apologize for the delay in reviewing the documentation for the above projects; thank you for your patience!

If you have any additional questions, please let me know.

Missy

**Missy Morrison**

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557

U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
United States Coast Guard  
Fifth Coast Guard District

431 Crawford Street  
Portsmouth, VA 23704-5004  
Staff Symbol: dpb  
Phone: (757) 398-6422  
Fax: (757) 398-6334  
Email: [Crystal.k.tucker@uscg.mil](mailto:Crystal.k.tucker@uscg.mil)

16591  
18 NOV 2021

Ms. Brandy Rotz  
Pennsylvania Department of Transportation  
1002 Hamilton Street  
Allentown, PA 18101

Dear Ms. Rotz:

Coast Guard review of your proposed project as provided in your email dated August 18, 2021, is complete.

Based on the documentation provided and our research, it is determined that a Coast Guard bridge permit will not be required for the proposed highway fixed bridges – Lehigh River Bridges over Lehigh River, mile 5.7, 41.055423, -75.770451 between Luzerne and Carbon County, PA.

The project will be placed in our Coast Guard Authorization Act of 1982 exemption category for the location and structure described above and **is valid for five years from the date of this letter**. The Coast Guard Authorization Act of 1982 exempts bridge projects from Coast Guard Bridge permits when the bridge project crosses non-tidal waters which are not used, susceptible to use in their natural condition, or susceptible to use by reasonable improvement as a means to transport interstate commerce. The following conditions apply to this determination:

- a. If the construction project on the above bridge does not commence within this time, you must contact this office for reaffirmation of this determination.
- b. Future bridge projects along the above waterway will have to be independently evaluated before they may be considered for placement in the Coast Guard Authorization Act of 1982 exemption category. This includes modification, replacement and removal of the above bridge, following its initial construction.

In addition, the requirement to display navigational lighting at the aforementioned bridge is hereby waived, as per Title 33 Code of Federal Regulations, Part 118.40(b). This waiver may be rescinded at any time in the future should nighttime navigation through the proposed bridge be increased to a level determined by the District Commander to warrant lighting.

The fact that a Coast Guard bridge permit is not required does not relieve you of the responsibility for compliance with the requirements of any other Federal, State, or local agency who may have jurisdiction over any aspect of the project.

16591  
18 NOV 2021

If you have any further questions, please contact Ms. Crystal K. Tucker at the above listed address or telephone number.

Sincerely,



HAL R. PITTS  
Bridge Program Manager  
By direction

Copy: Mr. John Baumgardner, HDR Inc.  
CG Sector Delaware Bay, Waterways Management  
U. S. Army Corps of Engineers, Philadelphia District  
Federal Highways Administration, Harrisburg, PA



**Appendix E**  
**Threatened and Endangered Species**



## Pennsylvania Fish & Boat Commission

---

**Division of Environmental Services**  
Watershed Analysis Section  
595 E Rolling Ridge Dr.  
Bellefonte, PA 16823

October 15, 2021

**IN REPLY REFER TO**  
SIR# 55187

HDR  
Kathleen Krommes  
4900 Ritter Road  
Suite 101  
Mechanicsburg, Pennsylvania 17055

**RE: Species Impact Review (SIR) – Rare, Candidate, Threatened and Endangered Species  
PNDI Search No.  
I-80 Bridge Replacement over Lehigh River  
CARBON County: East Side Borough, Kidder Township - LUZERNE County: Foster  
Township, White Haven Borough**

Dear Kathleen Krommes:

This responds to your inquiry about a Pennsylvania Natural Diversity Inventory (PNDI) Internet Database search “potential conflict” or a threatened and endangered species impact review. These projects are screened for potential conflicts with rare, candidate, threatened or endangered species under Pennsylvania Fish & Boat Commission jurisdiction (fish, reptiles, amphibians, aquatic invertebrates only) using the Pennsylvania Natural Diversity Inventory (PNDI) database and our own files. These species of special concern are listed under the Endangered Species Act of 1973, the Wild Resource Conservation Act, and the Pennsylvania Fish & Boat Code (Chapter 75), or the Wildlife Code.

An element occurrence of a rare, candidate, threatened, or endangered species under our jurisdiction is known from the vicinity of the proposed project. However, given the nature of the proposed project, the immediate location, or the current status of the nearby element occurrence(s), no adverse impacts are expected to the species of special concern.

This response represents the most up-to-date summary of the PNDI data and our files and is valid for two (2) years from the date of this letter. An absence of recorded species information does not necessarily imply species absence. Our data files and the PNDI system are continuously being updated with species occurrence information. Should project plans change or additional information on listed or proposed species become available, this determination may be reconsidered, and consultation shall be re-initiated.

### Our Mission:

[www.fish.state.pa.us](http://www.fish.state.pa.us)

*To protect, conserve and enhance the Commonwealth's aquatic resources and provide fishing and boating opportunities.*

**If you have any questions regarding this review, please contact Joshua Wisor at 814-359-5135 and refer to the SIR # 55187.** Thank you for your cooperation and attention to this important matter of species conservation and habitat protection.

Sincerely,

A handwritten signature in blue ink that reads "Joshua M. Wisor". The signature is written in a cursive style with a large initial 'J' and 'W'.

Joshua Wisor, Fisheries Biologist  
Watershed Analysis Section

HAS/JMW/dn



## 1. PROJECT INFORMATION

Project Name: **PennDOT - I-80 Lehigh River Bridges; Pathways Program**

Date of Review: **10/13/2021 12:43:51 PM**

Project Category: **Transportation, Structures and Bridges, Bridge Replacement adjacent to existing alignment (within 100 feet up/down stream)**

Project Area: **136.70 acres**

County(s): **Carbon; Luzerne**

Township/Municipality(s): **EAST SIDE; FOSTER TOWNSHIP; KIDDER TOWNSHIP; WHITE HAVEN**

ZIP Code:

Quadrangle Name(s): **HICKORY RUN; WHITE HAVEN**

Watersheds HUC 8: **Lehigh**

Watersheds HUC 12: **Black Creek-East Side of Lehigh River; Stony Creek-Lehigh River; Wright Creek-Lehigh River**

Decimal Degrees: **41.057305, -75.764172**

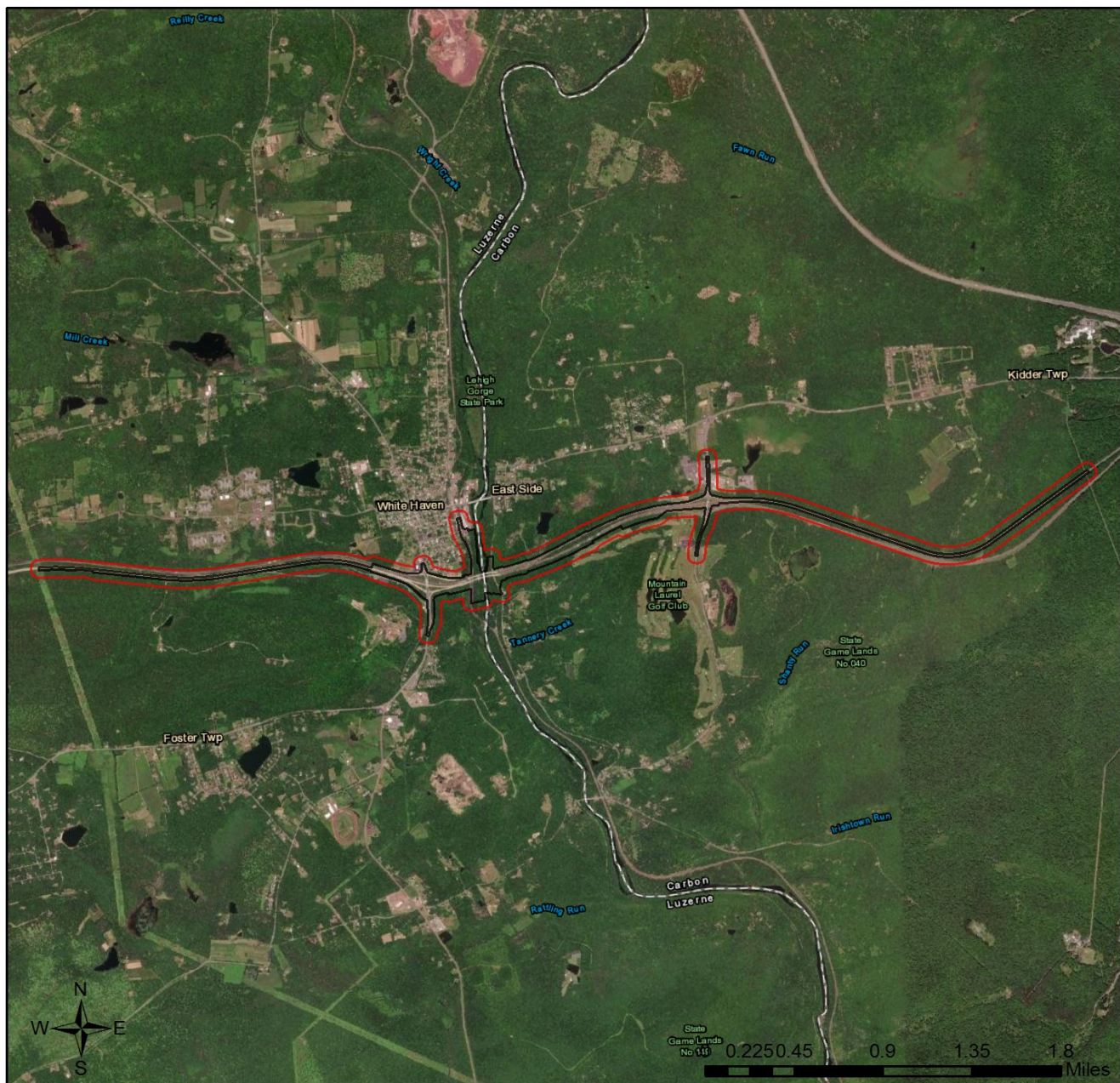
Degrees Minutes Seconds: **41° 3' 26.2964" N, 75° 45' 51.190" W**

## 2. SEARCH RESULTS

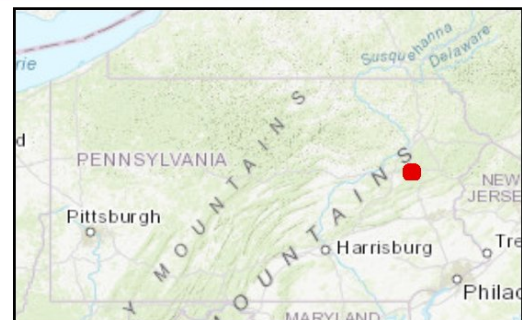
Agency	Results	Response
PA Game Commission	No Known Impact	No Further Review Required
PA Department of Conservation and Natural Resources	No Known Impact	No Further Review Required
PA Fish and Boat Commission	<b>Potential Impact</b>	<b>FURTHER REVIEW IS REQUIRED, See Agency Response</b>
U.S. Fish and Wildlife Service	No Known Impact	No Further Review Required

As summarized above, Pennsylvania Natural Diversity Inventory (PNDI) records indicate there may be potential impacts to threatened and endangered and/or special concern species and resources within the project area. If the response above indicates "No Further Review Required" no additional communication with the respective agency is required. If the response is "Further Review Required" or "See Agency Response," refer to the appropriate agency comments below. Please see the DEP Information Section of this receipt if a PA Department of Environmental Protection Permit is required.

## PennDOT - I-80 Lehigh River Bridges; PATHways Program



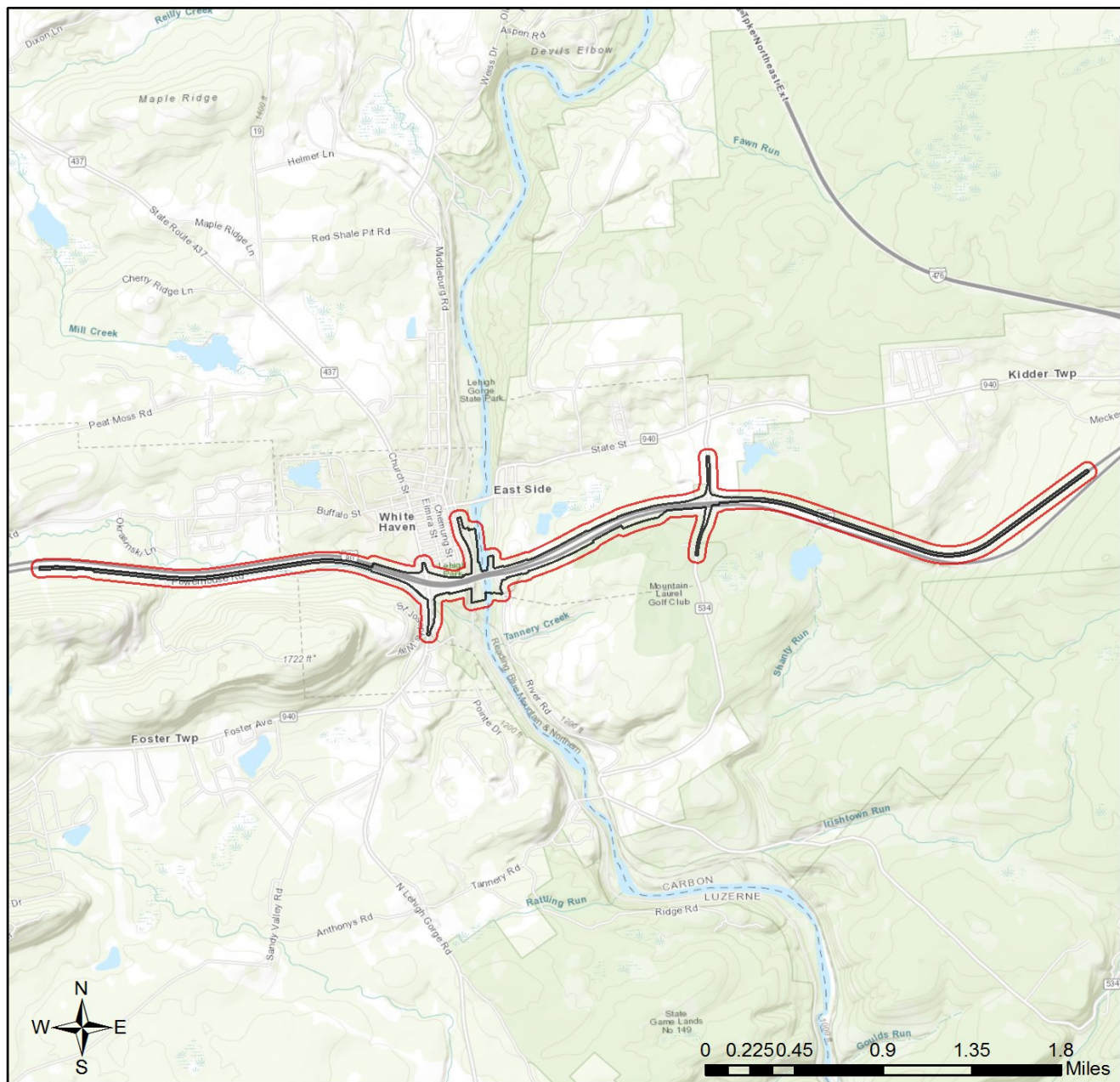
- ☐ Project Boundary
- ☐ Buffered Project Boundary



Service Layer Credits: Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community  
Esri, HERE, Garmin, (c) OpenStreetMap contributors, and the GIS user community  
Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China

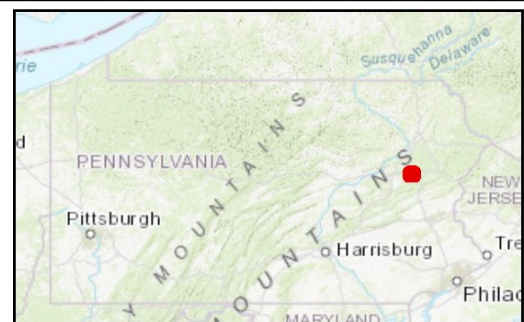


## PennDOT - I-80 Lehigh River Bridges; PATHways Program



- Project Boundary
- Buffered Project Boundary

Service Layer Credits: Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), (c) OpenStreetMap contributors, and the GIS User Community





### 3. AGENCY COMMENTS

Regardless of whether a DEP permit is necessary for this proposed project, any potential impacts to threatened and endangered species and/or special concern species and resources must be resolved with the appropriate jurisdictional agency. In some cases, a permit or authorization from the jurisdictional agency may be needed if adverse impacts to these species and habitats cannot be avoided.

These agency determinations and responses are **valid for two years** (from the date of the review), and are based on the project information that was provided, including the exact project location; the project type, description, and features; and any responses to questions that were generated during this search. If any of the following change: 1) project location, 2) project size or configuration, 3) project type, or 4) responses to the questions that were asked during the online review, the results of this review are not valid, and the review must be searched again via the PNDI Environmental Review Tool and resubmitted to the jurisdictional agencies. The PNDI tool is a primary screening tool, and a desktop review may reveal more or fewer impacts than what is listed on this PNDI receipt. The jurisdictional agencies **strongly advise against** conducting surveys for the species listed on the receipt prior to consultation with the agencies.

#### PA Game Commission

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Department of Conservation and Natural Resources

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Fish and Boat Commission

##### RESPONSE:

Further review of this project is necessary to resolve the potential impact(s). Please send project information to this agency for review (see WHAT TO SEND).

**PFBC Species:** (Note: The Pennsylvania Conservation Explorer tool is a primary screening tool, and a desktop review may reveal more or fewer species than what is listed below.)

Scientific Name	Common Name	Current Status
Sensitive Species**		Endangered

#### U.S. Fish and Wildlife Service

##### RESPONSE:

No impacts to **federally** listed or proposed species are anticipated. Therefore, no further consultation/coordination under the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq. is required. Because no take of federally listed species is anticipated, none is authorized. This response does not reflect potential Fish and Wildlife Service concerns under the Fish and Wildlife Coordination Act or other authorities.

\* Special Concern Species or Resource - Plant or animal species classified as rare, tentatively undetermined or candidate as well as other taxa of conservation concern, significant natural communities, special concern populations (plants or animals) and unique geologic features.

\*\* Sensitive Species - Species identified by the jurisdictional agency as collectible, having economic value, or being susceptible to decline as a result of visitation.

## WHAT TO SEND TO JURISDICTIONAL AGENCIES

If project information was requested by one or more of the agencies above, upload\* or email the following information to the agency(s) (see AGENCY CONTACT INFORMATION). Instructions for uploading project materials can be found [here](#). This option provides the applicant with the convenience of sending project materials to a single location accessible to all three state agencies (but not USFWS).

\*If information was requested by USFWS, applicants must email, or mail, project information to [IR1\\_ESPenn@fws.gov](mailto:IR1_ESPenn@fws.gov) to initiate a review. USFWS will not accept uploaded project materials.

### Check-list of Minimum Materials to be submitted:

\_\_\_\_ Project narrative with a description of the overall project, the work to be performed, current physical characteristics of the site and acreage to be impacted.

\_\_\_\_ A map with the project boundary and/or a basic site plan (particularly showing the relationship of the project to the physical features such as wetlands, streams, ponds, rock outcrops, etc.)

**In addition to the materials listed above, USFWS REQUIRES the following**

\_\_\_\_ **SIGNED** copy of a Final Project Environmental Review Receipt

### The inclusion of the following information may expedite the review process.

\_\_\_\_ Color photos keyed to the basic site plan (i.e. showing on the site plan where and in what direction each photo was taken and the date of the photos)

\_\_\_\_ Information about the presence and location of wetlands in the project area, and how this was determined (e.g., by a qualified wetlands biologist), if wetlands are present in the project area, provide project plans showing the location of all project features, as well as wetlands and streams.

## 4. DEP INFORMATION

The Pa Department of Environmental Protection (DEP) requires that a signed copy of this receipt, along with any required documentation from jurisdictional agencies concerning resolution of potential impacts, be submitted with applications for permits requiring PNDI review. Two review options are available to permit applicants for handling PNDI coordination in conjunction with DEP's permit review process involving either T&E Species or species of special concern. Under sequential review, the permit applicant performs a PNDI screening and completes all coordination with the appropriate jurisdictional agencies prior to submitting the permit application. The applicant will include with its application, both a PNDI receipt and/or a clearance letter from the jurisdictional agency if the PNDI Receipt shows a Potential Impact to a species or the applicant chooses to obtain letters directly from the jurisdictional agencies. Under concurrent review, DEP, where feasible, will allow technical review of the permit to occur concurrently with the T&E species consultation with the jurisdictional agency. The applicant must still supply a copy of the PNDI Receipt with its permit application. The PNDI Receipt should also be submitted to the appropriate agency according to directions on the PNDI Receipt. The applicant and the jurisdictional agency will work together to resolve the potential impact(s). See the DEP PNDI policy at <https://conservationexplorer.dcnr.pa.gov/content/resources>.

## 5. ADDITIONAL INFORMATION

The PNDI environmental review website is a preliminary screening tool. There are often delays in updating species status classifications. Because the proposed status represents the best available information regarding the conservation status of the species, state jurisdictional agency staff give the proposed statuses at least the same consideration as the current legal status. If surveys or further information reveal that a threatened and endangered and/or special concern species and resources exist in your project area, contact the appropriate jurisdictional agency/agencies immediately to identify and resolve any impacts.

For a list of species known to occur in the county where your project is located, please see the species lists by county found on the PA Natural Heritage Program (PNHP) home page ([www.naturalheritage.state.pa.us](http://www.naturalheritage.state.pa.us)). Also note that the PNDI Environmental Review Tool only contains information about species occurrences that have actually been reported to the PNHP.

## 6. AGENCY CONTACT INFORMATION

### PA Department of Conservation and Natural Resources

Bureau of Forestry, Ecological Services Section  
400 Market Street, PO Box 8552  
Harrisburg, PA 17105-8552  
Email: [RA-HeritageReview@pa.gov](mailto:RA-HeritageReview@pa.gov)

### PA Fish and Boat Commission

Division of Environmental Services  
595 E. Rolling Ridge Dr., Bellefonte, PA 16823  
Email: [RA-FBPACENOTIFY@pa.gov](mailto:RA-FBPACENOTIFY@pa.gov)

### U.S. Fish and Wildlife Service

Pennsylvania Field Office  
Endangered Species Section  
110 Radnor Rd; Suite 101  
State College, PA 16801  
Email: [IR1\\_ESPenn@fws.gov](mailto:IR1_ESPenn@fws.gov)  
NO Faxes Please

### PA Game Commission

Bureau of Wildlife Habitat Management  
Division of Environmental Planning and Habitat Protection  
2001 Elmerton Avenue, Harrisburg, PA 17110-9797  
Email: [RA-PGC\\_PNDI@pa.gov](mailto:RA-PGC_PNDI@pa.gov)  
NO Faxes Please

## 7. PROJECT CONTACT INFORMATION

Name: Kathleen I. Krommes  
Company/Business Name: HDR  
Address: 4900 Bitter Road, Suite 101  
City, State, Zip: Mechanicsburg, PA 17055  
Phone: (717) 516-3158 Fax: (717) 516-3145  
Email: kathy.krommes@hdrinc.com

## 8. CERTIFICATION

I certify that ALL of the project information contained in this receipt (including project location, project size/configuration, project type, answers to questions) is true, accurate and complete. In addition, if the project type, location, size or configuration changes, or if the answers to any questions that were asked during this online review change, I agree to re-do the online environmental review.

Kath I. Krommes  
applicant/project proponent signature

3/22/2022  
date



**Appendix F**  
**Section 4(f) Evaluation**



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**SELECT ONE:**   ☐ EIS                      ☐ EA                      ☒ CE                      ☐ EER                      ☐ ED

**PROJECT DESCRIPTION:**

(Provide a concise but thorough description of the proposed action.)

The proposed project consists of the replacement of the I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The project location is shown on Figure 1.

The purpose of the project is to address the deterioration of the I-80 Lehigh River bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and residents.

The needs for this project include:

- The existing I-80 dual bridges are more than 50 years old. The fracture critical bridges show deterioration and are approaching the end of their serviceable lifespan.
- I-80 provides a critical link in the interstate commerce system, and as such is vital to the efficient movement of goods and people. Further deterioration of the I-80 Lehigh River bridges could cause the bridges to be load posted.

Originally constructed in 1965, the I-80 Eastbound and Westbound structures are 836 feet long and 757 feet long, respectively. The structures consist of steel two-girder superstructures. The substructures consist of reinforced concrete abutments and reinforced concrete piers that reach heights of over 110 feet.

I-80 in the vicinity of the project has two 12' lanes of travel in both directions with 8' left shoulders and 10' right shoulders. The median width varies from 60' to 150'. This section of I-80 carries east-west traffic across Pennsylvania's northern tier with Average Daily Traffic (ADT) of 28,774 vehicles per day (2021) with 34 percent truck traffic and is projected to grow to 53,736 vehicles per day by 2046 (design year).

The Eastbound bridge will be replaced on a new alignment immediately to the south of the existing Eastbound bridge. The new Eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Westbound bridge will be replaced on the same approximate alignment as the existing Westbound bridge. At Exit 273 (SR 940), the Eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the Westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert.

**IDENTIFICATION OF SECTION 4(f)/SECTION 2002 PROPERTY:**

(List the property and provide a description of the property as per Chapter 6 of the *Section 4(f)/Section 2002 Handbook*. Attach a map, photo(s), etc. as appropriate.)

Lehigh Gorge State Park (LGSP) extends on both east and west sides of the Lehigh River on land owned by the Commonwealth of Pennsylvania and administered by PA Department of Conservation and Natural Resources (DCNR). According to DCNR's website, the 6,107-acre park follows the Lehigh River from Francis E. Walter Dam north of I-80 to Jim Thorpe to the south. Figure 2 shows the area of LGSP in the vicinity of the I-80 bridges.

The current main entrance to the LGSP in the project area is from Main Street in White Haven Borough. From the LGSP park entrance, a shared-use trail extends north to south in abandoned railroad grade along the river and provides opportunities for hiking, bicycling, sightseeing, and photography. The majority of the trail is closed to motor vehicles, although the entrance from Main Street to a parking area near the I-80 bridges is open for motor vehicle access. Within the state park, the trail is named the Lehigh Gorge Trail. According to DCNR's LGSP website, the Lehigh Gorge Trail follows more than 20 miles of the Delaware & Lehigh (D&L) Trail, the foundation of the 165-mile Delaware & Lehigh National Heritage Corridor.

