

Level 2 Categorical Exclusion Evaluation

SR0080 Section 550

I-80 North Fork Bridges Project

Brookville Borough and Pine Creek Township, Jefferson County

October 2022

Prepared by:
District 10-0
2550 Oakland Avenue
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LEVEL 2 CATEGORICAL EXCLUSION EVALUATION
for the
SR 0080 SECTION 550 JEFFERSON COUNTY
INTERSTATE 80 NORTH FORK BRIDGES PROJECT

MPMS #106275

Prepared by:
US Department of Transportation
Federal Highway Administration
and
Pennsylvania Department of Transportation
Engineering District 10-0

Pursuant to 42 U.S.C. 4332(2)(c) and, as applicable:
Executive Order 11990, Protection of Wetlands; Executive Order 11988, Floodplain Management;
Executive Order 12898, Environmental Justice; and 49 U.S.C. Section 303(c), Section 4(f)

Level 2 CE Approval

As supported by the attached Categorical Exclusion, this project qualifies for a Level 2 Categorical Exclusion in accordance with 23 CFR 771.117(d), Item Number 13. Furthermore, the project will not result in any of the four circumstances cited in 23 CFR 771.117(b).

County: Jefferson SR/Sec: 0080/550 MPMS: 106275 Project: I-80 North Fork Bridges

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ACRONYMS AND ABBREVIATIONS

ABACT	Antidegradation Best Available Combination of Technologies
ALCAB	Agricultural Lands Condemnation Approval Board
APE	Area of Potential Effect
ATON	Aids to Navigation
BMPs	Best Management Practices
CE	Categorical Exclusion
CFR	Code of Federal Regulations
CRPs	Cultural Resource Professionals
CWF	Cold Water Fishes
DCNR	Department of Conservation and Natural Resources
DEP	Department of Environmental Resources
E&S	Erosion and Sedimentation
EA	Environmental Assessment
EB	Eastbound
ECMTS	Environmental Commitments & Mitigation Tracking System
EDD	Environmental Due Diligence
ESA	Environmental Site Assessment
ESF	Environmental Stewardship Fund Act
EV	Exceptional Value
ft	feet
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FIRM	Flood Insurance Rate Map
FPPA	Farmland Protection Policy Act
GG2	Growing Greener Bond Fund
H&H	Hydrology and Hydraulics
HOV	High Occupancy Vehicle
HQ	High Quality
HQ-CWF	High Quality-Cold Water Fishes
I-79	Interstate 79
I-80	Interstate 80

I-99	Interstate 99
Key 93	Keystone Recreation, Park and Conservation Fund
LOD	Limits of Disturbance
LWCF	Land and Water Conservation Fund
MBP3	Major Bridge Public Private Partnership
MF	Migratory Fishes
MIT	Massachusetts Institute of Technology
MPMS	Multimodal Project Management System
MPO	Metropolitan Planning Organization
MSATs	Mobile Source Air Toxics
mvm	million vehicle miles
NAC	Noise Abatement Criteria
NAAQS	National Ambient Air Quality Standards
NB	Northbound
NEPA	National Environmental Policy Act
NESHAP	National Emission Standards for Hazardous Air Pollutants
NPDES	National Pollutant Discharge Elimination System
NPS	National Park Service
NRCS	Natural Resources Conservation Service
NRHP	The National Register of Historic Places
NSAs	Noise Sensitive Areas
OHWM	Ordinary High-Water Mark
OSHA	Occupational Safety and Health Administration
P3	Public Private Partnership
PA	Pennsylvania
PADEP	Pennsylvania Department of Environmental Protection
PAGWIS	Pennsylvania Groundwater Information System
PASPGP	Pennsylvania State Programmatic General Permit
PCSM	Post Construction Stormwater Management
PEL	Alternative Funding Planning and Environmental Linkages Study
PEM	Palustrine Emergent
PennDOT	Pennsylvania Department of Transportation

PFBC	Pennsylvania Fish and Boat Commission
PFO	Palustrine Forested
PNDI	Pennsylvania Natural Diversity Inventory
PSA	Project Study Area
PSS	Palustrine Scrub Shrub
RFFAs	Reasonably Foreseeable Future Actions
RIRA	Recreational Improvement and Rehabilitation Act
ROW	Right-of-Way
SB	Southbound
s.f.	square feet
SR	State Route
TCE	Temporary Construction Easement
TIP	Transportation Improvement Program
TMC	Traffic Management Center
TNM	Traffic Noise Model
TNW	Traditional Navigable Waters
TOYRs	Time-of-Year Restrictions
TSF	Trout Stocked Fishes
TYP	Twelve Year Program
UE	United Electric REA
USACE	United States Army Corps of Engineers
USDA	United States Department of Agriculture
USDOT	United States Department of Transportation
USFWS	United States Fish and Wildlife Service
USGS	United States Geological Survey
UST	Underground Storage Tank
UNT	Unnamed Tributary
VMT	Vehicle Miles Traveled
WB	Westbound
WSE	Water Surface Elevation
WUS	Waters of the United States
WWF	Warm Water Fishes

1.0 INTRODUCTION

This Level 2 Categorical Exclusion (CE) Evaluation has been prepared to replace the Environmental Assessment (EA) previously made available on April 18, 2022, because PennDOT is no longer going to toll the Interstate 80 (I-80) North Fork Bridges. This CE compares the effects of the Build Alternative without tolling to the No Build (or do nothing) Alternative.

Supporting documentation for Chapter 1 includes:

- *Alternative Funding: Planning and Environmental Linkages Study (September 2021)*

Project History

In fall of 2020, PennDOT began a statewide Planning and Environmental Linkages (PEL) study to identify potential funding options to fill an \$8.1 billion (and growing) funding gap for maintaining and improving the State's highways and bridges. The *Alternative Funding PEL Study* identified near-term and long-term potential funding solutions that could be implemented. Tolling major bridges and using the toll money to cover the costs of rehabilitating or replacing and maintaining the bridge over a period of time was identified as a near-term solution that could be implemented relatively quickly. In February 2021, PennDOT identified nine candidate bridges for tolling, one of which was the I-80 North Fork Bridges project.

Upon identification as a candidate bridge, the effects of tolling the I-80 North Fork bridges were evaluated, including: effects on low-income persons using the bridges, effects associated with constructing toll equipment, and effects associated with people choosing to divert onto local roadways to avoid paying the toll. A low-income program was adopted to off-set effects on low-income persons and improvements along diversion routes were incorporated into the project to off-set the effects on local roadways. Diversion route improvements included:

- Install a traffic control system connecting intersections along the primary diversion route to the PennDOT regional traffic management center (TMC), including monitoring cameras and possibly including adaptive signal control, to permit PennDOT to remotely monitor and adjust traffic signal operations.
- In conjunction with the recommendation above, upgrade the following traffic signals to meet current PennDOT and municipal standards, including pedestrian accommodations, phasing upgrades where warranted, detection, and monitoring cameras.
 - SR 36 at I-80 WB Ramps
 - SR 36 at I-80 EB Ramps
 - SR 36 (Allegheny Boulevard) at SR 28/SR 322
 - SR 28/SR 322 at SR 36 (White Street)
 - SR 322 at Pickering Street
 - SR 322 at Richardsville Road

Replace all signal equipment, including controllers, poles, mast arms, and conduit, using decorative poles within the Brookville Historic District.

- Install a center left turn lane on SR 322 (West Main Street) including the intersections with SR 28/SR 36 (Allegheny Boulevard) and Progress Street as well as intervening driveways and local streets.
- Improve sidewalks along Jenks Street between the Brookville Area schools and Valley Street to provide a continuous, accessible pedestrian route along at least one side of the street, replacing or repairing sidewalk where necessary and installing missing sidewalks.

- Improve sidewalks along Madison Avenue between West Main Street and Franklin Ave to provide a continuous, accessible route along at least one side of the street, replacing or repairing sidewalk where necessary and installing missing sidewalks.
- Install rectangular rapid-flashing beacons (RRFB) at the existing uncontrolled crosswalks of Jenks Street just south of the I-80 overpass, and of Valley Street at Jenks Street near the Rebecca N. Arthurs Memorial Library.
- Provide advanced warning signage on I-80 WB approximately 2 miles in advance of the Exit 81, SR 28 (Hazen Interchange), advising trucks of steep grades and narrow lanes in Brookville Borough, and recommending use of Exit 78 to SR 36 (Brookville Interchange).
- Add a 100-foot left turn lane to the WB off-ramp from I-80 at SR 28.

An EA comparing the effects of the No Build Alternative and the Build Alternative with bridge tolling was prepared and was made available for official public review and comment on April 18, 2022. A Public Hearing was held on May 3, 2022.

On May 18, 2022, as a result of a lawsuit, the court issued an injunction and all work related to the Major Bridge Public Private Partnership (MBP3) initiative ceased. Other litigation resulted in a ruling on the viability of the MBP3 as a Public-Private Transportation Project (P3). Subsequently, Act 84 of 2022 amended the P3 law and revoked PennDOT’s ability to implement mandatory tolls such as the proposed bridge tolling under the MBP3, but preserved the contract resulting from the MBP3.

As a result of the lawsuits and the subsequent enactment of Act 84 of 2022, **PennDOT is moving the I-80 North Fork Bridges project forward, but without tolling. Since tolling will not be initiated, diversion of traffic onto local roads to avoid the tolls will not occur; therefore, the proposed improvements along the diversion routes will no longer be included in the project.**

The PennDOT MBP3 was established to accelerate the replacement or rehabilitation of major bridges. Under MBP3, PennDOT entered into an agreement with a Development Entity to design, build, finance, and maintain (DBFM) a “package” (or group) of PennDOT bridges – including the I-80 North Fork Bridges. PennDOT will repay the amounts financed by the Development Entity through recurring availability payments over 30 years. Act 84 of 2022 authorizes the bridges identified in the MBP3 to be carried out via DBFM by the Development Entity, without mandatory tolling.

Funding to make the availability payments will consist of a blend of federal and state funds that could have been used for other projects. PennDOT will take advantage of additional funding opportunities arising out of the federal Infrastructure Investment Jobs Act (“IIJA”), also known as the Bipartisan Infrastructure Law (“BIL”) and potentially supplemented by funds that are currently included in the outer years of the Twelve Year Program (TYP) or by the deferral or elimination of some other TYP projects. NOTE: IIJA (BIL) funding was not available at the start of MBP3, but those additional funding sources will provide additional opportunities for PennDOT to pursue the Build Alternative without tolling with less effect to other projects.

This CE documents and compares the effects associated with the No Build Alternative and the Build Alternative without tolling. Effects associated with constructing tolling equipment, improving diversion routes, and paying tolls have been removed from the document.

The comments received during the EA comment period (April 18 to May 18, 2022), including testimony and comments received at the public hearing, have been reviewed and considered. The overwhelming majority of

comments received during the EA comment period were related to tolling and diversion of traffic and are no longer applicable to the project since tolling is no longer being implemented. Comments received on the EA relevant to the project without tolling were considered and additional information incorporated into the respective sections within this document as appropriate. A comment regarding lighting at the Brookville interchange, which is part of the adjacent I-80 Brookville East Project, was received and provided to that project's team for consideration.

2.0 I-80 NORTH FORK BRIDGES PROJECT OVERVIEW

2.1 Project Bridges

PennDOT, in cooperation with the FHWA, is advancing the replacement of two bridges carrying Interstate 80 (I-80), Section 550 Westbound (WB) and Eastbound (EB) over North Fork Redbank Creek and Water Plant Road in Brookville Borough and Pine Creek Township, Jefferson County, Pennsylvania. In addition, the I-80 bridges over State Route (SR) 4003 to the west of the project and the SR 4005 bridges over I-80 to the east will be replaced. The I-80 WB bridge is a 7-span steel girder/floorbeam/stringer and steel multi-beam bridge (BMS 33-0080-0795-1290, Bridge Key 19566) that carries I-80 WB over North Fork Redbank Creek and Water Plant Road. The I-80 WB bridge is 1,076.48 feet (ft) long. The I-80 EB bridge is a 1,082.00-ft long, 8-span steel girder/floorbeam/stringer and steel multi-beam bridge (BMS 33-0080- 0794-1178, Bridge Key 19565) that carries I-80 EB over North Fork Redbank Creek and Water Plant Road. **Figure 1 – Project Location Map** shows the location of the I-80 North Fork bridges and the Project Study Area (PSA).

Approximately 15,500 vehicles (approximately 43% trucks) travel I-80 in each direction at this location every day. During construction, two lanes of traffic will need to be maintained at all times on I-80 WB and I-80 EB.

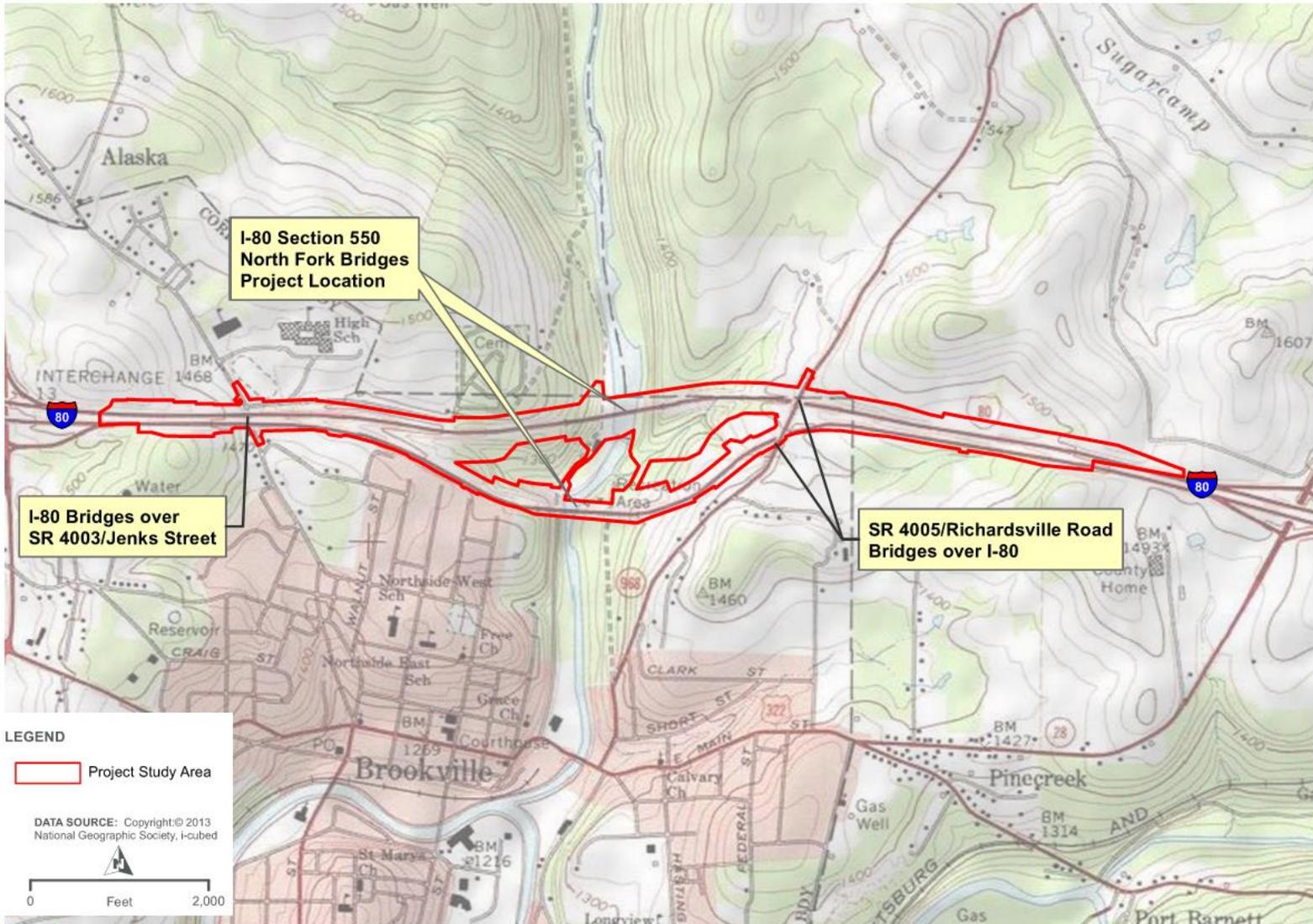
2.2 Project Purpose and Needs

Purpose: The purpose of the project is to provide safe, efficient, and effective crossings of I-80 over North Fork Redbank Creek and Water Plant Road that appropriately accommodate interstate traffic with respect to connectivity, mobility, loading, and geometry.

Needs: The project need is based on addressing the condition of the I-80 bridges and the geometric deficiencies of I-80 as follows.

Structure Deficiencies: The existing I-80 WB and EB bridges have several deficiencies. The main span of each bridge is a steel two girder system with floorbeams and stringers and lacks required load path redundancy. Both bridges possess problematic fatigue details which have received multiple retrofits during the service lives of the structures. Both bridges have fatigue cracking of the main girders, floorbeams, and web-to-stiffener welds related to the out-of-plane girder bending. Arrest holes have been drilled in the girders and floorbeams to stop the fatigue cracking. There are several locations of unarrested cracks in the toe of the weld at the top of the stiffener of the main girders.

Standard inspection frequency for bridge structures is 24 months. However, in the case of North Fork bridges, the EB bridge is in poor condition and on a 6-month inspection frequency schedule and the WB bridge is in satisfactory condition and on a 12-month inspection frequency schedule. The existing structure type, fatigue details, and frequency of inspection further underline the urgency to address these issues.



LEGEND

Project Study Area

DATA SOURCE: Copyright © 2013
National Geographic Society, I-cubed


 0 Feet 2,000

**I-80 NORTH FORK BRIDGE REPLACEMENT PROJECT
PROJECT LOCATION MAP**



PATH: G:\JOBS\PENNDOT\I-80 NORTH FORK\MAP_DOCS\DRIFT\PENNDOT_NORTH FORK_FIG1 PROJECT LOCATION MAP_07272022.MXD - USER: LSMITHHA - DATE: 8/29/2022

FIGURE 1

Geometric Deficiencies: I-80 is currently posted at 70 miles per hour (mph) in the vicinity of the project. The existing I-80 EB horizontal alignment contains three horizontal curves and two vertical sag curves in the vicinity of the existing bridge that do not meet the required 70 mph design speed for interstates. The existing I-80 WB horizontal alignment meets the required criteria for 70 mph; however, the two vertical sag curves in the vicinity of the existing bridge do not meet the required criteria. The existing vertical grades along I-80 WB and along the west approach of I-80 EB in the vicinity of the bridges are approximately 4%, which is the maximum grade per design requirements for interstates.

Each existing bridge carrying I-80 over North Fork Redbank Creek and Water Plant Road accommodates two 12 ft lanes and 4 ft shoulders (32 ft curb-to-curb) which are narrower than the approach roadway sections. The approach roadway sections of I-80 EB and WB accommodate two 12 ft lanes, an 8 ft inside shoulder, and a 10 ft outside shoulder (42 ft curb-to-curb).

Within the project limits, I-80 has an average crash rate of 0.57 crashes/million vehicle miles (mvm), which is over 1 1/2 times the statewide average of 0.36 crashes/mvm. A total of 62 crashes occurred between 2015 and 2019 with no fatalities.

2.3 Project Setting and Distinct Project Features

The project is located to the north of Brookville. North Fork Redbank Creek is used for recreation within the project area. Walter Dick Memorial Park is located between the I-80 bridges, with two fishing access points to the north of the I-80 WB bridge. A municipal water treatment plant is located to the north of the I-80 WB bridge. Farmland is located between I-80 WB and I-80 EB on the east side of the bridges. The farmland is accessed from SR 4005. Brookville Cemetery and Brookville High School/athletic complex are located within the northwest quadrant of the I-80 WB bridge. A previously recorded prehistoric archaeological site is located in the vicinity of the southeast quadrant of the EB bridge.

Describe the involvement with utilities with this project:

Utilities in the area include underground electric, gas, water, and sanitary sewers, as well as aerial electric, communication, and fiber optic lines. Notable gas lines include an 8-in high pressure gas main that crosses under I-80 just to the west of SR 4003 and an 8-in medium pressure gas main that runs parallel to and along the southern right-of-way (ROW) line of I-80 EB between SR 4003 and SR 4005. Sanitary sewer lines are located along SR 4003, SR 4005, Water Plant Road, and on both sides of I-80 EB/WB in the vicinity of and crossing under I-80 around Sta 372+50. Water lines are located along SR 4003 and Water Plant Road, and on both sides of I-80 EB/WB in the vicinity of and crossing under I-80 around Sta 377+00. Aerial electric, communication, and fiber optic lines are located along SR 4003, SR 4005, and Water Plant Road. Aerial electric lines are located north of and parallel to I-80 WB and along the west side of Water Plant Road and parallel to North Fork Redbank Creek. The aerials that parallel I-80 WB are owned by United Electric REA (UE), provide 12.47 kV service to the Brookville Water Authority, and are located within a 50-foot ROW. The aerials that run along the west side of Water Plant Road are owned by Penelec and provide a three-phase 34.5 kV service to the Brookville Reservoir and park property. A virtual preliminary utility coordination meeting with the utility companies in the project limits was held on January 29, 2021.

Describe the involvement with any railroad (active or inactive) including all rail lines, crossings, bridges, or signals:

There would be no involvement with active or inactive railroads.

Describe changes to access control:

No changes to access control are needed.

3.0 ALTERNATIVES

3.1 No-Build Alternative

Under the no-build alternative, regular maintenance would be assumed to occur. This alternative would fail to address other project needs such as fixing the nonredundant critical elements of the bridge and addressing the identified bridge and roadway deficiencies. The I-80 North Fork Bridges are nearing the end of their useful life. Currently, the EB bridge is in poor condition and the WB bridge is in satisfactory condition, and both require more frequent inspections than the standard 24-month schedule. Without replacement, these bridge structures will need more frequent maintenance and repairs. However, such maintenance can only extend the service life of these bridges for so long before they are at risk of failure.

I-80 is the longest east-west interstate in the Commonwealth of Pennsylvania. Within Pennsylvania, I-80 extends 311 miles across the northern tier of Pennsylvania, providing access to Ohio and Midwestern states to the west and New Jersey, the New York City Metropolitan Area and New England to the east. In the project area, the I-80 corridor is the only interstate serving the local area. Interstate 79 (I-79) and Interstate (I-99) are about an hour drive west and east, respectively. As a critical link in daily travel and the regional and national highway network, allowing the deterioration of these bridges to reach a level of failure is not reasonable; therefore, due to the project needs, the no-build alternative would not be a reasonable alternative.

The no-build alternative is presented in this CE as a baseline for comparison purposes only.

3.2 Proposed Action

The project will consist of the replacement of two bridges carrying I-80, Section 550 WB and EB over North Fork Redbank Creek and Water Plant Road in Brookville Borough and Pine Creek Township, Jefferson County, Pennsylvania. In addition, the I-80 bridges over SR 4003 (Jenks Street) to the west of the project and the SR 4005 (Richardsville Road) bridges over I-80 to the east will be replaced. Two lanes of traffic in both directions of I-80 will be maintained during construction.

A detour for users of Jenks Street is needed during the replacement of the I-80 bridges over SR 4003 (Jenks Street) and is anticipated to be in place for approximately one construction season. This 3.1-mile detour route would use SR 0028 (West Main Street), SR 0036 (Allegheny Boulevard) and SR 0322 (US 322; West Main Street) (see map in Appendix A). PennDOT will continue to coordinate with the Brookville Area School District and EMS providers throughout the design and construction process.

A detour for users of Richardsville Road is also anticipated during the replacement of the Richardsville Road bridges over I-80 and is anticipated to be in place for approximately one construction season. The detour route would use SR 0322 (US 322, East Main Street), SR 0028 and T-430 (Butler Cemetery Road) and would be approximately 5.3 miles overall (see map in Appendix A). No improvements or impacts to roads on the detour routes are anticipated; however, PennDOT will assess the conditions of T-430 (Butler Cemetery Road) before and after construction and make repairs if warranted.

**Table 1
Construction Station and Length**

Limits of Work (Segment/Offset)		Construction Stations	
Start:	End:	Start:	End:
0785/1578 (WB)	0811/0617 (WB)	333+50.00 (WB)	456+50.00 (WB)
0784/1258 (EB)	0810/0344 (EB)	332+00.00 (EB)	451+25.00 (EB)
Total Length:			
12,088 ft (WB); 11,732 ft (EB)			

I-80 North Fork Bridges

The proposed bridges both consist of one 12-ft inside shoulder, two 12-ft lanes, one 20-ft outside shoulder, and two 1'-8¼" wide by 45" tall barriers. The curb-to-curb width is 56 ft and the out-to-out width is 59'-4½" due to the required future redecking.

I-80 bridges over SR 4003

The proposed bridges both consist of one 11-ft inside shoulder, two 12-ft lanes, one 27-ft outside shoulder, one 1'-8¼" wide by 45" tall barrier on the inside of the bridge and one 2'-2¼" wide by 45" tall barrier on the outside of the bridge which will accommodate the sound wall barrier. The curb-to-curb width is 62 ft and the out-to-out width is 65'-10½" due to the required future redecking.

SR 4005 bridge over I-80

The proposed bridge consists of two 11-ft lanes, a 6-ft left shoulder and a 4-ft right shoulder. The curb-to-curb width is 32-ft and the out-to-out width is 35'-4½".

Right-of-Way (ROW) and driveway improvements for bridges over SR 4003 and SR 4005

In order to maintain two lanes of traffic in each direction during construction, the I-80 bridges for both EB and WB over SR 4003 will be replaced. To meet the required vertical clearance requirements over SR 4003, the horizontal alignment along SR 4003 will be maintained and the profile along SR 4003 will be lowered by approximately 1.8 ft. As a result of lowering SR 4003, several driveway adjustments along SR 4003 will be required. The existing grades along Driveways 3, 4 and 5 range from 10 to 20 percent. Driveway 3 on Parcel 2 will be lowered approximately 1 foot and will likely impact the steps that lead up to the adjacent residence. Driveway 4 (a secondary driveway also on Parcel 2) will be lowered approximately 2 ft, however most of this driveway is located within Limited Access ROW that will likely be needed to construct proposed I-80 EB. Driveway 5 will be lowered approximately 1.5 ft. All of these driveways will be adjusted to tie to the lower profile along SR 4003, and the proposed driveway grades will be similar to the existing grades. Details associated with these driveway adjustments will be determined in final design.

Due to the realignment of I-80 EB closer to I-80 WB, the bridges carrying SR 4005 over I-80 EB and WB will be replaced with a new 2-span bridge that will span over the realigned interstate. To meet the vertical clearance requirements over I-80, the horizontal alignment along SR 4005 will be maintained and the profile along SR 4005 will be raised by approximately 1.5 ft. The existing grades along Driveways 6 and 7 range from approximately 15 to 20 percent. Both of these driveways will be adjusted to tie to the raised profile along SR 4005, and the proposed driveway grades and will be similar to the existing grades.

Based on the current design, required ROW for limited access, permanent slope, drainage, and channel easements, and temporary construction easements (TCEs) are anticipated along I-80 EB and WB. Twenty-one parcels are affected, 19 with partial permanent takes and 2 parcels with TCE only. A required aerial easement is

anticipated along and to the south side of the new mainline bridge carrying I-80 EB over the Brookville Municipal Authority property, North Fork Redbank Creek, and Water Plant Road. Temporary construction easements and drainage or channel easements are anticipated through several of the properties located between existing I-80 EB and I-80 WB in order to construct access roads down into the North Fork Redbank Creek gorge and to construct the proposed embankments and piers. Sliver takes of required ROW and TCEs are anticipated along both SR 4003 and SR 4005 for roadway and driveway improvements.

Additional information is provided in Appendix A – Engineering Information and Appendix B – Preliminary Design Plans.