The Lehigh Gorge Trail extends north from the LGSP northern property line near the Main Street entrance within a public easement DCNR procured for the purpose of connecting the trail to LGSP property at the northern end of White Haven Borough. For the purposes of this Section 4(f) Evaluation, the trail segment within DCNR Easement is included along with the Lehigh Gorge Trail within LGSP because it is one continuous trail and has the same Official With Jurisdiction (DCNR).





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

After crossing through private property within DCNR Easement, the D&L Trail continues north following Main Street public right-of-way in White Haven Borough.

**FOR PARKS, IDENTIFY KEY COMPONENTS OF ANY EXISTING MANAGEMENT PLAN (if it exists):**

The LGSP does not have an existing management plan.

**OFFICIAL WITH JURISDICTION OVER SECTION 4(f)/SECTION 2002 PROPERTY:**

1. Identify agency with jurisdiction:  
Pennsylvania Department of Conservation and Natural Resources (DCNR)
2. Name and title of contact person at agency:  
Rex Bradish, Manager, Hickory Run State Park Complex  
Ryan Dysinger, Assistant Director, PA DCNR Bureau of State Parks



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**APPLICABILITY DETERMINATION:**

1. Provide the total acreage of the property: Lehigh Gorge State Park occupies 6,107 acres.

Describe the use of land from the property (identify amount of the property to be used, including temporary and permanent acquisition):

The proposed bridge replacement project will take approximately 0.4 acre of Proposed Slope Easement, 0.5 acre Proposed Aerial Easements, 4.9 acres of Temporary Construction Easements (TCE) within LGSP, and 0.3 acres of TCE within DCNR Easement. Proposed Slope Easement is necessary to replace the eastbound bridge on an alignment south of the existing bridge. Proposed Aerial Easement is needed for future access, maintenance, and construction. TCE is necessary to provide contractor access and staging. Construction of the bridge replacements is expected to take approximately four years. Figure 2, Section 4(f) / 6(f) Location Map, shows impact areas and includes acreages in the legend.

DCNR has initiated a capital improvements project that started in 2021 and is expected to be completed in 2022, prior to PennDOT's bridge replacement project. DCNR's project will construct a new LGSP entrance from SR 940 south of the I-80 bridges. In addition to the new entrance, improvements will include parking lots, flush facilities, and four boat launches. DCNR intends for all park vehicular traffic, including commercial biking and boating outfitters, to utilize the new park entrance from SR 940. Anticipated temporary impacts to park property and facilities, including the future facilities, are described below and shown on Figure 3.

**Lehigh Gorge Trail:**

- April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
- Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**LGSP driveway and parking:**

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges. If necessary, reconfiguration of the driveway and parking due to pier locations will be coordinated with DCNR.
- The future main park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

**Boat launches:**

- The boat launch directly under the bridges will be closed during construction. The planned improvements in this area, including upgraded boat launch, turnaround and handicap parking will be disturbed and will be reconstructed at the end of the project. These facilities may need to be reconfigured depending upon the chosen bridge pier location. Any such reconfiguration will be coordinated with DCNR.
- The three future boat launches to the south (downstream) of the bridges will not be impacted and will remain open during construction. These boat launches can be used for commercial outfitters and general public during construction.

**LGSP flush facilities:**

- The future flush facilities north of the bridges will not be disturbed.
- Access to these facilities from the Lehigh Gorge Trail will be maintained, though access to the facilities from south of the bridges will be cut off during trail closures as discussed above. Temporary portable facilities can be provided south of the bridges when the trail is closed at the bridges.
- Public vehicular access to the flush facilities will not be provided.
- DCNR access to the flush facilities from the driveway, if needed, will be coordinated with the contractor.

**Lehigh River:**

- Temporary causeways will extend from both riverbanks but will not extend completely across the river.





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- Boat traffic through the bridge construction site will be maintained for most of construction. Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.

**Lehigh Gorge State Park:**

- Proposed Slope Easement of approximately 0.4 acre and Proposed Aerial Easements of approximately 0.5 acre are necessary due to shifting the eastbound bridge and on-ramp to the south of existing alignment.
- Temporary Construction Easements are needed on both sides of the river, totaling approximately 5.2 acres within LGSP and DCNR Easement, to provide contractor access and staging areas.

2. The project **does not** adversely affect the activities, features, and attributes of the resource that qualify it for protection under Section 4(f) or Section 2002. **(If this statement cannot be verified as true, *de minimis*/no adverse use does not apply.)** ☒ YES

Describe the effect to the qualities, activities, features, or attributes of the resource that qualify it for protection. Include a description of any mitigation included when making the determination regarding effects to the resource:

The project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridges will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within the Proposed Aerial Easement adjacent to the bridges will be available for use by LGSP after construction.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Mitigation measures will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion (Proposed Slope Easement, Proposed Aerial Easements, and TCE) within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

3. The public was afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the resource. ☒ YES

Identify the opportunity(ies) for public comment and describe the input received (provide attachments as appropriate to document the public involvement activity):

A public notice was prepared and included a map of LGSP amenities and a written notice describing the anticipated impacts to LGSP (Attachment C). The notice requested public input and provided an email and physical address for comments. On September 24, 2021, laminated copies of the public notice were delivered to DCNR for posting in LGSP and nearby Hickory Run State Park. On the same day, the public notice was emailed to White Haven Borough, East Side Borough, Kidder Township, D&L National Heritage Corridor, and the whitewater and bicycle outfitters that operate in the park. Additionally, the public notice was handed out to local businesses in White Haven Borough. Upon the suggestion of LGSP Manager, Rex Bradish, a newspaper advertisement was published in the local newspaper, the Journal-Herald. Proof of publication in the October 7, 2021, and October 14, 2021 issues of the Journal-Herald are included in Attachment C.

The public comment period began September 24, 2021 and ended October 26, 2021. Three comments were received during this 30-day comment period. Only one comment was related to Section 4(f), LGSP, and the potential impacts to park amenities and visitors. The comment from a local resident and business owner included concerns of negative affects to White Haven Borough's local economy due to a four-year closure of the Lehigh Gorge Trail and requested highway signs to the downtown business district. Comments are included in Attachment C.

LGSP's Manager has reported that this comment and request for signs was voiced at a prior public meeting conducted for DCNR's LGSP improvement project, which moves the state park main entrance from its current location in downtown White Haven to SR 940 south of I-80. PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

In addition to the public outreach conducted September 24, 2021, to October 26, 2021, one written comment related to the Lehigh Gorge Trail was received from D&L National Heritage Corridor during the NEPA public meeting and comment period November 17, 2021 to December 17, 2021. PennDOT will continue coordination with D&L National Heritage Corridor as the project progresses. The full comment is included in the Attachment C.





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

4. The official with jurisdiction over the property was informed of FHWA's and/or PennDOT's intent to make a *de minimis*/no adverse use finding. ☒ YES

Identify the method used to notify the official with jurisdiction, and attach appropriate correspondence.

A letter describing the impacts to Lehigh Gorge State Park and PennDOT's intention of making a Section 4(f) De Minimis Use determination was provided to DCNR. Mitigation measures were detailed in the letter. A signed copy of the letter is attached to this form.

Correspondence documenting notification of the official with jurisdiction is included in the following Attachment: Section 4(f) De Minimis Concurrence Letter signed 8/17/2022

5. The official with jurisdiction over the property concurred in writing with FHWA's and/or PennDOT's determination that the project will not adversely affect the property. (NOTE: Public input must be received and considered prior to the official with jurisdiction making a final determination.) ☒ YES

Identify the official with jurisdiction and date of concurrence and attach written concurrence:

Ryan Dysinger, Assistant Director, Bureau of State Parks, DCNR

Written concurrence from the official with jurisdiction is included in the following Attachment: Section 4(f) De Minimis Concurrence Letter signed 8/17/2022

6. If the Section 4(f)/Section 2002 use involves State Game Land, verify that the use is considered *de minimis* in accordance with the *Cooperative Interagency Agreement for Interdepartmental Land Transfer of State Game Lands*. (Describe and obtain PA Game Commission concurrence signature below for use of a State Game Land Bank and/or Interdepartmental Land Transfer.) ☐ YES

☐ **State Game Land Bank**

Debiting [Click here to enter text.](#) (acres)

From [Click here to enter text.](#) SGL bank

PGC Signature:

Date: [Click here to enter a date.](#)

☐ **Interdepartmental Land Transfer**



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

PGC Signature:

Date: [Click here to enter a date.](#)

7. Have Federal or State funds [LWCF 6(f)/Project 70/Project 500/other recreation grants] been used in the acquisition of, or for any improvements to, the Section 4(f) property? ☒ YES  
☐ NO

If Yes, the appropriate Federal agency has been coordinated with and is in agreement with the land conversion or transfer. ☒ YES

**Provide more information regarding the Section 6(f)/Project 70/Project 500/other recreation grants coordination:**

DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for the Lehigh Gorge State Park. The entire state park is afforded protection under Section 6(f). As such, any land necessary to be acquired for construction of the project would be considered a conversion from public outdoor recreation use to transportation use. Temporary non-conforming use of Section 6(f) protected land (i.e., temporary construction easements) lasting longer than 180 days is considered a permanent conversion also requiring replacement of property. The National Park Service can approve such conversion only if it is found to be in accord with the comprehensive statewide outdoor recreation plan and with the replacement of property of at least equal fair market value and of reasonably equivalent usefulness and location. Approximately 6 acres of land will be replaced. The concurrence letters from DCNR and NPS are in Attachment D.

8. The project does not involve any uses that would require an individual Section 4(f) evaluation. (It is acceptable if there are other Section 4(f) uses that are covered by one of the nationwide programmatic Section 4(f) evaluations or meet temporary occupancy criteria.) ☒ YES

If there are other Section 4(f) properties used, list them here, briefly describe the use, and identify which form(s) will be completed to address the use:

Construction of the project will involve construction activities around, over and in the Lehigh River. The section of the Lehigh River within the project area is designated by PA Fish and Boat Commission as the Northern Section of the Lehigh River Water Trail. The Lehigh River is also designated as a Pennsylvania Scenic River within the project area. The Section 4(f) Evaluation for the Lehigh River Water Trail and PA Scenic River is documented in a separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No*



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

*Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form.*

In accordance with PA Act 120 Section 2002 requirements, briefly summarize the impacts to other Section 2002 areas of concern that would occur if the use of the public park, recreation area, or wildlife or waterfowl refuge was avoided. Other Section 2002 areas of concern to be discussed could include the following:

(1) residential and neighborhood character and location, (2) conservation including air, erosion, sedimentation, wildlife and general ecology of area, (3) noise, and air and water pollution, (4) multiple use of space, (5) replacement housing, (6) displacement of families and business, (7) aesthetics, (8) public health and safety, (9) fast, safe and efficient transportation, (10) civil defenses, (11) economic activity, (12) employment, (13) fire protection, (14) public utilities, (15) religious institutions, (16) conduct and financing of government including the effect on the local tax base and social service costs, (17) property values, (18) education, including the disruption of school district operations, (19) engineering, right-of-way and construction costs of the project and related facilities, (20) maintenance and operating costs of the project and related facilities, and (21) operation and use of existing transportation routes and programs during construction and after completion.

The project will replace the I-80 Section 08B, Lehigh River Bridges, which span over the Lehigh River and Lehigh Gorge State Park and Trail. To meet the project's purpose and need, there is no alternative that would avoid the use of the Lehigh Gorge State Park. The No-Build Alternative would impact public health and safety as the existing I-80 dual bridges are more than 50 years old and are approaching the end of their service life. Vehicular traffic and interstate commerce would be impacted as further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

**Ecological Services Evaluation**

As part of DCNR's review under the Environmental Rights Amendment, the following mitigation addresses ecological and other impacts associated with the Lehigh Gorge State Park (LGSP) and Trail:

- The I-80 Lehigh River Bridges are nearing the end of their serviceable life. This means the bridges will need ever increasing inspections and repairs. These repairs have the potential to interrupt access to the park and to the boat launch area under the bridges. If the condition of the bridges continues to deteriorate, they may need to be weight posted, or at some point be closed. Much of the tourist traffic headed to LGSP uses I-80 to get to the park, making replacement of the bridges important not just for interstate commerce, but also for tourism and park access.





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- PennDOT is responsible for providing safe and reliable facilities for the traveling public. The replacement of the I-80 Lehigh River Bridges is needed to maintain a safe and reliable crossing of the Lehigh River not only for those traveling on the interstate, but also for those biking, hiking, rafting, kayaking and fishing in the vicinity of the bridge within LGSP, the Lehigh Gorge Trail, and the Lehigh River Water Trail.
- Replacement land is being provided for the Section 6(f) conversion (Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easement) within LGSP totaling approximately 6 acres. This replacement land also serves as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon project completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during final design.
- The area within proposed aerial easements adjacent to the bridges will be available for use by LGSP after construction.
- The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridge will be temporarily closed). Efforts will be made to minimize effects to the park and trail throughout construction.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- An Aids to Navigation Plan will be implemented and will include advanced signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**Include any additional information related to the park impact that is relevant to the determination of *de minimis*/no adverse use:**

[Click here to enter text.](#)

**SUMMARY AND DETERMINATION:**

The project involves a *de minimis*/no adverse use on the Section 4(f)/Section 2002 property as evidenced through the minimization of harm to a public park, recreation land, or wildlife and waterfowl refuge as a result of mitigation to or avoidance of impacts to the qualifying characteristics and/or the functions of the resource. Based on the scope of the undertaking; the fact that the undertaking does not adversely affect the function/qualities of the Section 4(f)/Section 2002 property on a permanent or temporary basis; and with agreement from the official with jurisdiction, the proposed action constitutes a *de minimis*/no adverse use; and therefore, no analysis of avoidance alternatives is required.

Name and Organization of Preparer: Kathleen Krommes, HDR

Date: 8/15/2022

Project Manager: *Brandy L Rotz*

Date: 08/18/2022

Environmental Manager: *[Signature]*

Date: 8/18/2022

PennDOT, BOPD: Nina Ertel

Digitally signed by Nina Ertel  
Date: 2022.08.18 11:22:29 -04'00'

Date:

FHWA: CAMILLE A OTTO

Digitally signed by CAMILLE A OTTO  
Date: 2022.08.18 12:59:10 -04'00'

Date:

**LIST OF ATTACHMENTS**

Attachment A: Figures

Figure 1 – Project Location Map

Figure 2 – Section 4(f) / 6(f) Location Map

Figure 3 – Temporary Closures to Lehigh Gorge State Park

Attachment B: Photographs

Attachment C: Public Outreach

Public Notice Flyer, Temporary Closure to Lehigh Gorge State Park

Newspaper Advertisement Proof of Publication

Comments Received

Attachment D: Concurrence Letters

Section 4(f) *De Minimis* Concurrence Letter, signed 8/17/2022

DCNR Section 6(f) Concurrence Letter, dated 2/22/2022

NPS Section 6(f) Concurrence Emails, dated 6/15/2022 and 11/22/2021





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**List Section 4(f) mitigation measures associated with this use that are part of this project:**

The project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridges will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within Proposed Aerial Easements adjacent to the bridges will be available for use by LGSP after construction.

Mitigation measures will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

PennDOT will provide replacement land for Section 6(f) conversion (Proposed Slope Easement, Proposed Aerial Easements, and TCE) within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area. Concurrence letters from DCNR and NPS are in Attachment D.

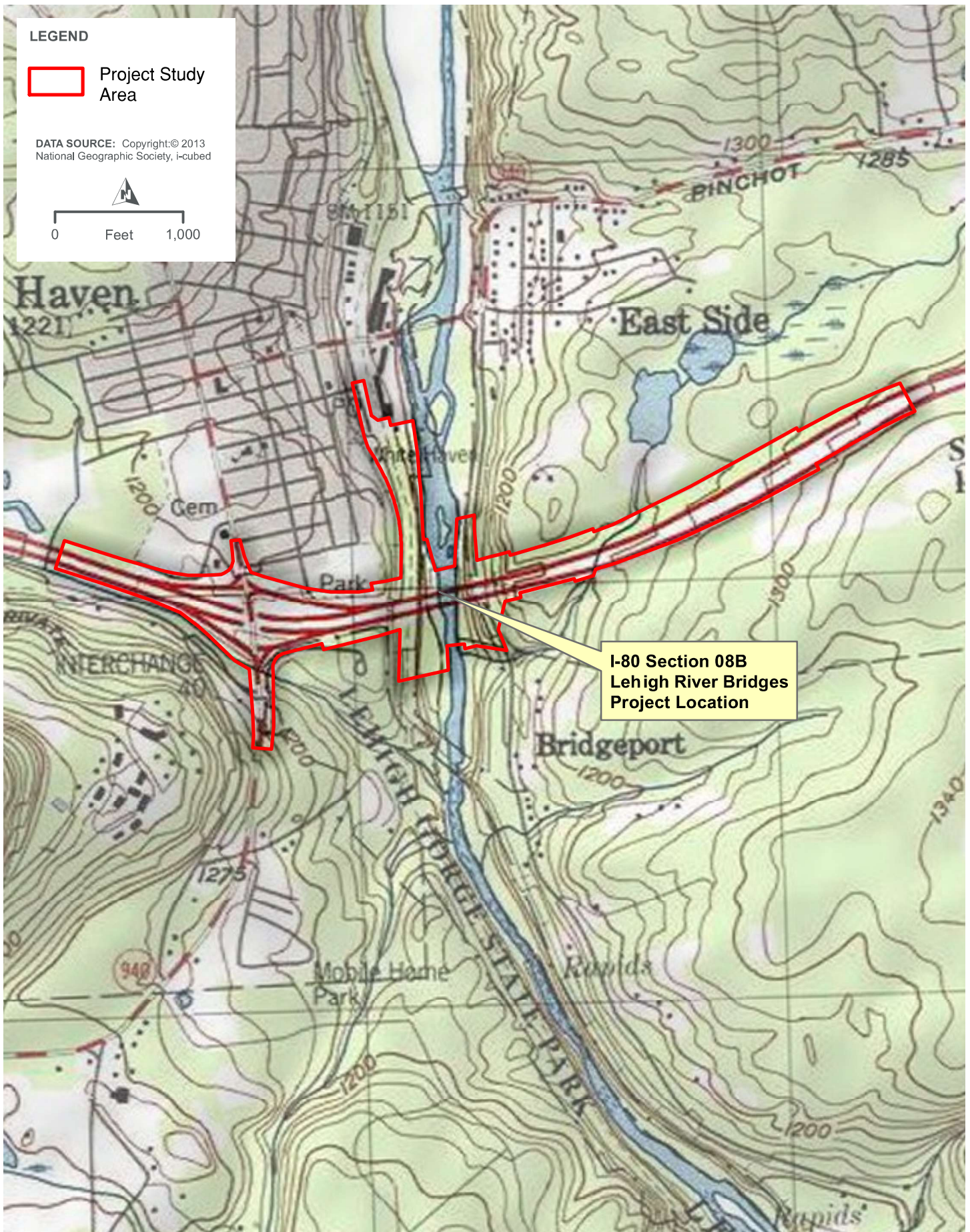
**Typical attachments for this form include, but are not limited to:**

- Project location map
- Map of affected Section 4(f) property and other Section 4(f) property(ies) in the project vicinity
- Photographs of the Section 4(f) property
- Project plan sheet to show impacts
- Correspondence with the official with jurisdiction
- Public involvement information

# **Attachment A**

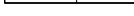
## **Figures**





**I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP**

**FIGURE 1**



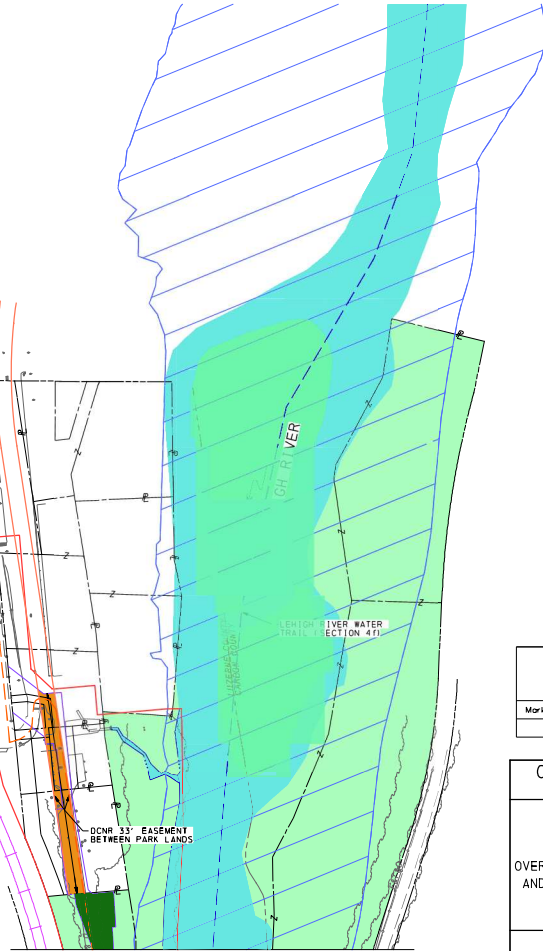


LEHIGH RIVER  
FLOODPLAIN  
WETLANDS  
STREAM AREA  
LEHIGH GORGE STATE PARK  
LEHIGH PARK  
FLOODPLAIN  
TEMPORARY CONSTRUCTION EASEMENT AREA  
DCNR EASEMENT  
PROPOSED SLOPE EASEMENT AREA  
PROPOSED AERIAL EASEMENT AREA  
STREAM LINES  
WETLANDS  
LEHIGH RIVER WATER TRAIL  
LEHIGH GORGE TRAIL  
NATIONAL HERITAGE CORRIDOR  
READING BLUE MOUNTAIN AND NORTHERN RAILROAD  
PROPOSED R/W LINE  
TEMPORARY CONSTRUCTION EASEMENT  
PROJECT STUDY AREA  
DCNR EASEMENT BETWEEN PARK LANDS

- LEGEND:**
- LEHIGH GORGE STATE PARK (SECTION 4f/6f)
  - LEHIGH PARK (SECTION 4f)
  - FLOODPLAIN
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 4.91 ac)
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN DCNR EASEMENT (SECTION 4f AREA = 0.27 ac)
  - PROPOSED SLOPE EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.42 ac)
  - PROPOSED AERIAL EASEMENT WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.51 ac)
  - STREAM AREA
  - STREAM LINES
  - WETLANDS
  - LEHIGH RIVER WATER TRAIL (SECTION 4f)
  - LEHIGH GORGE TRAIL (SECTION 4f/6f) AND DAL NATIONAL HERITAGE CORRIDOR (SECTION 4f)
  - READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)
  - PROPOSED R/W LINE
  - TEMPORARY CONSTRUCTION EASEMENT
  - PROJECT STUDY AREA
  - DCNR EASEMENT BETWEEN PARK LANDS

READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)

DAL NATIONAL HERITAGE CORRIDOR



SEE SHEET 1 OF 2

50 0 50 100 FEET



Mark	Description	By	Chk'd.	Rec'd.	Date
REVISIONS					

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

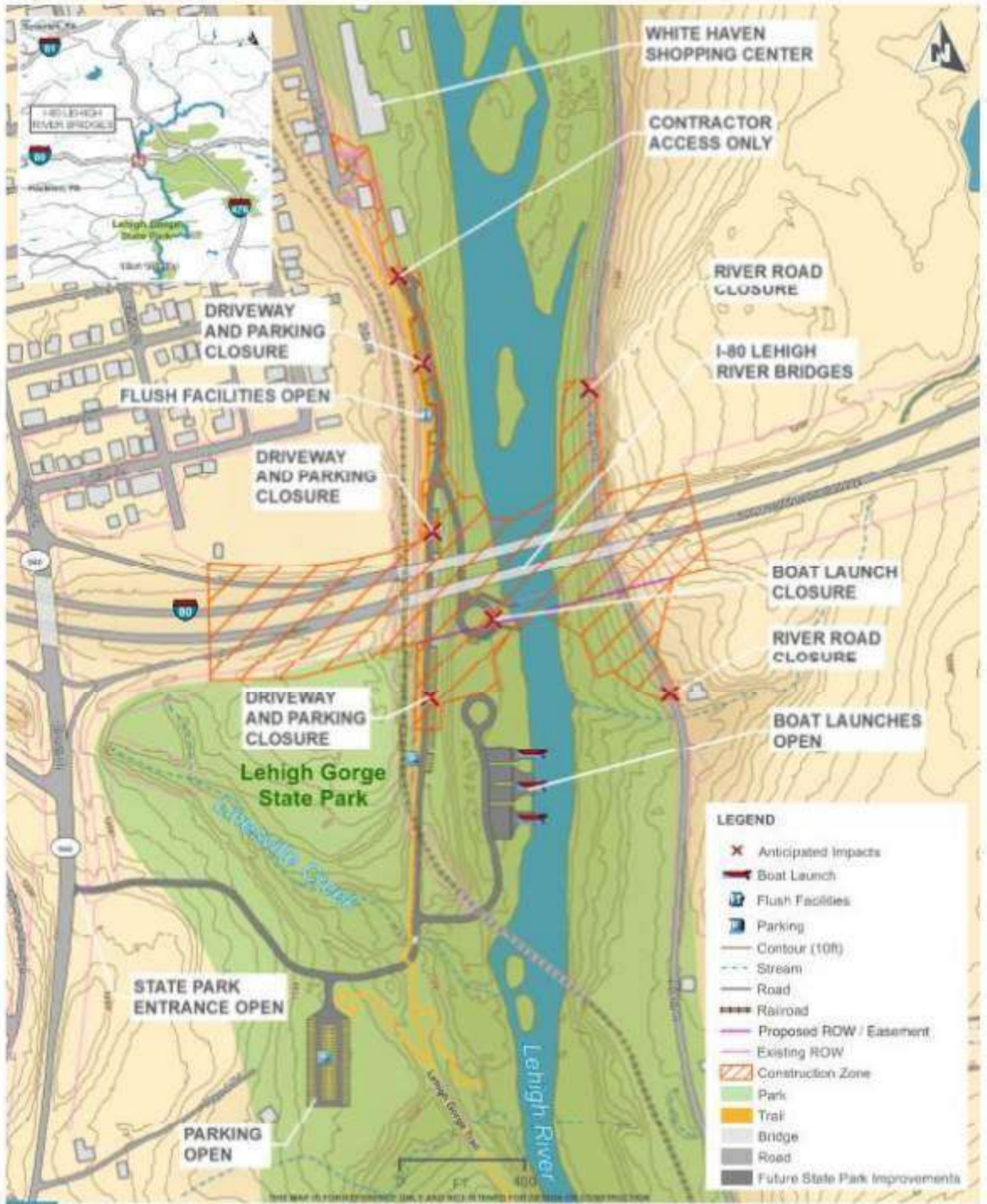
LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B  
SEC. 2728 OFF. 0000

OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
SECTION 4(f)/6(f) LOCATION MAP

**FIGURE 2**

SHEET 2 OF 2

S-XXXX



**I-80 LEHIGH RIVER BRIDGE PROJECT**  
**TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK**  
 WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP  
 LUZERNE & CARBON COUNTIES, PA

I-80, SECTION 09B, LEHIGH RIVER BRIDGES PROJECT

**FIGURE 3**



**Attachment B**  
**Photographs**

## Photographs



Lehigh Gorge State Park White Haven Access, facing south



Lehigh Gorge Trail and driveway, facing south





Lehigh Gorge Trail and driveway to existing boat launch, facing south



Boat launch area under I-80 Lehigh River Bridge on west shore of the Lehigh River, facing east





Underneath the I-80 Lehigh River Bridges on west shore of the Lehigh River, facing east



River gauge and raft requirements posted on the eastbound I-80 Lehigh River bridge pier



West shore of Lehigh River in Lehigh Gorge State Park, facing upstream (north)



West shore of Lehigh River in Lehigh Gorge State Park, facing downstream (south)





View of I-80 Lehigh River Bridges and Lehigh River, facing southeast



Lehigh Gorge Trail and driveway, facing north



**Attachment C**  
**Public Outreach**



Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP – Consultant Project Manager  
c-brotz@pa.gov

**LEGAL NOTICE**

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK,  
ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

**ADVERTISEMENT FOR BIDS****SEALED BID**

Contractor Name

Contractor Address

Re: Lawn Mowing and Landscape Maintenance Borough of White Haven

Attn: Linda Szoke, Borough Manager

312 Main Street

White Haven PA 18661

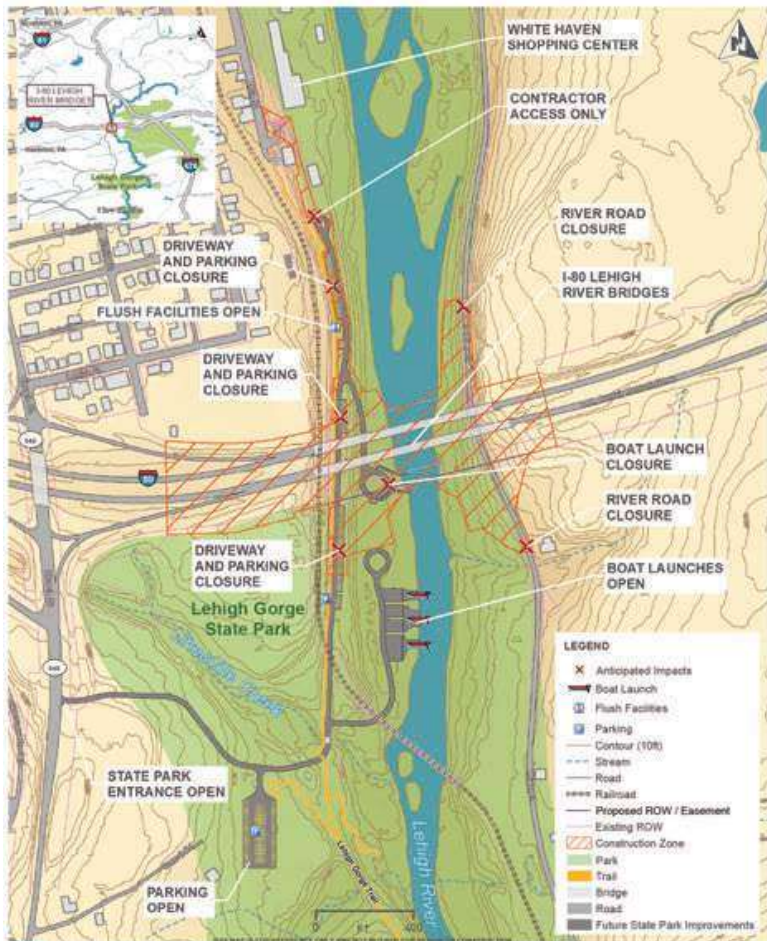
570-443-9129

The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



**I-80 LEHIGH RIVER BRIDGE PROJECT  
TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK  
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP  
LUZERNE & CARBON COUNTIES, PA**

**NOTICE OF PUBLIC INVOLVEMENT  
I-80, Section 08B, Lehigh River Bridges Project**

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

**Lehigh Gorge Trail:**

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

**Lehigh Gorge State Park driveway and parking:**

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

**Boat launches:**

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

**Lehigh River Water Trail:**

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, [c-brotz@pa.gov](mailto:c-brotz@pa.gov).

Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101

Attn: Brandy Rotz, AICP – Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)





# THE JOURNAL-HERALD

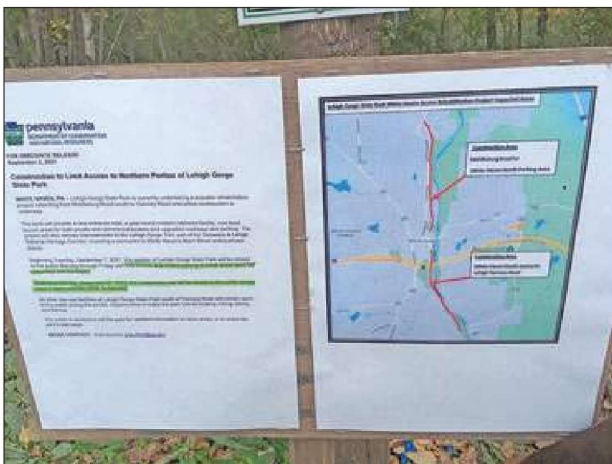
THURSDAY, OCTOBER 14, 2021 • Volume 41 – No. 7

©2021, THE JOURNAL-HERALD. All Rights Reserved

SINGLE COPY—75¢  
(USPS 277440)

CONTINUING: *THE WHITE HAVEN JOURNAL*  
ESTABLISHED 1879–142<sup>nd</sup> YEAR, NO. 46

CONTINUING: *THE WEATHERLY HERALD*  
ESTABLISHED 1880–142<sup>nd</sup> YEAR, NO. 19



**CLOSED:** Construction to the White Haven portion of Lehigh Gorge State Park will close the trail until spring. Above, a fence prevents access to the trail at Tannery; at left a posting gives details of the project. The fence was down over part of the weekend, but back up by Tuesday. Other signs around the parking area give details of the proposed Interstate 80 bridge project, also on page 3 of this issue. Comments on that plan are due by October 26. *JH: Ruth Isenberg*

## Both of Kidder's Fire Companies need new equipment

by Seth Isenberg

Some fire engines in the bays of the Albrightsville (AVFD) and Lake Harmony Volunteer Fire Companies (LHVFD) have gotten old. Through 2021, both fire companies have been working on plans to get new equipment.

Kidder Township supervisors called a public meeting on September 28 to talk to the companies about what they need, and how much the new equipment will cost.

Members of each fire company understood that in order to get something new, they had to retire two of their existing trucks. The LHVFD wants to replace its two oldest vehicles with a rescue pumper, with an estimated price tag of \$836,000 if ordered this year.

The AVFD is looking to replace both fire apparatus in the Albrightsville #2 Station with one new pumper tanker, AND buy a rescue pumper with an aerial flow tower for the Albrightsville main station, at a combined cost of just over \$1.5 million.

Placing the orders on these trucks before the end of the year will save about 3%

each — over \$70,000 total for the three.

Kidder Township has about \$1.2 million in its Emergency Services Capital Equipment Fund. It has been getting \$200,000 added from the township budget each year. There was discussion as to how much the township is allowed to spend from the fund. As the ordinance governing the fund is now written, the township can only cover about 60% of the value of the trucks. The ordinance also does not allow payments over time, which will have to be amended.

It was suggested that, given how fire company fundraisers are raising less and less, the ordinance should also be amended to allow 100% funding. While the trend was acknowledged, supervisors are more likely to boost the percentage over time, and as township finances allow.

For now, the 60% limit remains in place.

Supervisors were unanimous in approving a letter to

See KIDDER FIRE, page 7





# PUBLIC NOTICES

## LEGAL NOTICE

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK,  
ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

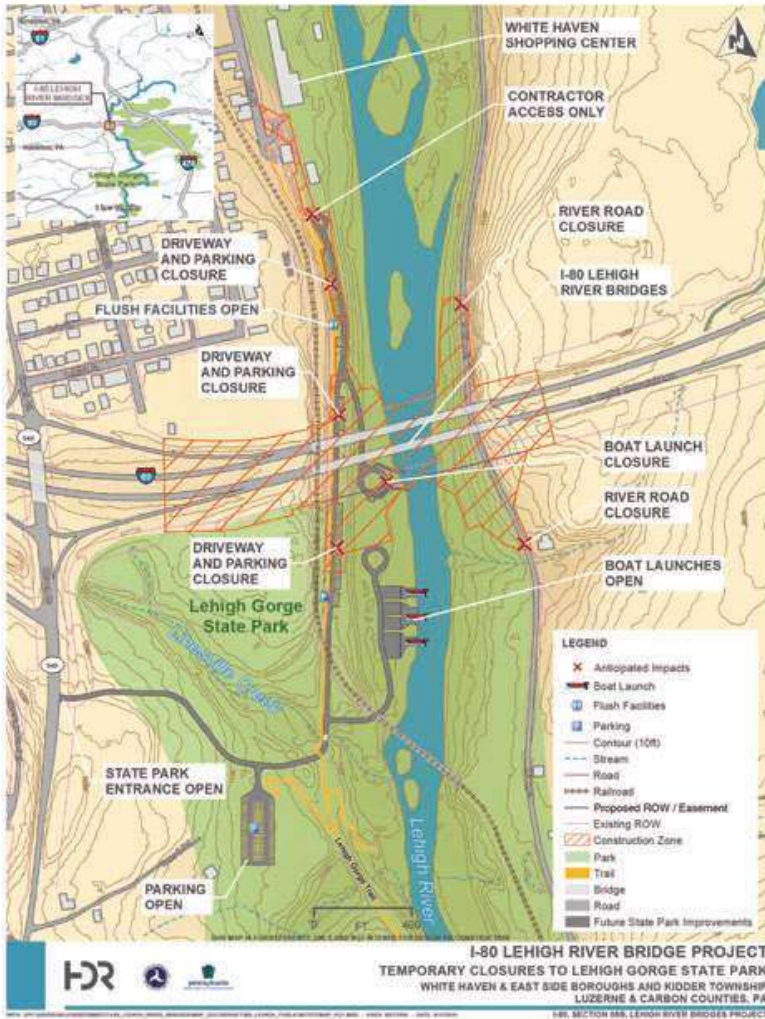
## ADVERTISEMENT FOR BIDS

SEALED BID  
Contractor Name  
Contractor Address  
Re: Lawn Mowing and Landscape Maintenance Borough of White Haven  
Attn: Linda Szoke, Borough Manager  
312 Main Street  
White Haven PA 18661  
570-443-9129  
The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



## NOTICE OF PUBLIC INVOLVEMENT I-80, Section 08B, Lehigh River Bridges Project

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

### Lehigh Gorge Trail:

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

### Lehigh Gorge State Park driveway and parking:

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

### Boat launches:

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

### Lehigh River Water Trail:

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, [c-brotz@pa.gov](mailto:c-brotz@pa.gov).

Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP — Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:08 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] Lehigh River Bridges I-80 08B Project Feedback

**From:** Cwalina Construction <cwalinaconstruction@gmail.com>  
**Sent:** Friday, October 15, 2021 2:43 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] Lehigh River Bridges I-80 08B Project Feedback

**ATTENTION:** *This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.*

Hi,

In response to your request for public feedback, I'd like you to consider noise reduction barriers on the bridge. We reside at 100 Towanda St in White Haven (directly up the path of the river to the other side of town) and the truck traffic traveling on the bridge causes significant noise to echo up the river which can be heard in the house even if the windows are closed. Additionally, a barrier might deter any people from attempting suicide off the bridge.

Is it possible to deaden sound, keep the public safe and keep the aesthetic view of the town as part of this project?

Thank you,

Valerie Love  
570-947-2690

--

Brian Cwalina  
Cwalina Construction  
570-606-6016  
[cwalinaconstruction@gmail.com](mailto:cwalinaconstruction@gmail.com)



**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge - Public Comment

---

**From:** Barbara Murphy <bmurphy@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 2:34 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] White Haven bridge

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Consultant Project Manager  
Brandy Rotz,

As a concerned citizen/neighbor of the Borough of White Haven we would like to know if the proposed new bridge (east and west bound route 80) will be built in the exact same area as the existing bridge? If so how will this be accomplished? For instance: lane closures , new on and off ramps which could possibly include taking land from our Lehigh Ballpark. How will the construction impact our local neighborhood? Please respond to my email [bmurphy@pa.metro cast.net](mailto:bmurphy@pa.metrocast.net) or

[patmurf81@gmail.com](mailto:patmurf81@gmail.com).

Thank you ,

Patrick and Barbara Murphy  
909 Elmira St.  
White Haven, PA 18661

Sent from my iPhone

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge project

---

**From:** Seth Isenberg <journalseth@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 11:21 PM  
**To:** Rotz, Brandy  
**Cc:** Seth Isenberg  
**Subject:** [External] White Haven bridge project

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Brandy Rotz  
Consultant Project Manager

October 26, 2021

Four years of closure of the Lehigh Gorge Heritage Corridor trail could be very damaging to the economy of White Haven borough. Even the temporary closure of the trail this year has had a measurable impact on plaza retailers this month - as trail traffic is just not around even during the foliage season.

Looking at the plans, White Haven's downtown will be cut off from the south trail for an amazing four years. This year alone, local businesspeople were working with the Heritage Corridor to enhance the trail experience. We were looking to establish a "trail tenders" group of volunteers as ambassadors to visitors who start their trip from here, or would be passing through.

As a Main Street business ourselves, The Journal-Herald was hoping to see continued reviving of our downtown. Slowly, we have seen stores open, and recently, there is a prospect of a new building on Main Street. Losing the tourists is going to hurt. We are going to need some help from PennDOT, including highway signs indicting that we have a downtown business district, and signs on the highway for the remaining downtown trailhead.

Is it also possible for help to route bikers and hikers through town via the new driveway? Trail travelers may take the detour as an adventure.

I will add my concerns about any traffic that comes in heavy onto Route 940 that clogs up White Haven's roads. When there is an accident and the highway is closed, the simple task of pulling out from Main Street can become a ten to 15 minute wait. Through vehicles will not yield. Going to the grocery store or drug store becomes a fight with trucks and cars stuck on our little state road. We also have foot traffic, mainly young people, and I am very concerned about their safety when Route 940 becomes the only path for Interstate-using vehicles.

If an accident with a detour happens in bad weather, vehicles get stuck on the big hill on Route 940.

So, as a wrap up -

I am asking for PennDOT to provide signs to aid the downtown commercial area, and signs to promote the northern trailhead in the downtown to help draw visitors.

I am also asking for help to improve the local roads to handle the surges in traffic, and an answer to the safety problem within White Haven when bumper to bumper traffic does appear.

Seth Isenberg

General manager

The Journal-Herald of White Haven

LAKE NEWS of Lake Harmony

Journal of the Pocono Plateau of Blakeslee and Tobyhanna

And

Journal of Penn-Kidder of Albrightsville

211 Main Street, White Haven, PA 18661

570 215-0204



Comment Date	Comment Origin	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Association
12/16/2021	web comment	<p>The Delaware and Lehigh National Heritage Corridor (D&amp;L Trail) needs to be kept in the chain of communication for this project along with the State Park. The D&amp;L was noted in some locations of this public meeting post, but not consistently. The trail within this project scope is part of a currently 145-mile network, soon to be 160+ miles- any notice regarding the disruption of trail activities must be shared with the D&amp;L so the trail users can be accurately updated. Please add the D&amp;L to your list of organizations to keep informed (this webpage noted DCNR and White Haven).</p> <p>For the timelines regarding trail closures- Please clarify the YEAR of these anticipated impacts to the D&amp;L Trail- is the schedule outlined for the full 4 years of the project or just a portion? Are the April to October overnight closures M-F or 7 days a week? When is "night" considered to begin- will this line up with the State Park's hours of operation? Is the November to March closure M-F or 7 days a week?</p> <p>Can you expand on the "replacement" land PennDOT will provide? Does the bridge permanently impact the current alignment of the D&amp;L Trail in any way and is PennDOT considering "replacing" any portion of the D&amp;L Trail if the alignment is affected?</p> <p>When the capacity analysis was conducted did it account for pedestrian and bicycle traffic utilizing the D&amp;L Trail along Main Street? White Haven Borough is exploring intersection improvements for pedestrians (RRFB's and crosswalk repainting like you mentioned)- how might this new "stop point" affect traffic congestion and does this need to be considered when evaluating the traffic controls on adjacent intersections?</p> <p>Please note when you conduct your before/after intersection study that the D&amp;L Trail is currently closed during the State Park improvements project and any trail user counts will not be accurate until the project is complete.</p>	neutral	Information Request, Trails/Recreation/Parks		Elizabeth	Rosencrans	Delaware and Lehigh National Heritage Corridor (D&L Trail)	trail@delawareandlehigh.org	6109233548227	I-80 White Haven

**Attachment D**

**Concurrence Letters**



August 16, 2022

Ryan Dysinger  
Assistant Director  
PA Department of Conservation and Natural Resources  
Bureau of State Parks  
PO Box 8551  
Harrisburg, PA 17105-8551

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Updated Concurrence with Section 4(f) Determination

Dear Mr. Dysinger:

In light of Act 84 of 2022, which amended the Public Private Partnership (P3) law and prohibited P3 initiatives with mandatory user fees (i.e., tolls), the Section 4(f) De Minimis form for Lehigh Gorge State Park has been updated to remove tolling facility features and associated impacts. This includes removing the 0.1 acre of right-of-way required for replacement of the SR 940 gabion wall that was part of the improvements to diversion routes associated with tolling. All other impacts remain the same as described in previous correspondence dated February 17, 2022, with your concurrence dated March 25, 2022.

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge. Due to the shift south of the existing bridge, Slope and Aerial Easements are required from Lehigh Gorge State Park property for replacement of the bridges. Additional Temporary Construction Easements are necessary for contractor access and staging. Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easements totaling approximately 6 acres are proposed to be acquired from Lehigh Gorge State Park.



Temporary impacts to State Park property and facilities are anticipated; however, the project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridge will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within the Proposed Aerial Easement adjacent to the bridges will be available for use by Lehigh Gorge State Park after construction.

As the work will take place within Lehigh Gorge State Park, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the recreational use of the property. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts to Lehigh Gorge State Park and proposed mitigation measures.

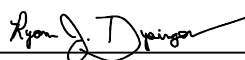
Thank you for your assistance in this matter. If you have any questions, please feel free to contact me at 717-705-1481 or via email, [johnname@pa.gov](mailto:johnname@pa.gov)

Sincerely,



Drew Ames  
Acting Chief  
Environmental Policy and Development Division

Enclosures: Lehigh Gorge State Park Draft Section 4(f) De Minimis Form

Concurrence:  Date: 8/17/2022  
Ryan J. Dysinger  
Assistant Director, Bureau of State Parks  
Department of Conservation and Natural Resources

BUREAU OF RECREATION AND CONSERVATION

February 22, 2022

Nicole L. Auker  
Environmental Planning Supervisor  
PA Department of Transportation  
400 North Street, 7<sup>th</sup> Floor  
Harrisburg, PA 17120

RE: I-80 WHITE HAVEN BRIDGE REPLACEMENT & GABION WALL REPLACEMENT  
SR 0080 & SR 0940 River Street  
Luzerne County, Carbon County  
LWCF 42-00033, 42-00081, Lehigh Gorge State Park

Dear Ms. Auker:

This letter is being written based on PennDOT's need to proceed with the environmental process for the above referenced bridge replacement project. The Federal Highway Administration requires acknowledgement of coordination between agencies to allow the project to continue through the environmental process, to begin the Right-Of-Way process, engage with the landowner for potential acquisition, and complete the National Park Service (NPS) Compliance and Stewardship Form (C&S Form).

The replacement of the I-80 bridge and Gabion wall along SR 0940 in East Side Borough will require permanent/TCE of 6.2 acres of impact to the Land and Water Conservation Fund protected Lehigh Gorge State Park. The impact area breakdown is as follows; 0.1 acre of Required Right-of-way, 0.4 acres of proposed Slope Easement, 0.5 acres of proposed Aerial Easement, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement.

The proposed replacement property acreage is approximately 6.45 acres, Parcel ID (65A-51-C402) in Penn Forest Township. DCNR received NPS concurrence on the proposed conversion and replacement property on 11/22/2021. DCNR Bureau of State Parks, DCNR Bureau of Recreation and Conservation, and PennDOT agree on the proposed replacement property to satisfy the value, location and recreational usefulness criteria under the Land and Water Conservation Fund Act as mitigation for both the I-80 bridge replacement and the SR 0940 gabion wall replacement.

DCNR recognizes PennDOT's need to proceed through the project planning and design phases and agrees with the proposed conversion footprint and replacement property while we await NPS concurrence. Please understand NPS has ultimate approval authority of the conversion package including replacement property.

If you have questions or concerns, please contact me at (717) 783-0318 or Ashley Rebert at (717) 772-3322.

Sincerely,

*Emma Harrison*

Emma Harrison  
Land Conversion Specialist  
Bureau of Recreation and Conservation

cc:, Ashley Rebert



**From:** Rebert, Ashley <arebert@pa.gov>  
**Sent:** Wednesday, June 15, 2022 1:50 PM  
**To:** Auker, Nicole <nauker@pa.gov>  
**Subject:** RE: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

I forwarded Missy's email to Mike this morning, but he must be on vacation until Monday. So glad we were able to get a quick turnaround on this!

Ashley Rebert  
Chief, Land Conservation and Stewardship  
DCNR Bureau of Recreation and Conservation  
717-772-3322

**From:** Auker, Nicole <[nauker@pa.gov](mailto:nauker@pa.gov)>  
**Sent:** Wednesday, June 15, 2022 1:45 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>; Brown, Corey <[corebrown@pa.gov](mailto:corebrown@pa.gov)>  
**Subject:** RE: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

This is wonderful news!

Thanks for sending this back so quickly. I will let Mike know and our staff here so that we can move forward with the purchase.

**Nicole L. Auker** | Environmental Planning Supervisor  
Environmental Policy and Development Division  
Bureau of Design and Delivery  
PA Department of Transportation  
400 North Street - 7th Floor | Harrisburg, PA 17120  
Phone: 717.787.0460 | Fax: 717.772.0834  
[www.dot.state.pa.us](http://www.dot.state.pa.us)

**From:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Sent:** Wednesday, June 15, 2022 1:40 PM  
**To:** Auker, Nicole <[nauker@pa.gov](mailto:nauker@pa.gov)>; Brown, Corey <[corebrown@pa.gov](mailto:corebrown@pa.gov)>  
**Subject:** FW: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

Good Afternoon,

Below is some good news 😊!!!

Ashley Rebert  
Chief, Land Conservation and Stewardship  
DCNR Bureau of Recreation and Conservation  
717-772-3322

**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Tuesday, June 14, 2022 4:00 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Subject:** Re: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

Thanks Ashley, I'm glad we were able to catch up. Below is NPS concurrence that this replacement property is suitable and complies with LWCF regulations.

-----

PA DCNR and PennDOT would like to purchase replacement land to satisfy a pending conversion at a PA State Park. Concurrence from NPS is necessary as soon as possible because the land owner wants to sell this property right away. The property to be purchased is adjacent to the Maurice K. Goddard State Park in Mercer County, PA. This purchased land would become part of the much larger 2,856 acre, LWCF assisted, MK. Goddard State Park, which is also encumbered by the LWCF requirements.

PA DCNR would like for this replacement land serve as replacement property for multiple pending small conversions, which is allowed as per the LWCF Post Completion Regulations (36 CFR 59.3(b)(3)(iii)). PennDOT summarized the proposal:

*DCNR Bureau of State Parks presented a potential 20-acre replacement land property adjacent to MK Goddard State Park in Mercer County. This would be an amicable sale. DCNR has already completed a preliminary appraisal, the property appraised for approximately \$70,000. This property would fulfill the replacement land requirement for several PennDOT projects located in state parks over the next several years.*

This replacement property will be purchased from a private land owner, the land is currently not dedicated or managed for outdoor recreation and meets the eligibility requirements for LWCF Acquisitions.

We concur with PennDOT and the PADCNr that the proposed replacement land meets the requirements for LWCF replacement land as outlined in 36 CFR 59.3 and the LWCF Financial Assistance Manual.

An official UASFLA Appraisal and Appraisal review will be performed to determine the fair market value of this property, and is currently underway.

The PA DCNR and PennDOT have provided a list of PennDOT projects expected to trigger conversions within approximately 13 LWCF assisted parks in PA. At this time, no conversion is proposed, however, at the time an application for a conversion is submitted to NPS (expected later this year), the NPS and PA DCNR will finalize the list of small conversions and agree to the

specific conversions that will utilize the MK Goddard Bank as replacement land. Fair Market Value and Recreation Utility of each parcel will be reviewed at this time to determine if the replacement land will satisfy each proposed future conversion.

We understand that the PennDOT will move forward with the purchase of this Goddard replacement property immediately upon hearing from NPS that the land is suitable replacement land for LWCF purposes. PennDOT will then transfer the land to PA DCNR. This will take place prior to NPS approval of the conversion package. Formal review and approval of the conversion will take place once the appraisals are completed and the NPS receives the complete and final conversion application from PA DCNR.

let me know if you have any questions.

Missy

**Missy Morrison**

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557

---

**From:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>

**Sent:** Tuesday, June 14, 2022 1:54 PM

**To:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>

**Subject:** RE: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

Good Afternoon Missy,

Thank you for your email and for taking the time to speak with me! Based on our records we believe MK Goddard State Park is LWCF protected. Attached is the only piece of documentation we have for the project that funded park rehabilitation in State Park Districts II and III. Would NPS have access to this project record where we could get some additional details?

We are in the process of obtaining a yellow book appraisal and review appraisal of the replacement property at MK Goddard State Park. Once we have those documents we will sign the Appraisal Certification in the C&S Form. In addition, we will update the PennDOT spreadsheet to include recreational utility justification for the properties identified to be completed in the short term.

I look forward to hearing back from you on the timing of the PennDOT acquisition and transfer of the replacement land to DCNR. As we discussed, PennDOT will be purchasing the property from a private landowner and intends to transfer it to DCNR immediately. Please let me know if there are any concerns with this, or if certain documentation needs to be provided to NPS prior to the purchase and transfer taking place.



As always, thank you for your guidance and assistance through the compliance process.

Kind Regards,  
Ashley

Ashley Rebert  
Chief, Land Conservation and Stewardship  
DCNR Bureau of Recreation and Conservation  
717-772-3322

**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Tuesday, June 14, 2022 11:36 AM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Subject:** Re: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

Good Morning Ashley  
has the appraisal review been completed? you'll want to get that done asap, and also provide us with the appraisal certification in the C&S Form.

The NPS can concur that this land qualifies as adequate replacement land.

We can talk more later today but I'm thinking we can set up a bank for all 13 properties, but we may need at least a little more information about the projects in the next 5 years - for instance the acreage if possible. Also we will want to understand what the "sliver takes" consist of, in other words, what's the land being used for specifically? I know it will prob seem redundant, but a description of the actual footprint be good so we can establish recreation utility.

Is the MKGSP an existing LWCF park?

looking forward to talking later  
Missy

**Missy Morrison**  
State and Local Assistance Programs / Compliance Team Lead  
National Park Service  
1849 C Street NW  
Washington, DC 20240  
Mobile: (202) 641-6557

---

**From:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Sent:** Friday, June 10, 2022 8:52 AM

To: Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>

Subject: [EXTERNAL] NEW Excess Value Bank Eligibility Determination

**This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.**

Good morning Missy,

I hope you are doing well! If you recall our earlier conversations I shared with you an opportunity to establish another excess value bank for a number of PennDOT bridge/road right of way sliver takes within PA State Parks. Attached is a list of those Park projects planned for the next 5 years and a Memo put together by PennDOT explaining the request. In addition, PA has identified replacement property next to MK Goddard State Park (documentation attached). The property has been valued at \$70,000 for 20 acres (a yellow book appraisal is being ordered).

This request for eligibility of the replacement property is URGENT. The landowner wishes to sell this summer. PennDOT has identified the funds to purchase the property and transfer it to DCNR...this was not easy for us to coordinate and getting this far was a monumental process in itself. I've set up some time for us to chat on Monday so I can brief you on this project and address any preliminary questions you may have before starting your review of eligibility.

Thank you and I hope you have a nice weekend.

Ashley

**Ashley D. Rebert** | Chief, Land Conservation & Stewardship Section  
Department of Conservation and Natural Resources  
Bureau of Recreation and Conservation  
Community Parks and Conservation Division  
Phone: 717.772.3322  
Email: [arebert@pa.gov](mailto:arebert@pa.gov)

## Krommes, Kathy

---

**From:** Krommes, Kathy  
**Sent:** Monday, March 28, 2022 3:06 PM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

---

**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Monday, November 22, 2021 3:26 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Cc:** Imgrund, Lauren <[limgrund@pa.gov](mailto:limgrund@pa.gov)>  
**Subject:** [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good Afternoon Ashley-

The NPS, LWCF program has reviewed the proposed conversion footprint and replacement property for the SR0080 Section 550 conversion at LWCF 42-00033 Walter Dick Memorial Park and the I-80 White Haven Bridge, conversion replacement property materials (Impacts Lehigh Gorge State Park, Luzerne County).

### Walter Dick Mem Park

The NPS, LWCF can concur that the proposed replacement property adjacent to Walter Dick Memorial Park satisfies the LWCF Act and Post Completion Compliance Regulations (36 CFR 59.3) for appropriate replacement property.

However, please confirm the final conversion footprint acreage: I had to go back and check through Jack's emails and a February email with attachment from you indicates the conversion footprint impact to this park is 7.6 acres; within the PennDot letter of August 31st 2021, PennDOT indicates a 5.5 acre footprint and approximately 6 acres replacement property.

An email from you with the updated/explanation of what the actual conversion footprint is meant to be will suffice. Also keep in mind that when the conversion package is submitted we will need maps indicating the correct acreage.

### Lehigh Gorge SP conversion - I-80 White Haven Bridge, Impacts Lehigh Gorge State Park, Luzerne County

The NPS LWCF also concurs with the proposed replacement property at Unionville Road for the replacement of 7 acres of land converted from Recreation use at Lehigh Gorge SP meets the LWCF Act and regulations.

I sincerely apologize for the delay in reviewing the documentation for the above projects; thank you for your patience!

If you have any additional questions, please let me know.

Missy



**Missy Morrison**

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**SELECT ONE:**   ☐ EIS                      ☐ EA                      ☒ CE                      ☐ EER                      ☐ ED

**PROJECT DESCRIPTION:**

(Provide a concise but thorough description of the proposed action.)

The proposed project consists of the replacement of the I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The project location is shown on Figure 1.

The purpose of the project is to address the deterioration of the I-80 Lehigh River bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and residents.

The needs for this project include:

- The existing I-80 dual bridges are more than 50 years old. The fracture critical bridges show deterioration and are approaching the end of their serviceable lifespan.
- I-80 provides a critical link in the interstate commerce system, and as such is vital to the efficient movement of goods and people. Further deterioration of the I-80 Lehigh River bridges could cause the bridges to be load posted.

Originally constructed in 1965, the I-80 Eastbound and Westbound structures are 836 feet long and 757 feet long, respectively. The structures consist of steel two-girder superstructures. The substructures consist of reinforced concrete abutments and reinforced concrete piers that reach heights of over 110 feet.

I-80 in the vicinity of the project has two 12' lanes of travel in both directions with 8' left shoulders and 10' right shoulders. The median width varies from 60' to 150'. This section of I-80 carries east-west traffic across Pennsylvania's northern tier with Average Daily Traffic (ADT) of 28,774 vehicles per day (2021) with 34 percent truck traffic and is projected to grow to 53,736 vehicles per day by 2046 (design year).

The Eastbound bridge will be replaced on a new alignment immediately to the south of the existing Eastbound bridge. The new Eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Westbound bridge will be replaced on the same approximate alignment as the existing Westbound bridge. At Exit 273 (SR 940), the Eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the Westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert.

**IDENTIFICATION OF SECTION 4(f)/SECTION 2002 PROPERTY:**

(List the property and provide a description of the property as per Chapter 6 of the *Section 4(f)/Section 2002 Handbook*. Attach a map, photo(s), etc. as appropriate.)

The stretch of the Lehigh River within the project study area is considered a navigable waterway by the U.S. Coast Guard (USCG) and a Water Trail designated by Pennsylvania Fish and Boat Commission (PFBC). Water based recreation, including recreational boating, is conducted along the 72-mile-long Lehigh River Water Trail, which emphasizes low-impact use and promotes stewardship of water resources. The 41-mile Northern section of the Lehigh River Water Trail begins at the White Haven boat access underneath the I-80 Lehigh River bridges within the Lehigh Gorge State Park. Figure 2 shows the Lehigh River in the vicinity of the I-80 bridges.

The stretch of the Lehigh River within the project study area is also designated as a Pennsylvania Scenic River by Pennsylvania Department of Conservation and Natural Resources (DCNR), with the scenic designation from the Francis E. Walter Dam north of the project to Jim Thorpe to the south. This designation encompasses a 32-mile upper section of the Lehigh River and is intended to help preserve the primitive qualities, as well as the natural aesthetic values, of our rivers and adjacent land environment. Designated rivers shall be free-flowing and capable of supporting both aquatic life and water-based recreation.

Approximately 650 linear feet of the Lehigh River extend within the project study area. The water trail's White Haven boat access is located underneath the I-80 Lehigh River Bridges on the west shore of the river within Lehigh Gorge State Park. Although not published on the Lehigh River Water Trail map, there is an access point on the east shore of the river within Lehigh Gorge State Park south of the project study area. This access is used by one





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

of the whitewater outfitters permitted by DCNR to operate within the Lehigh Gorge State Park. The other permitted outfitters and the general public use the White Haven boat access on the west shore under the I-80 Lehigh River bridges.

The river is used for both recreational boating and fishing. The segment of the Lehigh River within the project study area is classified as High Quality-Cold Water Fishes and Migratory Fishes (HQ-CWF, MF) according to the PA Code, Title 25, Chapter 93. It is also designated by PFBC as both an approved trout stream and naturally reproducing trout stream. These designations would prohibit in-stream construction from February 15 to June 1 and October 1 to December 31, respectively.

Photographs of Lehigh River and boat launch area are included in Attachment B.

**FOR PARKS, IDENTIFY KEY COMPONENTS OF ANY EXISTING MANAGEMENT PLAN (if it exists):**

In 2003, Wildlands Conservancy completed the Lehigh River Watershed Conservation Management Plan, which resulted in the Lehigh River's listing on the Pennsylvania DCNR's Rivers Conservation Program in February 2005. Through this process, the Lehigh River Watershed Conservation Management Plan was developed. The purpose of the Conservation Management Plan is to: identify the cultural, natural, biological, historical, and recreational resources of the watershed; identify problems; seek solutions; and list recommendations for the preservation, protection, and enhancement of the Lehigh River and its watershed. The Conservation Management Plan was updated in 2018 and provided the opportunity to compile the conservation accomplishments the watershed community has achieved in the 15 years since the original Conservation Management Plan was developed.

The PFBC developed the Lehigh River Fisheries Management Plan in 2007, which was designed to guide future fisheries management actions, including environmental protection efforts, of the PFBC, based on biological, environmental, and social data. The goal of this fisheries management plan is to protect, conserve, and where possible, enhance the fishery of the Lehigh River.

**OFFICIAL WITH JURISDICTION OVER SECTION 4(f)/SECTION 2002 PROPERTY:**

1. Identify agency with jurisdiction:  
 Pennsylvania Fish and Boat Commission (PFBC) for Water Trail; Pennsylvania Department of Conservation and Natural Resources (DCNR) for PA Scenic River System



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

2. Name and title of contact person at agency:

Paul Fawcett, Chief, Waterways and Marina Management Branch, PFBC

Kelly Rossiter, AICP, Rivers Program Specialist, DCNR

**APPLICABILITY DETERMINATION:**

1. Provide the total acreage of the property: The Lehigh River Water Trail is 72 miles long from White Haven to the river's confluence with the Delaware River in Easton. Approximately 650 linear feet of the Lehigh River extend within the project study area.

Describe the use of land from the property (identify amount of the property to be used, including temporary and permanent acquisition):

The anticipated layout of the replacement I-80 Lehigh River Bridges shown on Figure 3 would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). The final determination of the pier locations will be dependent on the contractor's selection of bridge structure type; however, no piers will be allowed to be placed in the middle portion of the river shown on Figure 4. By restricting the contractor from placing piers in the center of the river, the permanent pier footprint in the river is expected to be less than or equal to existing. Conceptual structure plans are included in Attachment C.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years. Attachment C includes the preliminary Aids to Navigation (ATON) Plan, which shows the proposed causeways and staging

The existing boat launch for the Lehigh River Water Trail is underneath the existing I-80 eastbound bridge within an existing aerial easement in Lehigh Gorge State Park. DCNR has initiated a capital improvements project in Lehigh Gorge State Park (LGSP) that started in 2021 and is expected to be completed in 2022, prior to PennDOT's bridge replacement project. DCNR's project will construct a new LGSP entrance from SR 940 south of the I-80



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

bridges. In addition to the new entrance, improvements will include four boat launches, parking lots, and flush facilities. DCNR intends for all park vehicular traffic, including commercial boating outfitters, to utilize the new park entrance from SR 940 and the new boat launches. Anticipated temporary impacts to park property and facilities, including the future facilities, are shown on Figure 5. Temporary impacts specific to the Lehigh River and access include:

**Lehigh River:**

- Temporary causeways will extend from both riverbanks but will not extend completely across the river.
- Boat traffic through the bridge construction site will be maintained for most of construction. Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- An Aids to Navigation (ATON) Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.

**Boat launches:**

- The boat launch directly under the bridges will be closed during construction. The planned improvements in this area, including upgraded boat launch, turnaround and handicap parking will be disturbed and will be reconstructed at the end of the project. These facilities may need to be reconfigured depending upon the chosen bridge pier location. Any such reconfiguration will be coordinated with DCNR.
- The three future boat launches to the south (downstream) of the bridges will not be impacted and will remain open during construction. These boat launches can be used for commercial outfitters and general public during construction.

A preliminary ATON plan was prepared and submitted to PFBC for review. PFBC approved the ATON plan on August 16, 2021. The ATON plan and approval letter is provided in Attachment C. Exclusion buoys are to be placed along the edges of the causeways to indicate area for safe passage of river traffic. Signs will be placed upstream and downstream of the bridges to warn boaters of the bridge construction site. If the ATON plan is revised by the contractor, additional coordination with PFBC is required.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

2. The project **does not** adversely affect the activities, features, and attributes of the resource that qualify it for protection under Section 4(f) or Section 2002. **(If this statement cannot be verified as true, *de minimis*/no adverse use does not apply.)** ☒ YES

Describe the effect to the qualities, activities, features, or attributes of the resource that qualify it for protection. Include a description of any mitigation included when making the determination regarding effects to the resource:

The project will not adversely affect the Lehigh River Water Trail/PA Scenic River. The river will remain open for recreational boating through the bridge construction site during most of the bridge construction (except for temporary closures noted below), and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the bridges (the existing boat launch under the bridge will be temporarily closed). The contractor will restore disturbed areas, and full access to the park amenities, including the boat launches, will be provided upon completion.

Mitigation measures will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.

In addition, due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river, as shown on Figure 4.





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
  - Where possible, riprap will be covered with soil and vegetation.
  - Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.
3. The public was afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the resource. ☒ YES

Identify the opportunity(ies) for public comment and describe the input received (provide attachments as appropriate to document the public involvement activity):

A public notice was prepared and included a map of LGSP amenities and a written notice describing the anticipated impacts to LGSP and the Lehigh River Water Trail (Attachment D). The notice requested public input and provided an email and physical address for comments. On September 24, 2021, laminated copies of the public notice were delivered to DCNR for posting in LGSP and nearby Hickory Run State Park. On the same day, the public notice was emailed to White Haven Borough, East Side Borough, Kidder Township, D&L National Heritage Corridor, and the whitewater and bicycle outfitters that operate in the park. Additionally, the public notice was handed out to local businesses in White Haven Borough. Upon the suggestion of LGSP Manager, Rex Bradish, a newspaper advertisement was published in the local newspaper, the Journal-Herald. Proof of publication in the October 7, 2021, and October 14, 2021 issues of the Journal-Herald are included in Attachment D.

The public comment period began September 24, 2021 and ended October 26, 2021. Three comments were received during this 30-day comment period. Only one comment was related to Section 4(f), LGSP, and the potential impacts to park amenities and visitors. The comment from a local resident and business owner included concerns of negative affects to White Haven Borough’s local economy due to a four-year closure of the Lehigh Gorge Trail and requested highway signs to the downtown business district. Comments are included in Attachment D.

LGSP’s Manager has reported that this comment and request for signs was voiced at a prior public meeting conducted for DCNR’s LGSP improvement project, which moves the state park main entrance from its current location in downtown White Haven to SR 940 south of



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

I-80. PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

In addition to the public outreach conducted September 24, 2021 to October 26, 2021, one written comment related to the Lehigh Gorge Trail was received from D&L National Heritage Corridor during the NEPA public meeting and comment period November 17, 2021 to December 17, 2021. PennDOT will continue coordination with D&L National Heritage Corridor as the project progresses. The full comment is included in Attachment D.

4. The official with jurisdiction over the property was informed of FHWA's and/or PennDOT's intent to make a *de minimis*/no adverse use finding. ☒ YES

Identify the method used to notify the official with jurisdiction, and attach appropriate correspondence.

Letters describing the impacts to Lehigh River Water Trail/PA Scenic River and PennDOT's intention of making a Section 4(f) De Minimis Use determination were provided to PFBC and DCNR. Mitigation measures were detailed in the letters. Signed copies of the letters are attached to this form.

Correspondence documenting notification of the official with jurisdiction is included in the following Attachment: PFBC Section 4(f) De Minimis Agreement Letter dated 2/25/2022; DCNR Section 4(f) De Minimis Agreement Letter dated 2/28/2022

5. The official with jurisdiction over the property concurred in writing with FHWA's and/or PennDOT's determination that the project will not adversely affect the property. (NOTE: Public input must be received and considered prior to the official with jurisdiction making a final determination.) ☒ YES

Identify the official with jurisdiction and date of concurrence and attach written concurrence:

Paul Fawcett, Chief, Waterways and Marina Management Branch, PFBC  
Kelly Rossiter, AICP, Rivers Program Specialist, DCNR

Written concurrence from the official with jurisdiction is included in the following Attachment: PFBC Section 4(f) De Minimis Agreement Letter dated 2/25/2022; DCNR Section 4(f) De Minimis Agreement Letter dated 2/28/2022



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

6. If the Section 4(f)/Section 2002 use involves State Game Land, verify that the use is considered *de minimis* in accordance with the *Cooperative Interagency Agreement for Interdepartmental Land Transfer of State Game Lands*. (Describe and obtain PA Game Commission concurrence signature below for use of a State Game Land Bank and/or Interdepartmental Land Transfer.) ☐ YES

☐ **State Game Land Bank**

Debiting [Click here to enter text.](#) (acres)

From [Click here to enter text.](#) SGL bank

PGC Signature:

Date: [Click here to enter a date.](#)

☐ **Interdepartmental Land Transfer**

PGC Signature:

Date: [Click here to enter a date.](#)

7. Have Federal or State funds [LWCF 6(f)/Project 70/Project 500/other recreation grants] been used in the acquisition of, or for any improvements to, the Section 4(f) property? ☒ YES  
☐ NO

If Yes, the appropriate Federal agency has been coordinated with and is in agreement with the land conversion or transfer. ☒ YES

**Provide more information regarding the Section 6(f)/Project 70/Project 500/other recreation grants coordination:**

Construction of the project will involve construction activities around, over and in the Lehigh Gorge State Park. DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for the state park. The entire state park is afforded protection under Section 6(f). The Section 4(f) Evaluation for the Lehigh Gorge State Park is documented in a separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form*.

8. The project does not involve any uses that would require an individual Section 4(f) evaluation. (It is acceptable if there are other Section 4(f) uses that are covered by one of the nationwide programmatic Section 4(f) evaluations or meet temporary occupancy criteria.) ☒ YES



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

If there are other Section 4(f) properties used, list them here, briefly describe the use, and identify which form(s) will be completed to address the use:

Construction of the project will involve construction activities around, over and in the Lehigh Gorge State Park. The Section 4(f) Evaluation for the Lehigh Gorge State Park is documented in a separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form*.

In accordance with PA Act 120 Section 2002 requirements, briefly summarize the impacts to other Section 2002 areas of concern that would occur if the use of the public park, recreation area, or wildlife or waterfowl refuge was avoided. Other Section 2002 areas of concern to be discussed could include the following:

(1) residential and neighborhood character and location, (2) conservation including air, erosion, sedimentation, wildlife and general ecology of area, (3) noise, and air and water pollution, (4) multiple use of space, (5) replacement housing, (6) displacement of families and business, (7) aesthetics, (8) public health and safety, (9) fast, safe and efficient transportation, (10) civil defenses, (11) economic activity, (12) employment, (13) fire protection, (14) public utilities, (15) religious institutions, (16) conduct and financing of government including the effect on the local tax base and social service costs, (17) property values, (18) education, including the disruption of school district operations, (19) engineering, right-of-way and construction costs of the project and related facilities, (20) maintenance and operating costs of the project and related facilities, and (21) operation and use of existing transportation routes and programs during construction and after completion.

The project will replace the I-80 Section 08B, Lehigh River Bridges, which span over the Lehigh River Water Trail and PA Scenic River. To meet the project's purpose and need, there is no alternative that would avoid the use of the Lehigh River Water Trail and PA Scenic River. The No-Build Alternative would impact public health and safety as the existing I-80 dual bridges are more than 50 years old and are approaching the end of their service life. Vehicular traffic and interstate commerce would be impacted as further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

#### Ecological Services Evaluation

As part of DCNR's review under the Environmental Rights Amendment, the following mitigation addresses ecological and other impacts associated with the Lehigh River Water Trail and PA Scenic River:

- The I-80 Lehigh River Bridges are nearing the end of their serviceable life. This means the bridges will need ever increasing inspections and repairs.





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

These repairs have the potential to interrupt access to the park and to the boat launch area under the bridges. If the condition of the bridges continues to deteriorate, they may need to be weight posted, or at some point be closed. Much of the tourist traffic headed to LGSP and this stretch of the Lehigh Water Trail/PA Scenic River uses I-80 to get to the park, making replacement of the bridges important not just for interstate commerce, but also for tourism and park and river access.

- PennDOT is responsible for providing safe and reliable facilities for the traveling public. The replacement of the I-80 Lehigh River Bridges is needed to maintain a safe and reliable crossing of the Lehigh River not only for those traveling on the interstate, but also for those biking, hiking, rafting, kayaking and fishing in the vicinity of the bridge within LGSP, the Lehigh Gorge Trail, and the Lehigh River Water Trail/PA Scenic River.
- An Aids to Navigation Plan will be implemented and will include advanced signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:
  - Vegetative buffers will be provided to the extent practicable.
  - Piers will not be allowed in the middle portion of the river, as shown on Figure 4.
  - Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
  - Where possible, riprap will be covered with soil and vegetation.
  - Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.
- Replacement land is being provided for the Section 6(f) conversion (Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easement) within LGSP totaling approximately 6 acres. This replacement land also serves as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- The contractor will restore disturbed areas, and full access to the park amenities, including boat launches, will be provided upon project completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during final design.
- The area within proposed aerial easements adjacent to the bridges will be available for use by LGSP after construction.
- The majority of the state park will remain open during bridge construction, and access to the Lehigh River Water Trail/PA Scenic River will be maintained via the three new boat launches currently being constructed by DCNR south of the bridges (the existing boat launch under the bridge will be temporarily closed). Efforts will be made to minimize effects to the park and Lehigh River Water Trail/PA Scenic River throughout construction.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**Include any additional information related to the park impact that is relevant to the determination of *de minimis*/no adverse use:**

[Click here to enter text.](#)

**SUMMARY AND DETERMINATION:**

The project involves a *de minimis*/no adverse use on the Section 4(f)/Section 2002 property as evidenced through the minimization of harm to a public park, recreation land, or wildlife and waterfowl refuge as a result of mitigation to or avoidance of impacts to the qualifying characteristics and/or the functions of the resource. Based on the scope of the undertaking; the fact that the undertaking does not adversely affect the function/qualities of the Section 4(f)/Section 2002 property on a permanent or temporary basis; and with agreement from the official with jurisdiction, the proposed action constitutes a *de minimis*/no adverse use; and therefore, no analysis of avoidance alternatives is required.

Name and Organization of Preparer: Kathleen Krommes, HDR

Date: 8/15/2022

Project Manager: *Brandy L Rotz*

Date: 08/25/22

Environmental Manager: *C. Ertel*

Date: 8/25/22

PennDOT, BOPD: Nina Ertel

Digitally signed by Nina Ertel  
Date: 2022.08.25 10:32:41 -04'00'

Date:

FHWA: CAMILLE A OTTO

Digitally signed by CAMILLE A OTTO  
Date: 2022.08.25 11:21:04 -04'00'

Date:

**LIST OF ATTACHMENTS**

**Attachment A: Figures**

Figure 1 – Project Location Map, Original Project Study Area

Figure 2 – Section 4(f) / 6(f) Location Map

Figure 3 – Lehigh River Water Trail and PA Scenic River Impact Map

Figure 4 – Pier Restriction Area

Figure 5 – Temporary Closure to Lehigh Gorge State Park, Public Notice

**Attachment B: Photographs**

**Attachment C: Design Plans**

Structure Plans (CTS&L)

ATON Plan and PFBC Approval

**Attachment D: Public Outreach**

Public Notice Flyer, Temporary Closure to Lehigh Gorge State Park

Newspaper Advertisement Proof of Publication

Comments Received



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Attachment E: Concurrence Letters

PFBC Section 4(f) De Minimis Agreement Letter, dated 2/25/2022

DCNR Section 4(f) De Minimis Agreement Letter, dated 2/28/2022





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**List Section 4(f) mitigation measures associated with this use that are part of this project:**

Mitigation measures will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in conjunction with DCNR during Final Design.