3.3 Impact Summary Table

**Table 2
Impact Summary Table**

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
Aquatic Resources			
Streams, Rivers, & Watercourses	No Impact	Streams: HQ-CWF, stocked trout 5,378 linear ft permanent impact 1,786 linear ft temporary impact	No work will be permitted in stocked trout streams from February 15 to June 1. Antidegradation Best Available Combination of Technologies (ABACT) Erosion and Sediment (E&S) Controls will be implemented. Compensatory mitigation will be determined with consultation with permitting agencies in final design and incorporated into the waterway permit application.
Wild & Scenic Rivers and Streams	Not Present	Not Present	None
Navigable Waterways	No Impact	Recreational Boating Waterway: canoeing No permanent impacts to stream navigability. Temporary impacts during construction.	Aids to Navigation (ATON) Plan to be implemented during construction
Groundwater	No Impact	Brookville Reservoir intake upstream, no impact. Two seeps in project area south of I-80 impacted; No groundwater resources developed for drinking water impacted.	None

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
Wetlands	No Impact	Wetlands: 0.163 acre permanent impact 0.009 acre temporary impact	For permanent impacts, credits debited from PennDOT's Dubois-Jefferson County Wetland Bank Site.
Floodplains	No Impact	No significant floodplain encroachment would occur.	None
Soil Erosion and Sedimentation	No Impact	E&S Control Plan will be implemented during construction.	Best Management Practices (BMPs) and E&S Control Plan will be defined, implemented, inspected and maintained. All areas of earth disturbance will be stabilized immediately following completion of earthwork Post Construction Stormwater Management (PCSM) controls will be evaluated in final design and included in the National Pollutant Discharge Elimination System (NPDES) permit application, if required.
Land Use			
Agricultural Resources	No Impact	Productive Agricultural Land: 0.276 acre Prime Farmland Soils: 0.35 acre Soils of Statewide Importance: 2.74 acres	Property owners will be compensated fair market value for impacted agricultural land. Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
			1964; and the Pennsylvania Eminent Domain Code of 1964.
Vegetation	No Impact	Wooded, landscaped, agricultural land, and roadside vegetation impacted.	Care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. All disturbed areas will be restored and revegetated with non-invasive vegetation as part of construction.
Geologic Resources	Not Present	Not Present	None
Parks and Recreation Facilities	No Impact	Walter Dick Memorial Park 1.2 acres permanent impact; 5.5 acres temporary impact 6(f): 6.7 acres conversion	Coordination with Brookville Borough (the official with jurisdiction for the park) regarding the proposed TCEs will continue throughout construction. Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions. The portion of the nature trail that is to be impacted will be relocated. A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
			Replacement land for 6(f) conversion will be purchased in accordance with Land and Water Conservation Fund (LWCF) policy.
State Forest and Gamelands	Not Present	Not Present	None
Wilderness, Natural, & Wild Areas	Not Present	Not Present	None
Hazardous or Residual Waste Sites	No Impact	<p>Potential for isolated pockets of residual petroleum contamination in soils within the ROW as a result of numerous vehicle crashes.</p> <p>Potential asbestos and lead in difficult-to-access portions of bridge structures.</p>	<p>Phase III Environmental Site Assessment (ESA) will be conducted in final design. Special provisions for managing waste materials will be developed and implemented during construction.</p> <p>Construction activities that disturb bridge paint should be performed in accordance with Occupational Safety and Health Administration (OSHA) requirements. All structures to be demolished should be inspected for asbestos-containing materials as per the National Emission Standards for Hazardous Air Pollutants (NESHAP).</p>
Wildlife			
Wildlife Refuges & Critical Habitat	Not Present	Not Present	None
Threatened & Endangered Species	Not Present	Potential impact to tri-colored bat	USFWS coordination for tri-colored bat: During final design, the project team will

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
			initiate conferencing with USFWS regarding the project's potential effects to the tri-colored bat and measures to avoid and minimize harm.
Cultural Resources			
Archaeological Resources	No Historic Properties Affected	No Historic Properties Affected	The Stonewall Spring site (36JE0201) and Haugh site (36JE0082) will be fenced off and avoided during construction. In addition, a metal plate will be placed over the open well at the Stonewall Spring site to mitigate any safety concerns and protect the resource.
Historic Resources	No Historic Properties Affected	No Historic Properties Affected	None
Section 4(f) Resources	No Impact	Walter Dick Memorial Park – <i>De minimis</i> Use 1.2 acres permanent impact	Coordination with Brookville Borough (the official with jurisdiction for the park) regarding the proposed TCEs will continue throughout construction. Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions. The portion of the nature trail that is to be impacted will be relocated.

Impact Summary Table Environmental Resource Category	No-Build Alternative ¹	Proposed Action	Mitigation for Proposed Action
			A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.
Air Quality and Noise			
Air Quality	No Impact	Exempt; no impact	None
Noise	No Impact	Type I Project; predicted noise levels approach or exceed Noise Abatement Criteria (NAC) for NSA 1, 2, & 5.	Proposed sound barrier walls are considered both feasible and reasonable along I-80 EB and WB from a point just west of the I-80 over SR 4003 (Jenks Street) bridges to a point near the proposed I-80 EB and WB bridges over North Fork Redbank Creek and Water Plant Road to mitigate for impacts to NSA 1 & 2. Additional coordination and evaluation for the proposed sound barrier walls will continue in final design.
Socioeconomic Areas			
Regional & Community Growth	No Impact	No Impact	None
Public Facilities & Services	No Impact	Positive Impacts: Access for public facilities and services will be improved due to design improvements resulting from the project.	None

Impact Summary Table Environmental Resource Category	No-Build Alternative¹	Proposed Action	Mitigation for Proposed Action
Community Cohesion	No Impact	No impact	None
Right-of-Way Acquisitions	No Impact	21 parcels: 19 parcels with partial takes and 2 parcels with TCEs only	Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.
Displacements	No Impact	No relocation of people, businesses, or farms	None
Aesthetics	No Impact	No impact	None
Environmental Justice	No Impact	No disproportionately high and adverse effects on low-income or minority populations have been identified.	None

¹ While the no-build alternative would not directly affect resources, should the bridge deteriorate to the point where it would have to be weight-posted, closed, or should it experience a partial collapse, there would be impacts to the resources below the bridge. A full or partial closure would have a profound effect on commerce reliant on I-80 and would detour vehicles onto local roads.

4.0 AFFECTED ENVIRONMENT AND ENVIRONMENTAL CONSEQUENCES

4.1 Aquatic Resources

	PRESENCE	IMPACTS
STREAMS, RIVERS & WATERCOURSES	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Intermittent (streams only)	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Perennial	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Wild trout streams	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Stocked trout streams	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

Identify all streams and their classifications per Chapter 93 of 25 PA Code (e.g. CWF, WWF, HQ, EV)

North Fork Redbank Creek is identified as a National Wetlands Inventory riverine habitat (Cowardin classification R2UBH and PUBHh) and is designated as a High Quality-Cold Water Fishes (HQ-CWF) resource per Pennsylvania Title 25, Chapter 93 Water Quality Standards. Per the Pennsylvania Fish and Boat Commission (PFBC) and Chapter 93 Water Quality Standards, North Fork Redbank Creek is not identified as supporting migratory fishes or wild trout populations; however, it is designated by the PFBC as a stocked trout stream. In-stream construction restrictions for Stocked Trout Waters (February 15th through June 1st) will be observed on North Fork Redbank Creek, S-02, and S-09.

Streams S-19, S-20, and S-21 contribute flow to the Mill Creek watershed via Tributary 48563 to Mill Creek (see Appendix C – Environmental Constraints Map). The Mill Creek Watershed is a sub-watershed of North Fork Redbank Creek. Tributary 48563 to Mill Creek and Mill Creek assume a designation of Cold Water Fishes (CWF) per Pennsylvania Title 25, Chapter 93 Water Quality Standards. Per the PFBC and Chapter 93 Water Quality Standards, neither Tributary 48563 to Mill Creek nor Mill Creek are identified as supporting migratory fishes or wild trout populations and are not approved trout waters; therefore, Streams S-19, S-20, and S-21 are not subject to in-stream construction restrictions.

Linear ft of Streams permanently impacted: 5,378

Describe Any Permanent Impacts

Permanent impacts associated with bridge replacement will include installation of bridge piers and scour protection within North Fork Redbank Creek as well as several stream relocations, culvert extensions, and impacts associated with cut and fill. (See Appendix C – Environmental Constraints Map.)

Describe Any Temporary Impacts

Approximately 1,786 linear ft of temporary stream impacts will result from the project. Temporary water access is anticipated for the demolition of the existing I-80 EB bridge and construction of the new I-80 EB and WB bridges, in-stream cofferdam diversions, sandbag diversion dikes, and concrete barrier cofferdams. (See Appendix C – Environmental Constraints Map.)

Supporting documentation for Chapter 4.1 includes:

- *I-80 North Fork Bridges Wetland and Watercourse Identification and Delineation Report* (October 2020)
- *I-80 North Fork Bridges Wetland and Watercourse Identification and Delineation Report (WWIDR) Addendum* (February 2021)
- *I-80 North Fork EB and WB Bridges H&H Report* (January 2021)

Is mitigation incorporated? No Yes

Proposed Project Specific Restoration/Enhancement: 5,378 linear ft

Mitigation Remarks

In-stream construction restrictions for Stocked Trout Waters (February 15th through June 1st) will be observed on North Fork Redbank Creek, S-02, and S-09.

Pennsylvania Code Title 25, Chapter 93 Water Quality Standards, identifies the management designation for North Fork Redbank Creek as HQ-CWF. Antidegradation Best Available Combination of Technologies (ABACT) Erosion and Sediment Controls should be specified for all projects within High Quality or Exceptional Value watersheds.

Compensatory mitigation is required for this project. Through consultation with permitting agencies, mitigation details will be determined in final design and incorporated into the waterway permit application. During final design, efforts to avoid and minimize impacts to the on-site aquatic resources will continue.

	PRESENCE	IMPACTS
FEDERAL WILD & SCENIC RIVERS & STREAMS	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Remarks	Review of the United States Geological Survey (USGS) Quadrangle and Federal Wild and Scenic Rivers System website has confirmed there are no federal wild and scenic rivers and streams within the project area.	

	PRESENCE	IMPACTS
STATE SCENIC RIVERS & STREAMS	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Remarks	Review of the USGS Quadrangle and the Department of Conservation and Natural Resources (DCNR) Scenic Rivers website has confirmed there are no state scenic rivers and streams within the project area.	

	PRESENCE	IMPACTS
NAVIGABLE WATERWAYS	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Coast Guard Navigable	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
PFBC Water Trail	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Recreational Boating Waterway	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

Documentation

- PFBC Aids to Navigation Plan
- Coast Guard Coordination

Describe Any Permanent and Temporary Impacts

North Fork Redbank Creek is listed by "Keystone Canoeing" (Edward Gertler 1993) as a canoeable waterway. Temporary and permanent impacts to North Fork Redbank Creek are anticipated. Permanent impacts associated with the installation of piers and scour protection should have no long-term impact to recreation in this area, as "Keystone Canoeing" suggests canoers recreate on the waterway portage upstream of, and around, the existing dam. Signage will be utilized in accordance with the Aids to Navigation (ATON) Plan to alert the canoers of the construction.

Is mitigation incorporated? No Yes

Describe Mitigation

An ATON Plan to be approved by the PFBC will be developed during final design in conjunction with the waterway permit and implemented during construction to alert boaters of temporary construction measures. This plan will be in place throughout construction.

Remarks

North Fork Redbank Creek is not identified by the PFBC as a water trail.

	PRESENCE	IMPACTS
OTHER SURFACE WATERS	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	

Remarks

Review of aerial imagery, eMapPA, topographic data, and National Wetland Inventory mapping, as well as an on-site investigation, did not identify any other surface waters within the project area.

	PRESENCE	IMPACTS
GROUNDWATER RESOURCES	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
State, County, Municipal or Local Public Supply Wells	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Residential Well	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Well Head Protection Area	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Springs, Seeps	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Potable Water Source	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Sole Source and/or Exceptional Value Aquifers	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

Describe Any Permanent and Temporary Impacts

The Brookville Reservoir and three seeps are located within the project area. Coordination was conducted with the Brookville Water Authority to determine the location of the freshwater intake in relation to the project. The freshwater intake is located upstream of all proposed construction activities and no impacts are anticipated. The Water Authority had no special requirements for the work being conducted within the area. Seeps are present south of I-80 EB. Two seeps are located north of stream S-02 and wetland W-03. One seep is located southwest of stream S-08. None of these seeps have been developed into potable water resources. The two seeps south of I-80 EB, north of stream S-02 and wetland W-03 will be permanently impacted. The seep southwest of stream S-08 will not be impacted (see Appendix C - Environmental Constraints Map). Project-related construction is not anticipated to have the potential to impact groundwater resources including quantity and quality of any residential wells.

Is mitigation incorporated? No Yes

Remarks

Review of the Pennsylvania Groundwater Information System (PaGWIS) website did not identify any private, residential wells with proximity to the project area.

	PRESENCE	IMPACTS
WETLANDS	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Open Water	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Vegetated Emergent	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

Scrub Shrub	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Forested	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Exceptional Value	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

Documentation

- Data Forms
- Wetland Identification and Delineation Report
- Conceptual Mitigation Plan
- 404 (b)(1) Alternative Analysis
- Jurisdictional Determination
- Functional Assessment Analysis

Methodology

A wetland and waterways delineation was conducted for the project. Areas identified with potential wetland indicators (hydric soils, hydrophytic vegetation, and hydrology), which were located waterward of the Ordinary High-Water Mark (OHWM) of the North Fork Redbank Creek, were classified as part of the Riverine system and not as a regulated palustrine wetland. The Wetland and Watercourse Identification and Delineation Report is included in the Project Technical File.

Number of Wetlands permanently impacted: 5

Acreage of Wetlands permanently impacted: 0.163

Describe Any Permanent Impacts

Permanent wetland impacts are anticipated as a result of permanent grading changes (cut and fill slopes). (See Appendix C – Environmental Constraints Map.)

Describe Any Temporary Impacts

Approximately 0.009 acre of temporary wetland impacts are anticipated as a result of temporary grading and access roads. (See Appendix C – Environmental Constraints Map.)

Is mitigation incorporated? No Yes

Project Specific Replacement/Construction:

Banking: 0.163 *acre*

Bank to be Debited: Dubois-Jefferson County Wetland Bank

Mitigation Remarks

- Temporary construction fencing will be placed around wetland boundaries not to be impacted by the project.
- Timber matting will be used to protect wetland crossings, when necessary.
- Graded areas will be returned to the original contour and the area seeded, mulched, and stabilized once construction in these areas is complete.

Executive Order 11990 Compliance

Compliance requires the determination that there is no practicable alternative to the proposed construction in wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from such use.

Options/design modifications were investigated to avoid impacts to wetlands: Yes No N/A

There are no practicable alternatives to construction within the wetlands: Yes No N/A

Alternative chosen (proposed project) includes all practicable measures to minimize harm to wetlands:

Yes No N/A

COASTAL ZONE	PRESENCE <input checked="" type="radio"/> Not Present <input type="radio"/> Present	IMPACTS <input checked="" type="radio"/> No <input type="radio"/> Yes
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Remarks

There are no coastal zones located within the project area.

FLOODPLAINS	PRESENCE <input type="radio"/> Not Present <input checked="" type="radio"/> Present	IMPACTS <input type="radio"/> No <input checked="" type="radio"/> Yes
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No significant floodplain encroachment would occur.

Describe Any Permanent and Temporary Impacts

North Fork Redbank Creek has an established 100-year floodplain; however, there would be no anticipated changes to the floodplain or floodway due to the proposed project. According to preliminary Hydrology and Hydraulics (H&H) reports and HEC-RAS modeling (v5.0.7), the proposed project will not result in statistically significant increases to water surface elevations (a <0.001% increase was seen to the 100-Year Federal Emergency Management Agency (FEMA) model for the North Fork Bridges structures). It was therefore determined the project will not result in significant floodplain encroachments or impacts, as defined by 23 CFR 650.105 (q) because the proposed project will not: (1) Have significant potential for interruption or termination of a transportation facility which is needed for emergency vehicles or provides a community's only evacuation route. (2) Pose a significant risk, or (3) Have a significant adverse impact on natural and beneficial floodplain values. The H&H report is in the project technical files.

Is mitigation incorporated? No Yes

SOIL EROSION & SEDIMENTATION

Are there activities that could cause erosion or sedimentation and would require E&S Controls? Yes No N/A

Documentation

- Coordination w/County Conservation
- District E&S Control Plan
- NPDES Stormwater Construction Permit

Is mitigation incorporated? No Yes

Mitigation:

- BMPs will be defined and implemented as a component of the E&S plan and waterway encroachment permit.
- The E&S Control Plan will be reviewed by the Pennsylvania Department of Environmental Protection (PADEP) and coordination will be conducted to ensure the selected BMPs are adequate for the project.
- The approved E&S Control Plan will be implemented prior to any earth disturbance, during construction.
- Installed BMPs will be inspected and maintained throughout the duration of construction.
- All areas of earth disturbance will be stabilized immediately following completion of earthwork.
- PCSM controls will be evaluated in final design and included in the NPDES permit application, if required.

4.2 Land

	PRESENCE	IMPACTS
AGRICULTURAL RESOURCES	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Productive Agricultural Land	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Agricultural Security Areas	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Prime Agricultural Land	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Conservation Easements	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Farmland Enrolled in Preferential Tax Assessments	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Zoning	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Soil Capability Classes I, II, III, IV	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Prime or Unique Soil	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Statewide/Locally Important Soils	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

Documentation

- Farmland Assessment Report
- Agricultural Lands Condemnation Approval Board (ALCAB) Approval
- Agricultural Land Preservation Policy Conformance Statement
- Form AD-1006 - Farmland Conversion Impact Rating or Form NRCS-CPA-106 for Corridor Type Projects
- Coordination with County Tax Assessor

Describe Any Permanent and Temporary Impacts

The project will result in minor impacts to productive agricultural land. Approximately 0.091 acre of parcel 19 (which is utilized for silage corn and soybean production) will be impacted by permanent roadway cuts associated with the I-80 WB construction work.

Approximately 0.185 acre of parcel 18 (which is utilized for hay, oats, and silage corn production) will also be impacted by permanent roadway cuts associated with the I-80 EB construction work.

Overall, the project will impact approximately 0.276 acre of productive agricultural land. These minor impacts, consisting of sliver takes, are not anticipated to affect the overall farming operations being conducted at these properties.

Examination of United States Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS) web soil survey mapping for the project area identified several farmlands of statewide importance and prime farmland soils within the project area. Overall, approximately 2.74 acres of farmland of statewide importance and 0.35 acre of prime farmland soils will be impacted by the project. The Farmland Protection Policy Act (FPPA) Farmland Conversion Impact Rating (Form CPA-106) has been prepared for the proposed project. The Site Assessment score for Part VI was calculated to be 16 points, which is below the 60-point threshold that requires completion of the remaining portions of the form. The form and rationale are included in the Project Technical File.

Is mitigation incorporated? No Yes

Property owners will be compensated fair market value for impacted agricultural land. Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

	PRESENCE	IMPACTS
VEGETATION	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Landscaped	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Agricultural	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Forest Land	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Rangeland	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

Describe Any Permanent and Temporary Impacts

Minor permanent impacts to wooded areas, landscaped residential areas, agricultural land, and roadside vegetation would be required for installation of the proposed bridges, abutments and piers, realignment of the I-80 EB bridge, cut and fill, and the installation of stormwater controls. Clearing and grubbing within wooded areas and landscaped residential areas would be required for temporary access to the existing I-80 bridge, watercourses, and location of the proposed bridge; installation of E&S control BMPs; and a temporary shoring tower.

Invasive Non-Native Plants are Present

Mitigation:

Are measures being taken to minimize movement of invasive plant parts (roots, tubers, seeds)? Yes No

Will native plants be used in project landscaping or mitigation? Yes No

Other? Yes No

Describe Mitigation

In accordance with PennDOT’s invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

Remarks

Vegetation within the project area includes maintained residential lawns and landscaping, roadside vegetation, herbaceous and wooded floodplain, graminoid stands, stands of herbaceous invasive species, and woodlots. Much of the project area is forested, with some stands of mixed deciduous trees, some stands of mixed coniferous and deciduous trees, and some homogenous stands of hemlock. Areas immediately adjacent to I-80 contain grasses and weedy plants or crown vetch. The remainder of the project area contains a mix of maintained lawn and landscaped areas associated with residences; active and fallow agricultural fields, which are located primarily along the western one- third of the project area; as well as shrub and brush, which are scattered throughout.

Invasive species observed within the project area include Japanese knotweed (*Fallopia japonica*), Japanese stiltgrass (*Microstegium vimineum*), reed canary grass (*Phalaris arundinacea*), and periwinkle (*Vinca sp.*).

	PRESENCE	IMPACTS
GEOLOGIC RESOURCES	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	

Remarks

According to the DCNR Heritage Geology Sites website, there are no Heritage Geology Sites in the project area. Additionally, the project area is not located near an Outstanding Scenic Geological Feature according to review of the Outstanding Scenic Geological Features of Pennsylvania Part 2.

	PRESENCE	IMPACTS
PARKS & RECREATION FACILITIES	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
National	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
State	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Local	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Other (describe in remarks)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

Were any of the impacted properties acquired through the use of :

- Land and Water Conservation Fund (Section 6(f))
- Project 70 Fund
- Project 500 Fund
- Recreational Improvement and Rehabilitation Act (RIRA)
- Growing Greener Bond Fund (GG2)
- Keystone Recreation, Park and Conservation Fund (Key 93)
- Environmental Stewardship Fund Act (ESF)

Documentation

- Coordination with DCNR as representative for National Park Service (NPS) (LWCF)
- Coordination with DCNR and PA General Assembly (Project 70)
- Coordination with DCNR (Project 500, GG2, Key93, RIRA or ESF)

Describe Any Permanent and Temporary Impacts

Walter Dick Memorial Park is situated below the I-80 North Fork Bridges. The 16.5-acre park is owned and maintained by Brookville Borough and is located on Brookville Municipal Authority property. Park amenities include the North Fork Environmental Education Center, four picnic shelters, a restroom facility, playground, concession building, swimming area, nature trails, and a suspension footbridge that connects both sides of North Fork Redbank Creek. Walter Dick Memorial Park received a Land and Water Conservation Fund (LWCF) grant and is therefore considered both a Section 6(f) and Section 4(f) resource.

The park will be impacted due to the permanent ROW needed for the new pier locations. No active recreation areas will be permanently impacted; however, a portion of a nature trail will need to be relocated on the eastern side of North Fork Redbank Creek near the bridge pier where the trail ends. Trail users will not be able to use this last bit of the trail during construction of the bridge pier, but it will be for less than the time needed overall for construction. The trail dead ends at the bridge pier and is seldom used at this location.

Additionally, the project will require temporary access roads and temporary easements for access during construction. Approximately 1.2 acres of park property will be permanently impacted and approximately 5.5 acres will be temporarily impacted. Since the TCEs will be needed for approximately three years, the project will result in a permanent Section 6(f) conversion and replacement land will need to be acquired for the TCEs based on LWCF policy. DCNR and NPS concurrence with the choice of replacement land is included in Appendix E.

Is mitigation incorporated? No Yes

Describe Mitigation

- Coordination with Brookville Borough regarding the proposed TCEs will continue throughout construction.
- Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.
- The portion of the nature trail that is to be impacted will be relocated. Replacement property for the permanent and temporary impacts to park property will be acquired in accordance with LWCF requirements and upon agreement by DCNR and NPS. DCNR and NPS concurrence with the choice of replacement land is included in Appendix E.
- A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

Remarks

Coordination with the DCNR indicated that the entire park property is considered a Section 6(f) resource as a clear Section 6(f) boundary had not been established. The Section 6(f) conversion package will be completed in final design.

In addition, Walter Dick Memorial Park has been improved, in part, by Keystone Recreation Park and Conservation Fund Program grants (Act 50 of 1993) from the DCNR. Two grants were received. The first was for the construction of the bath house and concession stand, installation of picnic shelters, ADA access, paving, landscaping, signs, and related improvements. The second grant included construction of the playground, safety surface, and related improvements. No impacts to these areas are anticipated as a result of the proposed project.

PRESENCE

FOREST & GAMELANDS

Not Present Present

Remarks

There are no national or state forests or state gamelands within the project area.

PRESENCE

WILDERNESS, NATURAL & WILD AREAS Not Present Present

Remarks

Review of USGS mapping, eMapPA, and site investigations confirmed there are no federal and/or state wilderness, natural, or wild areas within the project area.

PRESENCE

IMPACTS

NATIONAL NATURAL LANDMARKS

Not Present Present

No Yes

Remarks

There are no National Natural Landmarks present within the project area.

PRESENCE

IMPACTS

HAZARDOUS OR RESIDUAL WASTE SITES

Not Present Present

No Yes

Documentation

- Phase I
- Phase II
- Phase III
- Other
- No Documentation Required

Supporting documentation for Chapter 4.2 includes:

- *I-80 North Fork Bridges Phase I ESA report (February 2021)*
- *I-80 North Fork Environmental Due Diligence (EDD) Phase 1 Visual Inspection Form (July 2021)*

Describe Any Permanent and Temporary Impacts

A Phase I ESA was conducted in accordance with PennDOT Publication 281 to determine if hazardous, residual, or municipal waste sites exist within the project area. There could be isolated pockets of residual petroleum contamination in soils within the ROW as the result of numerous vehicle accidents, especially along I-80 EB. Soils under the I-80 WB bridge, between the abutment and first pier, appear to contain some black particles that appear different than the surrounding bridge soils. This material may contain heavy metals including lead. In addition, asbestos or lead paint may still be present in portions of the structures that are difficult to access. The Phase I ESA and Environmental Due Diligence (EDD) Phase 1 Visual Inspection Form are in the Project Technical File.

Is remediation/mitigation incorporated? No Yes Unknown at this time

Describe Remediation/Mitigation

The P3 development entity will conduct a Phase III ESA during final design. In accordance with PennDOT Publication 281, a work investigation plan will be prepared for review and approval by PennDOT prior to conducting the Phase III ESA. Based on recommendations of the Phase I ESA, the Phase III ESA should include:

- Soil under the I-80 WB bridge should be tested for Priority Pollutant Metals.
- The geotechnical borings logs should be reviewed for evidence of historic fill or contamination during final design.
- Special provisions for managing waste materials should be developed for potential isolated pockets of residual petroleum contamination within the ROW.
- Construction activities that disturb bridge paint should be performed in accordance with Occupational Safety and Health Administration (OSHA) requirements. All structures to be demolished should be inspected for asbestos-containing materials as per the National Emission Standards for Hazardous Air Pollutants (NESHAP).

4.3 Wildlife

PRESENCE

IMPACTS

WILDLIFE & HABITAT

Not Present Present

Remarks

Verified through a review of the Pennsylvania Game Commission State Game Land Maps and USGS quadrangle map.

PRESENCE

IMPACTS

THREATENED & ENDANGERED PLANTS & ANIMALS

Not Present
 Present
 No Coordination Needed

No Potential Impacts
 Potential Impacts with Avoidance Measures
 Potential Impacts with Conservation Measures
 Potential Impacts

Documentation

PNDI ER Receipt

Remarks

A search of the Pennsylvania Natural Heritage Program's Pennsylvania Natural Diversity Inventory (PNDI) database conducted on December 7, 2021, indicated "No Known Impact" to threatened and endangered species, special concern species, or special resources. The PNDI receipt is valid for two years. The PNDI receipt is included in Appendix D.

Although not addressed in the PNDI review, a decision is expected in 2023 to list the tri-colored bat as Endangered. A mitigation commitment is added to Chapter 7.0: During final design, the project team will initiate conferencing with USFWS regarding the project's potential effects to the tri-colored bat and measures to avoid and minimize harm.

4.4 Cultural Resources

Were Cultural Resource Professionals (CRPs) needed for project scoping? Yes No

CRP Scoping Field View Date: 06/26/19

CRP Architectural Historian in Attendance: Don Burden – PennDOT Architectural Historian

CRP Archaeologist in Attendance: Susanne Haney – PennDOT Archaeologist

Was a Project Early Notification / Scoping Results Form completed? Yes No

Is the project exempted from review by the District Designee or CRP as per Appendix C of the Statewide Section 106 Programmatic Agreement? Yes No

Is the project exempted from review by the District Designee or CRP as per Stipulation III of the Emergency Relief Projects Programmatic Agreement (2005)? Yes No

	<u>PRESENCE</u>				<u>LEVEL OF EFFECTS</u>		
	Not Present	Potentially Eligible Resource Present	Eligible Resource Present	Listed Resource Present	No Historic Properties Affected	No Adverse Effect	Adverse Effect
CULTURAL RESOURCES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Archaeology</u>							
Pre-Contact:	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Contact Native American:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic:	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Above-Ground Historic Properties</u>							
Structure/Building:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
District:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Above-Ground Historic Properties

- Above-Ground Historic Properties Field Assessment and Finding
- Above-Ground Historic Properties Finding Letter
- Section 106 (Above-Ground Historic Properties) Effect Concurrence Letter
- TE Project Field Assessment and Finding Checklist

Archaeology

- Archaeology Field Assessment and Finding
- Archaeology Finding Letter
- Section 106 (Archaeology) Effect Concurrence Letter
- TE Project Field Assessment and Finding Checklist
- Deferred Archaeological Testing Form
- Project Specific Programmatic Agreement

Supplemental documentation should be completed as warranted:

- Historic Structures Survey / Determination of Eligibility Report
- Phase Ia Archaeological Sensitivity Report
- Geomorphological Survey Report
- Archaeological Disturbance Report
- Archaeology Identification (Phase I) Report
- Archaeology Negative Survey Form
- Archaeology Evaluation (Phase II) Report
- Combined Archaeology Identification/Evaluation Report
- Determination of Effects Report
- (Bridge) Feasibility Report
- Other

Describe Any Permanent and Temporary Impacts

No permanent or temporary impacts are anticipated to the previously recorded Haugh site (36JE0082), a Pre-Contact habitation site with Archaic, Transitional, and Early woodland period cultural components located adjacent to the Area of Potential Effect (APE), or the Stonewall Spring site (36JE0201), a 19th to early 20th century domestic site located within APE. The National Register of Historic Places (NRHP)-listed Brookville Historic District (Key# 050628) and NRHP-eligible Boundary Increase (Key# 140793) are located southwest of and immediately adjacent to the APE but will not be impacted by the project.

Are mitigation and/or standard treatments required? No Yes

Describe Mitigation / Standard Treatments

The Stonewall Spring site (36JE0201) and Haugh site (36JE0082) will be fenced off and avoided during construction. In addition, a metal plate will be placed over the open well at the Stonewall Spring site to mitigate any safety concerns and protect the resource.

Remarks

Section 106 cultural resource documentation is located in Project Path (multiple postings in 2021) at <https://path.penndot.gov/ProjectDetails.aspx?ProjectID=57864>.

Cultural resources findings addendums were posted 3/8/2022 after the toll facility and proposed traffic improvements along the diversion route were included in the project. A subsequent addendum was posted 8/5/2022 to remove the toll facility and diversion route traffic improvements from the proposed project. The project will have no effect to archaeological and above ground historic properties (No Historic Properties Affected).