In addition, due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

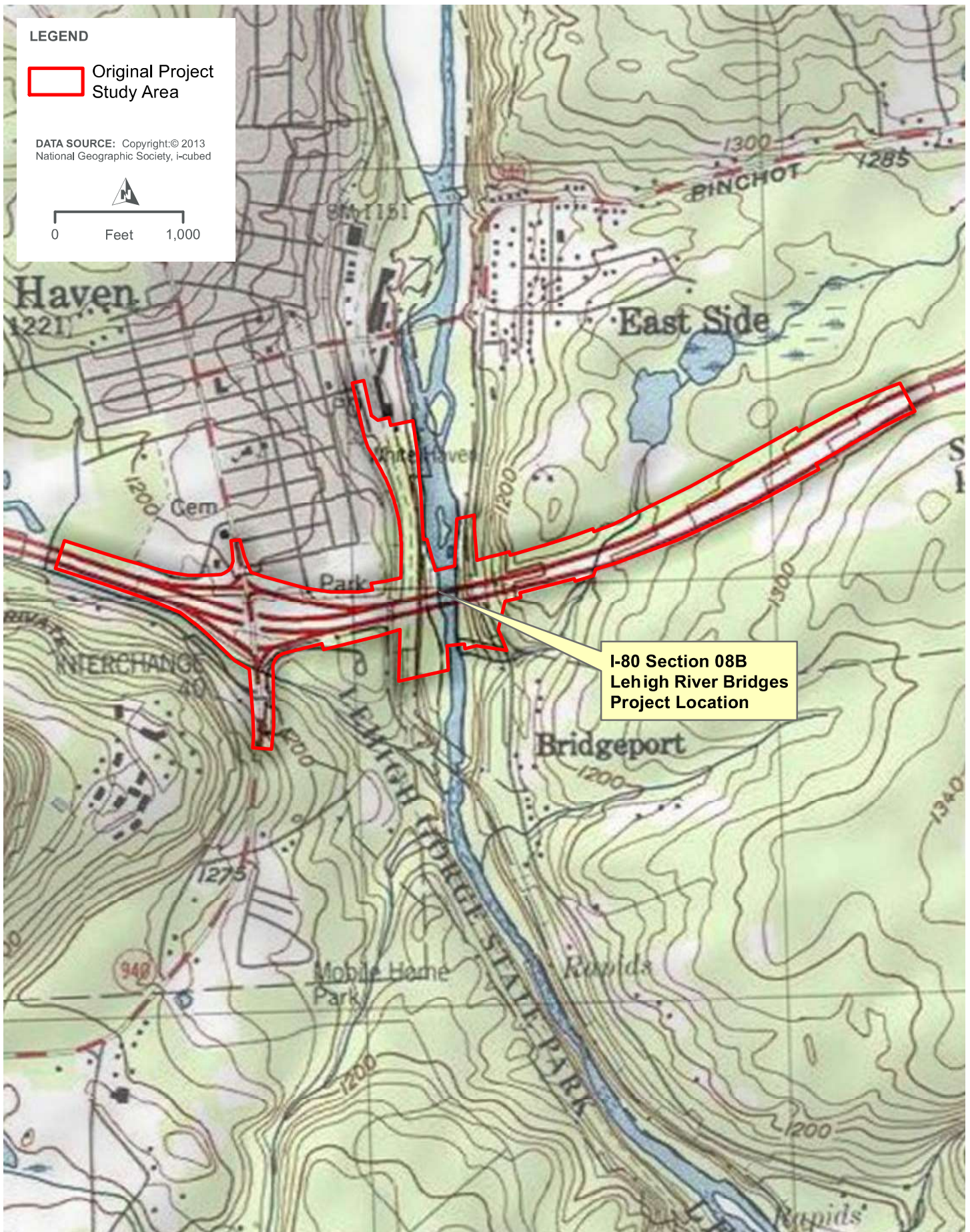
- Vegetative buffers will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river, as shown on Figure 4.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.

**Typical attachments for this form include, but are not limited to:**

- Project location map
- Map of affected Section 4(f) property and other Section 4(f) property(ies) in the project vicinity
- Photographs of the Section 4(f) property
- Project plan sheet to show impacts
- Correspondence with the official with jurisdiction
- Public involvement information

# **Attachment A**

## **Figures**



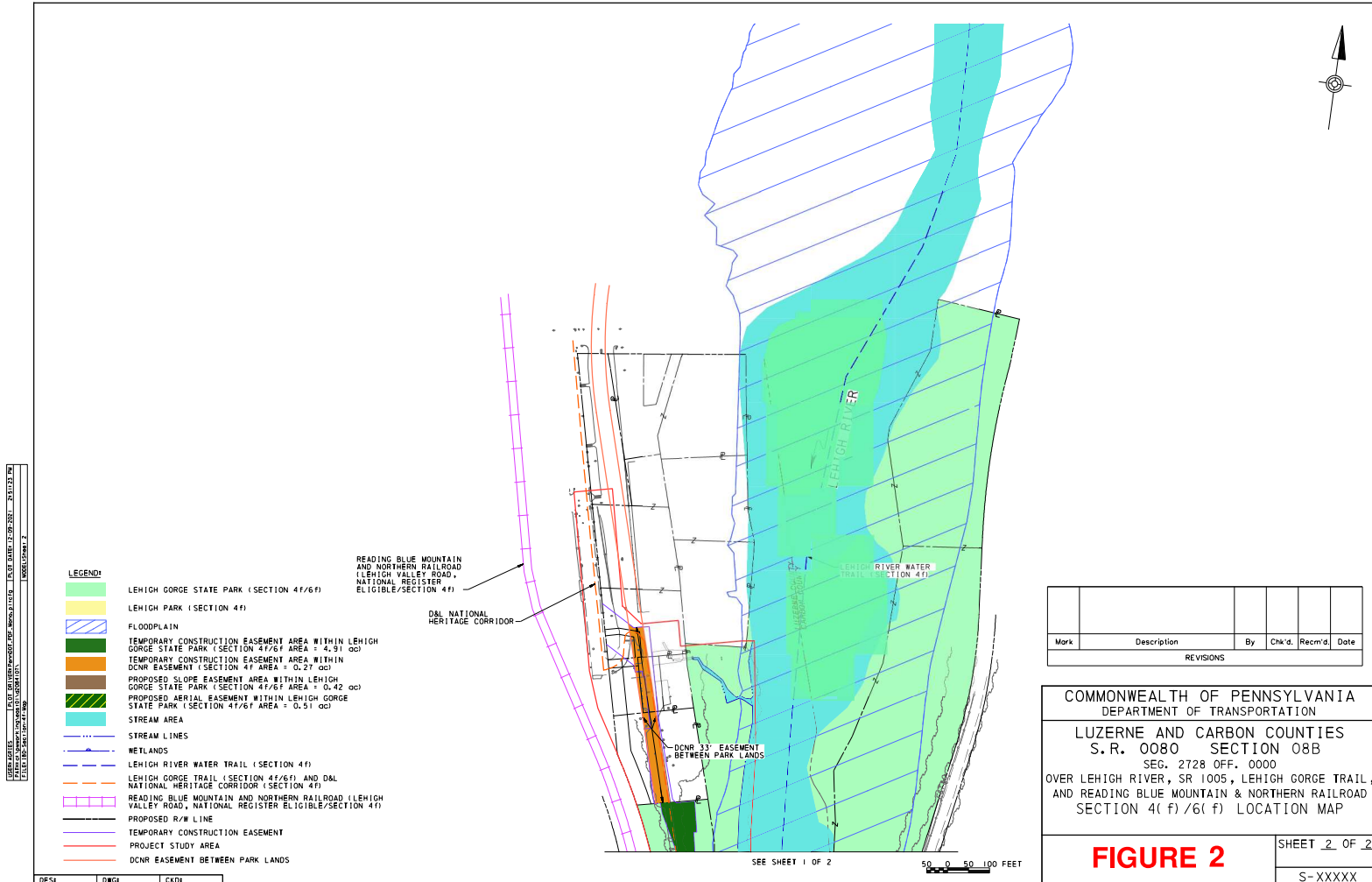
**I-80 SECTION 08B, LEHIGH RIVER BRIDGES**  
**PROJECT LOCATION MAP**  
**ORIGINAL PROJECT STUDY AREA**

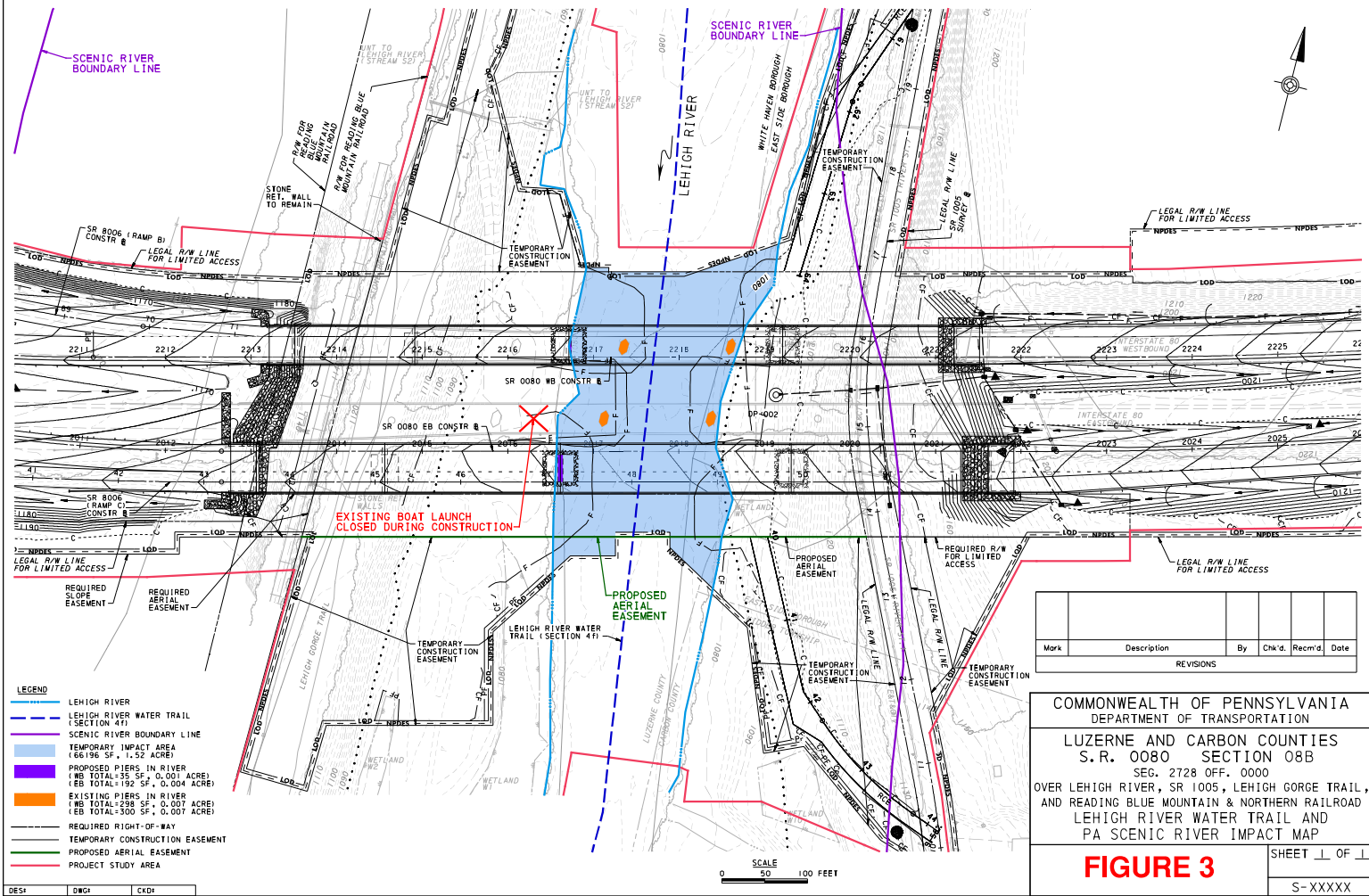
**FIGURE 1**

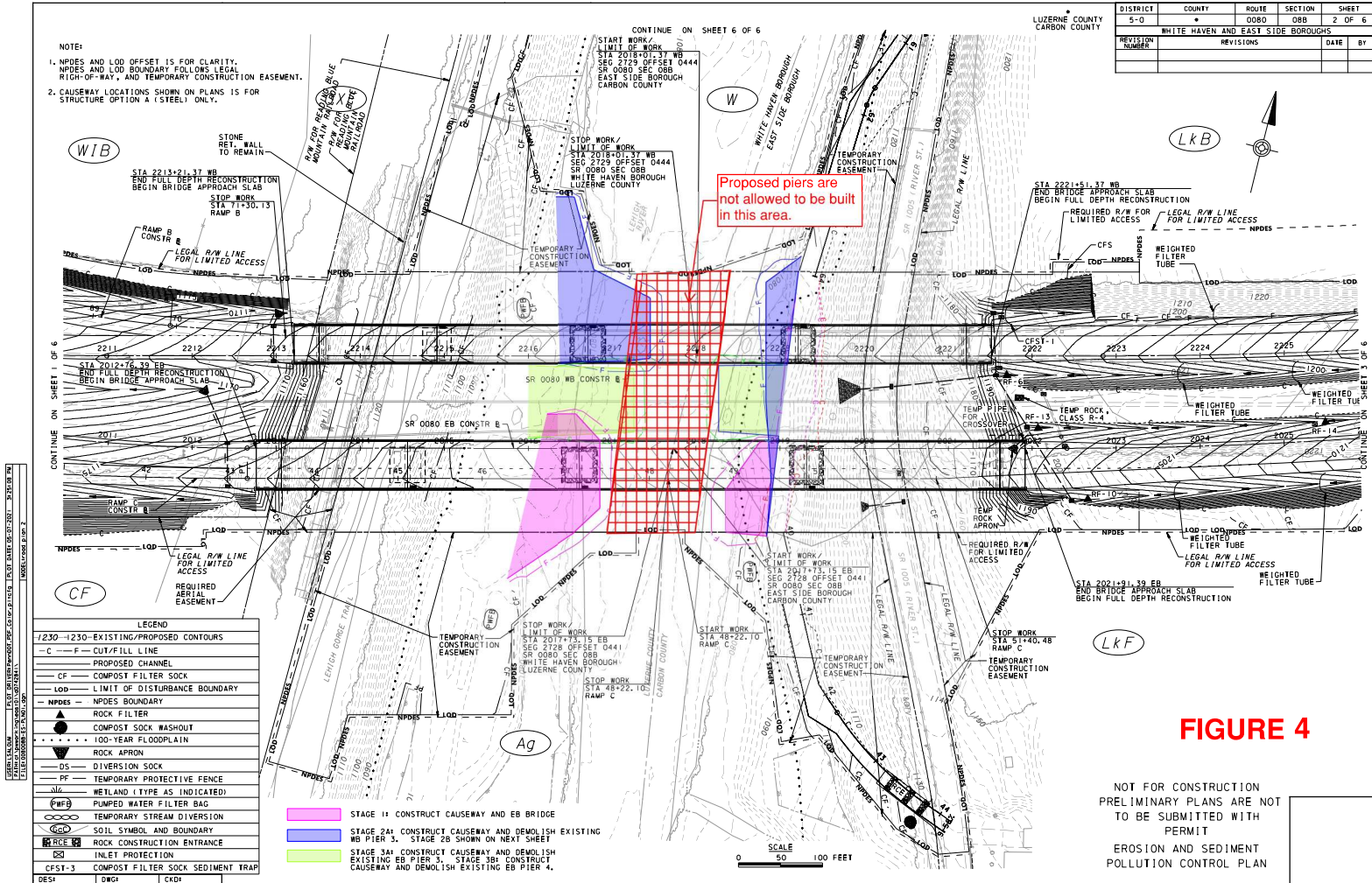




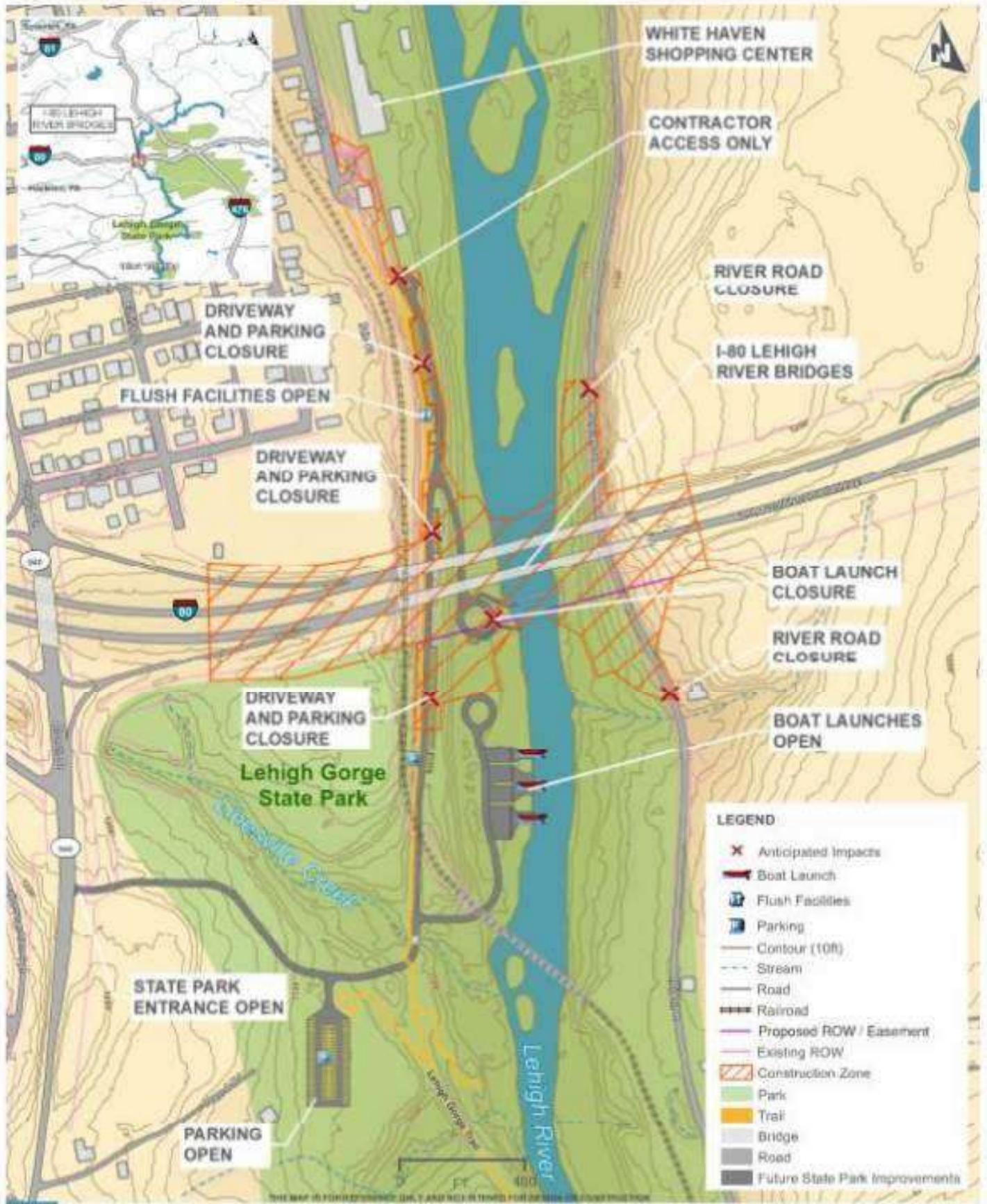












**I-80 LEHIGH RIVER BRIDGE PROJECT**  
**TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK**  
 WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP  
 LUZERNE & CARBON COUNTIES, PA

**FIGURE 5**



**Attachment B**  
**Photographs**

## Photographs



Lehigh Gorge State Park White Haven Access, facing south



Lehigh Gorge Trail and driveway, facing south





Lehigh Gorge Trail and driveway to existing boat launch, facing south



Boat launch area under I-80 Lehigh River Bridge on west shore of the Lehigh River, facing east





Underneath the I-80 Lehigh River Bridges on west shore of the Lehigh River, facing east



River gauge and raft requirements posted on the eastbound I-80 Lehigh River bridge pier





West shore of Lehigh River in Lehigh Gorge State Park, facing upstream (north)



West shore of Lehigh River in Lehigh Gorge State Park, facing downstream (south)





View of I-80 Lehigh River Bridges and Lehigh River, facing southeast

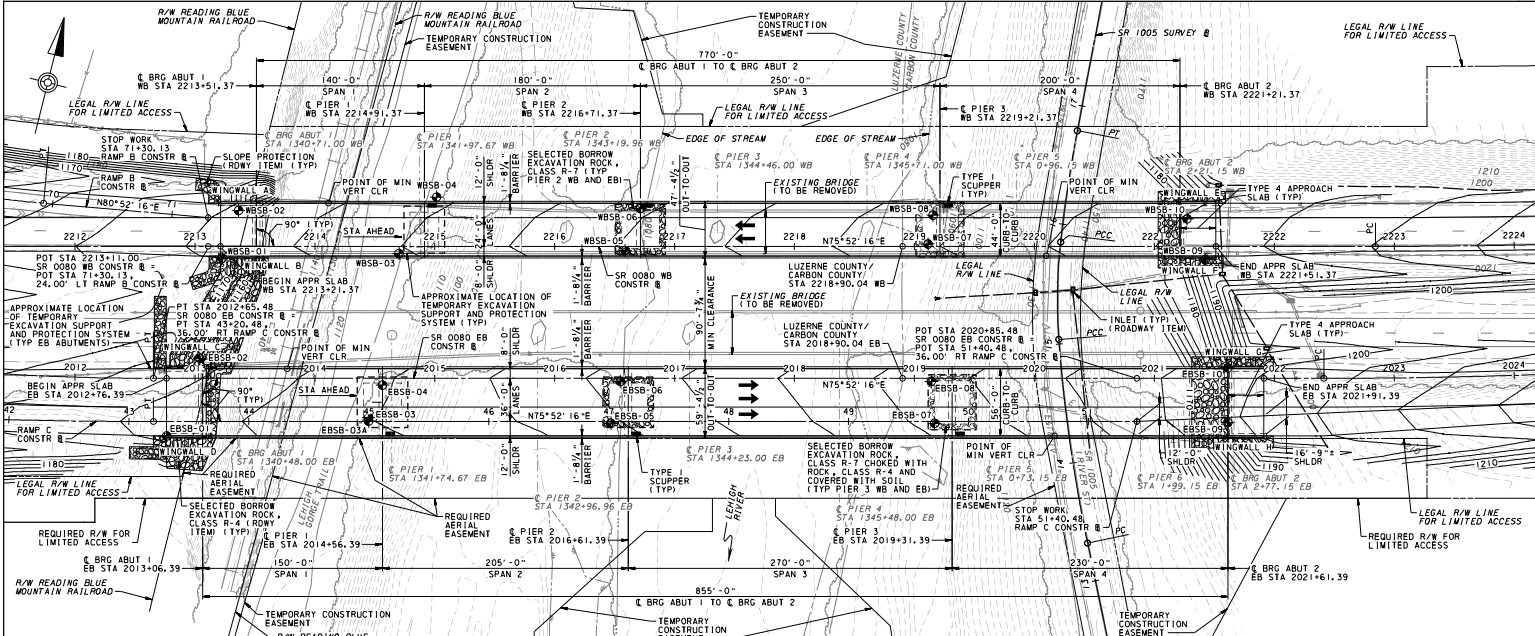


Lehigh Gorge Trail and driveway, facing north

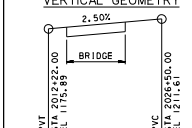
## **Attachment C**

### **Design Plans**

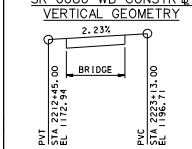
5-9000 C400 102-901 REVISED 10/01/11



### SR 0080 EB CONSTR B VERTICAL GEOMETRY



### SR 0080 WB CONSTR B VERTICAL GEOMETRY



### HYDRAULIC DATA

EXISTING SR 0080 WB STRUCTURE		PROPOSED SR 0080 WB STRUCTURE	
DESIGN FLOOD (50 YEAR)	DESIGN FLOOD (50 YEAR)	DESIGN FLOOD (50 YEAR)	DESIGN FLOOD (50 YEAR)
Q = 13,500 CFS	Q = 13,500 CFS	Q = 13,500 CFS	Q = 13,500 CFS
VELOCITY = 6.7 FPS	VELOCITY = 7.2 FPS	VELOCITY = 7.2 FPS	VELOCITY = 7.4 FPS
WS EL = 1086.64	WS EL = 1086.14	WS EL = 1086.14	WS EL = 1086.79
100 YEAR FLOOD	100 YEAR FLOOD	100 YEAR FLOOD	100 YEAR FLOOD
Q = 15,375 CFS	Q = 15,375 CFS	Q = 15,375 CFS	Q = 15,375 CFS
VELOCITY = 6.9 FPS	VELOCITY = 7.4 FPS	VELOCITY = 7.4 FPS	VELOCITY = 7.4 FPS
WS EL = 1087.30	WS EL = 1086.79	WS EL = 1086.79	WS EL = 1086.79

AS-DRILLED BORING LOCATIONS (WB)			
BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
WB5B-01	2213+22.7	9.7 RT	1174.4
WB5B-02	2213+36.4	30.0 LT	1174.4
WB5B-03	2214+70.2	6.0 RT	1120.0
WB5B-04	2215+01.5	41.1 LT	1119.1
WB5B-05	2216+58.9	3.5 RT	1083.1
WB5B-06	2216+71.4	31.6 LT	1084.8
WB5B-07	2219+11.3	2.0 LT	1084.8
WB5B-08	2219+15.5	26.1 LT	1084.4
WB5B-09	2221+42.4	11.1 RT	1190.8
WB5B-10	2221+27.3	23.0 LT	1192.0

AS-DRILLED BORING LOCATIONS (EB)			
BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EB5B-01	2012+76.4	48.0 RT	1166.3
EB5B-02	2013+04.1	16.6 LT	1175.7
EB5B-03	2014+44.8	32.7 RT	1118.4
EB5B-04	2014+44.8	35.7 RT	1118.4
EB5B-05	2014+56.4	6.6 RT	1118.9
EB5B-06	2016+46.8	37.5 RT	1080.4
EB5B-07	2016+46.8	2.5 RT	1080.4
EB5B-08	2019+16.8	37.5 RT	1089.9
EB5B-09	2019+14.0	2.5 RT	1086.7
EB5B-10	2021+61.4	8.8 LT	1185.0

- LEGEND
- AS-DRILLED TEST BORINGS
  - 1100 - EXISTING CONTOUR
  - 1100 - PROPOSED CONTOUR
  - EDGE OF STREAM
  - WETLAND LINES
  - DIRECTION OF TRAVEL
  - APPROXIMATE LOCATION OF TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM

### NOTES

- FOR GENERAL NOTES AND LIST OF SUPPLEMENTAL DRAWINGS, SEE SHEET 3.
- FOR TYPICAL SECTION, SEE SHEET 4.
- FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
- THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING R.
- FOR SR 1005 AND RAMP B AND C VERTICAL GEOMETRY, SEE SHEET 4.
- FOR SR 0080 HORIZONTAL GEOMETRY, SEE SHEET 4.