4.5 Section 4(f) Resources

SECTION 4(f) RESOURCES **PRESENCE** **USE**
 Not Present Present No Yes

Documentation

- Individual Section 4(f) Evaluation
- Programmatic Section 4(f) Evaluation
- Section 2002 Evaluation
- De Minimis Use/No Adverse Use Checklist
- Non-Applicability/No Use Checklist
- Temporary Use Checklist
- FHWA Coordination Documents

Will temporary easements during construction be necessary from Section 4(f) resources? No Yes

Describe Any Permanent and Temporary Impacts

Walter Dick Memorial Park is situated below the I-80 North Fork Bridges. The 16.5-acre park is owned and maintained by Brookville Borough and is located on Brookville Municipal Authority property. Park amenities include the North Fork Environmental Education Center, four picnic shelters, a restroom facility, playground, concession building, swimming area, nature trails, and a suspension footbridge that connects both sides of North Fork Redbank Creek. Walter Dick Memorial Park received a Land and Water Conservation Fund (LWCF) grant and is therefore considered both a Section 6(f) and Section 4(f) resource.

Walter Dick Memorial Park will be impacted due to the permanent ROW needed for the new pier locations. No active recreation areas will be permanently impacted; however, a portion of a nature trail will need to be relocated on the eastern side of North Fork Redbank Creek where the trail ends. Trail users will not be able to use this last bit of the trail during construction of the bridge pier, but it will be for less than the time needed overall for construction. The trail dead ends at the bridge pier and is seldom used at this location.

Additionally, the project will require temporary access roads and TCEs for access during construction. Approximately 1.2 acres of park property will be permanently impacted. Approximately 5.5 acres will be temporarily impacted, but these impacts are not a use under Section 4(f) because the use is temporary and minor, there is no anticipated permanent impacts to this area, the area will be restored, and the Borough has agreed that these impacts to the park will be temporary. The Determination of Section 4(f) De Minimis Use form is included in Appendix E to satisfy Section 4(f) and Section 2002 of Pa Act 120.

Is mitigation incorporated? No Yes

Describe Mitigation

Coordination with Brookville Borough (the official with jurisdiction for the park) regarding the proposed TCEs will continue throughout construction.

Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.

The portion of the nature trail that is to be impacted will be relocated.

A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

Remarks

North Fork Redbank Creek is not identified by the PFBC as a water trail and therefore is not a Section 4(f) resource.

4.6 Air Quality and Noise

4.6.1. Air Quality

Is the project exempt from regional ozone conformity analysis and a CO, PM10 & PM2.5 Hot-Spot analysis? Yes No

Mobile Source Air Toxics (MSATs)

Is the project exempt from an analysis for MSATs based on Pub #321? Yes No

Air Quality Remarks

This project is exempt from a regional conformity analysis. Table 3 of PennDOT Publication 321 notes that “Changes in vertical and horizontal alignment” projects are exempt. This project also does not require a project level (microscale) analysis because the project is located in Jefferson County, which is in attainment for all NAAQS pollutants.

4.6.2. Noise

Noise is defined as unwanted or disturbing sound, which can occur when it interferes with normal activities such as sleep, work, speech, or recreation. State highway agencies must complete a noise analysis for any federal or Federal-aid Highway Program projects where a project is considered a Type I Transportation Improvement Project, defined by 23 CFR 772.5. A Type I project involves a substantial horizontal or vertical alteration of the existing highway. This project is a Type I project because there is a substantial horizontal change in shifting of the EB traffic closer to the WB traffic in the current diverged section of I-80. For this reason, the detailed analysis procedures and methods for a Type I analysis were followed. The existing analysis year traffic is 2018. The design year is 2046 for both the no-build and preferred alternative based on the proposed opening year of 2026 plus 20 years.

Supporting documentation for Chapter 4.6 includes:

- *I-80 North Fork Bridges Noise Report (March 2021)*

The analysis was conducted in accordance with PennDOT/FHWA procedures as outlined in PennDOT’s Project Level Highway Traffic Noise Handbook, Publication 24 (May 2019). The FHWA-approved model used for the analysis is Traffic Noise Model (TNM) 2.5. The TNM incorporates engineering design information and project mapping elements to evaluate traffic-induced noise levels. The information applied to the modeling effort includes existing and proposed roadway and grading geometry, traffic volumes, travel speeds, vehicle types, building rows and tree zones, existing local roadways with measurable noise influences

To evaluate existing noise levels and provide data to assist with noise model validation, noise monitoring was conducted at 7 monitoring locations in the project area. Modeled receptor sites were chosen to establish highway traffic noise impacts, to evaluate possible noise barrier locations, lengths, and heights (as applicable), and at specific locations due to the nature of the transportation improvement project (i.e., topography, locations of special concern). Modeled receptor sites are used to represent the potential impacts experienced by noise sensitive areas.

Sound pressure is measured in terms of decibels (dB). A-weighted decibels (dBA) are an expression of the relative loudness of sounds in air, with an emphasis on frequencies that can be perceived by the human ear. Noise is measured on a logarithmic scale, which means that the doubling of sound energy increases the level by 3 decibels. On this scale, 0 dBA cannot be heard, and 120 dBA is uncomfortably loud and painful to human hearing. An increase in sound levels of 1 to 2 dBA is generally not perceptible by the human ear. For most people to begin to perceive a change in sound level, a 3 dBA increase would be necessary. An increase of 10-dBA is perceived as a doubling of sound levels. Relative to traffic noise, doubling the traffic volume yields an approximate 3 dBA increase.

FHWA has established (adopted by PennDOT in its *Project Level Highway Traffic Noise Handbook*, Publication 24) Noise Abatement Criteria (NACs) for five categories of land uses or activities, as shown in **Table 3**. Under FHWA criteria, a noise impact occurs when traffic noise levels approach or exceed the NAC shown in **Table 3**. PennDOT interprets a noise level “approaching” the criteria as a noise level that is 1 dBA less than the NAC level. In addition to the absolute criteria defined in **Table 3**, noise impacts can occur when design-year noise levels “substantially” exceed existing noise levels. PennDOT defines the substantial noise increase criteria for Categories A through E as increases of 10 dBA or greater.

Table 3
PennDOT and FHWA Hourly Weighted Sound Levels (dBA) for Various Land Use Activity Categories

Land Use Activity Category	NAC	Land Use Activity Category
A	57 (exterior)	Lands on which serenity and quiet are of extraordinary significance and serve an important public need, and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose
B ^a	67 (exterior)	Residential
C ^a	67 (exterior)	Active sports areas, amphitheaters, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails, and trail crossings
D	52 (interior)	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios
E	72 (exterior)	Hotels; motels; offices; restaurants/bars; and other developed lands, properties, or activities not included in A, B or C
F	--	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing
G	--	Undeveloped lands that are not permitted

^a Includes undeveloped lands permitted for this activity category

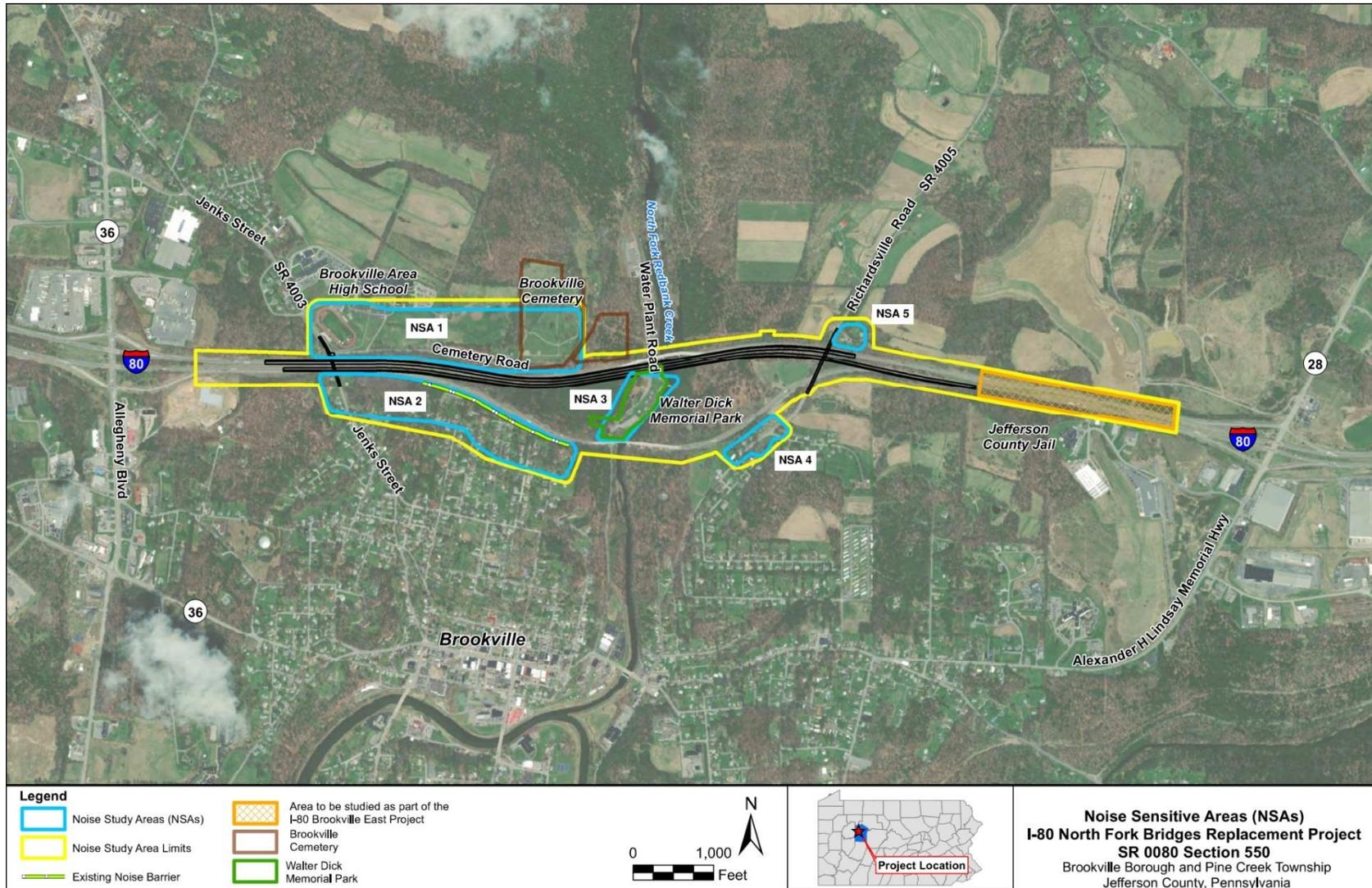
Affected Environment

In the noise analysis area, the existing land use north of I-80 WB primarily consists of school recreational fields, a cemetery, open/forested land and sparsely scattered single family residences. South of I-80 EB, the land use is primarily single and multi-family residential dwelling units. Additionally, Walter Dick Memorial Park is in between the bifurcated section of I-80, approximately 100-120 ft below the interstate (or more, depending on the exact location).

For analysis, the project area was divided into five Noise Sensitive Areas (NSAs) which are shown on **Figure 2**.

- **NSA 1.** Noise sensitive receptors include single-isolated residential land use (NAC B), several school recreational fields (NAC C) and the Brookville Cemetery (NAC C). Worst case sound levels in NSA 1 range from 60-70 dBA. Noise monitors recorded a sound level of 67.7 dBA at the Brookville Cemetery and 65.0 dBA at the Brookville High School football/track field.
- **NSA 2.** Noise receptors include single and multi-family residential land uses (NAC B). Worst case modeled sound levels in NSA 2 range from 55-71 dBA. Noise monitors recorded a sound level of 55.6 dBA at the Hickory Apartments and 61.5 dBA at the Sayer Street residence. This area includes an existing sound wall which would be removed due to ROW considerations.
- **NSA 3.** NSA 3 is comprised of the Walter Dick Memorial Park (NAC C). Noise sensitive receptors include exterior people activity areas, such as the picnic pavilions. Worst case modeled sound levels in NSA 3 are about 64 dBA. The noise monitor recorded a sound level of 62.4 dBA at the Walter Dick Memorial Park pavilion.
- **NSA 4.** Noise sensitive receptors include single-family homes (NAC B). Worst case sound levels in NSA 4 range from 64-66 dBA. The monitor placed within this NSA recorded a sound level of 63.8 dBA between two single family homes on SR 4005 (Richardsville Road).
- **NSA 5.** This NSA includes one single-isolated residential land use (NAC B). Modeled worst case sound levels are at 71 dBA. The monitor placed within this NSA recorded a sound level of 68.1 dBA at a single-family residence along SR 4005 (Richardsville Road).

Figure 2 – Noise Sensitive Areas



Noise Impacts

Each of the NSAs was modeled to determine potential noise impacts to receptors in that NSA. In summary, there were 20 representative locations in NSA 1, eight locations in NSA 2, eight locations in NSA 3, seven in NSA 4 and one location in NSA 5 that were modeled to be approaching or exceeding the PennDOT impact criteria for the design year no-build alternative.

- **NSA 1.** The NAC criteria was approached or exceeded at 27 representative locations under the build alternative. These locations include the Brookville HS football and baseball fields and a practice field, a residence on Cemetery Lane, a residence on Jenks Street, and 22 locations in the Brookville Cemetery. There were no substantial noise increases over existing noise level impacts. Because there are predicted impacts, consideration of noise mitigation was undertaken for NSA 1.
- **NSA 2.** The NAC criteria was approached or exceeded at 20 locations under the build alternative without the existing wall. These locations include 5 residences on Jenks Street, 8 residences on Caldwell Street, 2 residences on Porter Street, 2 residences on Oak Street, 2 residences on N. Barnett Street, 1 residence on Sayer Street. Five of those locations had substantial noise increase over existing noise level impacts. Because there are predicted impacts, consideration of noise mitigation was undertaken for NSA 2.
- **NSA 3.** The NAC criteria was not approached or exceeded under the build alternative as I-80 EB has been relocated north of this NSA. Both EB and WB bridges will also be higher in elevation than the current structure. Additionally, the proposed EB bridge will help block sound generated from the WB bridge as it travels “down” toward the park. Because there are no predicted impacts noise mitigation is not warranted.
- **NSA 4.** The NAC criteria was not approached or exceeded under the build alternative as I-80 EB has been relocated away from this NSA. Because there are no predicted impacts noise mitigation is not warranted.
- **NSA 5.** The single-isolated residence is predicted to be impacted. Although there was no substantial noise increase over the existing noise level, the NAC criteria was exceeded under existing conditions, the no build alternative, and the build alternative. Therefore, noise mitigation was considered.

The I-80 North Fork Bridges Noise Report (March 2021) is included in the Project Technical File.

Mitigation

PennDOT considered a number of potential mitigation measures to reduce highway traffic noise including reductions in speed, truck restrictions, alignment shifts, sound insulation for public institutions, or acquisition. These measures were determined to not be practical and/or not effective for noise control on this project.

PennDOT also examined noise barriers considering acoustical and engineering factors, reasonableness, and cost effectiveness using established procedures and parameters. PennDOT considers building up to 2,000 square ft (s.f.) of wall per benefitted receptor to be reasonable.

- **NSA 1.** A noise barrier (11-16 ft high by 2,920 ft long) is proposed to mitigate for noise impacts in NSA 1. The mitigation analysis determined a noise wall would be feasible and reasonable given the amount of noise wall needed (38,765 s.f.) and the number of receptors benefitted (32), 1,211 s.f. per benefitted receptor.
- **NSA 2.** A noise barrier (9-18 ft high by 3,410 ft long) is proposed to mitigate for noise impacts in NSA 2. The mitigation analysis determined a noise wall would be feasible and reasonable given the amount of noise wall needed (56,004 s.f.) and the number of receptors benefitted (37), 1,514 s.f. per benefitted receptor.
- **NSA 5.** The mitigation analysis determined a noise wall would be feasible but not reasonable given the amount of noise wall needed (3,579 s.f.) for the number of receptors benefitted (1).

The results presented in this CE are based on preliminary design. Additional coordination and evaluation for the proposed sound barrier walls will continue in final design.

4.7 Socioeconomic Areas

REGIONAL & COMMUNITY GROWTH

Will the project induce impacts (positive and negative) on planned growth, land use, or development patterns for the area? Yes No

Is the project consistent with planned growth? Yes No

Basis of this determination:

The project is programmed on the 2023-2026 Interstate Transportation Improvement Program (TIP). The project replaces existing infrastructure and is not anticipated to induce growth.

Will the project induce secondary growth? Yes No

PUBLIC FACILITIES & SERVICES

Will the project induce negative impacts on health and educational facilities; public utilities; fire, police and emergency services; civil defense; religious institutions; or public transportation? Yes No

Does the project incorporate bicycle or pedestrian facilities into the overall design or operations? (including construction)? Yes No

A new ADA-compliant sidewalk will be constructed along SR 4003 (Jenks Street). The completed bicycle/pedestrian checklist is in Appendix F.

Will the project have a positive impact to the public facilities and services listed above? Yes No
Access will be improved due to the design improvements resulting from the project.

COMMUNITY COHESION

Will the project induce impacts to community cohesion? Yes No

Will the project induce impacts to the local tax base or property values? Yes No

ENVIRONMENTAL JUSTICE *(See Chapter 6.0 of this CE)*

RIGHT-OF-WAY ACQUISITIONS OR DISPLACEMENTS OF PEOPLE, BUSINESSES OR FARMS

How many parcels require right-of-way acquisition, either partial or total?

21 parcels affected; 19 parcels with partial permanent takes, 2 parcels with TCEs only.

The P3 development entity will be responsible for final design and construction of the project. If area is required outside of the defined Project Study Area, the P3 development entity is required to coordinate with PennDOT to determine necessary NEPA Reevaluation studies and documentation (Chapter 7.0, Environmental Commitments and Mitigation).

Describe the extent and locations of acquisitions. Indicate for each acquisition whether it is temporary or permanent.

Based on the current design, required ROW for limited access, permanent slope, drainage, and channel easements, and TCEs are anticipated along I-80 EB and WB. A required aerial easement is anticipated along and to the south side of the new mainline bridge carrying I-80 EB over the Brookville Municipal Authority property, North Fork Redbank Creek, and Water Plant Road. Temporary construction easements and drainage or channel

easements are anticipated through several of the properties located between existing I-80 EB and I-80 WB in order to construct access roads down into the North Fork Redbank Creek gorge and to construct the proposed embankments and piers. Sliver takes of required ROW and TCEs are anticipated along both SR 4003 and SR 4005 for roadway and driveway improvements.

The required ROW and permanent and temporary easement lines will be further refined in final design. Once the Eastbound lanes of I-80 have been relocated next to the existing Westbound lanes, the existing Eastbound lanes and existing bridge structure will be removed, and the area will be revegetated. Upon the completion of construction, the Department will evaluate the needs of the traveling public (both current and future), make a determination of excess land, and dispose of that land (if any) in accordance with Department policy, procedure and state highway law.

Will the project require the relocation of people, businesses, or farms? Yes No

Will the project induce impacts to economic activity, including employment gains and losses? Yes No

Short-term employment gains and activity may occur during construction.

Mitigation

Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

- While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
- Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.

MAINTENANCE AND OPERATING COSTS OF THE PROJECT AND RELATED FACILITIES

Will the project induce increases of operating or maintenance costs? Yes No

AESTHETIC AND OTHER VALUES

Will the project be visually intrusive to the surrounding environment? Yes No

Will the project include "multiple use" opportunities? Yes No

Will the project involve "joint development" activities? Yes No

4.8 Permits Checklist

- No Permits Required
- United States Army Corps of Engineers Section 404 and/or Section 10 Permit**
 - Individual Nationwide PASPGP
- DEP Waterway Encroachment (105) Permit**
 - Standard Small Project General Other
- DEP 401 Water Quality Certification**
- Coast Guard Permit**
- NPDES Permit**
 - General Individual Exempt
- Other Permits**

Remarks, Footnotes, Supplemental Data

Permit conditions will be added to the Environmental Commitments & Mitigation Tracking System (ECMTS) as mitigation commitments. ECMTS is a computer application for tracking mitigation commitments from inception during preliminary design through construction, to be used by construction inspectors to ensure mitigation measures are completed as intended for protection of environmental resources.

5.0 PUBLIC INVOLVEMENT

	#	Comments
<input checked="" type="checkbox"/> Plans Display	2	See Remarks
<input checked="" type="checkbox"/> Public Officials Meetings	2	See Remarks
<input checked="" type="checkbox"/> Public Meetings	2	See Remarks
<input checked="" type="checkbox"/> Public Hearing	1	EA Public Hearing 5/3/2022. See Remarks
<input checked="" type="checkbox"/> Special Purpose Meetings	2	Diversion Route Workshop 8/11/2021 and follow-up briefing 11/22/2021. See Remarks
<input type="checkbox"/> Section 106 Public Involvement / Consulting Parties		
<input checked="" type="checkbox"/> Section 106 Tribal Consultation		- Absentee Shawnee (no response) - Delaware Nation (responded they would like to be a Consulting Party; however, no potentially eligible sites will be impacted by the project) - Delaware Tribe (no response) - Eastern Shawnee (no response) - Seneca Nation of Indians (no response) - Seneca-Cayuga Nation (no response) - Shawnee Tribe (no response) - Tonawanda Band of Seneca (no response)
<input checked="" type="checkbox"/> Environmental Justice Community Involvement		Knowledgeable Parties outreach, see Remarks
<input checked="" type="checkbox"/> Other information dissemination activities		A flyer discussing the impacts to Walter Dick Memorial Park and asking for comments was posted in the park and in the Brookville Borough building. It was also posted to the Borough's Facebook page.
<input checked="" type="checkbox"/> Commitment for Further Public Involvement		The contractor will continue to coordinate with local municipalities and the public.

Remarks

Public outreach activities were conducted beginning in November 2020 for the PennDOT Pathways program. Since the I-80 North Fork Bridges Project was identified as a candidate for bridge tolling through PennDOT Pathways Program's Major Bridge P3 Initiative in February 2021, additional public outreach efforts were conducted specific to the I-80 North Fork Bridges Project.

- Project information was posted on a project-specific website in February 2021 at <https://www.penndot.pa.gov/RegionalOffices/district-10/ConstructionsProjectsAndRoadwork/Pages/I-80-North-Fork.aspx>
- A virtual public meeting was held for the project between March 1 and March 22, 2021. The on-line content is available on the project website and included: project overview, purpose and need, description of proposed improvements, project visualizations, detour information, noise analysis and noise wall considerations, local impacts, funding, and project schedule. Comments were accepted from March 1 to March 22, 2021.
- A diversion route workshop was conducted on August 11, 2021 to gather additional information from stakeholders regarding potential issues along the diversion route consisting of SR 28, SR 28/SR 322 (East Main Street), SR 28/SR 36/SR 322 (West Main Street), and SR 36 (Allegheny Boulevard).
- The diversion route workshop attendees were invited to attend a follow-up briefing on November 22, 2021 to review the proposed diversion route improvements included in the public meeting materials.
- A project-specific virtual public meeting was held from November 22, 2021 to December 22, 2021. The online meeting was comprised of text, graphics and videos that provided a project overview and explained the project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, comment process and next steps. The online meeting website provided a comment form that allowed individuals to submit their comments directly within the virtual public meeting. The website also noted other ways in which comments could be submitted, including the comment form on the general project website, project phone number, project email and a physical mailing address.
- An in-person public open house was held on Wednesday December 15, 2021 at Hickory Grove Elementary School in Brookville, PA. At the in-person public open house, display boards were provided for project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, and schedule. Comment forms were provided for individuals to submit their comment while in attendance or at their convenience. While the comment period for the public meeting and open house has closed, the online meeting materials are available for reference via the project website. In-person meeting materials were printed versions of the online content.

Supporting documentation for Chapter 5 includes:

- *I-80 North Fork Bridges Project Virtual Public Meeting (March 1 to March 22, 2021)*
- *I-80 North Fork Bridges Project Virtual Public Meeting (November 22 to December 22, 2021)*
- *I-80 North Fork Public Meeting Summary (January 2022)*

Prior to and during the public comment period for the second public meeting held on December 15, 2021, the project team executed several outreach strategies to maximize public participation at the public meeting or online consultation of the Virtual Public Meeting on the project website. The outreach activities are listed in **Table 4**.

**Table 4
Public Outreach Activities for Second Public Meeting**

Outreach Type	Number of Recipients	Type of Recipients	Date Sent
Virtual Public Meeting Website	N/A	- General Public via https://www.penndot.pa.gov/RegionalOffices/district-10/ConstructionsProjectsAndRoadwork/Pages/I-80-North-Fork-Bridges-VPM.aspx	Launched 11/22/21
Postcard	4,640	- General Public - Mailed via Every Door Direct Mail Service - Sent to all postal routes within the direct project area and along the diversion route.	Mailed week of 11/29/21
Legal Ad	Print circulation approx. 14,000	- General public - Placed in <i>The Courier Express</i> and <i>The Jeffersonian Democrat</i>	Ran 11/30/21 & 12/2/21
Stakeholder & Public Mailing List Email	266	- Key stakeholders, legislators and those who requested to be put on the project's mailing list. - Email with information about the virtual public meeting and in-person open house.	11/22/21
Knowledgeable Parties Email & Flyer	8	- Knowledgeable parties identified in environmental justice analysis - Email with information about virtual and in-person meetings, along with a flyer to be distributed in the community and copies of social media art for sharing	11/22/21
News Release	N/A	- Sent to area media to distribute via news stories and calendars of events for the general public.	11/22/21
Public Officials Briefing	N/A	- Invited public officials to a pre-launch briefing to get a first look at the materials to launch in the virtual public meeting	11/22/21 at 9 a.m.
Social Media Posts	20,562 people reached	- Social media posts on PennDOT social media regarding how to participate in the public meeting and comment period - 64 engagements across three posts	11/22/21 12/15/21 12/20/21

An EA comparing the effects of the No Build Alternative and the Build Alternative with bridge tolling was prepared and was made available for official public review and comment on April 18, 2022. A Public Hearing was held on May 3, 2022. The comments received during the EA comment period (April 18 to May 18, 2022), including testimony and comments received at the public hearing, have been reviewed, considered, and where appropriate, additional information was incorporated into this CE. During the public comment period for the EA, the project team executed several outreach strategies to maximize public participation as listed in **Table 5**.

**Table 5
Public Outreach Activities for the EA**

Outreach Type	Number of Recipients	Type of Recipients	Date Sent
Virtual Public Hearing Website	N/A	<ul style="list-style-type: none"> General public via https://www.penndot.pa.gov/RegionalOffices/district-10/ConstructionsProjectsAndRoadwork/Pages/I-80-North-Fork.aspx 	4/18/2022
Postcard	15,910	<ul style="list-style-type: none"> General Public Mailed via Every Door Direct Mail Service Sent to all postal routes within the direct project area and along the diversion route. 	Mailed week of 4/18/2022
Legal Ad	Print circulation approx. 10,000	<ul style="list-style-type: none"> General public Placed in the <i>Courier Express</i> 	4/18/2022
Stakeholder & Public Mailing List Email	210	<ul style="list-style-type: none"> Key stakeholders, legislators and those who requested to be put on the project’s mailing list. Email with information about the Virtual Open House and in-person Public Hearing. 	4/18/2022
Knowledgeable Parties Email & Flyer	8	<ul style="list-style-type: none"> Knowledgeable parties identified in environmental justice analysis Email with information about virtual open house and in-person hearing, along with a flyer to be distributed in the community and copies of social media art for sharing 	4/18/2022
News Release	N/A	<ul style="list-style-type: none"> Sent to area media to distribute via news stories and calendars of events for the general public. 	4/18/2022
Social Media Posts	2,751	<ul style="list-style-type: none"> Social media posts on PennDOT social media regarding how to participate in the public hearing and comment period 	4/29/2022
Elected Official Notification	Key Elected Officials List	<ul style="list-style-type: none"> Elected officials (State and Local) Direct reach out by PennDOT District 10-0 	4/15/2022

On May 18, 2022, as a result of a lawsuit, the court issued an injunction and all work related to the MBP3 initiative ceased. Subsequently, Act 84 of 2022 amended the P3 law and revoked PennDOT’s ability to implement mandatory tolls such as the proposed bridge tolling under the MBP3 initiative. As a result of the lawsuits and the subsequent enactment of Act 84 of 2022, PennDOT is moving the I-80 North Fork Bridges project forward, but without tolling.

As the project is reverting back to a bridge replacement with associated approach roadway work, this CE was prepared to document the current effects of the Build Alternative without tolling. The project team completed outreach in September 2022 to educate and inform the public about the CE with the removal of tolling. Outreach activities are summarized in **Table 6**.

Table 6
Public Outreach Activities for the CE

Outreach	Audience & Subject
News Releases	Two news releases to media: <ol style="list-style-type: none"> 1. Statewide release regarding removal of tolling from MBP3 program. 2. I-80 North Fork Bridges Project specific release with information on the CE.
Email Blasts	Two email blasts to mailing list sign-ups: <ol style="list-style-type: none"> 1. Statewide email blast regarding removal of tolling from MBP3 program. 2. I-80 North Fork Bridges Project specific email blast with information on the CE.
Social Posts	Two social posts on Facebook and Twitter. <ol style="list-style-type: none"> 1. Statewide social post regarding removal of tolling from MBP3 program. 2. I-80 North Fork Bridges Project specific social post with information on the CE.
Bridge Website Update & Online Educational Resource	The bridge project website was updated to include information on the project's current status, description and history. An online educational resource about the CE and potential impacts was also developed to provide information to the public on what is presented in the CE. A comment form was available on the website for those who wished to provide feedback on the project. Comments were considered as the CE was finalized.

Public involvement documentation covering the NEPA process for the project is located in the project technical files.

6.0 ENVIRONMENTAL JUSTICE

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations* (February 11, 1994), directs federal agencies to identify and address, as appropriate, disproportionately high, and adverse human health or environmental effects of programs, policies, and activities on minority and low-income populations. To achieve effective and equitable decision-making, the USDOT identifies three fundamental principles of environmental justice to consider in all USDOT programs, policies, and activities:

- To avoid, minimize, or mitigate disproportionately high and adverse human health and environmental effects, including social and economic effects, on environmental justice communities of concern.
- To ensure the opportunity for full and fair participation by all potentially affected communities in the transportation decision-making process.
- To prevent the denial of, reduction in, or substantial delay in the receipt of benefits by any environmental justice community of concern.