PREPARED BY:  
**HDR**  
HDR ENGINEERING, INC.  
3025 CHEMICAL RD  
SUITE 110  
PLYMOUTH MEETING, PA 19462

SIGNATURE & DATE

PRELIMINARY

Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					

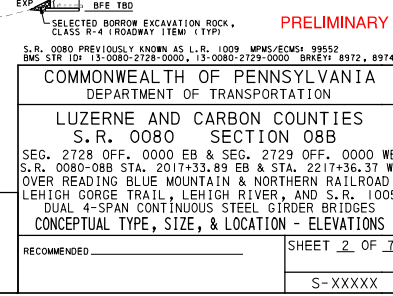
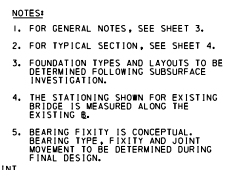
S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009. MPM/ECMS/ 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000. BMEY 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B  
SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
CONCEPTUAL TYPE, SIZE, & LOCATION - PLAN

RECOMMENDED	SHEET 1 OF 7 + SUPPLEMENTAL DRAWINGS
DISTRICT BRIDGE ENGINEER	S-XXXXX





CLASSIFICATION OF EARTHWORK FOR STRUCTURES	RC-11M	6/1/2010
BACKFILL AT STRUCTURES	RC-12M	2/8/2010
SUBSURFACE DRAINS	RC-30M	12/1/2010
ANCHORS	RC-31M	6/1/2010
ELECTRICAL DETAILS	BC-12M	2/19/2011
PERMANENT METAL DECK FORMS	BC-32M	1/30/2011
ANCHOR STUDS	BC-73M	2/19/2012
BAR CONSTRUCTION AND EXPANSION JOINT DETAILS	BC-735M	9/31/2011
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	1/31/2011
BRIDGE DRAINAGE	BC-75M	1/31/2011
CONCRETE SLAB DETAILS	BC-751M	6/1/2012
STEEL GIRDER DETAILS	BC-753M	1/31/2011
STEEL DIAPHRAGMS FOR STEEL BEAM/GIRDER STRUCTURES	BC-754M	1/31/2011
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	1/31/2011
DESCRIPTION	MENTAL DRAWINGS	DWG. NO.
		APP. DATE

## PRELIMINARY

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

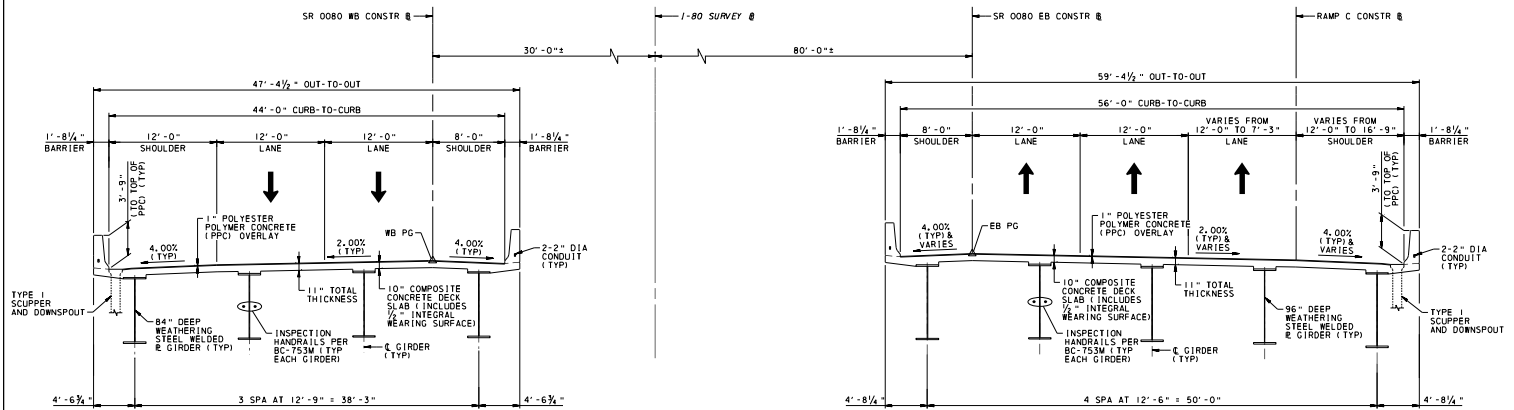
S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
RMS STR ID: 13-0080-2728-0000 13-0080-2729-0000 BRXBY: 8972 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.B. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 W  
OVER RAILROAD BLUE MOUNTAIN & NORTHERN RAILROAD  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1009  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

RECOMMENDED _____	SHEET <u>3</u> OF _____
	S-XXXXX



PROPOSED TYPICAL SECTION

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

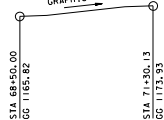
SR 0080 WB  
HORIZONTAL GEOMETRY  
WEST APPROACH

PI STA 2207+12.22  
 $\Delta = 23^{\circ}19'08''$  LT  
T = 397.22'  
L = 783.44'  
R = 1925.00'  
E = 40.56'  
PCC STA 2203+15.00  
PT STA 2210+98.44  
SUPERELEVATE 8.0%  
DESIGN SPEED TO MPH

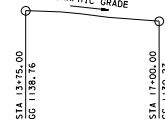
SR 0080 EB  
HORIZONTAL GEOMETRY  
WEST APPROACH

PI STA 2010+35.80  
 $\Delta = 12^{\circ}38'35''$  LT  
T = 231.95'  
L = 461.23'  
R = 2090.00'  
E = 12.79'  
PCC STA 2008+04.25  
PT STA 2012+65.48  
SUPERELEVATE 7.8%  
DESIGN SPEED TO MPH

SR 0080 RAMP B  
VERTICAL GEOMETRY



SR 1005  
VERTICAL GEOMETRY



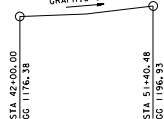
SR 0080 WB  
HORIZONTAL GEOMETRY  
EAST APPROACH

PI STA 2224+58.96  
 $\Delta = 3^{\circ}05'48''$  LT  
T = 174.61'  
L = 345.13'  
R = 6460.00'  
E = 2.36'  
PCC STA 2222+84.35  
PT STA 2226+33.48  
SUPERELEVATE 3.2%  
DESIGN SPEED TO MPH

SR 0080 EB  
HORIZONTAL GEOMETRY  
EAST APPROACH

PI STA 2025+76.65  
 $\Delta = 7^{\circ}35'47''$  LT  
T = 335.26'  
L = 669.53'  
R = 5050.00'  
E = 11.12'  
PCC STA 2022+41.39  
PT STA 2029+10.92  
SUPERELEVATE 4.0%  
DESIGN SPEED TO MPH

SR 0080 RAMP C  
VERTICAL GEOMETRY



NOTE:  
1. FOR GENERAL NOTES, SEE SHEET 3.

PRELIMINARY

Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS/ 99552  
BMS STR ID 13-0080-2728-0000, 13-0080-2729-0000 BMEY 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

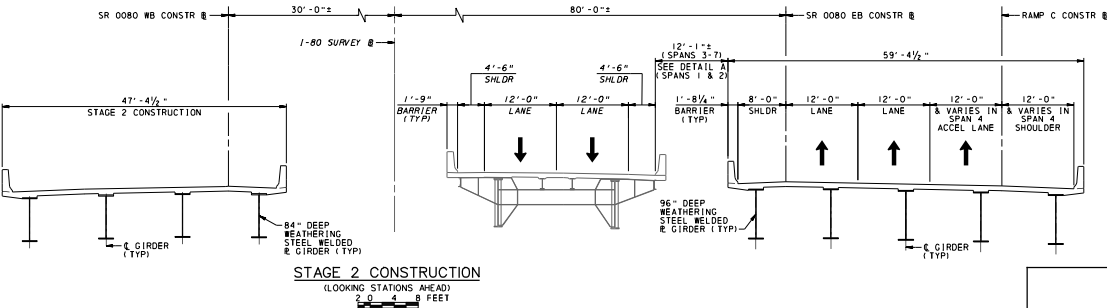
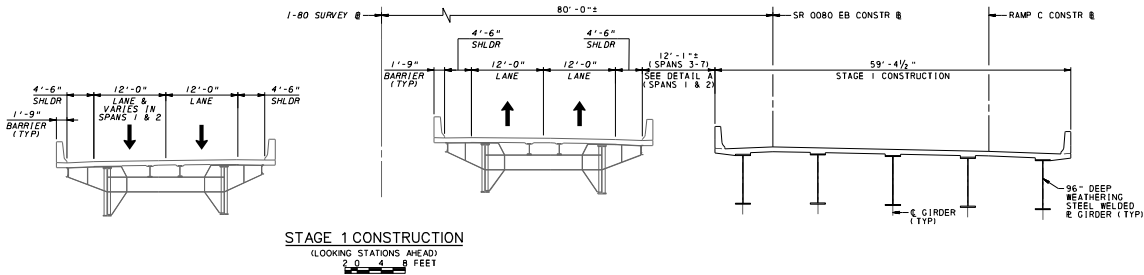
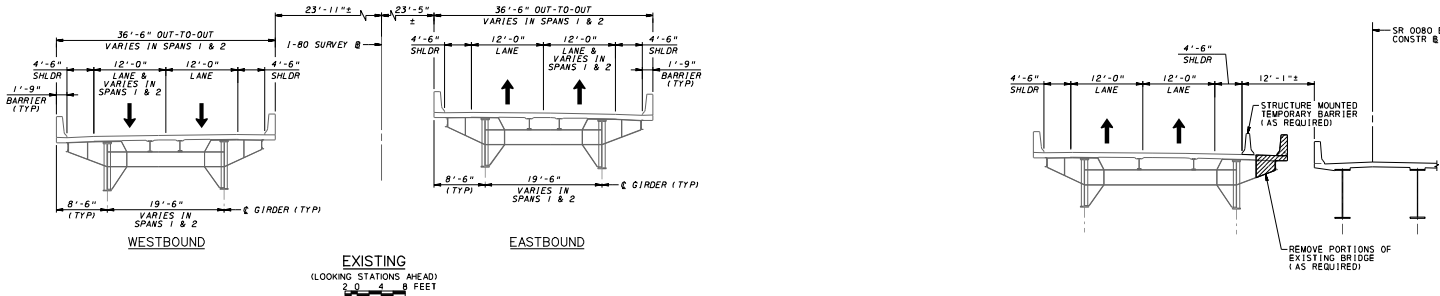
LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
TYPICAL SECTION

RECOMMENDED	SHEET 4 OF 7
	S-XXXX

DES: PJC DWG: ARG CKD: KRS





NOTE:  
1. FOR GENERAL NOTES, SEE SHEET 3.

**PRELIMINARY**

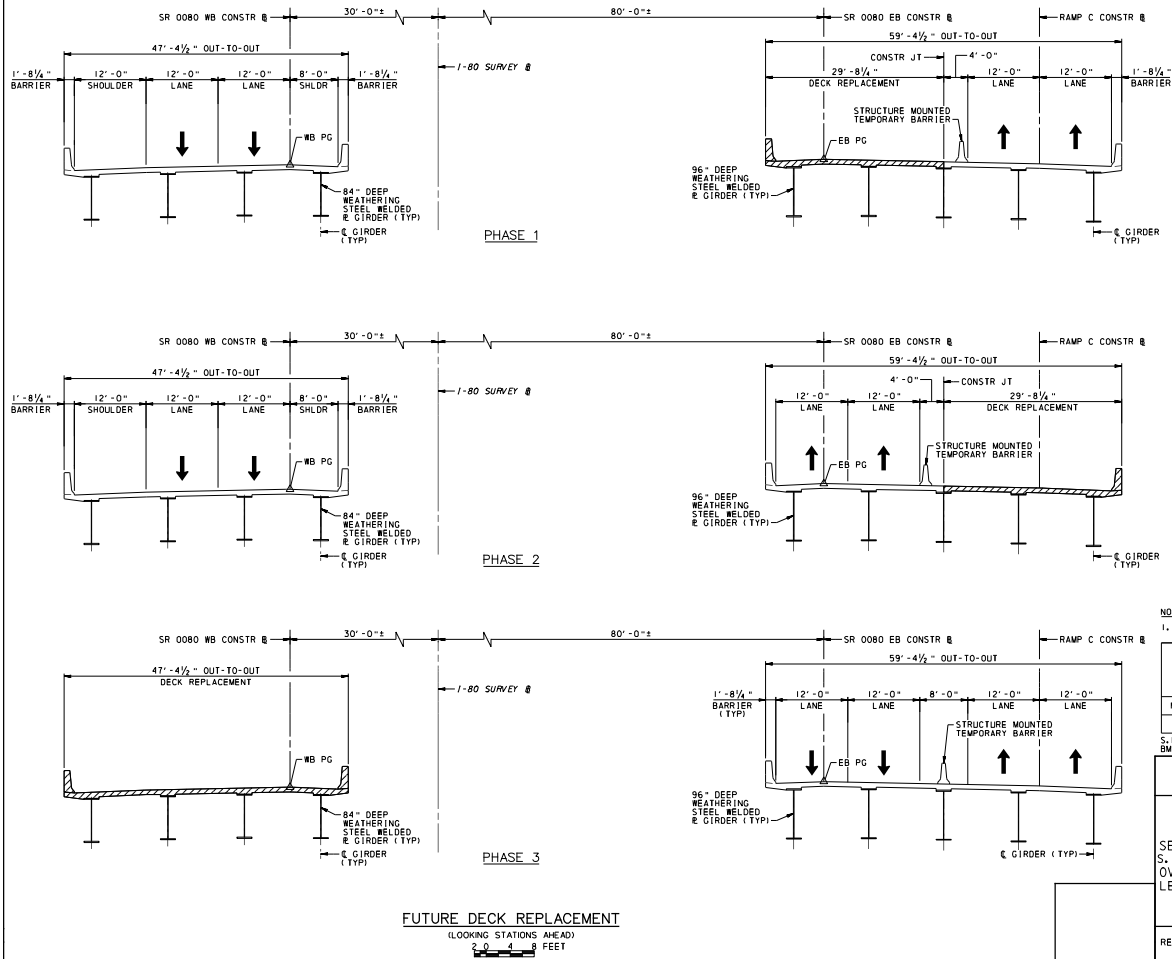
Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS# 99552  
BMS STR ID# 13-0080-2728-0000, 13-0080-2729-0000 BMEY# 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**  
SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONSTRUCTION STAGING**

RECOMMENDED \_\_\_\_\_ SHEET 5 OF 7  
S-XXXXX

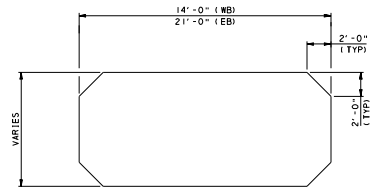
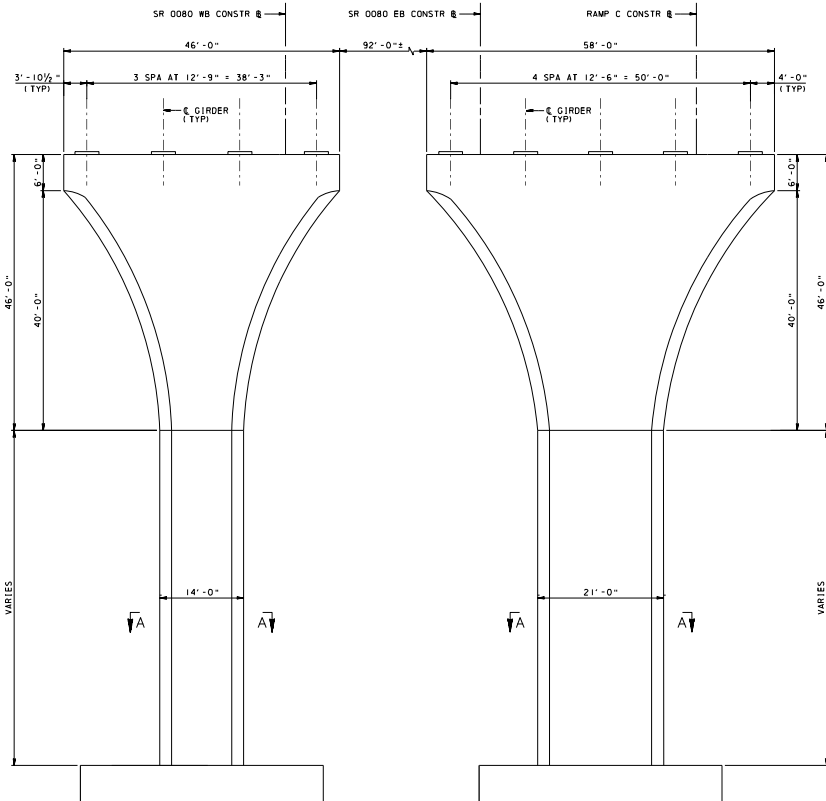


NOTE:  
1. FOR GENERAL NOTES, SEE SHEET 3.

PRELIMINARY

Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					
S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS# 99552 BMS STR ID# 13-0080-2728-0000, 13-0080-2729-0000 BMS# 8972, 8974					
COMMONWEALTH OF PENNSYLVANIA DEPARTMENT OF TRANSPORTATION					
LUZERNE AND CARBON COUNTIES S.R. 0080 SECTION 08B					
SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD, LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES FUTURE DECK REPLACEMENT					
RECOMMENDED _____					SHEET 6 OF 7
					S-XXXXX

S-9002 C400 (02-90) REVISED (06-97)



SECTION A-A  
2 0 4 FEET

NOTE:  
1. FOR GENERAL NOTES, SEE SHEET 3.

PRELIMINARY

Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS# 99552  
BMS STR ID# 13-0080-2728-0000, 13-0080-2729-0000 BMS# 8972, 8974

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B  
SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
PIER DETAILS

RECOMMENDED _____	SHEET 1 OF 1
	S-XXXX

DES: PJC DWG: ARG CKD: KRS



# Pennsylvania Fish & Boat Commission

---

August 16, 2021

Jennifer Nolan-Kremm, PE  
HDR  
301 Grant St. Suite 1700  
Pittsburgh, PA 15219

I have reviewed the recently submitted Aids to Navigation Plan for the SR 080 section 08B bridge replacement project over the Lehigh River in White Haven Borough, Luzerne County and East Side Borough, Carbon County. The plan is acceptable as submitted and has been approved. All signage/buoys must be installed prior to construction and inspected frequently to ensure it is always visible to boaters. Any changes to this plan must be submitted directly to this office for further review. If buoys are to be utilized PFBC-277 Application to Install Floating Structures/Private Aids to Navigation must be submitted and approved prior to installation.

I will share the approved ATON plan with the PA Fish & Boat Commission Division of Environmental Services and the Regional Law Enforcement Office to ensure they are aware of the project. Please advise the contractor that the project site may be inspected at any time by Bureau of Law Enforcement personnel for compliance with this plan. A copy of the approved plan along with this letter should be available for inspection if requested.

Please feel free to contact me if you have any questions concerning this project or the PA Fish & Boat Commission Aids to Navigation program in general.

Sincerely,

Clyde Warner, Colonel  
Bureau of Law Enforcement

Cc: (electronic) NE Region BLE-DES

---

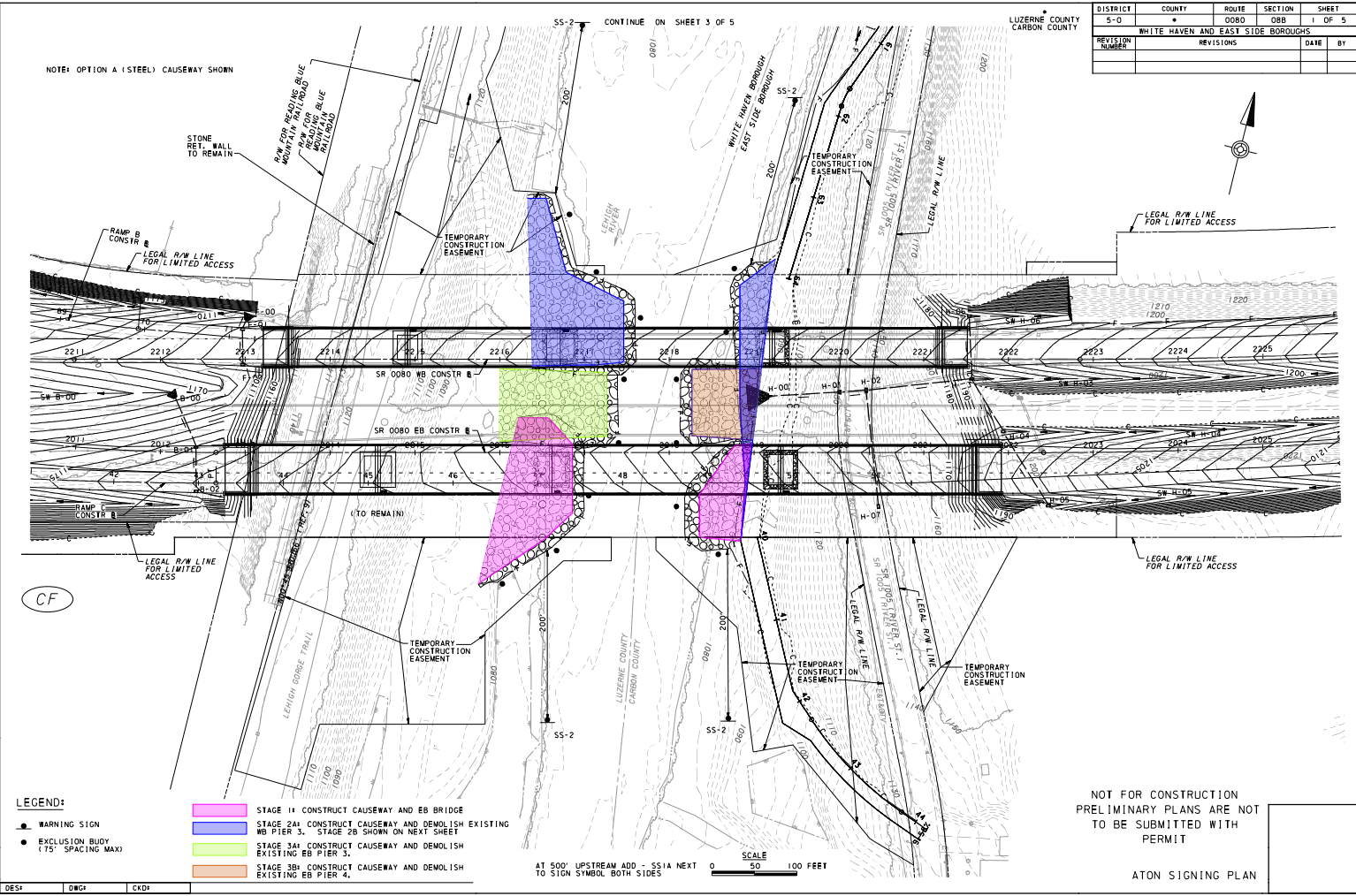
## Our Mission:

[www.fishandboat.com](http://www.fishandboat.com)

*To protect, conserve and enhance the Commonwealth's aquatic resources and provide fishing and boating opportunities.*



LUZERNE COUNTY  
CARBON COUNTY  
WHITE HAVEN AND EAST SIDE BOROUGH  
ROUTE 208  
SECTION 08B  
SHEET 1 OF 5  
DATE 11-07-2011  
BY J. J. JONES  
PROJECT NO. 2011-0001

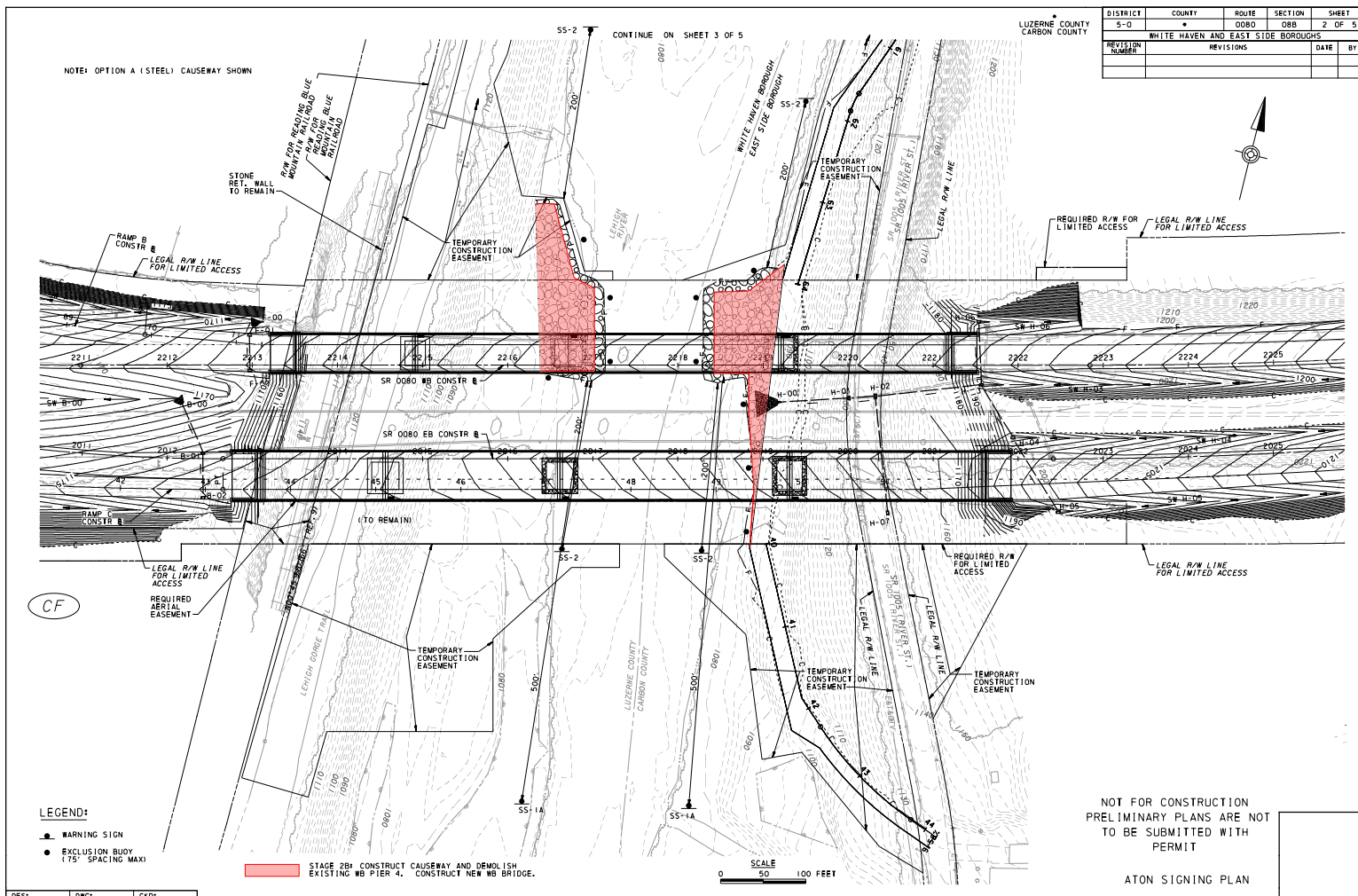


DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	•	0080	08B	1 OF 5
WHITE HAVEN AND EAST SIDE BOROUGH				
REVISION NUMBER	REVISIONS	DATE	BY	