The *I-80 North Fork Bridges Environmental Justice Analysis*, February 2022, was prepared to address the effects of bridge tolling and associated traffic diversion to avoid tolls on low-income and minority populations; a copy is included in the project technical files. While bridge tolling is no longer under consideration, the report contains relevant background information describing low income and minority populations in the vicinity of the proposed project.

The Environmental Justice analysis for the project was performed by completing the following process:

- **Step 1: Define the Study Area.** Consistent with NEPA practices, identify the reasonable and logical boundaries by considering the potential for direct and indirect impacts related to the project.

The project study area includes approximately 2.3 miles of I-80 between the SR 0036 Exit 78 Brookville interchange and the SR 0028 Exit 81 Hazen interchange. The project will replace two bridges carrying I-80 over North Fork Redbank Creek and Water Plant Road. In addition, the I-80 bridges over SR 4003 (Jenks Street) and the SR 4005 (Richardsville Road) bridges over I-80 will be replaced.

- **Step 2: Identify Low-income and Minority Populations.** Collect recent data on race, color, national origin, income, Tribal governments, and seasonal and migrant workers in the study area, and apply FHWA and PennDOT methodology to identify low-income and minority populations.

Low-income and minority populations are identified in the *I-80 North Fork Environmental Justice Analysis*, February 2022.

- **Step 3: Solicit Input from Low-income and Minority Populations.** Using PennDOT's *Public Involvement Handbook* and other environmental justice outreach guidance, identify appropriate outreach techniques. Through targeted outreach to potentially affected low-income and minority populations, identify transportation needs and concerns about the project to inform Steps 4, 5, and 6.

Public outreach was conducted throughout the development of the project including plans display/public meetings and additional stakeholder outreach targeted to parties knowledgeable about environmental justice issues (see Chapter 5.0 of this CE).

Supporting documentation for Chapter 6 includes:

- *I-80 North Fork Environmental Justice Analysis* (February 2022)

- **Step 4: Evaluate Adverse and Beneficial Effects.** Analyze whether the project would create impacts to communities or populations in the near, medium, or long term. Then, with input from the community, assess whether the impacts are adverse, beneficial, or both.

Since the project involves on-location reconstruction of the existing WB roadway lanes, reconstruction of the EB lanes adjacent to the WB lanes, and replacement of existing bridges in a rural setting, the effects on the local community are minimal. The project will impact Walter Dick Memorial Park; however, the recreational activities, features and attributes of the park will not be adversely affected, and replacement land adjacent to the park will be acquired to off-set the right-of-way and easements needed for construction of the replacement bridges (see Chapter 4.2 and 4.5 of the CE). During construction, some diversion through the community may occur as some travelers may opt to avoid the construction zone even with two lanes largely being retained in each direction; however, once the project is completed the reconstructed roadway and replaced bridges would provide improved service along the I-80 corridor. Once the Eastbound lanes of I-80 have been relocated next to the existing Westbound lanes, the existing Eastbound lanes and existing bridge structure will be removed and the area will be revegetated. Upon the completion of construction, the Department will evaluate the needs of the traveling public (both current and future), make a determination of excess land and dispose of that land (if any), in accordance with Department policy, procedure and state highway law.

- **Step 5: Identify Disproportionately High and Adverse Effects.** Determine whether adverse effects are predominately borne by low-income persons and minorities, and if these effects are more or greater than those effects borne by the general population.

As a result of this analysis and associated outreach effort, no disproportionately high and adverse effects on low-income or minority populations have been identified for the I-80 North Fork Bridges Project since adverse effects to these populations are not anticipated as a result of the project.

- **Step 6. Evaluate Mitigation Measures.** If adverse effects would be predominately borne by low- income and minority populations and are more or greater in magnitude than the adverse effect that would be suffered by the general population, consult with the community to identify measures to avoid, minimize, or mitigate the impacts. Determine whether the mitigation measures are practical. Practical mitigation measures are those that are: effective and do not create other adverse effects that are more severe; feasible in terms of implementation and operation; and cost effective, while maintaining the financial viability of the project.

As no disproportionately high and adverse effects on low-income or minority populations are anticipated to occur, evaluation of mitigation measures was not necessary.

- **Step 7: Re-evaluate Disproportionately High and Adverse Effects and Document Decision.** If practical mitigation measures have been identified, re-evaluate whether adverse effects borne by low-income and minority populations are appreciably more severe or greater than those effects borne by non-environmental justice populations.

Re-evaluation of effects on low-income and minority populations was not necessary.

7.0 ENVIRONMENTAL COMMITMENTS AND MITIGATION

The mitigation measures summarized in this section shall be incorporated into the project's design documents. In order to track and transfer mitigation commitments through the project development process, Environmental Commitments & Mitigation Tracking System (ECMTS) documentation shall be prepared and submitted through the appropriate channels, as the project moves through final design and construction.

Impacts and mitigation commitments are based on Preliminary Design and may change as the project moves through final design and construction. Final design information and final mitigation commitments will be included in the ECMTS documentation.

STREAMS

Permanent Stream Impacts: 5,378 linear ft

Proposed Project Specific Restoration/Enhancement: 5,378 linear ft

Mitigation Remarks:

- In-stream construction restrictions for Stocked Trout Waters (February 15th through June 1st) will be observed on North Fork Redbank Creek, S-02, and S-09.
- Pennsylvania Code Title 25, Chapter 93 Water Quality Standards, identifies the management designation for North Fork Redbank Creek as HQ-CWF. Antidegradation Best Available Combination of Technologies (ABACT) Erosion and Sediment Controls should be specified for all projects within High Quality or Exceptional Value watersheds.
- Compensatory mitigation is required for this project. Through consultation with permitting agencies, mitigation details will be determined in final design and incorporated into the waterway permit application.

WETLANDS

Permanent Wetland Impacts: 0.163 acre

Project Specific Replacement/Construction:

Banking: 0.163 acre

Bank to be Debited: Dubois-Jefferson County Wetland Bank

Mitigation Remarks:

- Permanent impacts to wetlands will be mitigated by utilizing credits from PennDOT's Dubois-Jefferson County Wetland Bank Site.
- Temporary construction fencing will be placed around wetland boundaries not to be impacted by the project.
- Timber matting will be used to protect wetland crossings when necessary.
- Graded areas will be returned to the original contour and the area seeded, mulched, and stabilized once construction in these areas is complete.

NAVIGABLE WATERWAYS

An Aids to Navigation (ATON) Plan, to be approved by the PFBC, will be developed during final design in conjunction with the waterway permit and implemented during construction to alert boaters of temporary construction measures. This plan will be in place throughout construction.

SOIL EROSION & SEDIMENTATION

- BMPs will be defined and implemented as a component of the E&S plan and waterway encroachment permit.
- The E&S Control Plan will be reviewed by the PADEP, and coordination will be conducted to ensure the selected BMPs are adequate for the project.
- The approved E&S Control Plan will be implemented prior to any earth disturbance during construction.
- Installed BMPs will be inspected and maintained throughout the duration of construction.
- All areas of earth disturbance will be stabilized immediately following completion of earthwork.
- PCSM controls will be evaluated in final design and included in the NPDES permit application, if required.

VEGETATION

In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

PARKS & RECREATION FACILITIES

- Coordination with Brookville Borough regarding the proposed TCEs will continue throughout construction.
- Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.
- The portion of the nature trail that is to be impacted will be relocated.
- Replacement property for the permanent and temporary impacts to park property will be acquired in accordance with LWCF requirements and policies. DCNR and NPS concurrence with the choice of replacement land is included in Appendix E.
- A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

HAZARDOUS OR RESIDUAL WASTE SITES

The P3 development entity will conduct a Phase III ESA during final design. In accordance with PennDOT Publication 281, a work investigation plan will be prepared for review and approval by PennDOT prior to conducting the Phase III ESA. Based on recommendations of the Phase I ESA, the Phase III ESA should include:

- Soil under the I-80 WB bridge should be tested for Priority Pollutant Metals.
- The geotechnical borings logs should be reviewed for evidence of historic fill or contamination during final design.
- Special provisions for managing waste materials should be developed for potential isolated pockets of residual petroleum contamination within the ROW.
- Construction activities that disturb bridge paint should be performed in accordance with OSHA requirements. All structures to be demolished should be inspected for asbestos-containing materials

as per the NESHAP.

THREATENED & ENDANGERED PLANTS & ANIMALS

USFWS coordination for tri-colored bat:

During final design, the project team will initiate conferencing with USFWS regarding the project's potential effects to the tri-colored bat and measures to avoid and minimize harm.

CULTURAL RESOURCES

The Stonewall Spring site (36JE0201) and Haugh site (36JE0082) will be fenced off and avoided during construction. In addition, a metal plate will be placed over the open well at the Stonewall Spring site to mitigate any safety concerns and protect the resource.

SECTION 4(f) RESOURCES

- Coordination with Brookville Borough (the official with jurisdiction for the park) regarding the proposed TCEs will continue throughout construction.
- Impacts to mature trees will be minimized as much as practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.
- The portion of the nature trail that is to be impacted will be relocated.
- A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

NOISE

Proposed sound barrier walls are considered both feasible and reasonable along I-80 EB and WB from a point just west of the I-80 over SR 4003 (Jenks Street) bridges to a point near the proposed I-80 EB and WB bridges over North Fork Redbank Creek and Water Plant Road. Additional coordination and evaluation for the proposed sound barrier walls will continue in final design.

RIGHT-OF-WAY ACQUISITION

Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

- While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
- Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated

COMMITMENTS FOR FURTHER PUBLIC INVOLVEMENT

The contractor will continue to coordinate with local municipalities and the public.

NON-RESOURCE SPECIFIC MITIGATION COMMITMENTS

- The NPDES and waterway permit conditions will be added to ECMTS as mitigation commitments.
- If the P3 Development Entity requires area outside of the PSA delineated in this CE, the P3 Development

Entity is required to coordinate with PennDOT to determine necessary NEPA Reevaluation studies and documentation.

This NEPA Reevaluation may include but not be limited to:

- Delineation of aquatic resources in accordance with PennDOT and USACE protocol;
 - Section 106 consultation;
 - Phase I ESA or Environmental Due Diligence (EDD) statement;
 - PNDI review and coordination with resource protection agencies; and
 - Public outreach.
-

Appendix A
Engineering Information

Project Identification

Originating Office: 10

Date: 07/19/21

Federal Project Number: N/A

Township/Municipality: Brookville Borough and Pine Creek Township

Local Name: I-80 North Fork Bridge Replacements

Limits of Work (Segment/Offset)

Construction Stations

Start:

End:

Start:

End:

0785/1578 (WB)

0811/1467 (WB)

333+50.00 (WB)

463+70.00 (WB)

0784/1258 (EB)

0810/0344 (EB)

332+00.00 (EB)

451+25.00 (EB)

Total Length: 12,908 (WB)/11,732 (EB) ft

Date of First Federal Authorization for Preliminary Engineering:

01-28-2019

Date of Federal Authorization Time Extension(s) for Preliminary Engineering (if applicable):

N/A

Design Criteria

Roadway Description: Interstate 80

Functional Classification: Freeways/Interstates

Urban Rural

Current ADT: 26241

Design Year No-Build ADT: 46,789

Current LOS: N/A

Design Year Build ADT: 46,789

Design Year Build LOS: N/A

DHV: 3275

Truck %: 43

D (Directional Distribution) %: 100

Design Speed: 70 mi/h

Posted Speed: 70 mi/h

Required Minimum Widths

Lane Width: 12 ft



Shoulder Width: 12 RT / 8 LT ft

Bridge Curb-to-Curb: 44 ft

Design Exception Required?

Yes No

Typology: Limited Access Freeway – Rural Interstate

Topography: Level Rolling Mountainous

Proposed Design Criteria: New and Reconstruction

Traffic Control Measures

The following traffic control measures will be implemented:

- Temporary Bridge(s)
- Temporary Roadway
- Detour
- Ramp Closure
- Other (specify)
- None

Other Description: Phased construction for SR 0080. Cross overs will be utilized to move traffic from I-80 WB to Temporary Roadway. Temporary Roadway will eventually be the new relocated I-80 EB roadway.

Provisions for access by local traffic will be made and so posted. True False

Through-traffic dependent business will not be adversely affected. True False

There will be no interference with any local special event or festival. True False

There will be no substantial environmental consequences associated with the traffic control measure(s). True False

There is no substantial controversy associated with the traffic control measure(s). True False

There are no substantial impacts to bicycle or pedestrian routes. True False

An alternate, ADA compliant bicycle/pedestrian access route is available. True False

There will be an impact to the sidewalk along SR 4003 for one construction season.

Approximate length of planned detour: See attached maps

Detour Map

Make the selection that best describes the planned detour:

- Detour will use local roads with no improvements.
- Detour will involve improvements to local roads with no resulting impacts on safety or the environment.
- Detour will involve improvements to local roads and will impact safety and/or the environment.
- Detour will use only state owned roads.

Describe impacts

The detour for SR 4003 (Jenks Street) will use state-owned roads only while the detour for SR 4005 (Richardsville Road) will use a combination of state-owned and local roads. No improvements or impacts are anticipated.

Estimated Costs

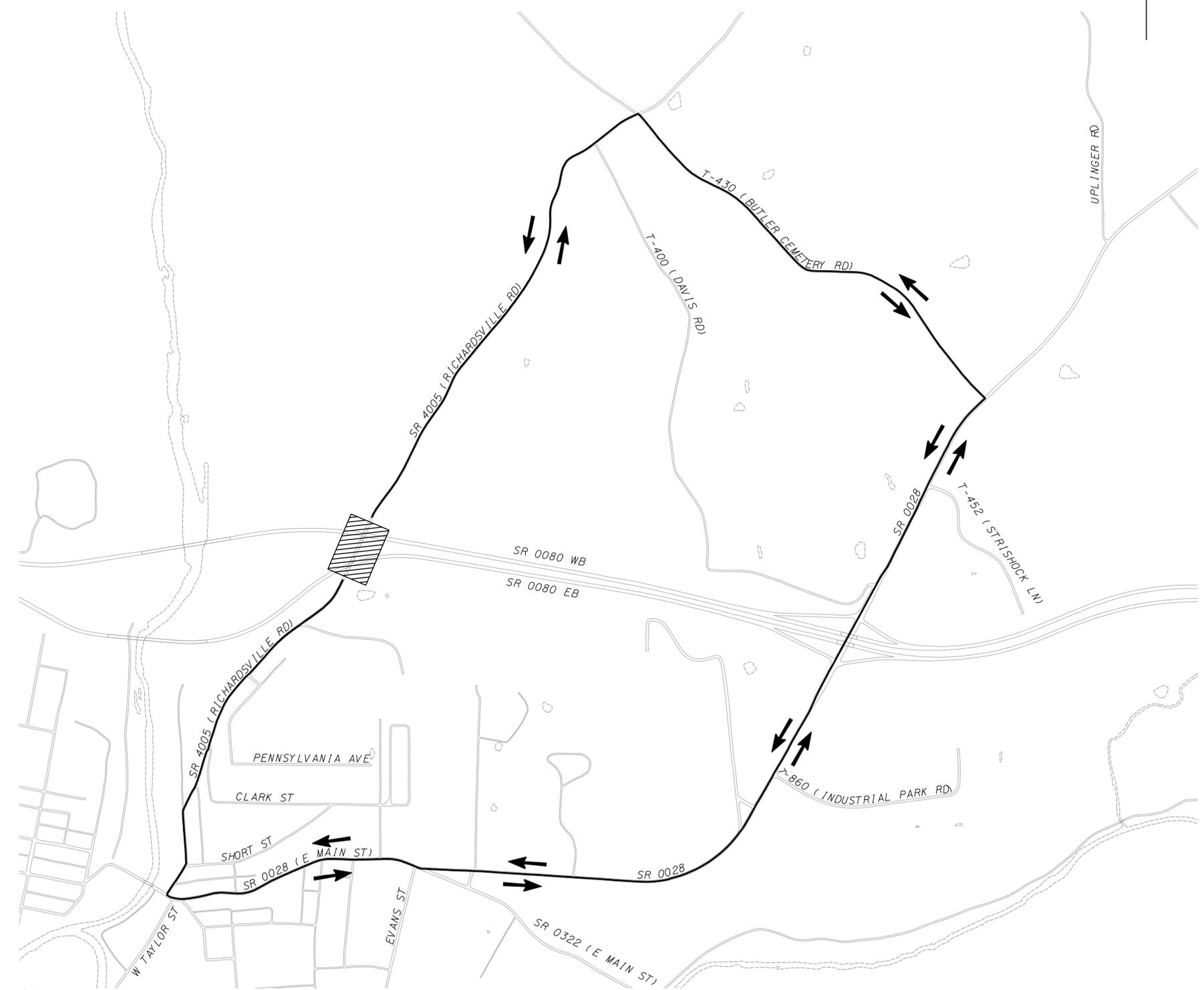
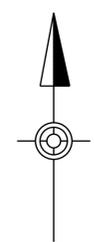
Engineering: \$ 6,953,000

Right-of-Way: \$ 1,093,000

Construction: \$ 176,887,000

Utilities: \$ 274,000

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	5 OF 362
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



TOTAL DETOUR LENGTH 5.6 MILES

NOTE:
SIGNING WILL BE INCLUDED IN THE
FINAL DESIGN TRAFFIC CONTROL PLAN

NOT TO SCALE

**TRAFFIC CONTROL PLAN
SR 4005 DETOUR**

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

LEGEND

- DETOUR ROUTE
- TYPE "B" LIGHT
- TRAFFIC FLOW
- SIGN LOCATION (TYPE B POST)
- TYPE III BARRICADE
- WORK AREA

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Roadway

Roadway Description

SR 0080 / Sec 550

	Existing	Proposed
Number of Lanes:	2	2
Lane Width:	12 ft	12 ft
Shoulder Width:	8 LT; 10 RT ft	8 LT; 12 RT ft
Median Width:	60 - 1,000 (varies) ft	50 (min) ft
Sidewalk Width:	N/A ft	N/A ft
Bicycle Lane Width:	N/A ft	N/A ft
Clear Zone Width:	26-30 ft	26-30 ft

Roadway Description

SR 4003 / Sec 550

	Existing	Proposed
Number of Lanes:	2	2
Lane Width:	10 ft	10 ft
Shoulder Width:	3 ft	4 ft
Median Width:	N/A ft	N/A ft
Sidewalk Width:	5 ft	5 ft
Bicycle Lane Width:	N/A ft	N/A ft
Clear Zone Width:	12-14 ft	12-14 ft

Roadway Description

SR 4005 / Sec 550

	Existing	Proposed
Number of Lanes:	2	2
Lane Width:	10 ft	11 ft
Shoulder Width:	3 ft	4 to 6 ft
Median Width:	N/A ft	N/A ft
Sidewalk Width:	N/A ft	N/A ft
Bicycle Lane Width:	N/A ft	N/A ft
Clear Zone Width:	8-14 ft	8-14 ft

Structure

BMS Number: 33-0080-0795-1178

BRKEY: 19565

Description: SR 0080 EB over North Fork Redbank Creek

	Existing	Proposed
Structure Type:	Steel Girder/Floorbeam/Stringer&Steel Multi-Girder	4-span Continuous Steel Plate Girder
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	32 ft	56 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	4 ft	12 LT; 20 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	35.42 ft	59.375 ft
*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.		
Under Clearance:	85 ft	114.08 ft
Lateral Clearance:	N/A ft	N/A ft
Sufficiency Rating:	58.7	
Structure Length:	1,078 ft	1,050 ft

BMS Number: 33-0080-0795-1290

BRKEY: 19566

Description: SR 0080 WB over North Fork Redbank Creek

	Existing	Proposed
Structure Type:	Steel Girder/Floorbeam/Stringer&Steel Multi-Girder	4-span Continuous Steel Plate Girder
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	32 ft	56 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	4 ft	12 LT; 20 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	35.42 ft	59.375 ft
*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.		
Under Clearance:	75 ft	113.83 ft
Lateral Clearance:	N/A ft	N/A ft
Sufficiency Rating:	95.0	
Structure Length:	1,076 ft	1050 ft

Structure

BMS Number: 33-0080-0790-0383

BRKEY: 19563

Description: SR 0080 EB over SR 0043 (Jenks Street)

	Existing	Proposed
Structure Type:	3-span PS Conc Spread Box Beam	Single Span Galvanized Steel Plate Girder
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	42 ft	62 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	7 LT; 11 RT ft	11 LT; 27 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	45.5 ft	65.88 ft
*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.		
Under Clearance:	15.69 ft	15.08 ft
Lateral Clearance:	6 ft	16.60 ft
Sufficiency Rating:	75.2	
Structure Length:	106 ft	70 ft

BMS Number: 33-0080-0791-0349

BRKEY: 19564

Description: SR 0080 WB over SR 4003 (Jenks Street)

	Existing	Proposed
Structure Type:	3-span PS Conc Spread Box Beam	Single Span Galvanized Steel Plate Girder
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	42 ft	62 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	7 LT; 11 RT ft	11 LT; 27 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	45.5 ft	65.88 ft
*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.		
Under Clearance:	15.58 ft	14.96 ft
Lateral Clearance:	6 ft	16.53 ft
Sufficiency Rating:	73.1	
Structure Length:	106 ft	70 ft

Structure

BMS Number: 33-4005-0040-0412

BRKEY: 19810

Description: SR 4005 (Richardsville Road) over SR 0080 EB and WB

	Existing	Proposed
Structure Type:	3-span PS Conc Spread Box Beam	2-span Continuous Galvanized Steel Plate Girder
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	28 ft	32 ft
Lane Width:	10 ft	11 ft
Shoulder Width:	4 ft	6 LT; 4 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	33.5 ft	35.38 ft

*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.

Under Clearance:	16.27 ft	16.52 ft
Lateral Clearance:	8.0 ft	22.75 ft
Sufficiency Rating:	77.6	
Structure Length:	120.98 ft	250 ft

BMS Number: 33-0080-0801-0588

BRKEY: 19567

Description: SR 0080 EB and WB over UNT to North Fork Redbank Creek

	Existing	Proposed
Structure Type:	Single Cell Reinforced Conc Arch Culvert	Single Cell Reinforced Conc Box Arch Culvert Ext.
Weight Restrictions:	N/A ton	N/A ton
Height Restrictions:	N/A ft	N/A ft
Curb to Curb Width:	42 ft	44 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	8 LT; 10 RT ft	8 LT; 12 RT ft
Sidewalk Width:	N/A ft	N/A ft
Total Bridge Width*:	42 ft	44 ft

*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.

Under Clearance:	N/A ft	N/A ft
Lateral Clearance:	N/A ft	N/A ft
Sufficiency Rating:	85.9	
Structure Length:	12.17 ft	12.7 ft

Appendix B
Preliminary Design Plans

DISTRICT	COUNTY	TOWNSHIP	BOROUGH	ROUTE	SECTION	TOTAL SHEETS
10-0	JEFFERSON	PINE CREEK	BROOKVILLE	0080	550	86
				0080	550	

SR 0080 PREVIOUSLY KNOWN AS LR 1009

ECMS/MPMS NO 106275

COMMONWEALTH OF PENNSYLVANIA



DEPARTMENT OF TRANSPORTATION

DRAWINGS FOR CONSTRUCTION

OF
STATE ROUTE 0080 SECTION 550
IN JEFFERSON COUNTY
FROM STA 332+00.00 EB TO STA 451+25.00 EB LENGTH 11,732.00 FT 2.22 MI
FROM SEGMENT 0784 OFFSET 1258 TO SEGMENT 0810 OFFSET 0344
FROM STA 333+50.00 WB TO STA 456+50.00 WB LENGTH 12,088.00 FT 2.29 MI
FROM SEGMENT 0785 OFFSET 1578 TO SEGMENT 0811 OFFSET 0617

ALSO
STATE ROUTE 4003

FROM STA 33+45.00 TO STA 40+65.00

ALSO
STATE ROUTE 4005

FROM STA 41+50.00 TO STA 52+25.00

ALSO
WATER PLANT ROAD

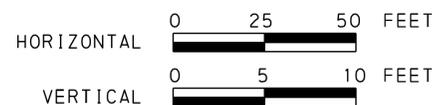
FROM STA 69+00.00 TO STA 76+10.00

ALSO INCLUDED:

TRAFFIC CONTROL PLAN	362 SHEETS
EROSION AND SEDIMENT POLLUTION CONTROL PLAN	101 SHEETS
ENVIRONMENTAL MITIGATION PLAN	21 SHEETS
POST CONSTRUCTION STORMWATER MANAGEMENT PLAN	36 SHEETS
STRUCTURE PLANS	
S-39935 (SR 0080 EASTBOUND BRIDGE OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD)	5 SHEETS
S-39936 (SR 0080 WESTBOUND BRIDGE OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD)	5 SHEETS
S-40035 (SR 4005 OVER SR 0080 BRIDGE)	4 SHEETS
S-40040 (NORTH FORK PARK, CULVERT NO. 2)	4 SHEETS
S-40041 (SR 0080 EASTBOUND OVER SR 4003)	6 SHEETS
S-40042 (SR 0080 WESTBOUND OVER SR 4003)	6 SHEETS
CROSS SECTIONS	336 SHEETS

SR 0080
ESTABLISHED AS A LIMITED ACCESS HIGHWAY FROM
334+00.00BK=
STATION 335+68.11AHD TO STATION 515+58.00 EASTBOUND
STATION 335+01.52 TO STATION 516+20.00 WESTBOUND
LEGISLATIVE ROUTE 1009 SECTION 3 R/W
APPROVED DECEMBER 20, 1960

SCALE



DESIGN DESIGNATION

SR 0080

HIGHWAY CLASSIFICATION	- INTERSTATE/FREEWAYS
HIGHWAY TYPOLOGY	- LIMITED ACCESS FREEWAY, RURAL INTERSTATE
DESIGN SPEED	- 70 MPH
PAVEMENT WIDTH	- 12'-0" LANES
SHOULDER WIDTH	- 8'-0" LT, 12'-0" RT
MEDIAN WIDTH	- 50'-0" MIN

TRAFFIC DATA

CURRENT YEAR ADT	- 15,506 EB (2026)
	- 15,613 WB (2026)
DESIGN YEAR ADT	- 23,314 EB (2046)
	- 23,475 WB (2046)
DHV	- 1,642 EB
DHV	- 1,643 WB
D	- 100%
T	- 43% EB
T	- 42% WB

DESIGN FIELD VIEW SUBMISSION SEPTEMBER 2021

Michael Baker

INTERNATIONAL

AIRSIDE BUSINESS PARK
100 AIRSIDE DRIVE
MOON TOWNSHIP, PA 15108
Phone: (412) 269-6300

PROJECT MANAGER DATE
REGISTERED PROFESSIONAL
ENGINEER

RECOMMENDED DATE: ____/____/20____

DISTRICT EXECUTIVE

RECOMMENDED DATE: ____/____/20____

DEPUTY SECRETARY

APPROVED DATE: ____/____/20____

SECRETARY OF TRANSPORTATION

(ON BEHALF OF THE GOVERNOR AS WELL AS
THE SECRETARY)

TABULATION OF SEGMENT EQUALITIES

SEGMENT 0784 OFFSET 2654 = STA 345+96 EB SR 0080 =
 SEGMENT 0790 OFFSET 0000 = STA 345+96 EB SR 0080 =
 SEGMENT 0790 OFFSET 2640 = STA 372+36 EB SR 0080 =
 SEGMENT 0794 OFFSET 0000 = STA 372+36 EB SR 0080 =
 SEGMENT 0794 OFFSET 2640 = STA 398+76 EB SR 0080 =
 SEGMENT 0800 OFFSET 0000 = STA 398+76 EB SR 0080 =
 SEGMENT 0800 OFFSET 2253 = STA 421+29 EB SR 0080 =
 SEGMENT 0804 OFFSET 0000 = STA 421+29 EB SR 0080 =
 SEGMENT 0804 OFFSET 2652 = STA 447+81 EB SR 0080 =
 SEGMENT 0810 OFFSET 0000 = STA 447+81 EB SR 0080 =

SEGMENT 0785 OFFSET 2757 = STA 345+29 WB SR 0080 =
 SEGMENT 0791 OFFSET 0000 = STA 345+29 WB SR 0080 =
 SEGMENT 0791 OFFSET 2742 = STA 372+71 WB SR 0080 =
 SEGMENT 0795 OFFSET 0000 = STA 372+71 WB SR 0080 =
 SEGMENT 0795 OFFSET 2615 = STA 398+86 WB SR 0080 =
 SEGMENT 0801 OFFSET 0000 = STA 398+86 WB SR 0080 =
 SEGMENT 0801 OFFSET 2640 = STA 425+26 WB SR 0080 =
 SEGMENT 0805 OFFSET 0000 = STA 425+26 WB SR 0080 =
 SEGMENT 0805 OFFSET 2507 = STA 450+33 WB SR 0080 =
 SEGMENT 0811 OFFSET 0000 = STA 450+33 WB SR 0080 =

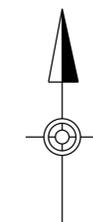
SEGMENT 0010 OFFSET 2440 = STA 23+29 SR 4003 =
 SEGMENT 0020 OFFSET 0000 = STA 23+29 SR 4003 =