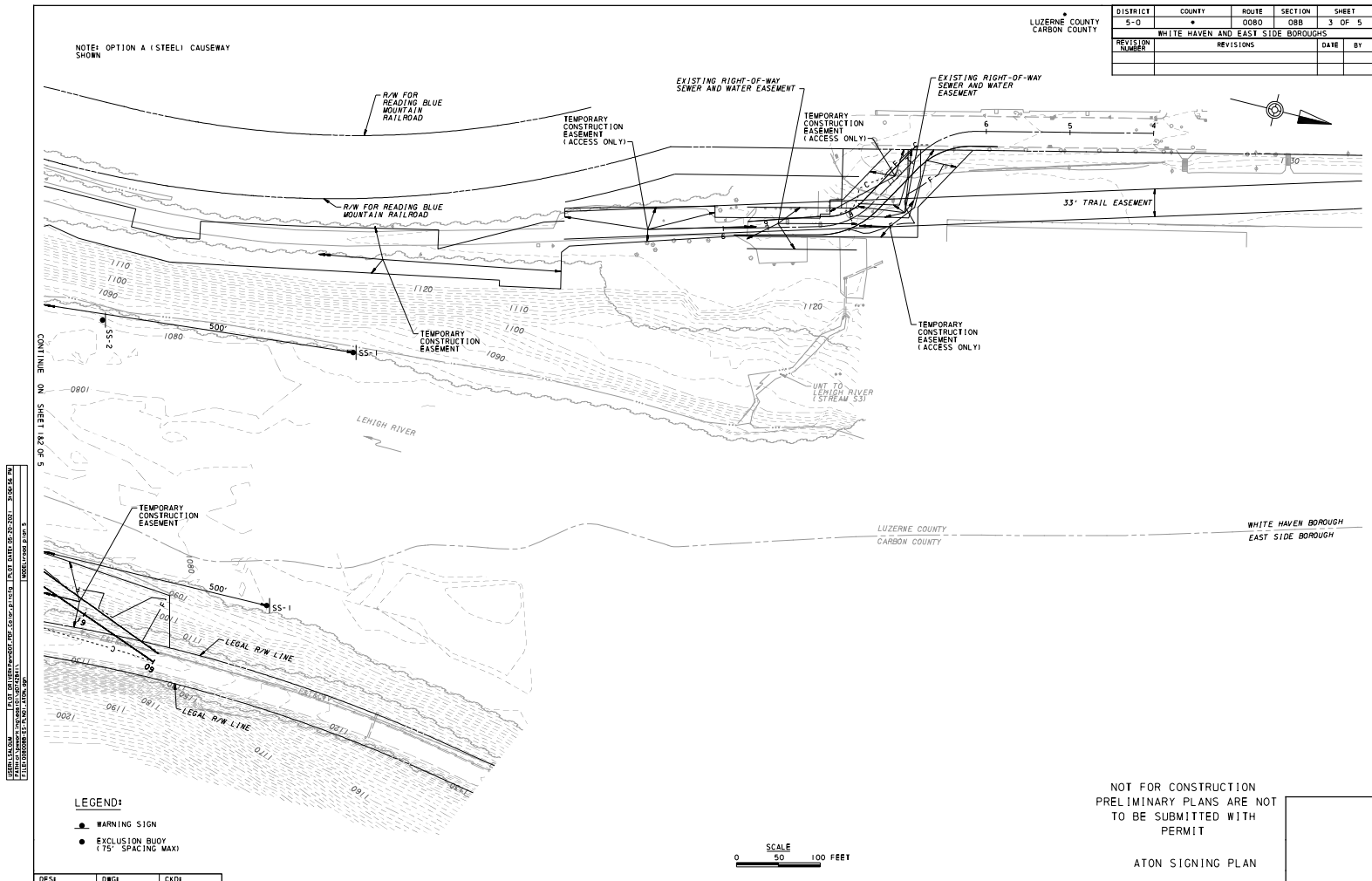
- LEGEND:**
- WARNING SIGN
  - EXCLUSION BUOY (75' SPACING MAX)
- STAGE 1A: CONSTRUCT CAUSEWAY AND EB BRIDGE
- STAGE 2A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 3. STAGE 2B SHOWN ON NEXT SHEET
- STAGE 3A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 3.
- STAGE 3B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 4.

NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT

ATON SIGNING PLAN



DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	•	0080	08B	3 OF 5	
WHITE HAVEN AND EAST SIDE BOROUGH					
REVISION NUMBER	REVISIONS			DATE	BY

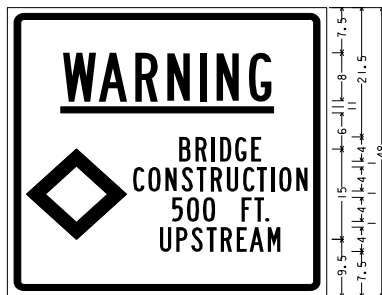


LUZERNE COUNTY  
CARBON COUNTY  
WHITE HAVEN AND EAST SIDE BOROUGH  
REVISIONS  
DATE  
BY

LUZERNE COUNTY  
CARBON COUNTY  
WHITE HAVEN AND EAST SIDE BOROUGH

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	088	4 OF 5
REVISION NUMBER	WHITE HAVEN AND EAST SIDE BOROUGH			
	REVISIONS			DATE BY

SS-1A (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON NONE;  
[WARNING] BLACK B; [HORIZONTAL LINE] FLUORESCENT ORANGE;  
[BRIDGE] BLACK B; [CONSTRUCTION] BLACK B;  
[500 FT.] BLACK B; [UPSTREAM] BLACK B;  
TABLE OF WIDTHS AND SPACES.

W	A	R	N	I	N	C
8.3	5.0	0.4	4.3	1.2	3.4	1.5

8.0	32.0	8.0
-----	------	-----

2.9	15.0
-----	------

B	I	R	N	I	N	C
7.9	1.7	0.8	1.6	0.8	1.5	0.8

C	O	N	S	T	R	U	C	T	I	O	N	2.9
18.9	1.7	0.6	1.8	0.7	1.7	0.8	1.5	0.6	1.6	0.8	1.7	0.7

S	O	O	F	T	-	8.7
24.7	1.7	0.6	1.8	0.6	1.8	4.0

U	P	S	T	R	E	A	W	7.0
22.9	1.7	0.8	1.7	0.6	1.5	0.6	1.6	0.8

SS-1 (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON NONE;  
[WARNING] BLACK B; [HORIZONTAL LINE] FLUORESCENT ORANGE;  
[BRIDGE] BLACK B; [CONSTRUCTION] BLACK B;  
[500 FT.] BLACK B; [DOWNSTREAM] BLACK B;  
TABLE OF WIDTHS AND SPACES.

W	A	R	N	I	N	C
8.3	5.0	0.4	4.3	1.2	3.4	1.5

8.0	32.0	8.0
-----	------	-----

2.9	15.0
-----	------

B	I	R	N	I	N	C
7.9	1.7	0.8	1.6	0.8	1.5	0.8

C	O	N	S	T	R	U	C	T	I	O	N	2.9
18.9	1.7	0.6	1.8	0.7	1.7	0.8	1.5	0.6	1.6	0.8	1.7	0.7

S	O	O	F	T	-	8.7
24.7	1.7	0.6	1.8	0.6	1.8	4.0

D	O	W	N	S	T	R	E	A	W	4.2
20.2	1.7	0.6	1.8	0.6	2.5	0.6	1.7	0.7	1.7	0.6

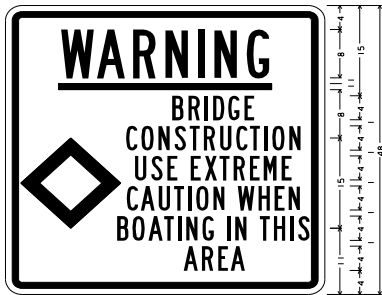
NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
ATON DETAIL SHEET

DES: DWG: CKD:



LUZERNE COUNTY  
CARBON COUNTY  
DISTRICT 5-0  
COUNTY  
ROUTE 0080  
SECTION 08B  
SHEET 5 OF 5  
WHITE HAVEN AND EAST SIDE BOROUGH  
REVISION NUMBER  
REVISIONS  
DATE  
BY

SS-2 (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON WHITE:  
[WARNING] BLACK B; [BRIDGE] BLACK B;  
[CONSTRUCTION] BLACK B; [USE EXTREME] BLACK B;  
[CAUTION WHEN] BLACK B; [BOATING IN THIS] BLACK B;  
[AREA] BLACK B

TABLE OF WIDTHS AND SPACES:

#	A	R	N	I	N	G
8.3	5.0	0.4	4.3	1.2	3.4	1.5
8.3	5.0	0.4	4.3	1.2	3.4	1.5

8.0	32.0	8.0
-----	------	-----

2.3	15.0
-----	------

7.8	1.7	0.7	1.7	0.7	0.5	0.8	1.7	0.6	1.7	0.7	1.5	10.6
-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	------

18.1	1.7	0.6	1.8	0.8	1.7	0.7	1.7	0.6	1.5	0.6	1.7	0.7	1.7	0.8	1.7	0.4	1.5	0.6	0.5	0.7	1.8	0.8	1.7	3.6
------	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----

19.5	1.7	0.7	1.7	0.7	1.5
------	-----	-----	-----	-----	-----

2.0	1.5	0.6	1.9	0.4	1.5	0.6	1.7	0.7	1.5	0.6	2.0	0.7	1.5	5.0
-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----

18.2	1.7	0.4	2.2	0.6	1.6	0.6	1.5	0.6	0.5	0.8	1.8	0.7	1.7
------	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----

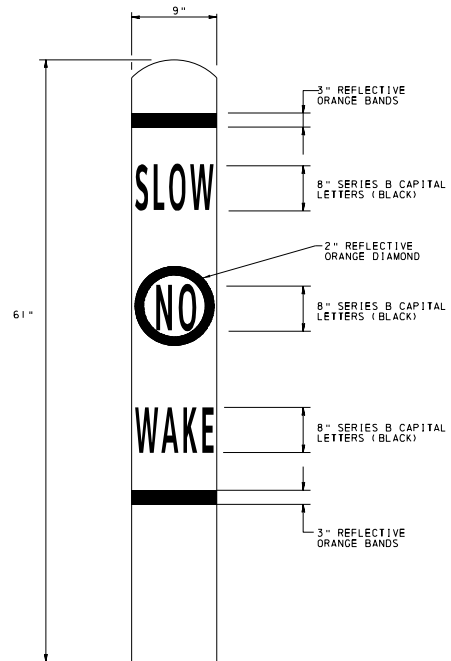
2.0	2.5	0.6	1.7	0.8	1.5	0.6	1.7	3.7
-----	-----	-----	-----	-----	-----	-----	-----	-----

16.8	1.7	0.6	1.8	0.6	2.1	0.2	1.5	0.6	0.5	0.8	1.7	0.7	1.7
------	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----

2.0	0.5	0.7	1.7	2.0	1.5	0.6	1.7	0.8	0.5	0.7	1.7	2.3
-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----

26.8	2.1	0.6	1.7	0.8	1.5	0.4	2.1	12.0
------	-----	-----	-----	-----	-----	-----	-----	------

NO WAKE BUOY



NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
ATON DETAIL SHEET

DES: DWG: CKD:

**Attachment D**

**Public Outreach**



Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP – Consultant Project Manager  
c-brotz@pa.gov



**LEGAL NOTICE**

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK,  
ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

**ADVERTISEMENT FOR BIDS****SEALED BID**

Contractor Name

Contractor Address

Re: Lawn Mowing and Landscape Maintenance Borough of White Haven

Attn: Linda Szoke, Borough Manager

312 Main Street

White Haven PA 18661

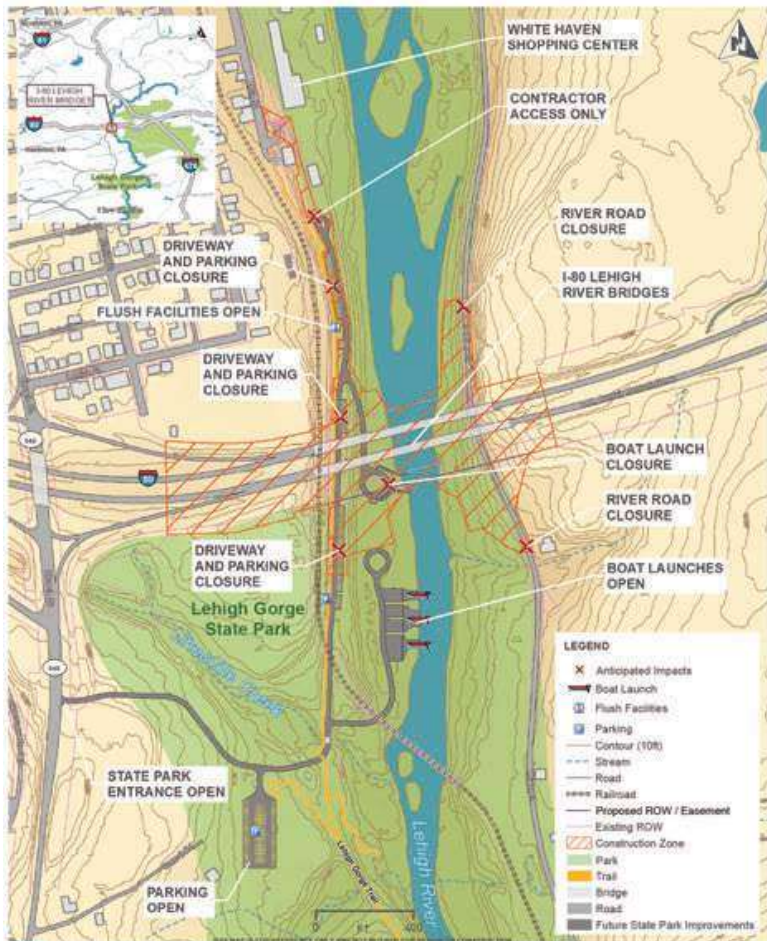
570-443-9129

The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



**I-80 LEHIGH RIVER BRIDGE PROJECT  
TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK  
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP  
LUZERNE & CARBON COUNTIES, PA**

**NOTICE OF PUBLIC INVOLVEMENT  
I-80, Section 08B, Lehigh River Bridges Project**

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

**Lehigh Gorge Trail:**

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

**Lehigh Gorge State Park driveway and parking:**

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

**Boat launches:**

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

**Lehigh River Water Trail:**

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, [c-brotz@pa.gov](mailto:c-brotz@pa.gov).

Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101

Attn: Brandy Rotz, AICP – Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)





# THE JOURNAL-HERALD

THURSDAY, OCTOBER 14, 2021 • Volume 41 – No. 7

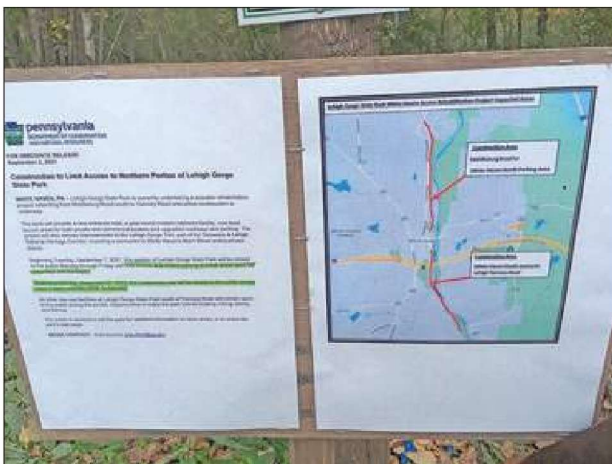
©2021, THE JOURNAL-HERALD. All Rights Reserved

SINGLE COPY—75¢

(USPS 277440)

CONTINUING: *THE WHITE HAVEN JOURNAL*  
ESTABLISHED 1879–142<sup>nd</sup> YEAR, NO. 46

CONTINUING: *THE WEATHERLY HERALD*  
ESTABLISHED 1880–142<sup>nd</sup> YEAR, NO. 19



**CLOSED:** Construction to the White Haven portion of Lehigh Gorge State Park will close the trail until spring. Above, a fence prevents access to the trail at Tannery; at left a posting gives details of the project. The fence was down over part of the weekend, but back up by Tuesday. Other signs around the parking area give details of the proposed Interstate 80 bridge project, also on page 3 of this issue. Comments on that plan are due by October 26. *JH: Ruth Isenberg*

## Both of Kidder's Fire Companies need new equipment

by Seth Isenberg

Some fire engines in the bays of the Albrightsville (AVFD) and Lake Harmony Volunteer Fire Companies (LHVFD) have gotten old. Through 2021, both fire companies have been working on plans to get new equipment.

Kidder Township supervisors called a public meeting on September 28 to talk to the companies about what they need, and how much the new equipment will cost.

Members of each fire company understood that in order to get something new, they had to retire two of their existing trucks. The LHVFD wants to replace its two oldest vehicles with a rescue pumper, with an estimated price tag of \$836,000 if ordered this year.

The AVFD is looking to replace both fire apparatus in the Albrightsville #2 Station with one new pumper tanker, AND buy a rescue pumper with an aerial flow tower for the Albrightsville main station, at a combined cost of just over \$1.5 million.

Placing the orders on these trucks before the end of the year will save about 3%

each — over \$70,000 total for the three.

Kidder Township has about \$1.2 million in its Emergency Services Capital Equipment Fund. It has been getting \$200,000 added from the township budget each year. There was discussion as to how much the township is allowed to spend from the fund. As the ordinance governing the fund is now written, the township can only cover about 60% of the value of the trucks. The ordinance also does not allow payments over time, which will have to be amended.

It was suggested that, given how fire company fundraisers are raising less and less, the ordinance should also be amended to allow 100% funding. While the trend was acknowledged, supervisors are more likely to boost the percentage over time, and as township finances allow.

For now, the 60% limit remains in place.

Supervisors were unanimous in approving a letter to

See KIDDER FIRE, page 7





# PUBLIC NOTICES

## LEGAL NOTICE

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK,  
ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

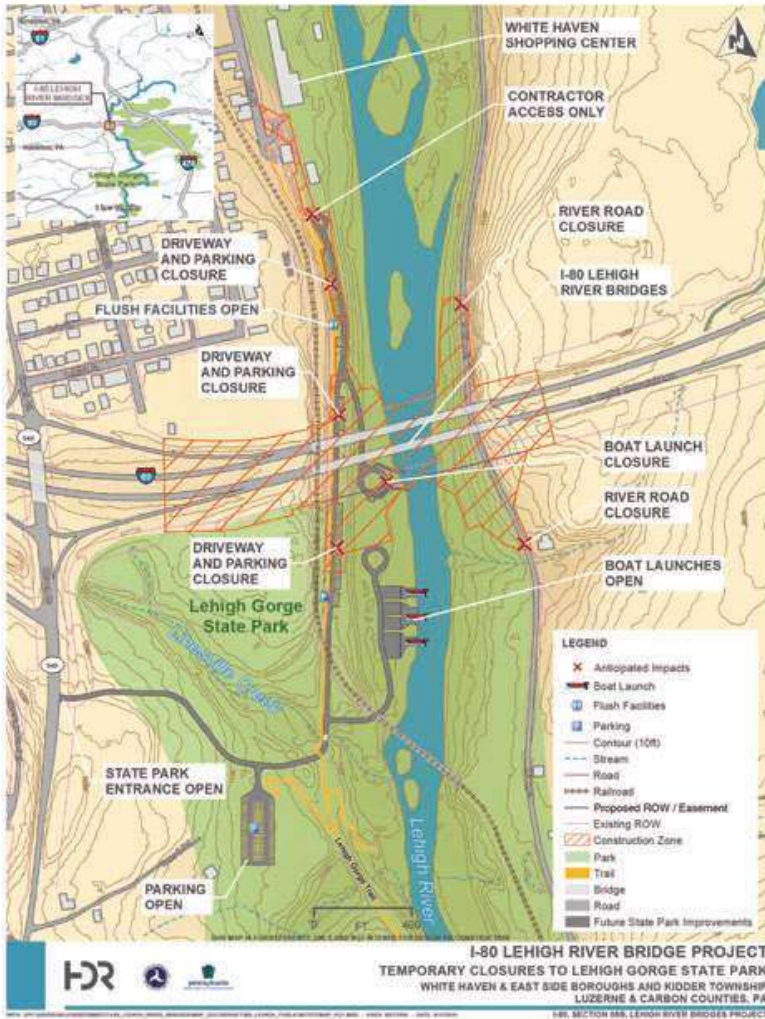
## ADVERTISEMENT FOR BIDS

SEALED BID  
Contractor Name  
Contractor Address  
Re: Lawn Mowing and Landscape Maintenance Borough of White Haven  
Attn: Linda Szoke, Borough Manager  
312 Main Street  
White Haven PA 18661  
570-443-9129  
The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



## NOTICE OF PUBLIC INVOLVEMENT I-80, Section 08B, Lehigh River Bridges Project

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

### Lehigh Gorge Trail:

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

### Lehigh Gorge State Park driveway and parking:

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

### Boat launches:

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

### Lehigh River Water Trail:

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, [c-brotz@pa.gov](mailto:c-brotz@pa.gov).

Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP — Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:08 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] Lehigh River Bridges I-80 08B Project Feedback

**From:** Cwalina Construction <cwalinaconstruction@gmail.com>  
**Sent:** Friday, October 15, 2021 2:43 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] Lehigh River Bridges I-80 08B Project Feedback

**ATTENTION:** *This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.*

Hi,

In response to your request for public feedback, I'd like you to consider noise reduction barriers on the bridge. We reside at 100 Towanda St in White Haven (directly up the path of the river to the other side of town) and the truck traffic traveling on the bridge causes significant noise to echo up the river which can be heard in the house even if the windows are closed. Additionally, a barrier might deter any people from attempting suicide off the bridge.

Is it possible to deaden sound, keep the public safe and keep the aesthetic view of the town as part of this project?

Thank you,

Valerie Love  
570-947-2690

--

Brian Cwalina  
Cwalina Construction  
570-606-6016  
[cwalinaconstruction@gmail.com](mailto:cwalinaconstruction@gmail.com)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge - Public Comment

---

**From:** Barbara Murphy <bmurphy@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 2:34 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] White Haven bridge

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Consultant Project Manager  
Brandy Rotz,

As a concerned citizen/neighbor of the Borough of White Haven we would like to know if the proposed new bridge (east and west bound route 80) will be built in the exact same area as the existing bridge? If so how will this be accomplished? For instance: lane closures , new on and off ramps which could possibly include taking land from our Lehigh Ballpark. How will the construction impact our local neighborhood? Please respond to my email [bmurphy@pa.metro cast.net](mailto:bmurphy@pa.metrocast.net) or

[patmurf81@gmail.com](mailto:patmurf81@gmail.com).

Thank you ,

Patrick and Barbara Murphy  
909 Elmira St.  
White Haven, PA 18661

Sent from my iPhone



**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge project

---

**From:** Seth Isenberg <journalseth@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 11:21 PM  
**To:** Rotz, Brandy  
**Cc:** Seth Isenberg  
**Subject:** [External] White Haven bridge project

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Brandy Rotz  
Consultant Project Manager

October 26, 2021

Four years of closure of the Lehigh Gorge Heritage Corridor trail could be very damaging to the economy of White Haven borough. Even the temporary closure of the trail this year has had a measurable impact on plaza retailers this month - as trail traffic is just not around even during the foliage season.

Looking at the plans, White Haven's downtown will be cut off from the south trail for an amazing four years. This year alone, local businesspeople were working with the Heritage Corridor to enhance the trail experience. We were looking to establish a "trail tenders" group of volunteers as ambassadors to visitors who start their trip from here, or would be passing through.

As a Main Street business ourselves, The Journal-Herald was hoping to see continued reviving of our downtown. Slowly, we have seen stores open, and recently, there is a prospect of a new building on Main Street. Losing the tourists is going to hurt. We are going to need some help from PennDOT, including highway signs indicting that we have a downtown business district, and signs on the highway for the remaining downtown trailhead.

Is it also possible for help to route bikers and hikers through town via the new driveway? Trail travelers may take the detour as an adventure.

I will add my concerns about any traffic that comes in heavy onto Route 940 that clogs up White Haven's roads. When there is an accident and the highway is closed, the simple task of pulling out from Main Street can become a ten to 15 minute wait. Through vehicles will not yield. Going to the grocery store or drug store becomes a fight with trucks and cars stuck on our little state road. We also have foot traffic, mainly young people, and I am very concerned about their safety when Route 940 becomes the only path for Interstate-using vehicles.

If an accident with a detour happens in bad weather, vehicles get stuck on the big hill on Route 940.

So, as a wrap up -

I am asking for PennDOT to provide signs to aid the downtown commercial area, and signs to promote the northern trailhead in the downtown to help draw visitors.

I am also asking for help to improve the local roads to handle the surges in traffic, and an answer to the safety problem within White Haven when bumper to bumper traffic does appear.

Seth Isenberg

General manager

The Journal-Herald of White Haven

LAKE NEWS of Lake Harmony

Journal of the Pocono Plateau of Blakeslee and Tobyhanna

And

Journal of Penn-Kidder of Albrightsville

211 Main Street, White Haven, PA 18661

570 215-0204

Comment Date	Comment Origin	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Association
12/16/2021	web comment	<p>The Delaware and Lehigh National Heritage Corridor (D&amp;L Trail) needs to be kept in the chain of communication for this project along with the State Park. The D&amp;L was noted in some locations of this public meeting post, but not consistently. The trail within this project scope is part of a currently 145-mile network, soon to be 160+ miles- any notice regarding the disruption of trail activities must be shared with the D&amp;L so the trail users can be accurately updated. Please add the D&amp;L to your list of organizations to keep informed (this webpage noted DCNR and White Haven).</p> <p>For the timelines regarding trail closures- Please clarify the YEAR of these anticipated impacts to the D&amp;L Trail- is the schedule outlined for the full 4 years of the project or just a portion? Are the April to October overnight closures M-F or 7 days a week? When is "night" considered to begin- will this line up with the State Park's hours of operation? Is the November to March closure M-F or 7 days a week?</p> <p>Can you expand on the "replacement" land PennDOT will provide? Does the bridge permanently impact the current alignment of the D&amp;L Trail in any way and is PennDOT considering "replacing" any portion of the D&amp;L Trail if the alignment is affected?</p> <p>When the capacity analysis was conducted did it account for pedestrian and bicycle traffic utilizing the D&amp;L Trail along Main Street? White Haven Borough is exploring intersection improvements for pedestrians (RRFB's and crosswalk repainting like you mentioned)- how might this new "stop point" affect traffic congestion and does this need to be considered when evaluating the traffic controls on adjacent intersections?</p> <p>Please note when you conduct your before/after intersection study that the D&amp;L Trail is currently closed during the State Park improvements project and any trail user counts will not be accurate until the project is complete.</p>	neutral	Information Request, Trails/Recreation/Parks		Elizabeth	Rosencrans	Delaware and Lehigh National Heritage Corridor (D&L Trail)	trail@delawareandlehigh.org	6109233548227	I-80 White Haven

**Attachment E**

**Concurrence Letters**





February 22, 2022

Paul Fawcett  
Chief, Waterways and Marina Management Branch  
PA Fish and Boat Commission, Bureau of Boating  
PO Box 6700  
Harrisburg, PA 17106-7000

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Concurrence with Section 4(f) Determination

Dear Mr. Fawcett:

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge.

The project will have temporary impacts to the Lehigh River but will not adversely affect the Lehigh River Water Trail. Temporary impacts to State Park property and facilities, including one boat launch directly under the I-80 bridges, are anticipated. The majority of the State Park will remain open during bridge construction, and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the I-80 bridges (the existing boat launch under the bridges will be temporarily closed). The contractor will restore disturbed areas upon completion.

An Aids to Navigation (ATON) Plan has been prepared and approved by Colonel Clyde Warner, PFBC Bureau of Law Enforcement. Implementation of the ATON Plan is included as a mitigation commitment for the project. PennDOT has agreed to additional mitigation measures through consultation with DCNR and PFBC.

As the work will occur over, around, and in the Lehigh River and will temporarily affect the Lehigh River Water Trail, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the recreational use of the Lehigh River Water Trail. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts to the Lehigh River Water Trail and proposed mitigation measures.

Thank you for your assistance in this matter.


If you have any questions, please feel free to contact me at 610-871-4550 or via email, [svottero@pa.gov](mailto:svottero@pa.gov)

Sincerely,



Scott G. Vottero, PE  
Acting Assistant District Executive – Design  
PennDOT District 5-0

Enclosures: Lehigh River Draft Section 4(f) De Minimis Form

Concurrence:  Date: 2/25/2022  
Paul Fawcett  
Chief, Waterways and Marina Management Branch  
Bureau of Boating  
PA Fish and Boat Commission



February 21, 2022

Kelly Rossiter, AICP  
Rivers Program Specialist  
PA Department of Conservation and Natural Resources  
400 Market Street, 5<sup>th</sup> Floor  
Harrisburg, PA 17101-2301

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Concurrence with Section 4(f) Determination

Dear Ms. Rossiter:

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge.

The project will have temporary impacts to the Lehigh River but will not adversely affect the scenic qualities of the Lehigh River. Temporary impacts to Lehigh Gorge State Park property and facilities, including one boat launch directly under the I-80 bridges, are anticipated. The majority of the State Park will remain open during bridge construction, and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the I-80 bridges (the existing boat launch under the bridges will be temporarily closed). The contractor will restore disturbed areas upon completion.

An Aids to Navigation (ATON) Plan has been prepared and approved by Colonel Clyde Warner, PFBC Bureau of Law Enforcement. Implementation of the ATON Plan is included as a mitigation commitment for the project. PennDOT has agreed to additional mitigation measures through consultation with DCNR and PFBC.

As the work will occur over, around, and in the Lehigh River and will temporarily affect the Lehigh River Water Trail and PA Scenic River, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the scenic qualities of the Lehigh River. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts

to the Lehigh River and proposed mitigation measures. The attached Section 4(f) Evaluation is specific to the Lehigh River Water Trail and PA Scenic River. A Section 4(f) Evaluation for de minimis use of the Lehigh Gorge State Park has been prepared and is being coordinated separately with PA DCNR Bureau of State Parks.

Thank you for your assistance in this matter.

If you have any questions, please feel free to contact me at 610-871-4550 or via email, [svottero@pa.gov](mailto:svottero@pa.gov)

Sincerely,



Scott G. Vottero, PE  
Acting Assistant District Executive – Design  
PennDOT District 5-0

Enclosures: Lehigh River Draft Section 4(f) De Minimis Form

Concurrence: Kelly Rossiter Date: 2/28/2022  
Kelly Rossiter, AICP  
Rivers Program Specialist  
PA Department of Conservation and Natural Resources



**Appendix G**  
**Bicycle and Pedestrian Checklist**



## Planning and Programming Checklist

Project I-80 Section 08B, White Haven Bridges Project

SR 0080, Section 08B Segment start 2720 EB, 2721 WB Offset start 1542 EB, 2330 WB

Team Members PennDOT District 5-0, Brandy Rotz, PM; HDR, John Baumgardner, PM

Date 4/30/2021

Item	Considerations	Check	Comments
1. Consistency with Bicycle/Pedestrian Planning Documents	Is the transportation facility included in or related to bicycle and pedestrian facilities identified in a master plan?  <ul style="list-style-type: none"> <li>• MPO/LDD bike/ped plan.</li> <li>• Local planning documents.</li> <li>• BicyclePA Routes.</li> <li>• Statewide Bicycle and Pedestrian Master Plan.</li> </ul>	Included in? No  Related to? Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. Bicycles and pedestrians are generally not allowed on interstate highways. During construction, the project will temporarily close and detour PA Bike Route L which follows River Road (SR 1005) along the east side of the Lehigh River through the project area. The PA Bike Route L detour will be established and implemented through coordination with PennDOT's Statewide Bicycle and Pedestrian Coordinator.
	Will the transportation facility provide continuity and linkages with existing or proposed bicycle/pedestrian facilities?	Yes	As described in multiple sections within this Checklist, the project will incorporate measures to ensure continuity and linkages to the existing PA Bike Route L and Lehigh Gorge Trail/D&L Trail that the I-80 bridges cross over. No permanent disruption of continuity or linkages will result from the project.
	Is the transportation facility included in or related to a regional/local recreational plan?  <ul style="list-style-type: none"> <li>• Rails-to-Trails.</li> <li>• Greenways.</li> <li>• Local, State, National Parks.</li> </ul>	Included in? No  Related to? Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. The bridges cross over Lehigh Gorge State Park and Lehigh Gorge Trail/D&L Trail. Mitigation measures will be incorporated into the project to ensure safety of bicycle riders and pedestrians during construction. It is anticipated that the trail will remain open for the majority of construction duration but may require short-term closures during certain construction activities such as bridge demolition, pier construction and setting beams. Agency coordination is ongoing to determine details of mitigation measures.
2. Existing and Future Usage	Do bicycle/pedestrian groups regularly use the transportation facility?  <ul style="list-style-type: none"> <li>• Bike clubs.</li> <li>• Bicycle commuters.</li> <li>• Hiking, walking, or running clubs.</li> <li>• Skateboarding or rollerblading groups.</li> <li>• Bicycle touring groups.</li> <li>• General tourism/sightseeing.</li> </ul>	No	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed.
	Does the existing transportation facility provide the only convenient transportation connection/linkage between land uses in the local area or region?	No	While I-80 is a heavily traveled east/west interstate highway in the region, it is not the only connection/linkage in the vicinity of the project. SR 940 crosses the Lehigh

			River north of the project and connects the communities of White Haven and East Side in the project area. PA Bike Route L crosses the Lehigh River on SR 940.
	<p>Could the transportation facility have favorable or unfavorable impacts upon the bike tourism/economy of an area/region? Consider:</p> <ul style="list-style-type: none"> <li>• Local businesses</li> <li>• Chamber of Commerce</li> <li>• Tourism Promotion Agencies.</li> </ul>	Yes	The transportation facility, I-80 and the bridges over Lehigh River, will be constructed in phases to maintain two lanes of traffic in both directions during construction to minimize disruption to local businesses and tourism. Mitigation measures will be incorporated into the project to ensure safety of bicycle riders and pedestrians in Lehigh Gorge State Park during construction. Agency coordination is ongoing to determine details of mitigation measures.
	Are there physical or perceived impediments to bicycle or pedestrian use of the transportation facility?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. Bicycles and pedestrians are generally not allowed on interstate highways.
	Is there a higher than normal incidence of bicycle/pedestrian crashes in the area?	No	
3. Safety	Is the transportation facility in a high-density land use area that has pedestrian/bike/motor vehicle traffic?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. The bridges cross over Lehigh Gorge State Park and Lehigh Gorge Trail/D&L Trail, a popular local and tourist destination for pedestrians and bicycle users.

3. Safety (continued)	Is there a high amount of crossing activity at intersections?  • Midblock • Night crossing activity • Adequate lighting.	Not Applicable	
	Would the transportation facility (and all users) benefit from widened or improved shoulders or improved markings (shoulders, crosswalks)?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. While not proposed due to pedestrian or bicycle use, wider shoulders will be provided on the bridges.
4. Community and Land Use	Is the transportation facility in a city, town, or village?	Yes	The boroughs of White Haven and East Side are located west and east of the project bridges, respectively. Portions of both boroughs are within the project area.
	Is the transportation facility within/near a community or neighborhood?	Yes	The boroughs of White Haven and East Side are located west and east of the project bridges, respectively. Portions of both boroughs are within the project area.
	Is the transportation facility the “main street” in a community or town?	No	
	Could bicycle or pedestrian usage impact economic development?	No	
	Are sidewalks needed in the area?  • Presence of worn paths along the facility. • Adjacent land uses generate pedestrian traffic. • Possible linkages/continuity with other pedestrian facilities.	No	
	Is the transportation facility a link between complimentary land uses?  • Residential and commercial. • Residential and business.	No	
	Is the transportation facility in close proximity to hospitals, elderly care facilities, or the residences or businesses of persons with disabilities?	No	
	Is the transportation facility within or near educational buildings?	No	
	Is the transportation facility in close proximity to transit stops or multi-modal centers (including airports, rail stations, intercity bus terminals, and water ports)?	No	
5. Transit	Is the transportation facility on a transit route?	No	The Lackawanna & Luzerne Counties Joint Comprehensive Plan & Long-Range Transportation Plan and Northeastern Pennsylvania (NEPA) MPO Long-Range Transportation Plan show no mass transit in the project area.
	Is the transportation facility near park and-ride lots?	Yes	A park-and-ride lot is located along SR 940 south of the I-80/SR 940 interchange.
	Are there existing or proposed bicycle racks, shelters or parking available? Are there bike racks on buses?	Yes	A bike rack is located at the park-and-ride lot along SR 940.



6. Traffic Calming	Is the community considering traffic calming as a possible solution to speeding and cut-through traffic?	Not Applicable	
--------------------	--	----------------	--

## Scoping Checklist

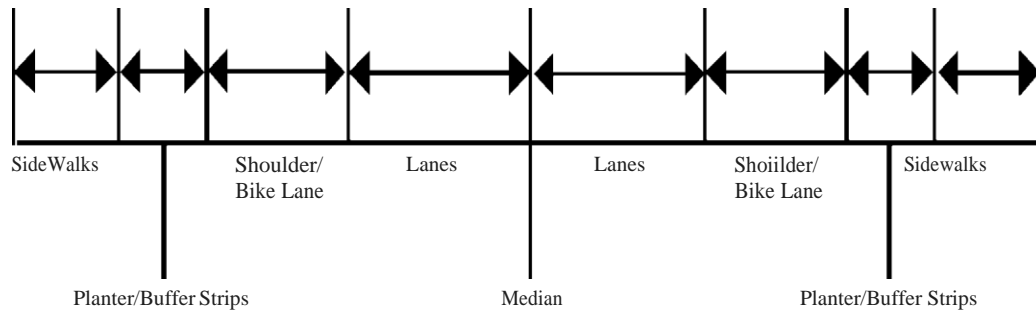
Project I-80 Section 08B, White Haven Bridges Project

SR 0080, Section 08B Segment start 2720 EB, 2721 WB Offset start 1542 EB, 2330 WB

Team Members PennDOT District 5-0, Brandy Rotz, PM; HDR, John Baumgardner, PM

Date 4/30/2021

## Right-of-Way Needs Diagrams



Element	Number Required	Width Required	Total Width
Sidewalks	0	0	0
Buffer Strips	0	0	0
Shoulders	2	8 ft (inside) 12 ft (outside)	40 ft
Lanes	4	12 ft	48 ft
Median	1	Varies, 60 - 110 ft	110 ft
<i>Total Right-of-Way Required</i>			198 ft

\* Table presents typical Roadway Section.

## Pedestrian Facilities

Item	Considerations	Check	Comments
1. Sidewalks	<p>Appropriate width:</p> <ul style="list-style-type: none"> <li>1.5 m — 2.1 m (3'-7') for residential, commercial, and industrial.</li> <li>2.5 m (8') minimum for high use areas/CBD.</li> <li>2.1 m (7') width for bridges.</li> <li>0.6 m (2') shy distance for vertical barriers.</li> <li>1.2 m — 2.1 m barrier separating traffic from pedestrians on bridges.</li> </ul>	Not Applicable	

1. Sidewalks (continued)	Applicability of planter or buffer strips.		
	Connectivity with other pedestrian facilities.		
	Proximity to transit bike/ped generators: <ul style="list-style-type: none"> <li>• Transit stops.</li> <li>• Schools.</li> <li>• Park &amp; rides.</li> <li>• Nursing homes.</li> <li>• Offices.</li> <li>• Business environments.</li> <li>• Athletic fields.</li> <li>• Recreation facilities.</li> </ul>		
	Observe pedestrian patterns for special needs such as: <ul style="list-style-type: none"> <li>• Midblock crossings.</li> <li>• Islands and refuges.</li> <li>• Night crossing activity.</li> </ul>		
	ADA needs and concerns.		
2. Signalized Intersections	Crosswalks provided and marked.	Not Applicable	
	Intersection bike/ped crash history reviewed.		
	Is there a dedicated pedestrian phase, if so how long?		
	Crossing distance is minimized.		
	Ped heads and ped pushbuttons provided.		
	ADA needs and concerns.	Retirement Homes	
		Schools	
		Medical Facilities	
3. Traffic Calming	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?	Not Applicable	

## Bicycle Facilities

Item	Considerations	Check	Comments
1. Bikelanes/Paved Shoulders	Appropriate width of bike lane: <ul style="list-style-type: none"> <li>• 1.5m (3') adjacent to curb</li> <li>• 1.8m (6') standard.</li> </ul>	Not Applicable	
	Connectivity with other facilities. <ul style="list-style-type: none"> <li>• Bike lanes</li> <li>• Shared use trails</li> <li>• Trail heads/parking areas.</li> </ul>		
	Maximize width of shoulders and provide appropriate markings as per AASHTO Green Book.		
	3 m (10') vertical clearance from fixed obstructions (excluding road signs).		
	Angle and smoothness of railroad crossings. Avoid angles of incidence of <70 degrees or re-design.		
	Bridge accesses provided/pinch points avoided.		
2. Signalized Intersections	Parking parallel or angled.	Not Applicable	
	Inventory existing bicycle facilities.		
	Intersection bike/ped crash history reviewed.		
	Crossing distance is minimized.		
	Considerations for bikes making turns.		
	Bike detection.		
3. Traffic Calming	Elevated push buttons.		
	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?	Not Applicable	



## **Appendix H**

### **List of Preparers**

<b>Name</b>	<b>Organization</b>	<b>EA Role</b>	<b>Education</b>	<b>Years</b>
Camille Otto Director of Planning, Environment, and Finance	FHWA PA Division	FHWA Approver	B.S. Biology	25
Jon Crum Senior Environmental Specialist	FHWA PA Division	FHWA Reviewer	B.S. Biology M.S. Environmental Science and Management	17
Sarah Cordek Transportation Engineer	FHWA PA Division	FHWA Engineering Reviewer	B. S. Civil Engineering Technology	9
Scott Vottero Acting Assistant District Execute for Design	PennDOT District 5-0	Engineer Reviewer	B.S. Civil Engineering	26
Jerry Neal District Environmental Supervisor	PennDOT District 5-0	Environmental Reviewer	B.A. Biology	32
Brandy Rotz Consultant Project Manager	PennDOT District 5-0	Consultant Project Manager	M.S. Geo-Environmental Studies	19
Drew Ames Environmental Planning Manager	PennDOT Central Office	Environmental Reviewer	B.H Communications M.S. Community and Regional Planning	26
Kenda Gardner Deputy Chief Counsel	PennDOT Office of Chief Counsel	Legal Review	B.S. Chemistry J.D.	28
Neal Brofee Environmental Counsel	PennDOT Office of Chief Counsel	Legal Review	B.A. Mathematics J.D.	24
Kristine Thompson Architectural Historian	PennDOT Central Office	Above-Ground Cultural Resources	B.S. Historic Preservation; M.A. Anthropology	28
Steven McDougal Archeologist	PennDOT Central Office	Archaeology	M.A. Anthropology	29
Nina Ertel, PE Project Development Engineer	PennDOT Central Office	Engineering Reviewer	B.S. Civil Engineering M.S. Civil Engineering	11
Diane Nulton Environmental Project Manager	HDR	EA Project Manager	B.S. Biology/Ecology	35
John Baumgardner, PE Structures Section Manager	HDR	Bridge Replacement Project Manger	B.S. Civil Engineering M.S. Civil Engineering	20
Kathleen Krommes, ENV SP Environmental Project Manager	HDR	Environmental Lead, Technical Writer/Editor	B.S. Chemical Engineering	35
Katherine Markowitz Environmental Scientist	HDR	EA Technical Writer/Editor	B.S. Marine and Environmental Biology and Policy	8
Jennifer Nolan-Kremm Transportation Hydraulics Lead	HDR	H&H and Permitting Lead	B.S. Physics & M.S. Civil Engineering	16
James Koval Project Manager	HDR	EA Technical Writer/Editor	BS Geography – Environmental Science	20
John McPherson, AICP Env. Services Director	HDR	EA, Cumulative Impacts	B.A. Math/Economics; M.U.P.	30

Name	Organization	EA Role	Education	Years
Linda Smith Senior Environmental Planner	HDR	Author, Cumulative Impacts	M.S.E. Civil Engineering	17
Jenn Walsh, PE Traffic & Planning Section Manager	HDR	Traffic Diversion Analysis	B.S. Civil Engineering; M.S. Civil Engineering	28
Ken O'Brien, PE Senior Project Manager	HDR	Traffic Diversion Analysis	B.S Civil Engineering;	27
Audrey Heffernan Senior Environmental Planner	HDR	Environmental Justice	B.A. Math; M.A. Math; M.S. City & Regional Planning	28
Connie Eskin Administrative Coordinator	HDR	Technical Editor	Pennsylvania State University	25
Tina Adair Technical Editor	HDR	Technical Editor	B.S. Communications	35
Frank Brilhante GIS Manager	HDR	GIS Analysis	B.S. Engineering; M.S Environmental Engineering	28
Ben Reiman Sr. Project Manager	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.S. Environmental Science	24
Margaret Parker Sr. Historian/ Project Manager	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Cultural Resources	B.A. History	30
Gabrielle Vicari Historian	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Cultural Resources	M.A. Historic Preservation	8
Connor Sullivan Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	M.S. Environmental Science	9
Alyssa Hyziak Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.A. Anthropology	6
Terri Slack National Discipline Lead, Trans. Revenue Systems & Operations	CDM Smith	Traffic Forecasting	BA Economics; BA Political Science M.B.A Management	33
Tarannum Rima Travel Demand Modeler	CDM Smith	Traffic Forecasting	B.S. Civil Engineering M.S Transportation Engineering M.S. Computer Systems Engineering	16
Nathaniel Weinstock Air Quality and Acoustical Group Leader, Sr. Air Quality and Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis	B.S. Public Service	22
Kyle Brubaker SR. Environmental Specialist, TD Environmental Task Leader	Navarro & Wright	Hazardous Materials	B.S. Environmental Science	13

Name	Organization	EA Role	Education	Years
Robert C. Kolmansberger Director of Environmental Services, Sr. Air Quality & Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis, QA/QC	B.A. Geography & Environmental Planning	30



## **Appendix I**

### **References**

## REFERENCES

- Ball, M. 2015. MIT Study: Traffic Jams Magnify How Roads Affect Fuel Consumption. *Informed Infrastructure: The magazine for civil & structural engineers*. Available at: <https://informedinfrastructure.com/16484/mit-study-traffic-jams-magnify-how-roads-affect-fuel-consumption/>
- Lotus Environmental Consulting. 2022. I-80 White Haven Bridges, EO9 White Haven Historical Background. Memorandum to PennDOT District 5.0 Architectural Historian.
- PennDOT (Pennsylvania Department of Transportation). 2005. Publication 217: Pennsylvania Department of Transportation Community Impact Assessment Handbook. Available at: <http://www.dot.state.pa.us/public/pubsforms/Publications/PUB%20217.pdf>
- PennDOT. 2008. Publication 640 - Indirect and Cumulative Effects (ICE) Desk Reference. Available at: <http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20640.pdf>
- PennDOT. 2014. Publication 756 - Pennsylvania Department of Transportation Invasive Species Best Management Practices. Available at: <http://www.dot.state.pa.us/public/pubsforms/Publications/PUB%20756.pdf>
- PennDOT. 2015. Publication 325 - Wetland Resources Handbook. Available at: <http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20325.pdf>
- PennDOT. 2016. Publication 324 - The Transportation Project Development Process: Agricultural Resources Evaluation Handbook. Available at: <https://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20324.pdf>
- PennDOT. 2016. Publication 746 - Project Level Environmental Justice Guidance. Available at: <http://www.dot.state.pa.us/public/pubsforms/Publications/Pub%20746.pdf>
- PennDOT. 2017. Publication 745 - Section 6(f), Project 70, Project 500, and Other Recreation Grant Guidance. October 2017 Update of April 2014 Edition. Available at: <http://www.dot.state.pa.us/public/pubsforms/Publications/PUB%20745.pdf>
- PennDOT. 2018. Publication 349 – Pennsylvania Department of Transportation Section 4(f)/Section 2002 Handbook. January 2018 Update of August 2014 Edition. Available at: <http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20349.pdf>
- PennDOT. 2018. Publication 783 - Environmental Permitting Handbook. Available at: <https://www.dot.state.pa.us/public/pubsforms/Publications/PUB%20783.pdf>
- PennDOT. 2019. Publication 10B: Design Manual Part 1B (Post-TIP NEPA Procedures). November 2015 Edition, July 2019 Change No. 1. Available at: <https://www.dot.state.pa.us/public/PubsForms/Publications/PUB%2010/Pub%2010B/July%202019%20Change%20No%201.pdf>
-

*SR0080 Section 08B, I-80 Over Lehigh River Bridges Project*  
**White Haven Borough, Luzerne County**  
**East Side Borough and Kidder Township, Carbon County**

PennDOT. 2019. Publication 24: Pennsylvania Department of Transportation Project Level Highway Traffic Noise Handbook. Available at:  
<http://www.dot.state.pa.us/public/pubsforms/Publications/PUB%2024.pdf>

PennDOT. 2019. Publication 281: Waste Site Evaluation Procedures Handbook: The Transportation Project Development Process. Available at:  
<http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20281.pdf>

PennDOT. 2020. Publication 10X: Design Manual Part 1X (Appendices to Design Manuals 1, 1A, 1B, and 1C). November 2015 Edition, May 2020 Change No. 3. Available at:  
<https://www.dot.state.pa.us/public/PubsForms/Publications/PUB%2010/Pub%2010X/May%202020%20Change%20No.%203.pdf>

PennDOT. 2020. Publication 319: Needs Study Handbook. Available at:  
<http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20319.pdf>

PennDOT. 2020. Publication 321. Pennsylvania Department of Transportation Project-Level Air Quality Handbook. Available at:  
<http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20321.pdf>

PennDOT. 2020. Publication 546 - Threatened and Endangered Species Desk Reference. January 2020 Update of December 2013 Edition. Available at:  
<http://www.dot.state.pa.us/public/pubsforms/Publications/PUB%20546.pdf>

PennDOT. 2021. Publication 295: Project Level Public Involvement Handbook. May 2021 Update of November 2011 Edition. Available  
at: <http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20295.pdf>

PennDOT. 2021. Publication 689 – The Transportation Project Development Process: Cultural Resources Handbook. Available at:  
<http://www.dot.state.pa.us/public/PubsForms/Publications/PUB%20689.pdf>

PennDOT. 2022. Transportation Improvement Program: Four & Twelve Year Plans (maps). Available at:  
<https://gis.penndot.gov/paprojects/TipVisMap.aspx>

Treiber, M., A. Kesting, and C. Thiemann. 2008. How Much Does Traffic Congestion Increase Fuel Consumption and Emissions? Applying Fuel Consumption Model to NGSIM Trajectory Data. Transportation Research Board (TRB) 87th Annual Meeting, January 13–17, 2008, Paper No. 08-2715. Available at: <https://trid.trb.org/view/848721>

Wikipedia. 2022. “White Haven, Pennsylvania.” Available at:  
[https://en.wikipedia.org/wiki/White\\_Haven,\\_Pennsylvania#Establishment](https://en.wikipedia.org/wiki/White_Haven,_Pennsylvania#Establishment)

---