SEGMENT 0030 OFFSET 2601 = STA 44+12 SR 4005 =
 SEGMENT 0040 OFFSET 0000 = STA 44+12 SR 4005 =

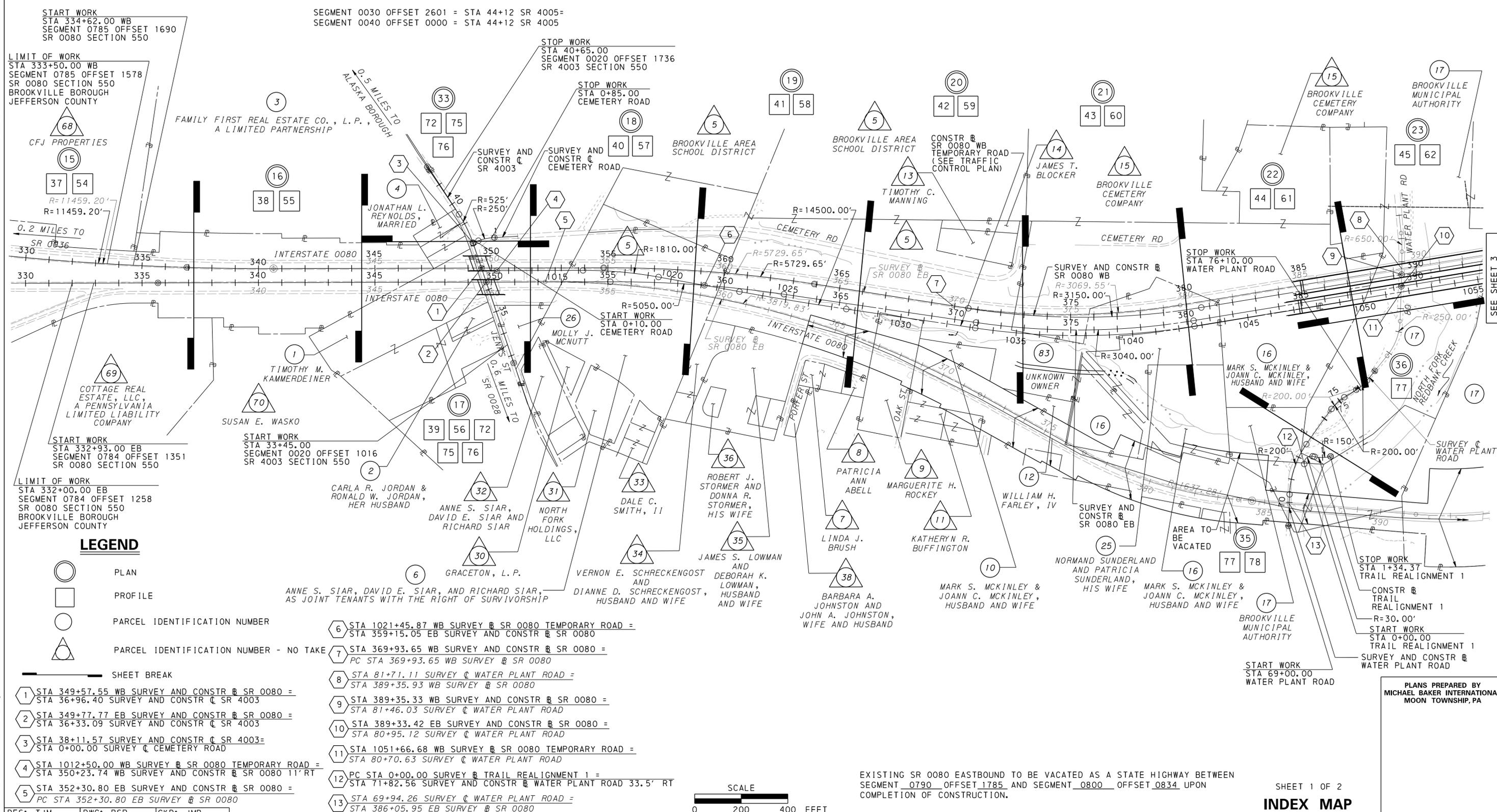
SHEET INDEX BLOCK

DESCRIPTION	SHEET
TITLE SHEET	1
INDEX MAPS	2 TO 3
GENERAL NOTES AND LOCATION MAP	4
PROJECT COORDINATES	5
TYPICAL SECTIONS AND DETAILS	6 TO 14
PLAN SHEETS	15 TO 36
PROFILE SHEETS	37 TO 78
CONCEPTUAL TEMPORARY ACCESS ROAD PLANS	79 TO 80
CONCEPTUAL TEMPORARY ACCESS ROAD PROFILES	81 TO 86

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	2 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



9/28/2021 11:58:30 AM I-80_Nor.th_Fork_.inc01.dgn



START WORK
 STA 334+62.00 WB
 SEGMENT 0785 OFFSET 1690
 SR 0080 SECTION 550

LIMIT OF WORK
 STA 333+50.00 WB
 SEGMENT 0785 OFFSET 1578
 SR 0080 SECTION 550
 BROOKVILLE BOROUGH
 JEFFERSON COUNTY

STOP WORK
 STA 40+65.00
 SEGMENT 0020 OFFSET 1736
 SR 4003 SECTION 550

STOP WORK
 STA 0+85.00
 CEMETERY ROAD

STOP WORK
 STA 0+10.00
 CEMETERY ROAD

STOP WORK
 STA 76+10.00
 WATER PLANT ROAD

START WORK
 STA 332+93.00 EB
 SEGMENT 0784 OFFSET 1351
 SR 0080 SECTION 550

LIMIT OF WORK
 STA 332+00.00 EB
 SEGMENT 0784 OFFSET 1258
 SR 0080 SECTION 550
 BROOKVILLE BOROUGH
 JEFFERSON COUNTY

START WORK
 STA 33+45.00
 SEGMENT 0020 OFFSET 1016
 SR 4003 SECTION 550

STOP WORK
 STA 1+34.37
 TRAIL REALIGNMENT 1

CONSTR &
 TRAIL
 REALIGNMENT 1
 R=30.00'

START WORK
 STA 0+00.00
 TRAIL REALIGNMENT 1

SURVEY AND CONSTR &
 WATER PLANT ROAD

START WORK
 STA 69+00.00
 WATER PLANT ROAD

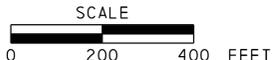
LEGEND

- PLAN
- PROFILE
- PARCEL IDENTIFICATION NUMBER
- PARCEL IDENTIFICATION NUMBER - NO TAKE
- SHEET BREAK

- 1 STA 349+57.55 WB SURVEY AND CONSTR & SR 0080 =
 STA 36+96.40 SURVEY AND CONSTR & SR 4003
- 2 STA 349+77.77 EB SURVEY AND CONSTR & SR 0080 =
 STA 36+33.09 SURVEY AND CONSTR & SR 4003
- 3 STA 38+11.57 SURVEY AND CONSTR & SR 4003 =
 STA 0+00.00 SURVEY & CEMETERY ROAD
- 4 STA 1012+50.00 WB SURVEY & SR 0080 TEMPORARY ROAD =
 STA 350+23.74 WB SURVEY AND CONSTR & SR 0080 11' RT
- 5 STA 352+30.80 EB SURVEY AND CONSTR & SR 0080 =
 PC STA 352+30.80 EB SURVEY & SR 0080

- 6 STA 1021+45.87 WB SURVEY & SR 0080 TEMPORARY ROAD =
 STA 359+15.05 EB SURVEY AND CONSTR & SR 0080
- 7 STA 369+93.65 WB SURVEY AND CONSTR & SR 0080 =
 PC STA 369+93.65 WB SURVEY & SR 0080
- 8 STA 81+71.11 SURVEY & WATER PLANT ROAD =
 STA 389+35.93 WB SURVEY & SR 0080
- 9 STA 389+35.33 WB SURVEY AND CONSTR & SR 0080 =
 STA 81+46.03 SURVEY & WATER PLANT ROAD
- 10 STA 389+33.42 EB SURVEY AND CONSTR & SR 0080 =
 STA 80+95.12 SURVEY & WATER PLANT ROAD
- 11 STA 1051+66.68 WB SURVEY & SR 0080 TEMPORARY ROAD =
 STA 80+70.63 SURVEY & WATER PLANT ROAD
- 12 PC STA 0+00.00 SURVEY & TRAIL REALIGNMENT 1 =
 STA 71+82.56 SURVEY AND CONSTR & WATER PLANT ROAD 33.5' RT
- 13 STA 69+94.26 SURVEY & WATER PLANT ROAD =
 STA 386+05.95 EB SURVEY & SR 0080

EXISTING SR 0080 EASTBOUND TO BE VACATED AS A STATE HIGHWAY BETWEEN
 SEGMENT 0790 OFFSET 1785 AND SEGMENT 0800 OFFSET 0834 UPON
 COMPLETION OF CONSTRUCTION.



SHEET 1 OF 2
INDEX MAP

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	3 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

LEGEND

- PLAN
- PROFILE
- PARCEL IDENTIFICATION NUMBER
- △ PARCEL IDENTIFICATION NUMBER - NO TAKE
- SHEET BREAK

LIMIT OF WORK
 STA 456+50.00 WB
 SEGMENT 0811 OFFSET 0617
 SR 0080 SECTION 550
 PINE CREEK TOWNSHIP
 JEFFERSON COUNTY

STOP WORK
 STA 455+50.00 WB
 SEGMENT 0811 OFFSET 0517
 SR 0080 SECTION 550

SURVEY # SR 0080 WB
 POT STA 448+71.26 BK =
 SURVEY # SR 0080 WB
 POT STA 452+49.87 AHD

SURVEY # SR 0080 WB
 PC STA 440+10.31 BK =
 SURVEY # SR 0080 WB
 PC STA 440+37.50 AHD

DIGGINS LLC,
 A PENNSYLVANIA LIMITED
 LIABILITY COMPANY

JAMES E. WINNER, JR.,
 AND
 DONNA C. WINNER,
 HIS WIFE

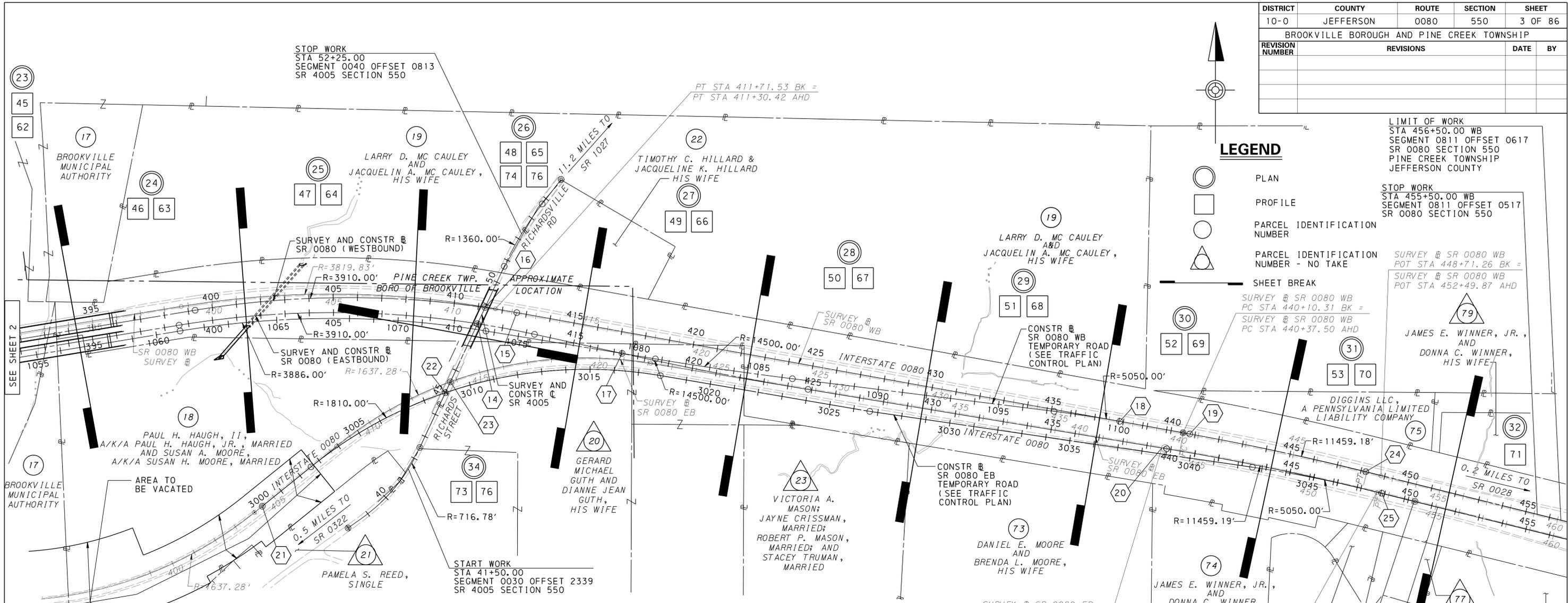
OUTDOORSMAN
 INVENTORY, LLC

LIMIT OF WORK
 STA 451+25.00 EB
 SEGMENT 0810 OFFSET 0344
 SR 0080 SECTION 550
 PINE CREEK TOWNSHIP
 JEFFERSON COUNTY

STOP WORK
 STA 450+25.00 EB
 SEGMENT 0810 OFFSET 0244
 SR 0080 SECTION 550

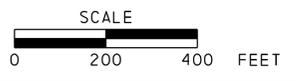
JEFFERSON COUNTY
 E. M. S., INC.,
 A NON-PROFIT
 CORPORATION

DICKINSON
 CENTER, INC.,
 A PENNSYLVANIA
 NON-PROFIT,
 NON-STOCK
 CORPORATION



RECORD OF EXISTING ROAD TYPES

SR 0080 EB	SR 0080 WB	SR 0080 WB	SR 4003	SR 4005
SEGMENT 0784 OFFSET 1258 TO * SEE BELOW OFFSET 2654 SEGMENT 0790 OFFSET 0000 TO OFFSET 0383 SEGMENT 0790 OFFSET 0490 TO OFFSET 2640 SEGMENT 0794 OFFSET 0000 TO OFFSET 2635 SEGMENT 0800 OFFSET 0000 TO OFFSET 2640 SEGMENT 0804 OFFSET 0000 TO OFFSET 2652 SEGMENT 0810 OFFSET 0 TO OFFSET 0344	SEGMENT 0785 OFFSET 1578 TO ** SEE BELOW OFFSET 2071 SEGMENT 0785 OFFSET 2028 TO OFFSET 2071 SEGMENT 0785 OFFSET 2071 TO OFFSET 2757 SEGMENT 0791 OFFSET 0000 TO OFFSET 2742 SEGMENT 0795 OFFSET 0000 TO OFFSET 2315 SEGMENT 0801 OFFSET 0000 TO OFFSET 2164 SEGMENT 0805 OFFSET 0000 TO OFFSET 2507 SEGMENT 0811 OFFSET 0000 TO OFFSET 0617	SEGMENT 0785 *** 2002 1.5" SPAV, HMA WRG, 64-22, 12.5MM, H 2002 1.0" SPAV, HMA WRGLVL, 64-22, 9.5, L 1985 2.0" ID-2 BINDER COURSE 1985 3.0" BCBC ON CONCRETE PAVEMENT 1961 10.0" RCCP 61.5' JNT SPACING/DOWEL 1961 8.0" UNKNOWN SUBBASE ** 2002 1.5" SPAV, HMA WRG, 76-22, 9.5MM, E 2002 0.75" SPAV, HMA WRGLVL, 76-22, 9.5, L 1985 2.0" ID-2 BINDER COURSE 1985 3.0" BCBC ON CONCRETE PAVEMENT 1961 10.0" RCCP 61.5' JNT SPACING/DOWEL 1961 8.0" UNKNOWN SUBBASE *** SEE ABOVE 2002 1.5" SPAV, HMA WRG, 76-22, 9.5MM, E 2002 1.0" SPAV, HMA WRGLVL, 76-22, 9.5, L 1985 2.0" ID-2 BINDER COURSE 1985 3.0" BCBC ON CONCRETE PAVEMENT 1961 10.0" RCCP 61.5' JNT SPACING/DOWEL 1961 8.0" UNKNOWN SUBBASE	SEGMENT 0020 OFFSET 1016 TO OFFSET 1736 SEGMENT 0030 OFFSET 2339 TO OFFSET 2601 SEGMENT 0040 OFFSET 0000 TO OFFSET 0813 NOTE: THE DEPTHS OF MATERIAL SHOWN ARE FOR DESIGN PURPOSES ONLY. ANY RISK OF UNANTICIPATED COSTS ASSOCIATED WITH DIFFERENCES BETWEEN THE LISTED DEPTHS AND THE ACTUAL DEPTHS SHALL BE ACCEPTED BY THE CONTRACTOR.	2002 1.5" SPAV, WMA WRG, 64-22, 9.5MM, G 2002 1.0" SPAV, WMA WRGLVL, 64-22, 9.5MM, L 1995 3.0" BITUM CONCRETE BASE COURSE 1969 1.0" BITUMINOUS WEARING COURSE ID-2 1956 2.0" BITUMINOUS WEARING COURSE ID-2 1956 2.0" ID-2 BINDER COURSE 1956 7.0" PARABOLIC RCCP 2002 1.5" BITUMINOUS WEARING COURSE 2002 0.5" SCRATCH BIT WEAR COURSE 1999 4.0" BITUM CONCRETE BASE COURSE 1963 2.5" BITUMINOUS WEARING COURSE ID-2 1963 9.0" CRUSHED AGGREGATE BASE COURSE 2002 1.5" BITUMINOUS WEARING COURSE 2002 0.5" SCRATCH BIT WEAR COURSE 1999 4.0" BITUM CONCRETE BASE COURSE 1963 2.5" BITUMINOUS WEARING COURSE ID-2 1963 9.0" CRUSHED AGGREGATE BASE COURSE
17 PT STA 424+84.42 EB SURVEY AND CONSTR # SR 0080 = STA 428+68.34 EB SURVEY # SR 0080 18 STA 437+85.03 WB SURVEY AND CONSTR # SR 0080 = POT STA 1100+03.81 WB SURVEY # SR 0080 TEMPORARY ROAD 19 PC STA 440+77.90 WB SURVEY AND CONSTR # SR 0080 = PC STA 440+64.69 WB SURVEY # SR 0080 20 POT STA 439+61.98 EB SURVEY AND CONSTR # SR 0080 = PC STA 443+60.23 EB AHD SURVEY # SR 0080 21 POT STA 3000+00.00 EB SURVEY # SR 0080 TEMPORARY ROAD = STA 404+39.16 EB SURVEY # SR 0080 22 PC STA 3008+93.16 EB SURVEY # SR 0080 TEMPORARY ROAD = POT STA 49+98.49 SURVEY AND CONSTR # SR 4005 23 POT STA 45+47.54 SURVEY AND CONSTR # SR 4005 = PC STA 413+73.40 EB SURVEY # SR 0080 24 PT STA 448+19.84 WB SURVEY AND CONSTR # SR 0080 = STA 448+06.57 WB, 1.76' RT SURVEY # SR 0080 25 PT STA 448+87.74 EB SURVEY AND CONSTR # SR 0080 = STA 452+70.56 EB, 1.64' RT SURVEY # SR 0080	76 JEFFERSON COUNTY E. M. S., INC., A NON-PROFIT CORPORATION 77 OUTDOORSMAN INVENTORY, LLC 78 DICKINSON CENTER, INC., A PENNSYLVANIA NON-PROFIT, NON-STOCK CORPORATION			

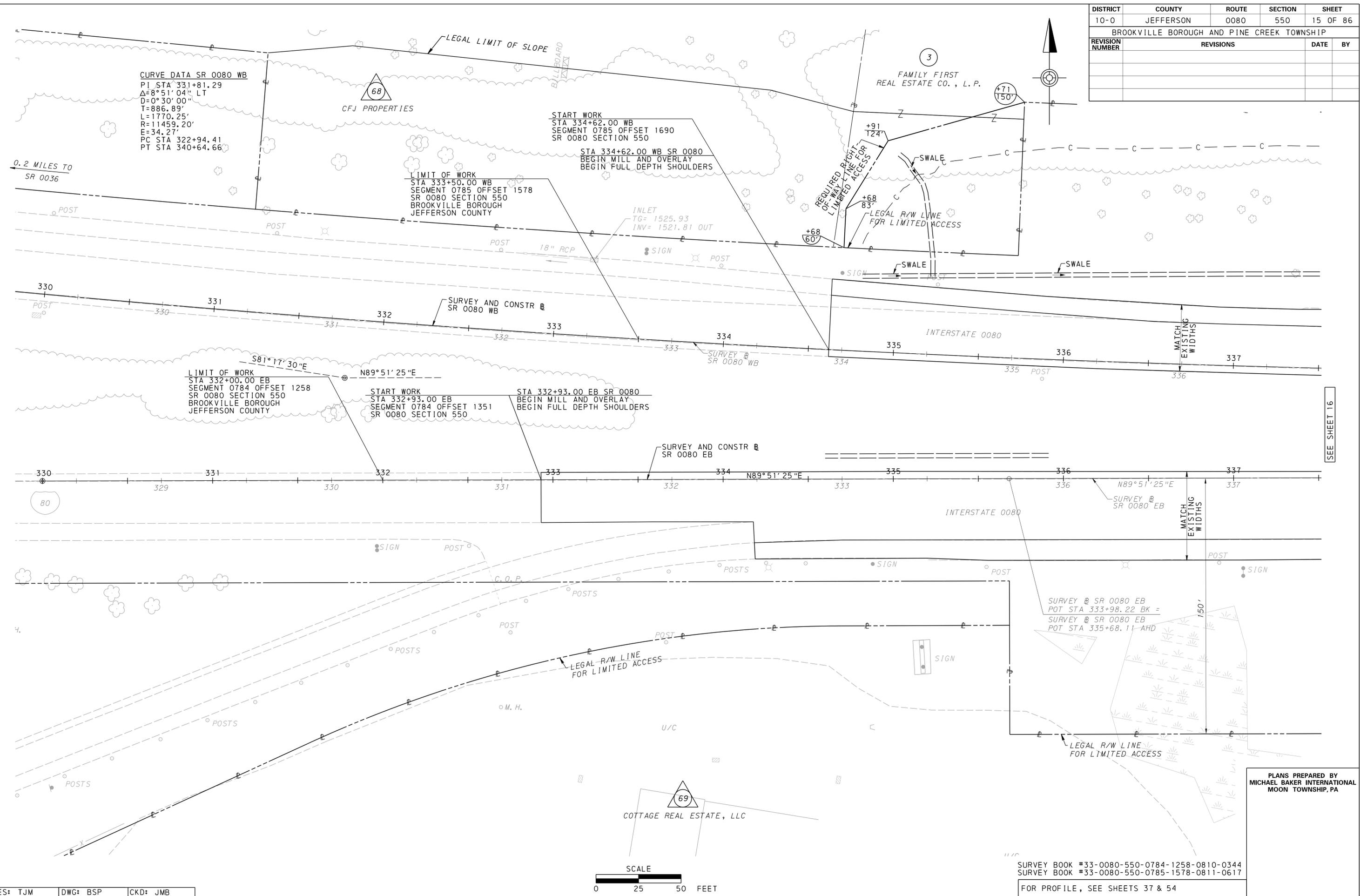


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 I-80_NorTh_Fork_inco2.dgn
 DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	15 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

9/28/2021 11:59:18 AM

I-80_Nor Th_Fork_Roadway_Plans.dgn



CURVE DATA SR 0080 WB
 PI STA 331+81.29
 $\Delta=8^{\circ}51'04''$ LT
 $D=0^{\circ}30'00''$
 $T=886.89'$
 $L=1770.25'$
 $R=11459.20'$
 $E=34.27'$
 PC STA 322+94.41
 PT STA 340+64.66

LIMIT OF WORK
 STA 332+00.00 EB
 SEGMENT 0784 OFFSET 1258
 SR 0080 SECTION 550
 BROOKVILLE BOROUGH
 JEFFERSON COUNTY

START WORK
 STA 332+93.00 EB
 SEGMENT 0784 OFFSET 1351
 SR 0080 SECTION 550

START WORK
 STA 334+62.00 WB
 SEGMENT 0785 OFFSET 1690
 SR 0080 SECTION 550

STA 334+62.00 WB SR 0080
 BEGIN MILL AND OVERLAY
 BEGIN FULL DEPTH SHOULDERS

LIMIT OF WORK
 STA 333+50.00 WB
 SEGMENT 0785 OFFSET 1578
 SR 0080 SECTION 550
 BROOKVILLE BOROUGH
 JEFFERSON COUNTY

SURVEY AND CONSTR #
 SR 0080 WB

SURVEY AND CONSTR #
 SR 0080 EB

SURVEY # SR 0080 EB
 POT STA 333+98.22 BK =
SURVEY # SR 0080 EB
 POT STA 335+68.11 AHD



SEE SHEET 16

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 37 & 54

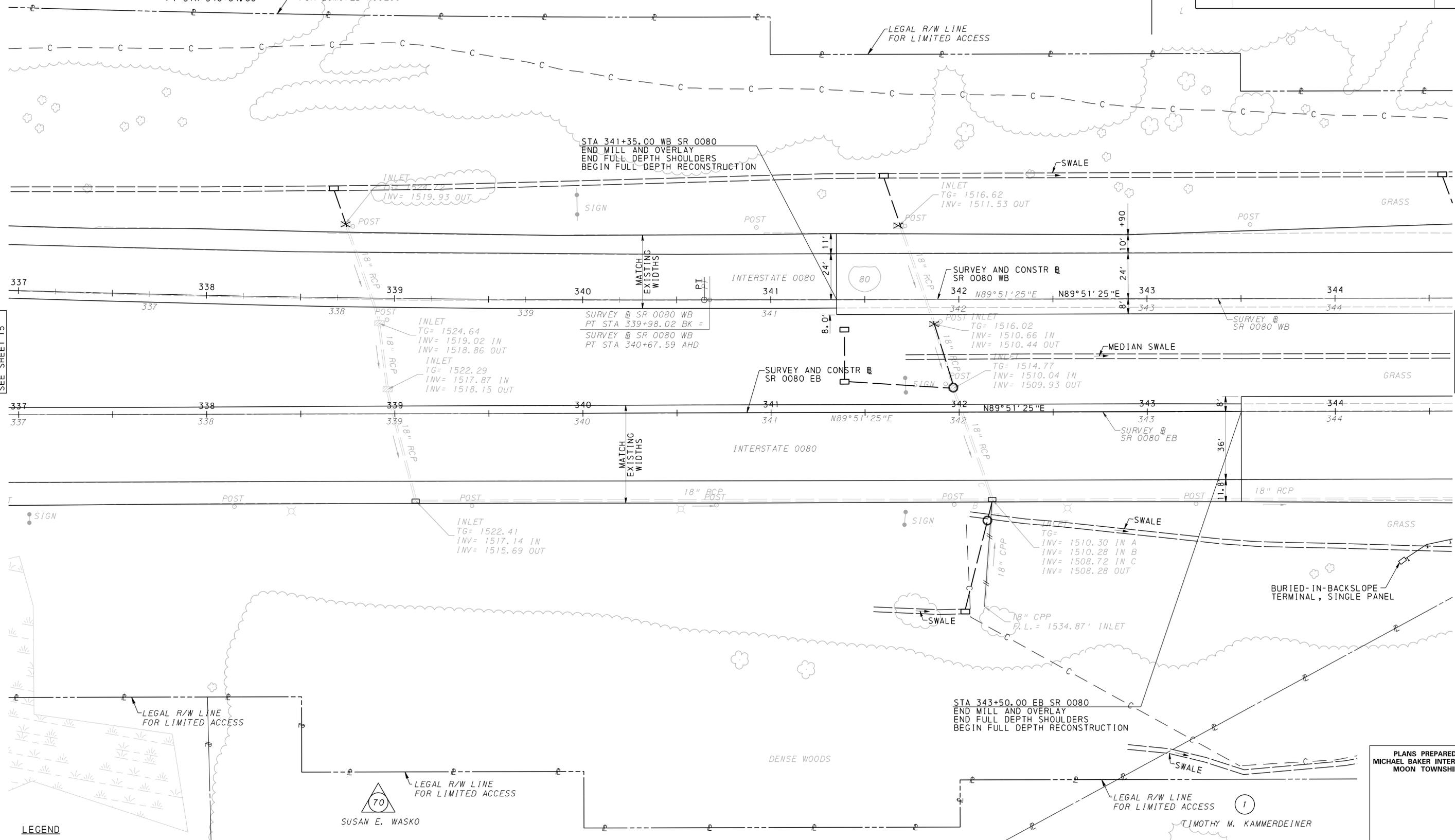
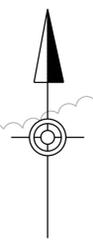
DES: TJM DWG: BSP CKD: JMB

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CURVE DATA SR 0080 WB
PI STA 331+81.29
 $\Delta=8^{\circ}51'04''$ LT
D=0°30'00"
T=886.89'
L=1770.25'
R=11459.20'
E=34.27'
PC STA 322+94.41
PT STA 340+64.66

3
FAMILY FIRST
REAL ESTATE CO., L.P.

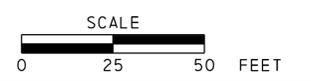
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	16 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



SEE SHEET 15

SEE SHEET 17

LEGEND
— PROPOSED DRAINAGE PIPE



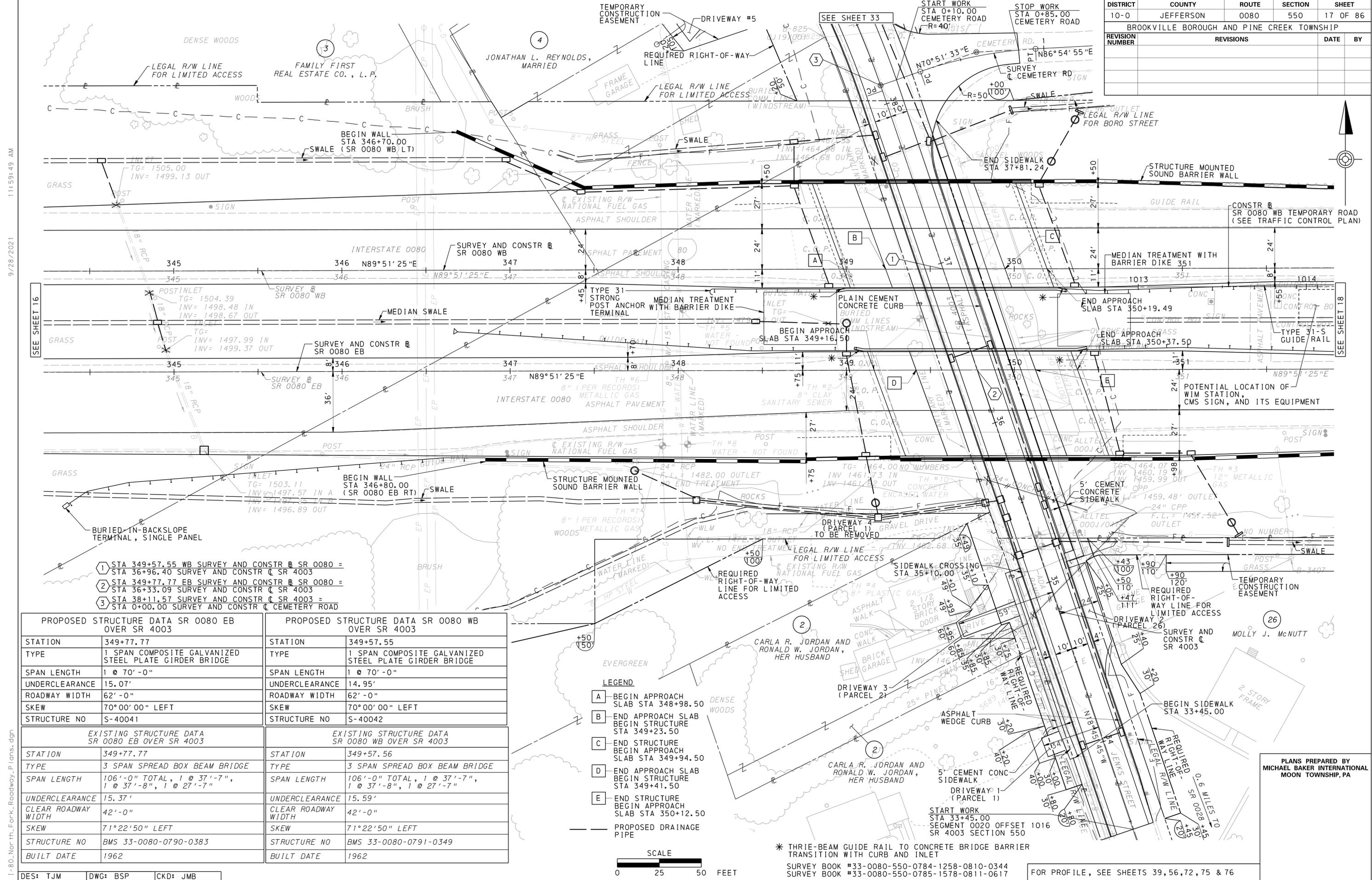
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 38 & 55

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

70
SUSAN E. WASKO

1
TIMOTHY M. KAMMERDEINER

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	17 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



- ① STA 349+57.55 WB SURVEY AND CONSTR SR 0080 = STA 36+96.40 SURVEY AND CONSTR SR 4003
- ② STA 349+77.77 EB SURVEY AND CONSTR SR 0080 = STA 36+33.09 SURVEY AND CONSTR SR 4003
- ③ STA 38+11.57 SURVEY AND CONSTR SR 4003 = STA 0+00.00 SURVEY AND CONSTR SR 0080

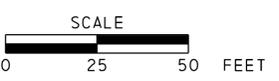
PROPOSED STRUCTURE DATA SR 0080 EB OVER SR 4003	
STATION	349+77.77
TYPE	1 SPAN COMPOSITE GALVANIZED STEEL PLATE GIRDER BRIDGE
SPAN LENGTH	1 @ 70'-0"
UNDERCLEARANCE	15.07'
ROADWAY WIDTH	62'-0"
SKEW	70°00'00" LEFT
STRUCTURE NO	S-40041

PROPOSED STRUCTURE DATA SR 0080 WB OVER SR 4003	
STATION	349+57.55
TYPE	1 SPAN COMPOSITE GALVANIZED STEEL PLATE GIRDER BRIDGE
SPAN LENGTH	1 @ 70'-0"
UNDERCLEARANCE	14.95'
ROADWAY WIDTH	62'-0"
SKEW	70°00'00" LEFT
STRUCTURE NO	S-40042

EXISTING STRUCTURE DATA SR 0080 EB OVER SR 4003	
STATION	349+77.77
TYPE	3 SPAN SPREAD BOX BEAM BRIDGE
SPAN LENGTH	106'-0" TOTAL, 1 @ 37'-7", 1 @ 37'-8", 1 @ 27'-7"
UNDERCLEARANCE	15.37'
CLEAR ROADWAY WIDTH	42'-0"
SKEW	71°22'50" LEFT
STRUCTURE NO	BMS 33-0080-0790-0383
BUILT DATE	1962

EXISTING STRUCTURE DATA SR 0080 WB OVER SR 4003	
STATION	349+57.56
TYPE	3 SPAN SPREAD BOX BEAM BRIDGE
SPAN LENGTH	106'-0" TOTAL, 1 @ 37'-7", 1 @ 37'-8", 1 @ 27'-7"
UNDERCLEARANCE	15.59'
CLEAR ROADWAY WIDTH	42'-0"
SKEW	71°22'50" LEFT
STRUCTURE NO	BMS 33-0080-0791-0349
BUILT DATE	1962

- LEGEND**
- A BEGIN APPROACH SLAB STA 348+98.50
 - B END APPROACH SLAB BEGIN STRUCTURE STA 349+23.50
 - C END STRUCTURE BEGIN APPROACH SLAB STA 349+94.50
 - D END APPROACH SLAB BEGIN STRUCTURE STA 349+41.50
 - E END STRUCTURE BEGIN APPROACH SLAB STA 350+12.50
- PROPOSED DRAINAGE PIPE



* THRIE-BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION WITH CURB AND INLET

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617

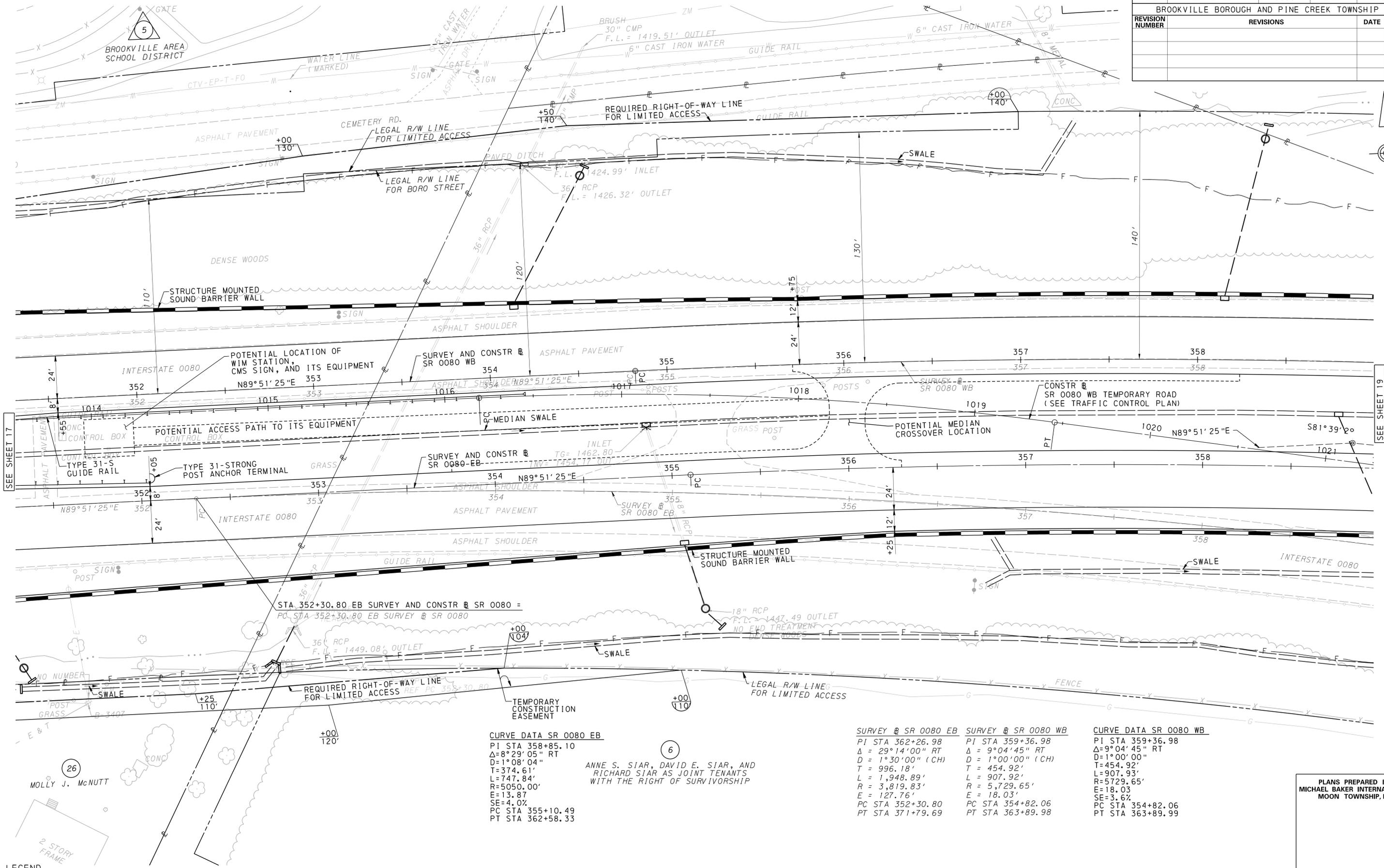
FOR PROFILE, SEE SHEETS 39, 56, 72, 75 & 76

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

9/28/2021 11:59:49 AM
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	18 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

9/28/2021 11:59:53 AM I-80_Nor.Th._Fork._Roadway_Plans.dgn



SEE SHEET 17

SEE SHEET 19

CURVE DATA SR 0080 EB
 PI STA 358+85.10
 $\Delta = 8^{\circ}29'05''$ RT
 $D = 1^{\circ}08'04''$
 $T = 374.61'$
 $L = 747.84'$
 $R = 5050.00'$
 $E = 13.87'$
 $SE = 4.0\%$
 PC STA 355+10.49
 PT STA 362+58.33

ANNE S. SIAR, DAVID E. SIAR, AND RICHARD SIAR AS JOINT TENANTS WITH THE RIGHT OF SURVIVORSHIP

SURVEY # SR 0080 EB
 PI STA 362+26.98
 $\Delta = 29^{\circ}14'00''$ RT
 $D = 1^{\circ}30'00''$ (CH)
 $T = 996.18'$
 $L = 1,948.89'$
 $R = 3,819.83'$
 $E = 127.76'$
 PC STA 352+30.80
 PT STA 371+79.69

SURVEY # SR 0080 WB
 PI STA 359+36.98
 $\Delta = 9^{\circ}04'45''$ RT
 $D = 1^{\circ}00'00''$ (CH)
 $T = 454.92'$
 $L = 907.92'$
 $R = 5,729.65'$
 $E = 18.03'$
 PC STA 354+82.06
 PT STA 363+89.98

CURVE DATA SR 0080 WB
 PI STA 359+36.98
 $\Delta = 9^{\circ}04'45''$ RT
 $D = 1^{\circ}00'00''$
 $T = 454.92'$
 $L = 907.93'$
 $R = 5729.65'$
 $E = 18.03'$
 $SE = 3.6\%$
 PC STA 354+82.06
 PT STA 363+89.99

LEGEND
 ——— PROPOSED DRAINAGE PIPE



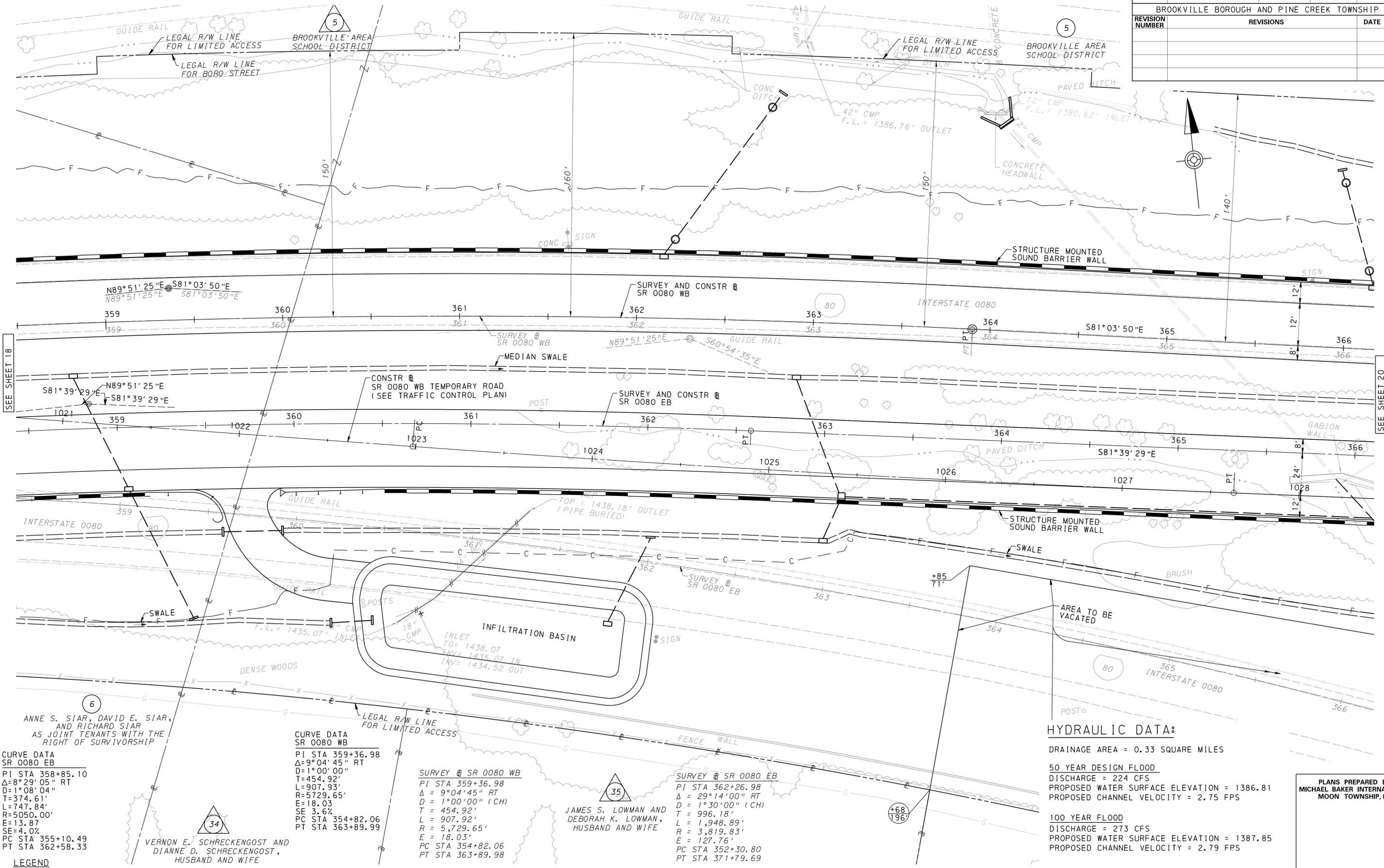
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 40 & 57

PLANS PREPARED BY MICHAEL BAKER INTERNATIONAL MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	19 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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CURVE DATA
 SR 0080 EB
 PI STA 358+85.10
 $\Delta = 8^{\circ}29'05''$ RT
 $D = 1^{\circ}08'04''$
 $T = 374.61'$
 $L = 747.84'$
 $R = 5050.00'$
 $E = 13.87'$
 $SE = 4.0\%$
 PC STA 355+10.49
 PT STA 362+58.33

34
 VERNON E. SCHRECKENGOST AND
 DIANNE D. SCHRECKENGOST,
 HUSBAND AND WIFE

CURVE DATA
 SR 0080 WB
 PI STA 359+36.98
 $\Delta = 9^{\circ}04'45''$ RT
 $D = 1^{\circ}00'00''$
 $T = 454.92'$
 $L = 907.93'$
 $R = 5729.65'$
 $E = 18.03'$
 $SE = 3.6\%$
 PC STA 354+82.06
 PT STA 363+89.99

SURVEY SR 0080 WB
 PI STA 359+36.98
 $\Delta = 9^{\circ}04'45''$ RT
 $D = 1^{\circ}00'00''$ (CH)
 $T = 454.92'$
 $L = 907.92'$
 $R = 5,729.65'$
 $E = 18.03'$
 PC STA 354+82.06
 PT STA 363+89.98

35
 JAMES S. LOWMAN AND
 DEBORAH K. LOWMAN,
 HUSBAND AND WIFE

SURVEY SR 0080 EB
 PI STA 362+26.98
 $\Delta = 29^{\circ}14'00''$ RT
 $D = 1^{\circ}30'00''$ (CH)
 $T = 996.18'$
 $L = 1,948.89'$
 $R = 3,819.83'$
 $E = 127.76'$
 PC STA 352+30.80
 PT STA 371+79.69

HYDRAULIC DATA:

DRAINAGE AREA = 0.33 SQUARE MILES
 50 YEAR DESIGN FLOOD
 DISCHARGE = 224 CFS
 PROPOSED WATER SURFACE ELEVATION = 1386.81
 PROPOSED CHANNEL VELOCITY = 2.75 FPS
 100 YEAR FLOOD
 DISCHARGE = 273 CFS
 PROPOSED WATER SURFACE ELEVATION = 1387.85
 PROPOSED CHANNEL VELOCITY = 2.79 FPS

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 41 & 58

LEGEND
 PROPOSED DRAINAGE PIPE

DES: TJM DWG: BSP CKD: JMB



PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

HYDRAULIC DATA:

DRAINAGE AREA = 0.33 SQUARE MILES

50 YEAR DESIGN FLOOD

DISCHARGE = 224 CFS
 PROPOSED WATER SURFACE ELEVATION = 1386.81
 PROPOSED CHANNEL VELOCITY = 2.75 FPS

100 YEAR FLOOD

DISCHARGE = 273 CFS
 PROPOSED WATER SURFACE ELEVATION = 1387.85
 PROPOSED CHANNEL VELOCITY = 2.79 FPS

CURVE DATA SR 0080 EB

PI STA 375+64.45
 $\Delta = 17^\circ 40' 20''$ LT
 $D = 1^\circ 53' 05''$
 $T = 472.58'$
 $L = 937.66'$
 $R = 3040.00'$
 $E = 36.51'$
 $SE = 6.2\%$
 PC STA 370+91.87
 PT STA 380+29.53

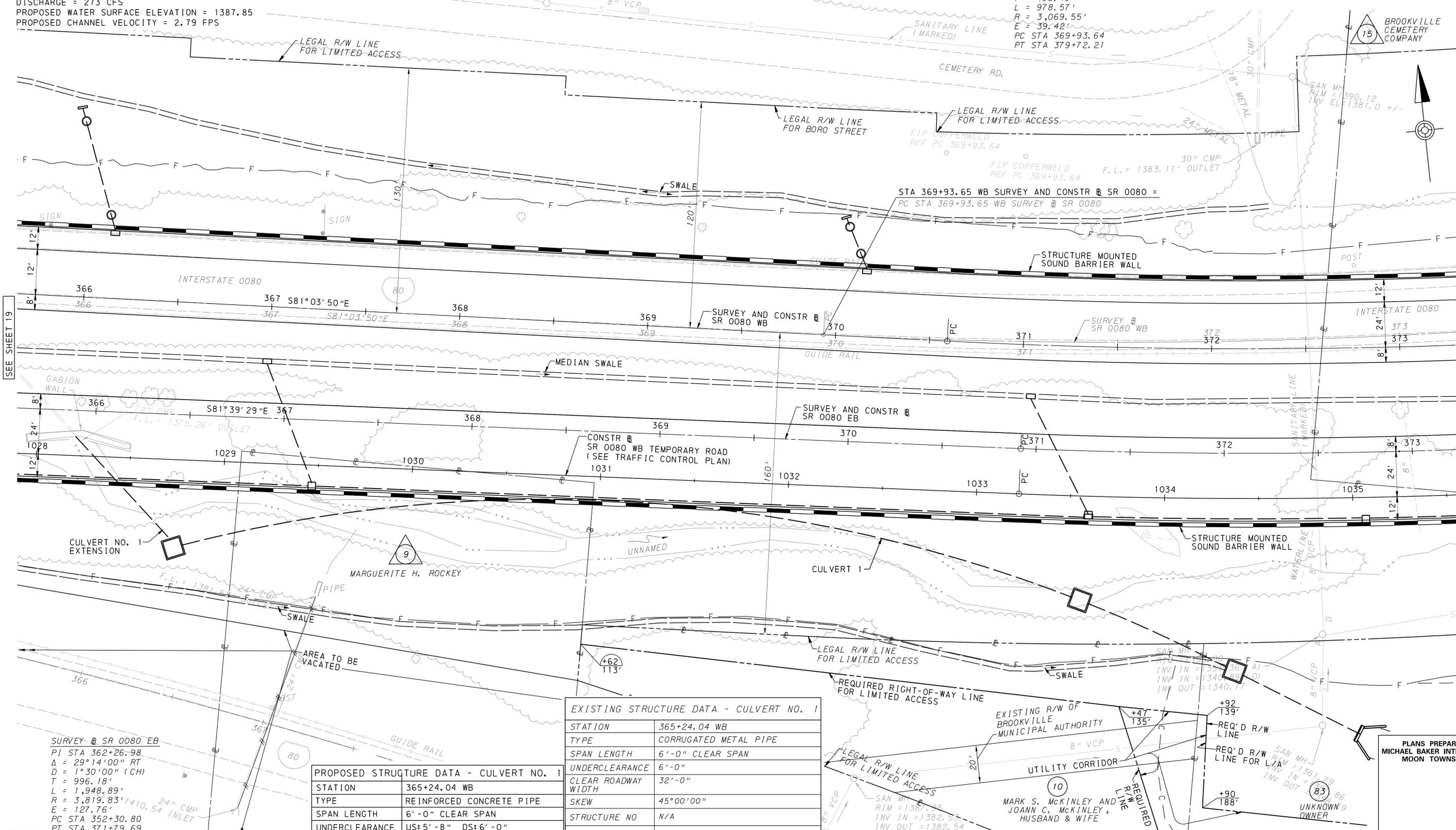
CURVE DATA SR 0080 WB

PI STA 375+65.90
 $\Delta = 18^\circ 16' 00''$ LT
 $D = 1^\circ 49' 08''$
 $T = 506.43'$
 $L = 1004.26'$
 $R = 3150.00'$
 $E = 40.45'$
 $SE = 6.0\%$
 PC STA 370+59.48
 PT STA 380+63.74

SURVEY # SR 0080 WB
 PI STA 374+87.13
 $\Delta = 18^\circ 16' 00''$ LT
 $D = 1^\circ 52' 00''$ (CH)
 $T = 493.49'$
 $L = 978.57'$
 $R = 3,069.55'$
 $E = 39.42'$
 PC STA 369+93.64
 PT STA 379+72.21

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	20 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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SEE SHEET 19

SEE SHEET 21

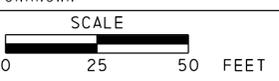
STATION	365+24.04 WB
TYPE	CORRUGATED METAL PIPE
SPAN LENGTH	6'-0" CLEAR SPAN
UNDERCLEARANCE	6'-0"
CLEAR ROADWAY WIDTH	32'-0"
SKEW	45°00'00"
STRUCTURE NO	N/A
BUILT DATE	UNKNOWN

STATION	365+24.04 WB
TYPE	REINFORCED CONCRETE PIPE
SPAN LENGTH	6'-0" CLEAR SPAN
UNDERCLEARANCE	US: 5'-8" DS: 6'-0"
ROADWAY WIDTH	42'-0"
SKEW	45°00'00"
STRUCTURE NO	N/A

SURVEY # SR 0080 EB
 PI STA 362+26.98
 $\Delta = 29^\circ 14' 00''$ RT
 $D = 1^\circ 30' 00''$ (CH)
 $T = 996.18'$
 $L = 1,948.89'$
 $R = 3,819.83'$
 $E = 127.76'$
 PC STA 352+30.80
 PT STA 371+79.69

LEGEND
 ——— PROPOSED DRAINAGE PIPE
 ——— PIPE

DES: TJM DWG: BSP CKD: JMB

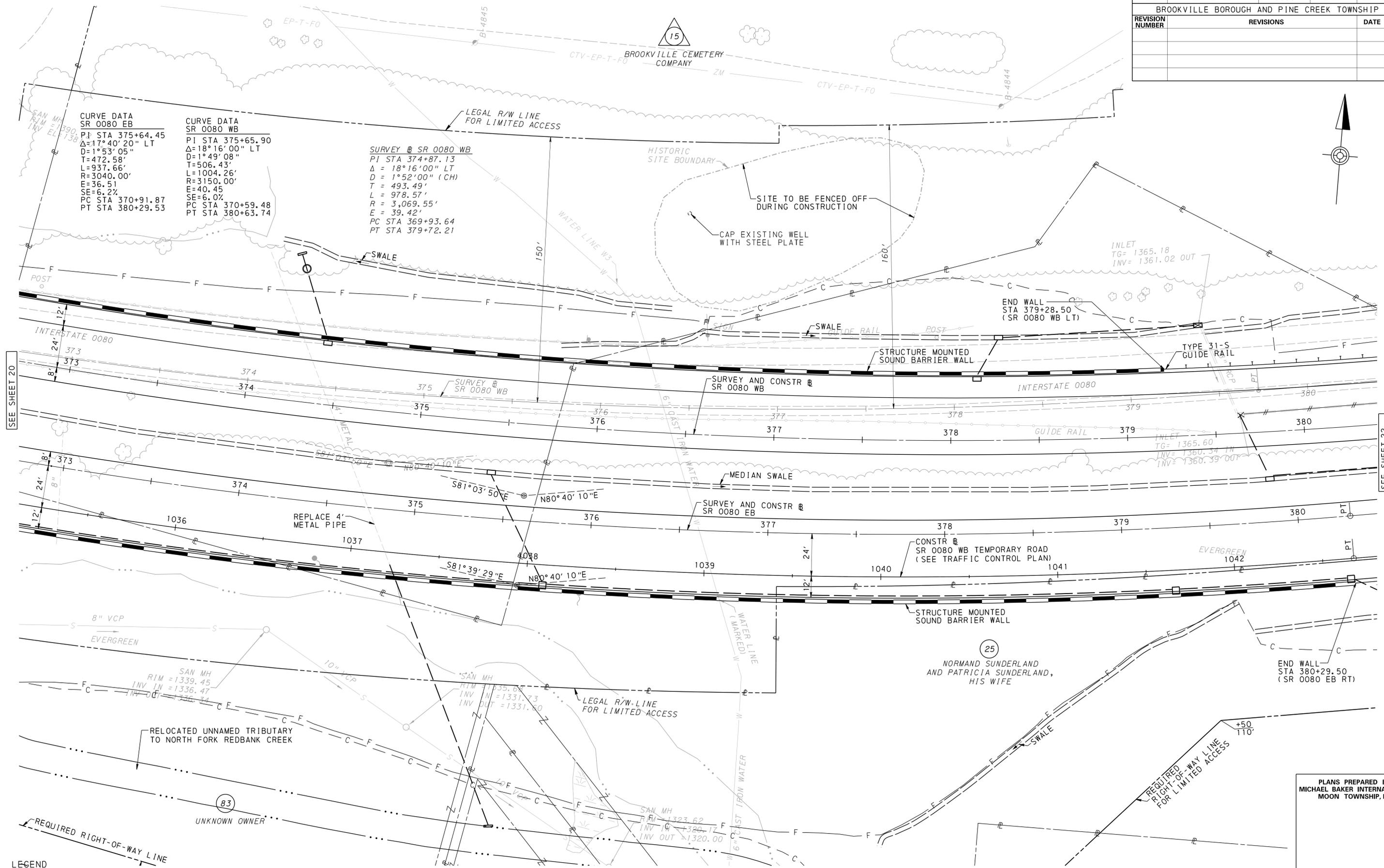


SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 42 & 59

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	21 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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SEE SHEET 20

SEE SHEET 22

LEGEND
 ——— PROPOSED DRAINAGE PIPE



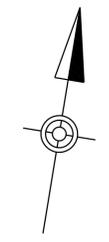
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 43 & 60

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	22 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

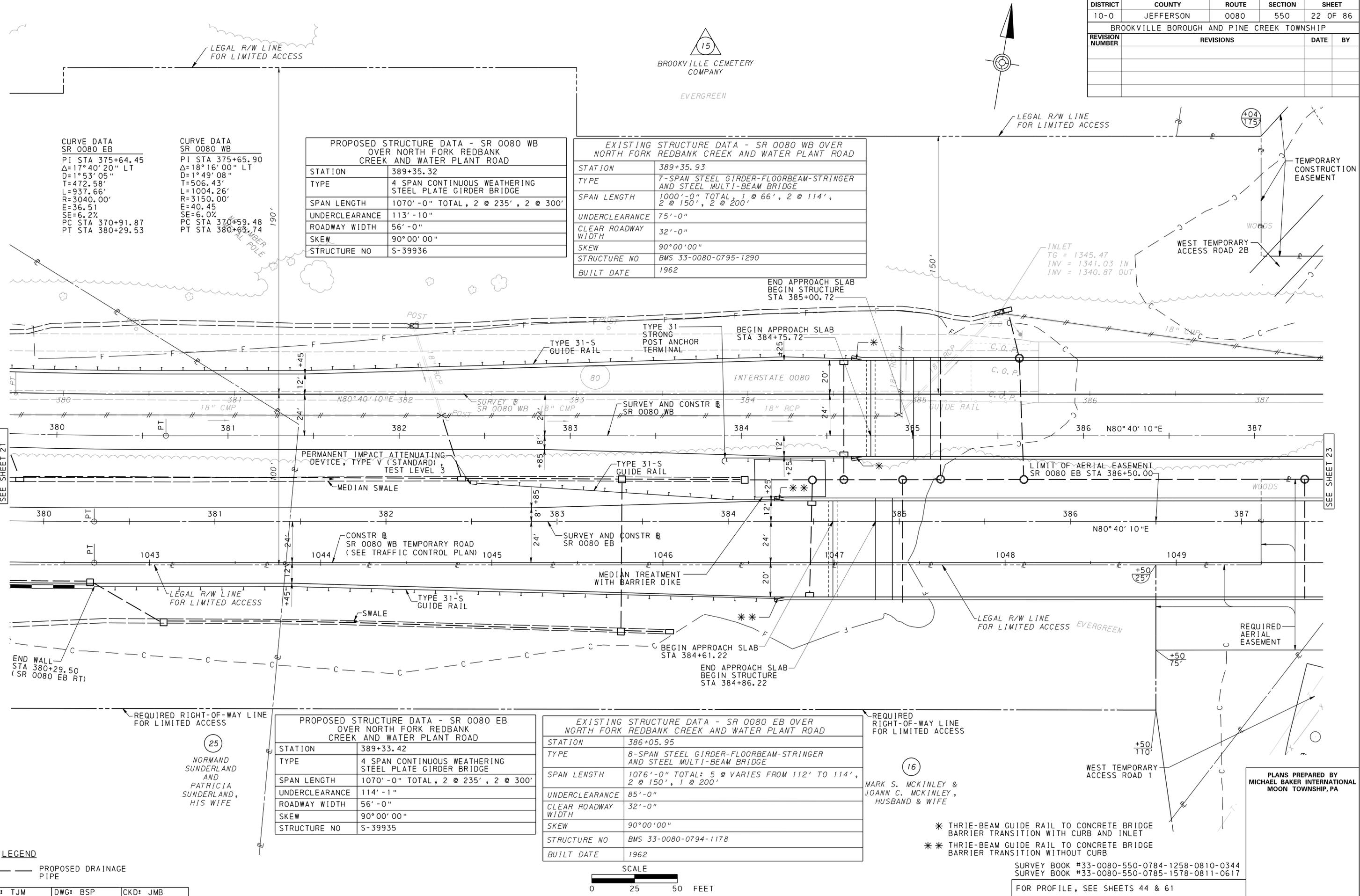
15
BROOKVILLE CEMETERY COMPANY
EVERGREEN



CURVE DATA SR 0080 EB	CURVE DATA SR 0080 WB
PI STA 375+64.45	PI STA 375+65.90
Δ=17°40'20" LT	Δ=18°16'00" LT
D=1°53'05"	D=1°49'08"
T=472.58'	T=506.43'
L=937.66'	L=1004.26'
R=3040.00'	R=3150.00'
E=36.51'	E=40.45'
SE=6.2%	SE=6.0%
PC STA 370+91.87	PC STA 370+59.48
PT STA 380+29.53	PT STA 380+63.74

PROPOSED STRUCTURE DATA - SR 0080 WB OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD	
STATION	389+35.32
TYPE	4 SPAN CONTINUOUS WEATHERING STEEL PLATE GIRDER BRIDGE
SPAN LENGTH	1070'-0" TOTAL, 2 @ 235', 2 @ 300'
UNDERCLEARANCE	113'-10"
ROADWAY WIDTH	56'-0"
SKEW	90°00'00"
STRUCTURE NO	S-39936

EXISTING STRUCTURE DATA - SR 0080 WB OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD	
STATION	389+35.93
TYPE	7-SPAN STEEL GIRDER-FLOORBEAM-STRINGER AND STEEL MULTI-BEAM BRIDGE
SPAN LENGTH	1000'-0" TOTAL, 1 @ 66', 2 @ 114', 2 @ 150', 2 @ 200'
UNDERCLEARANCE	75'-0"
CLEAR ROADWAY WIDTH	32'-0"
SKEW	90°00'00"
STRUCTURE NO	BMS 33-0080-0795-1290
BUILT DATE	1962



PROPOSED STRUCTURE DATA - SR 0080 EB OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD	
STATION	389+33.42
TYPE	4 SPAN CONTINUOUS WEATHERING STEEL PLATE GIRDER BRIDGE
SPAN LENGTH	1070'-0" TOTAL, 2 @ 235', 2 @ 300'
UNDERCLEARANCE	114'-1"
ROADWAY WIDTH	56'-0"
SKEW	90°00'00"
STRUCTURE NO	S-39935

EXISTING STRUCTURE DATA - SR 0080 EB OVER NORTH FORK REDBANK CREEK AND WATER PLANT ROAD	
STATION	386+05.95
TYPE	8-SPAN STEEL GIRDER-FLOORBEAM-STRINGER AND STEEL MULTI-BEAM BRIDGE
SPAN LENGTH	1076'-0" TOTAL: 5 @ VARIES FROM 112' TO 114', 2 @ 150', 1 @ 200'
UNDERCLEARANCE	85'-0"
CLEAR ROADWAY WIDTH	32'-0"
SKEW	90°00'00"
STRUCTURE NO	BMS 33-0080-0794-1178
BUILT DATE	1962

REQUIRED RIGHT-OF-WAY LINE FOR LIMITED ACCESS

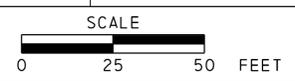
16
MARK S. MCKINLEY & JOANN C. MCKINLEY, HUSBAND & WIFE

- * THRIE-BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION WITH CURB AND INLET
- ** THRIE-BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION WITHOUT CURB

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
SURVEY BOOK #33-0080-550-0785-1578-0811-0617

FOR PROFILE, SEE SHEETS 44 & 61

LEGEND
— PROPOSED DRAINAGE PIPE



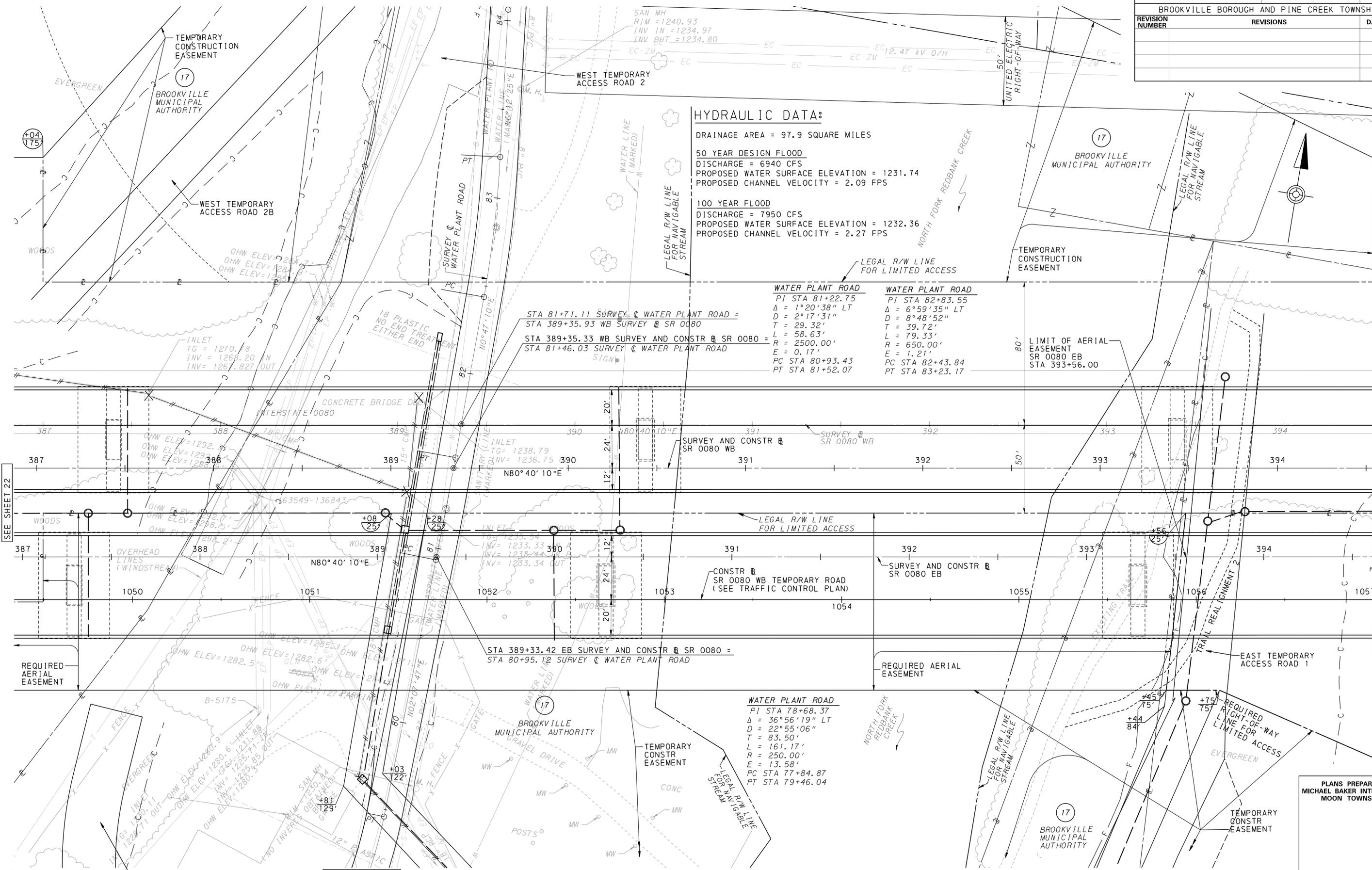
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DES: TJM DWG: BSP CKD: JMB

PLANS PREPARED BY
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MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	23 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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HYDRAULIC DATA:
 DRAINAGE AREA = 97.9 SQUARE MILES
50 YEAR DESIGN FLOOD
 DISCHARGE = 6940 CFS
 PROPOSED WATER SURFACE ELEVATION = 1231.74
 PROPOSED CHANNEL VELOCITY = 2.09 FPS
100 YEAR FLOOD
 DISCHARGE = 7950 CFS
 PROPOSED WATER SURFACE ELEVATION = 1232.36
 PROPOSED CHANNEL VELOCITY = 2.27 FPS

WATER PLANT ROAD	WATER PLANT ROAD
PI STA 81+22.75	PI STA 82+83.55
$\Delta = 1^{\circ}20'38''$ LT	$\Delta = 6^{\circ}59'35''$ LT
$D = 2^{\circ}17'31''$	$D = 8^{\circ}48'52''$
$T = 29.32'$	$T = 39.72'$
$L = 58.63'$	$L = 79.33'$
$R = 2500.00'$	$R = 650.00'$
$E = 0.17'$	$E = 1.21'$
PC STA 80+93.43	PC STA 82+43.84
PT STA 81+52.07	PT STA 83+23.17

STA 81+71.11 SURVEY & WATER PLANT ROAD =
 STA 389+35.93 WB SURVEY & SR 0080
 STA 389+35.33 WB SURVEY AND CONSTR @ SR 0080 =
 STA 81+46.03 SURVEY & WATER PLANT ROAD

STA 389+33.42 EB SURVEY AND CONSTR @ SR 0080 =
 STA 80+95.12 SURVEY & WATER PLANT ROAD

WATER PLANT ROAD
 PI STA 78+68.37
 $\Delta = 36^{\circ}56'19''$ LT
 $D = 22^{\circ}55'06''$
 $T = 83.50'$
 $L = 161.17'$
 $R = 250.00'$
 $E = 13.58'$
 PC STA 77+84.87
 PT STA 79+46.04

LEGEND
 — PROPOSED DRAINAGE ACCESS ROAD 1 PIPE
 — WEST TEMPORARY ACCESS ROAD 1
 DES: TJM DWG: BSP CKD: JMB



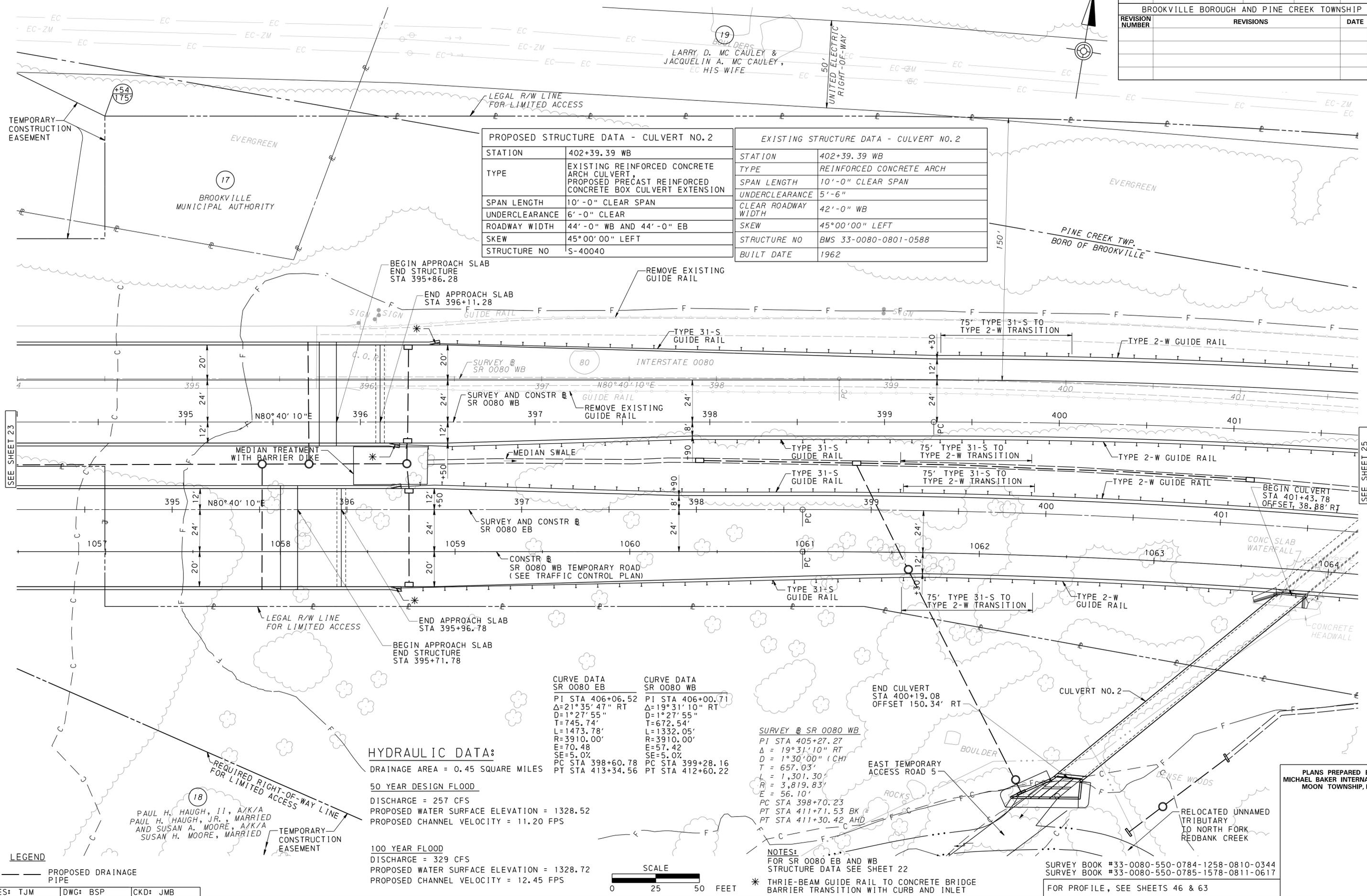
NOTE:
 FOR SR 0080 EB AND WB
 STRUCTURE DATA SEE SHEET 22

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 45 & 62

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	24 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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PROPOSED STRUCTURE DATA - CULVERT NO. 2	
STATION	402+39.39 WB
TYPE	EXISTING REINFORCED CONCRETE ARCH CULVERT, PROPOSED PRECAST REINFORCED CONCRETE BOX CULVERT EXTENSION
SPAN LENGTH	10'-0" CLEAR SPAN
UNDERCLEARANCE	6'-0" CLEAR
ROADWAY WIDTH	44'-0" WB AND 44'-0" EB
SKEW	45°00'00" LEFT
STRUCTURE NO	S-40040

EXISTING STRUCTURE DATA - CULVERT NO. 2	
STATION	402+39.39 WB
TYPE	REINFORCED CONCRETE ARCH
SPAN LENGTH	10'-0" CLEAR SPAN
UNDERCLEARANCE	5'-6"
CLEAR ROADWAY WIDTH	42'-0" WB
SKEW	45°00'00" LEFT
STRUCTURE NO	BMS 33-0080-0801-0588
BUILT DATE	1962

CURVE DATA SR 0080 EB	CURVE DATA SR 0080 WB
PI STA 406+06.52	PI STA 406+00.71
Δ=21°35'47" RT	Δ=19°31'10" RT
D=1°27'55"	D=1°27'55"
T=745.74'	T=672.54'
L=1473.78'	L=1332.05'
R=3910.00'	R=3910.00'
E=70.48'	E=57.42'
SE=5.0%	SE=5.0%
PC STA 398+60.78	PC STA 399+28.16
PT STA 413+34.56	PT STA 412+60.22

HYDRAULIC DATA:
 DRAINAGE AREA = 0.45 SQUARE MILES
50 YEAR DESIGN FLOOD
 DISCHARGE = 257 CFS
 PROPOSED WATER SURFACE ELEVATION = 1328.52
 PROPOSED CHANNEL VELOCITY = 11.20 FPS
100 YEAR FLOOD
 DISCHARGE = 329 CFS
 PROPOSED WATER SURFACE ELEVATION = 1328.72
 PROPOSED CHANNEL VELOCITY = 12.45 FPS



SURVEY # SR 0080 WB
 PI STA 405+27.27
 Δ = 19°31'10" RT
 D = 1°30'00" (CH)
 T = 657.03'
 L = 1,301.30'
 R = 3,819.83'
 E = 56.10'
 PC STA 398+70.23
 PT STA 411+71.53 BK E
 PT STA 411+30.42 AHD

NOTES:
 FOR SR 0080 EB AND WB
 STRUCTURE DATA SEE SHEET 22
 * THRIE-BEAM GUIDE RAIL TO CONCRETE BRIDGE
 BARRIER TRANSITION WITH CURB AND INLET

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 46 & 63

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

LEGEND
 — PROPOSED DRAINAGE PIPE

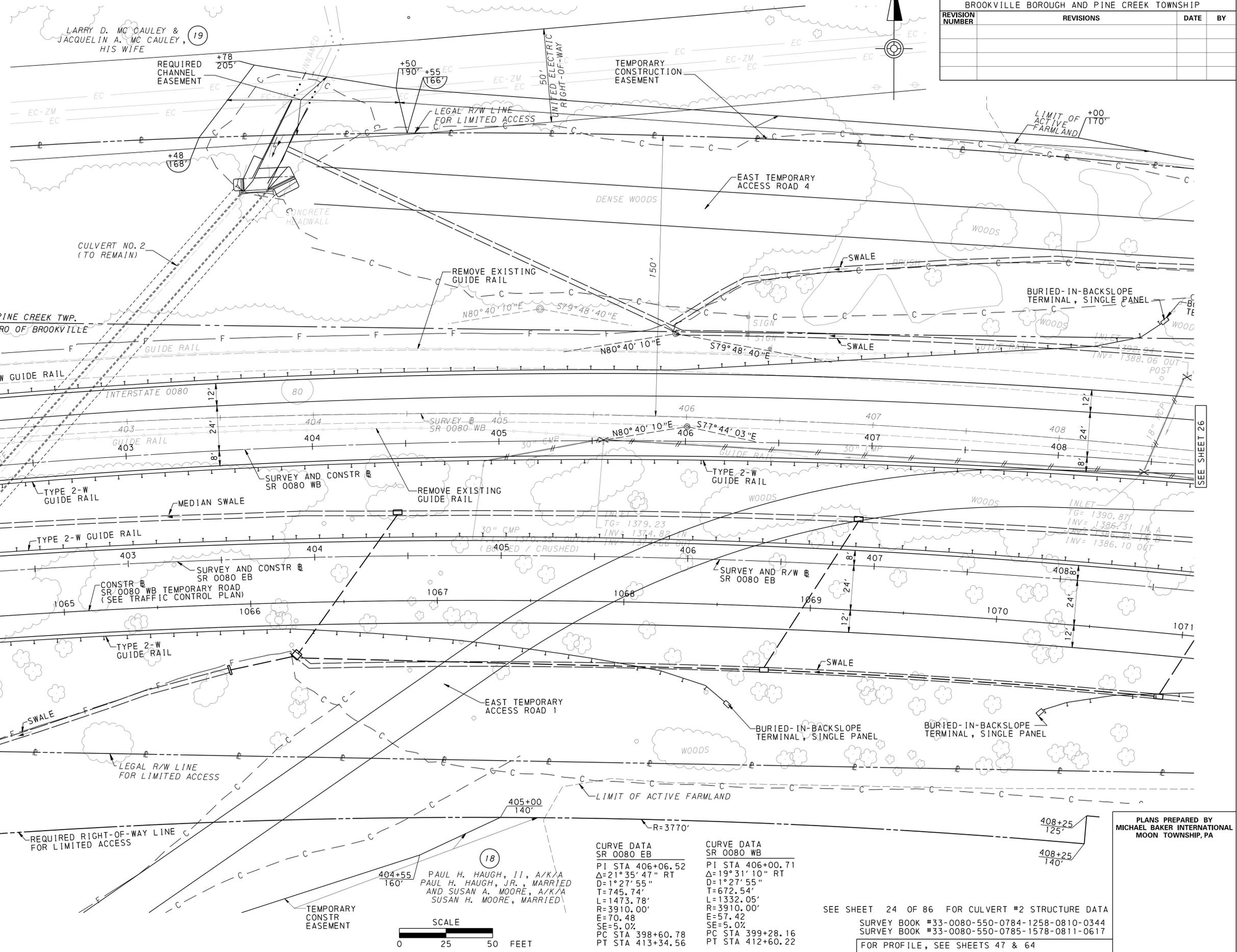
DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	25 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

HYDRAULIC DATA:

DRAINAGE AREA = 0.45 SQUARE MILES
50 YEAR DESIGN FLOOD
DISCHARGE = 257 CFS
PROPOSED WATER SURFACE ELEVATION = 1328.52
PROPOSED CHANNEL VELOCITY = 11.20 FPS

100 YEAR FLOOD
DISCHARGE = 329 CFS
PROPOSED WATER SURFACE ELEVATION = 1328.72
PROPOSED CHANNEL VELOCITY = 12.45 FPS



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SEE SHEET 24

SEE SHEET 26

LEGEND

PROPOSED DRAINAGE PIPE

SURVEY # SR 0080 WB
PI STA 405+27.27
 $\Delta = 19^\circ 31' 10''$ RT
D = 1° 30' 00" (CH)
T = 657.03'
L = 1,301.30'
R = 3,819.83'
E = 56.10'
PC STA 398+70.23
PT STA 411+71.53 BK
PT STA 411+30.42 AHD

PAUL H. HAUGH, II, A/K/A
PAUL H. HAUGH, JR., MARRIED
AND SUSAN A. MOORE, A/K/A
SUSAN H. MOORE, MARRIED



CURVE DATA
SR 0080 EB
PI STA 406+06.52
 $\Delta = 21^\circ 35' 47''$ RT
D = 1° 27' 55"
T = 745.74'
L = 1473.78'
R = 3910.00'
E = 70.48
SE = 5.0%
PC STA 398+60.78
PT STA 413+34.56

CURVE DATA
SR 0080 WB
PI STA 406+00.71
 $\Delta = 19^\circ 31' 10''$ RT
D = 1° 27' 55"
T = 672.54'
L = 1332.05'
R = 3910.00'
E = 57.42
SE = 5.0%
PC STA 399+28.16
PT STA 412+60.22

SEE SHEET 24 OF 86 FOR CULVERT #2 STRUCTURE DATA
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
SURVEY BOOK #33-0080-550-0785-1578-0811-0617
FOR PROFILE, SEE SHEETS 47 & 64

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

EXISTING STRUCTURE DATA SR 4005 OVER SR 0080 EB	
STATION	413+73.00
TYPE	3 SPAN SPREAD BOX BEAM BRIDGE
SPAN LENGTH	171'-0" TOTAL, 1 @ 52', 1 @ 67', 1 @ 52'
UNDERCLEARANCE	16.08'
CLEAR ROADWAY WIDTH	28'-0"
SKIEW	45°02'25" LEFT
STRUCTURE NO	BMS 33-4005-0040-0000
BUILT DATE	1962

EXISTING STRUCTURE DATA SR 4005 OVER SR 0080 WB	
STATION	411+31.23
TYPE	3 SPAN SPREAD BOX BEAM BRIDGE
SPAN LENGTH	121'-0" TOTAL, 1 @ 36', 1 @ 49'-6", 1 @ 36'
UNDERCLEARANCE	16.33'
CLEAR ROADWAY WIDTH	28'-0"
SKIEW	74°33'56" LEFT
STRUCTURE NO	BMS 33-4005-0040-0412
BUILT DATE	1962

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	26 OF 86

BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP

REVISION NUMBER	REVISIONS	DATE	BY

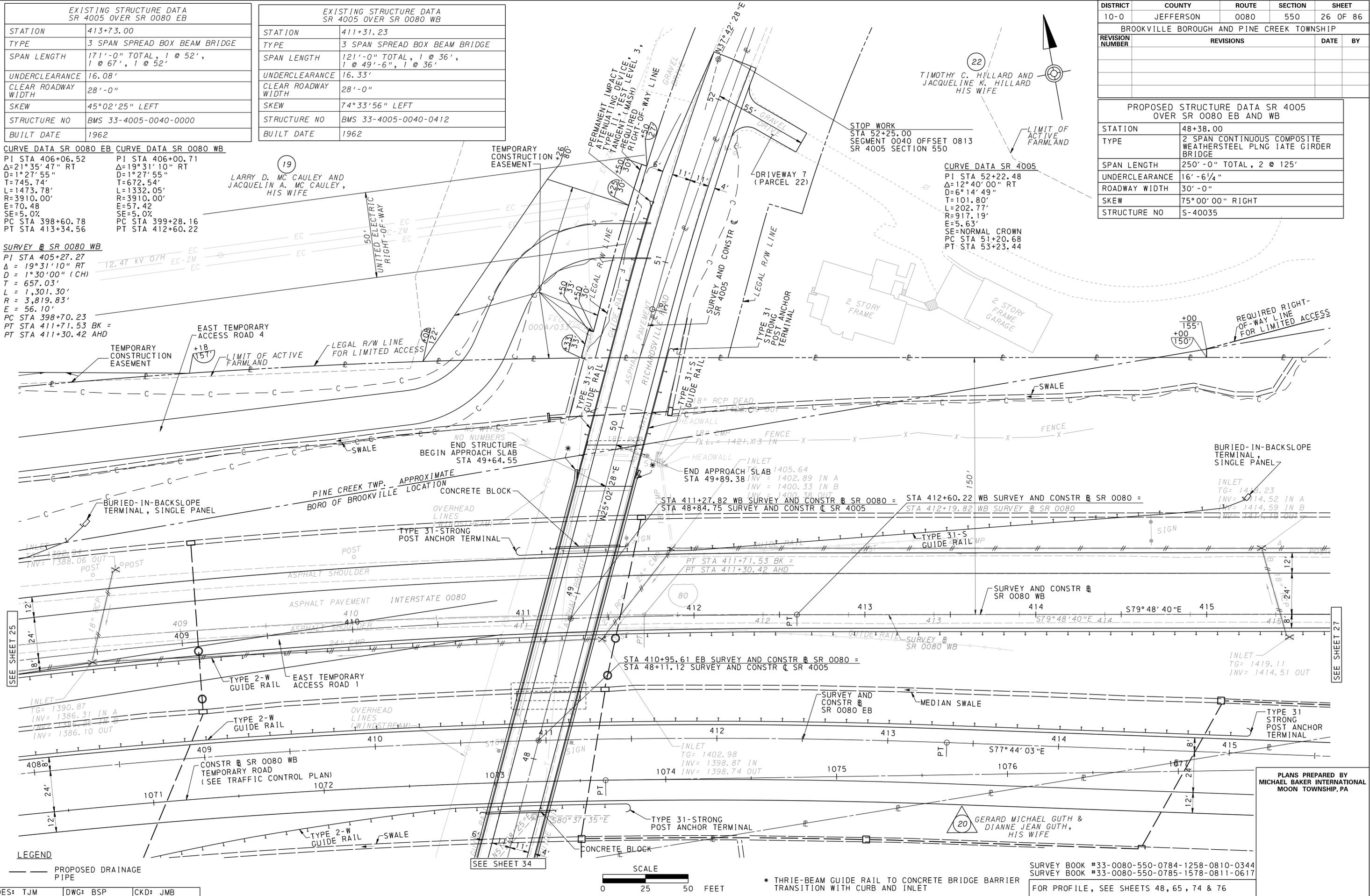
PROPOSED STRUCTURE DATA SR 4005 OVER SR 0080 EB AND WB	
STATION	48+38.00
TYPE	2 SPAN CONTINUOUS COMPOSITE WEATHERSTEEL PLNG IATE GIRDER BRIDGE
SPAN LENGTH	250'-0" TOTAL, 2 @ 125'
UNDERCLEARANCE	16'-6 1/4"
ROADWAY WIDTH	30'-0"
SKIEW	75°00'00" RIGHT
STRUCTURE NO	S-40035

CURVE DATA SR 0080 EB
 PI STA 406+06.52
 $\Delta = 21^\circ 35' 47''$ RT
 $D = 1^\circ 27' 55''$
 $T = 745.74'$
 $L = 1473.78'$
 $R = 3910.00'$
 $E = 70.48'$
 $SE = 5.0\%$
 PC STA 398+60.78
 PT STA 413+34.56

CURVE DATA SR 0080 WB
 PI STA 406+00.71
 $\Delta = 19^\circ 31' 10''$ RT
 $D = 1^\circ 27' 55''$
 $T = 672.54'$
 $L = 1332.05'$
 $R = 3910.00'$
 $E = 57.42'$
 $SE = 5.0\%$
 PC STA 399+28.16
 PT STA 412+60.22

SURVEY # SR 0080 WB
 PI STA 405+27.27
 $\Delta = 19^\circ 31' 10''$ RT
 $D = 1^\circ 30' 00''$ (CH)
 $T = 657.03'$
 $L = 1,301.30'$
 $R = 3,819.83'$
 $E = 56.10'$
 PC STA 398+70.23
 PT STA 411+71.53 BK =
 PT STA 411+30.42 AHD

CURVE DATA SR 4005
 PI STA 52+22.48
 $\Delta = 12^\circ 40' 00''$ RT
 $D = 6^\circ 14' 49''$
 $T = 101.80'$
 $L = 202.77'$
 $R = 917.19'$
 $E = 5.63'$
 $SE = \text{NORMAL CROWN}$
 PC STA 51+20.68
 PT STA 53+23.44



LEGEND

	PROPOSED DRAINAGE PIPE
--	------------------------



SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 48, 65, 74 & 76

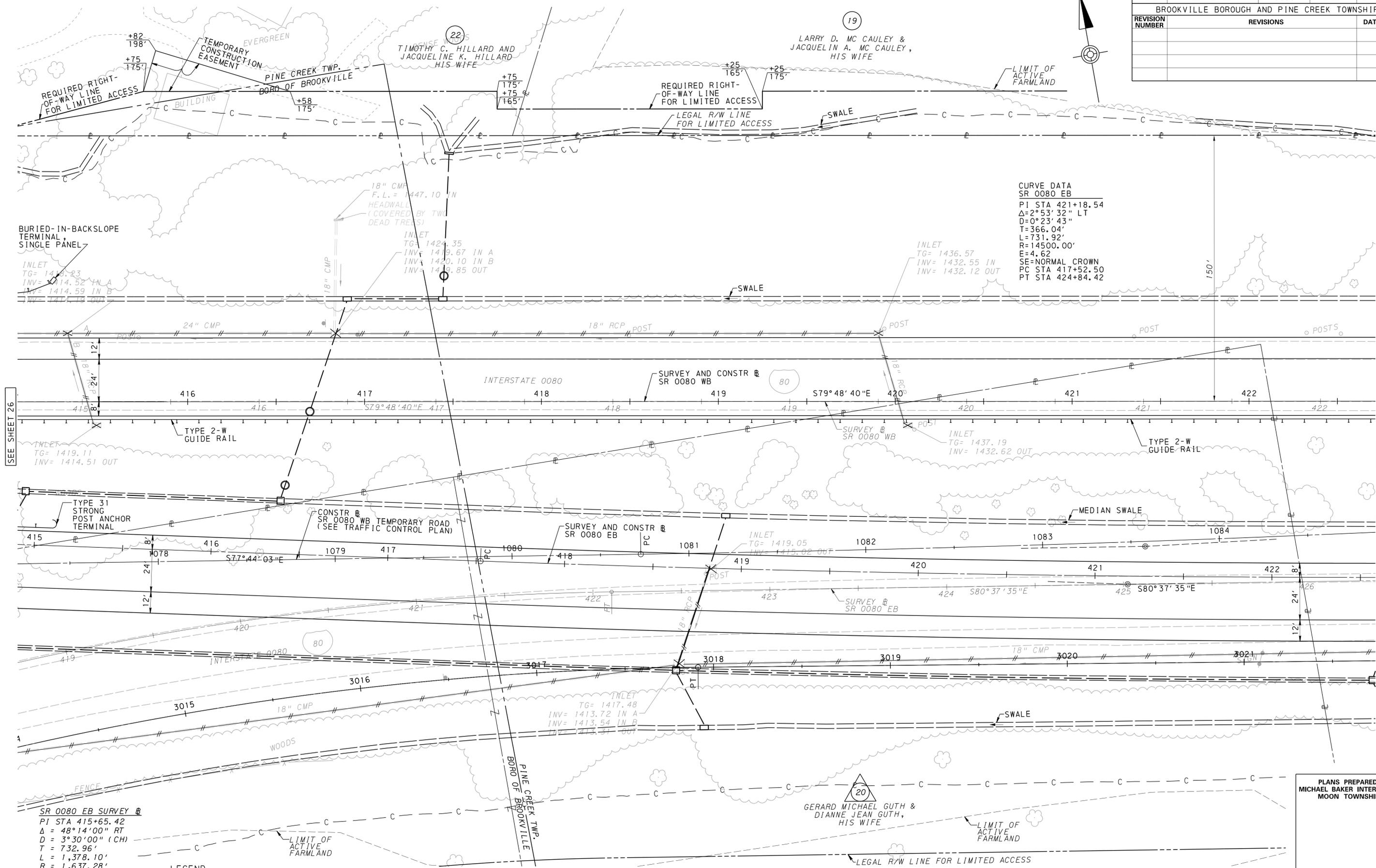
* THREE-BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER
 TRANSITION WITH CURB AND INLET

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

12:00:14 PM
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	27 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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CURVE DATA
 SR 0080 EB
 PI STA 421+18.54
 $\Delta = 2^{\circ}53'32''$ LT
 $D = 0^{\circ}23'43''$
 $T = 366.04'$
 $R = 14500.00'$
 $E = 4.62$
 SE=NORMAL CROWN
 PC STA 417+52.50
 PT STA 424+84.42

SR 0080 EB SURVEY #
 PI STA 415+65.42
 $\Delta = 48^{\circ}14'00''$ RT
 $D = 3^{\circ}30'00''$ (CH)
 $T = 732.96'$
 $L = 1,378.10'$
 $R = 1,637.28'$
 $E = 156.58'$
 PC STA 408+32.46
 PT STA 422+10.56

LEGEND
 ——— PROPOSED DRAINAGE PIPE



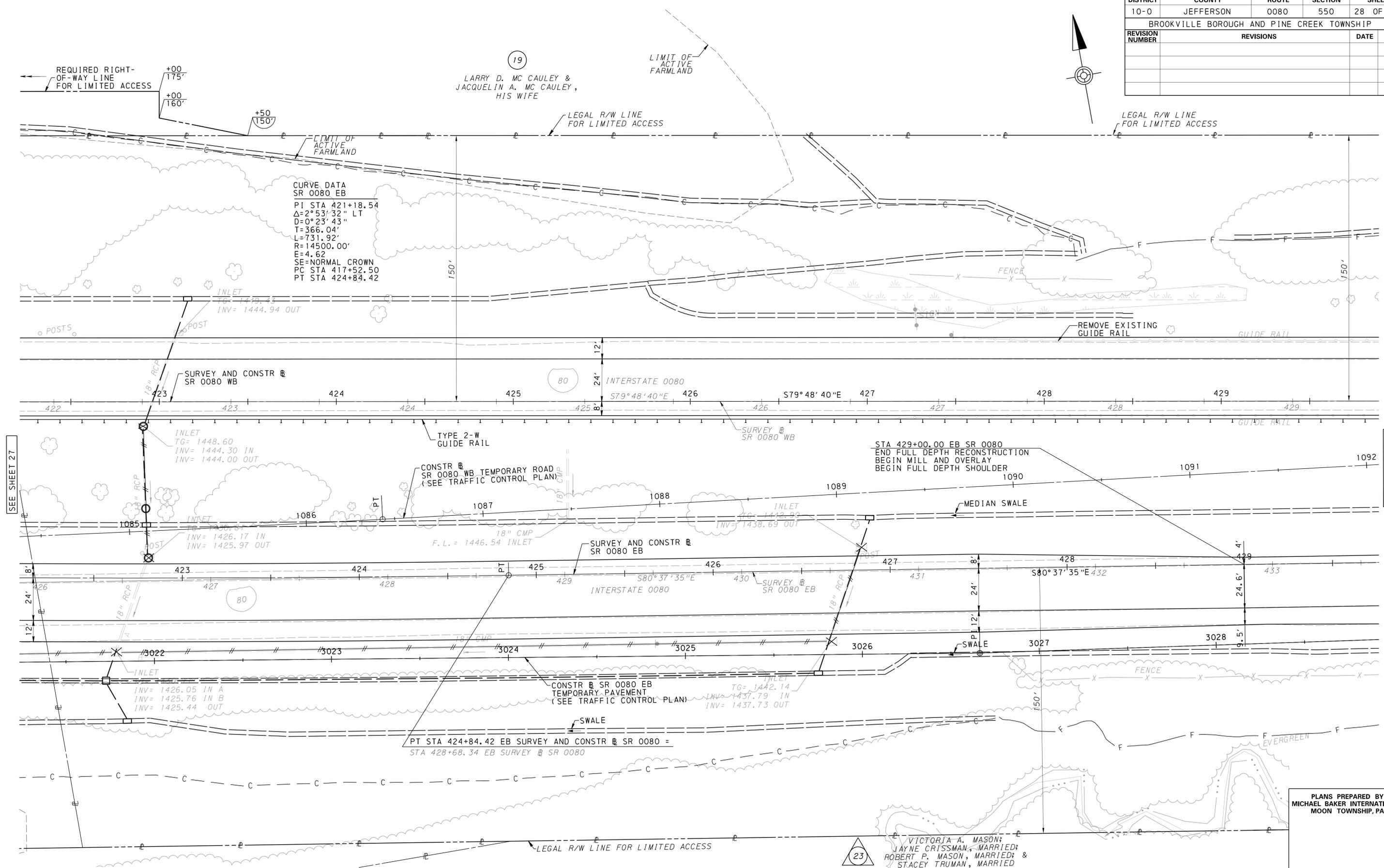
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 49 & 66

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	28 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY

9/28/2021 12:00:19 PM I:\80_Nor Th_Fork_Roadway_Plans.dgn



CURVE DATA
 SR 0080 EB
 PI STA 421+18.54
 $\Delta = 2^\circ 53' 32''$ LT
 $D = 0^\circ 23' 43''$
 $T = 366.04'$
 $L = 731.92'$
 $R = 14500.00'$
 $E = 4.62$
 SE=NORMAL CROWN
 PC STA 417+52.50
 PT STA 424+84.42

INLET
 TG= 1448.60
 INV= 1444.30 IN
 INV= 1444.00 OUT

INLET
 TG= 1436.61
 INV= 1426.17 IN
 INV= 1425.97 OUT

INLET
 INV= 1426.05 IN A
 INV= 1425.76 IN B
 INV= 1425.44 OUT

INLET
 TG= 1442.90
 INV= 1438.69 OUT

INLET
 TG= 1442.14
 INV= 1437.79 IN
 INV= 1437.73 OUT

PT STA 424+84.42 EB SURVEY AND CONSTR SR 0080 =
 STA 428+68.34 EB SURVEY SR 0080

STA 429+00.00 EB SR 0080
 END FULL DEPTH RECONSTRUCTION
 BEGIN MILL AND OVERLAY
 BEGIN FULL DEPTH SHOULDER

23
 VICTORIA A. MASON;
 JAYNE CRISSMAN, MARRIED;
 ROBERT P. MASON, MARRIED; &
 STACEY TRUMAN, MARRIED

LEGEND
 — PROPOSED DRAINAGE PIPE



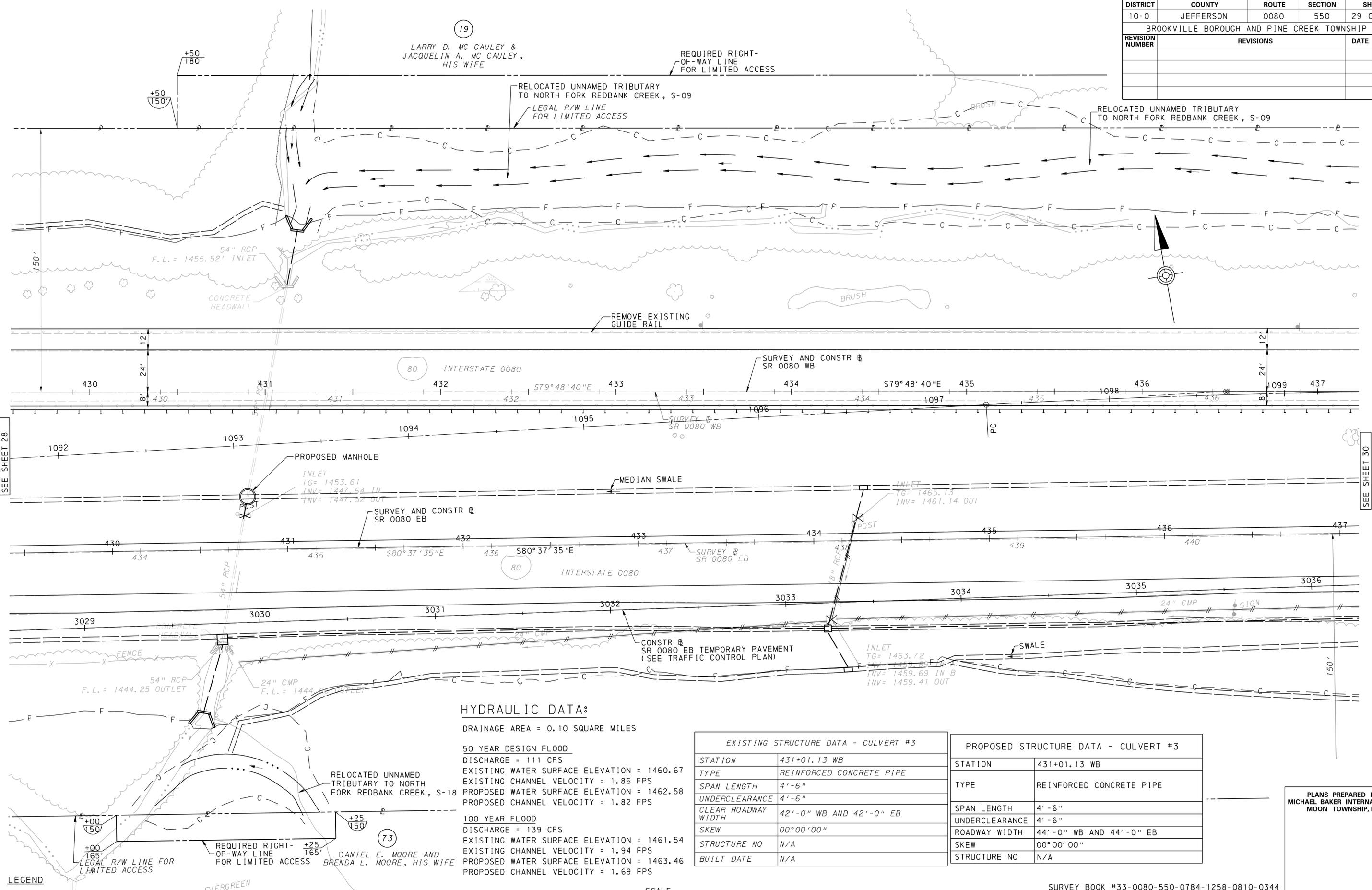
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 50 & 67

DES: TJM DWG: BSP CKD: JMB

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 MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	29 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
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HYDRAULIC DATA:

DRAINAGE AREA = 0.10 SQUARE MILES

50 YEAR DESIGN FLOOD
 DISCHARGE = 111 CFS
 EXISTING WATER SURFACE ELEVATION = 1460.67
 EXISTING CHANNEL VELOCITY = 1.86 FPS
 PROPOSED WATER SURFACE ELEVATION = 1462.58
 PROPOSED CHANNEL VELOCITY = 1.82 FPS

100 YEAR FLOOD
 DISCHARGE = 139 CFS
 EXISTING WATER SURFACE ELEVATION = 1461.54
 EXISTING CHANNEL VELOCITY = 1.94 FPS
 PROPOSED WATER SURFACE ELEVATION = 1463.46
 PROPOSED CHANNEL VELOCITY = 1.69 FPS

EXISTING STRUCTURE DATA - CULVERT #3	
STATION	431+01.13 WB
TYPE	REINFORCED CONCRETE PIPE
SPAN LENGTH	4'-6"
UNDERCLEARANCE	4'-6"
CLEAR ROADWAY WIDTH	42'-0" WB AND 42'-0" EB
SKEW	00°00'00"
STRUCTURE NO	N/A
BUILT DATE	N/A

PROPOSED STRUCTURE DATA - CULVERT #3	
STATION	431+01.13 WB
TYPE	REINFORCED CONCRETE PIPE
SPAN LENGTH	4' - 6"
UNDERCLEARANCE	4' - 6"
ROADWAY WIDTH	44' - 0" WB AND 44' - 0" EB
SKEW	00°00'00"
STRUCTURE NO	N/A

LEGEND
 — PROPOSED DRAINAGE PIPE

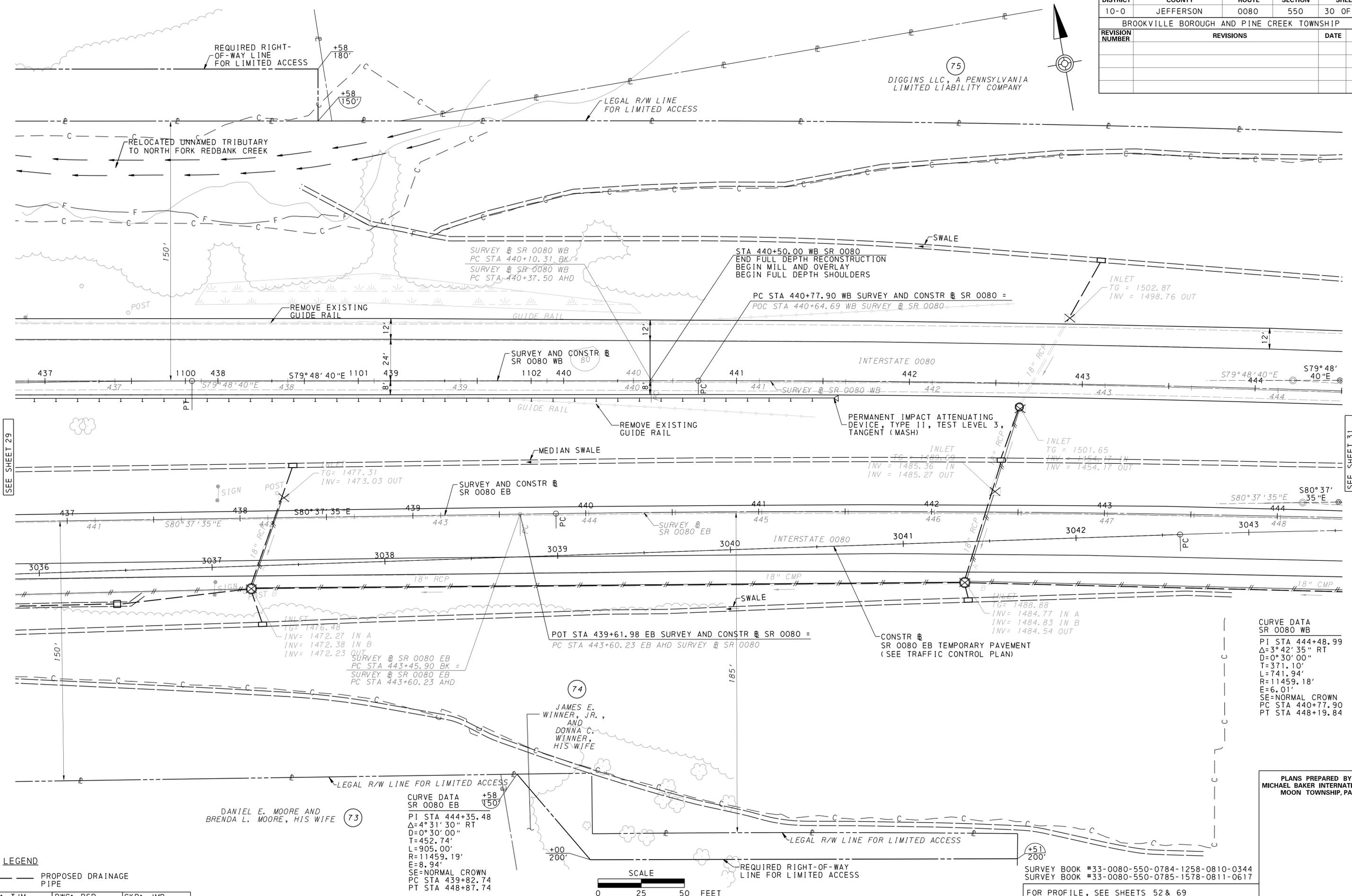


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 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 51 & 68

PLANS PREPARED BY
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 MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	30 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

9/28/2021 12:00:24 PM



SEE SHEET 29

SEE SHEET 31

I-80_Nor.Th._Fork._Roadway_Plans.dgn

LEGEND
 --- PROPOSED DRAINAGE PIPE

CURVE DATA
 SR 0080 EB
 PI STA 444+35.48
 $\Delta=4^{\circ}31'30''$ RT
 $D=0^{\circ}30'00''$
 $T=452.74'$
 $L=905.00'$
 $R=11459.19'$
 $E=8.94'$
 SE=NORMAL CROWN
 PC STA 439+82.74
 PT STA 448+87.74



CURVE DATA
 SR 0080 WB
 PI STA 444+48.99
 $\Delta=3^{\circ}42'35''$ RT
 $D=0^{\circ}30'00''$
 $T=371.10'$
 $L=741.94'$
 $R=11459.18'$
 $E=6.01'$
 SE=NORMAL CROWN
 PC STA 440+77.90
 PT STA 448+19.84

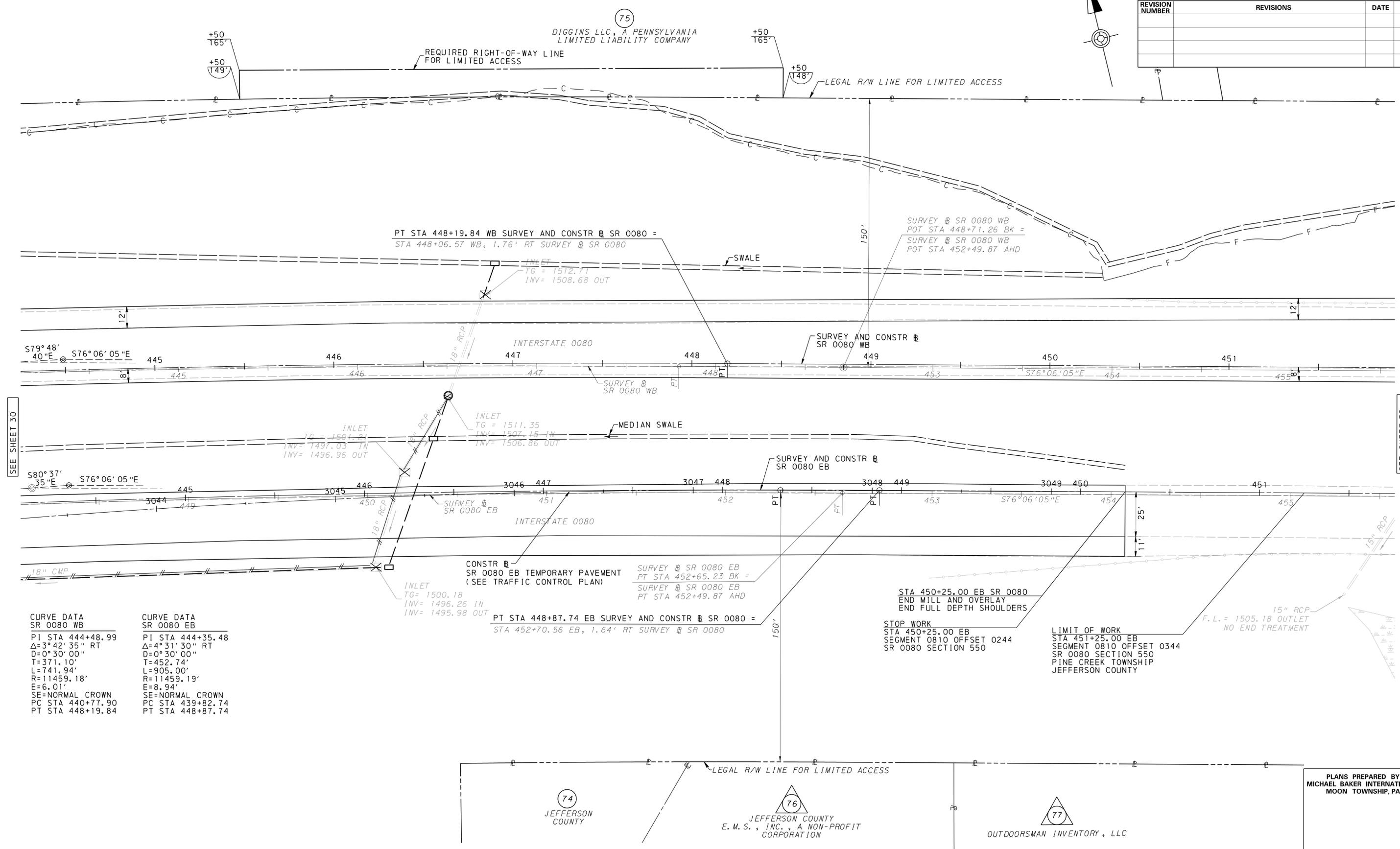
PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 52 & 69

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	31 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

9/28/2021 12:00:26 PM



CURVE DATA SR 0080 WB	CURVE DATA SR 0080 EB
PI STA 444+48.99	PI STA 444+35.48
Δ=3°42'35" RT	Δ=4°31'30" RT
D=0°30'00"	D=0°30'00"
T=371.10'	T=452.74'
L=741.94'	L=905.00'
R=11459.18'	R=11459.19'
E=6.01'	E=8.94'
SE=NORMAL CROWN	SE=NORMAL CROWN
PC STA 440+77.90	PC STA 439+82.74
PT STA 448+19.84	PT STA 448+87.74

CONSTR SR 0080 EB TEMPORARY PAVEMENT (SEE TRAFFIC CONTROL PLAN)

INLET
TG= 1500.18
INV= 1496.26 IN
INV= 1495.98 OUT

PT STA 448+87.74 EB SURVEY AND CONSTR SR 0080 =
STA 452+70.56 EB, 1.64' RT SURVEY SR 0080

SURVEY SR 0080 EB
PT STA 452+65.23 BK =
SURVEY SR 0080 EB
PT STA 452+49.87 AHD

STA 450+25.00 EB SR 0080
END MILL AND OVERLAY
END FULL DEPTH SHOULDERS

STOP WORK
STA 450+25.00 EB
SEGMENT 0810 OFFSET 0244
SR 0080 SECTION 550

LIMIT OF WORK
STA 451+25.00 EB
SEGMENT 0810 OFFSET 0344
SR 0080 SECTION 550
PINE CREEK TOWNSHIP
JEFFERSON COUNTY

15" RCP
F.L. = 1505.18 OUTLET
NO END TREATMENT

LEGEND
— PROPOSED DRAINAGE PIPE



SURVEY BOOK #33-0080-550-0784-1258-0810-0344
SURVEY BOOK #33-0080-550-0785-1578-0811-0617
FOR PROFILE, SEE SHEETS 53 & 70

DES: TJM DWG: BSP CKD: JMB

PLANS PREPARED BY
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MOON TOWNSHIP, PA

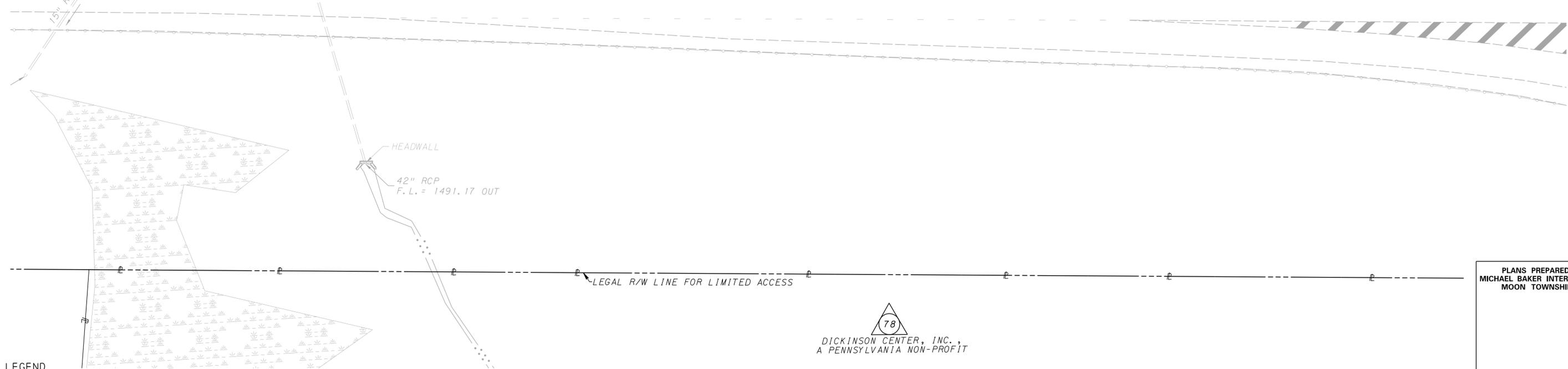
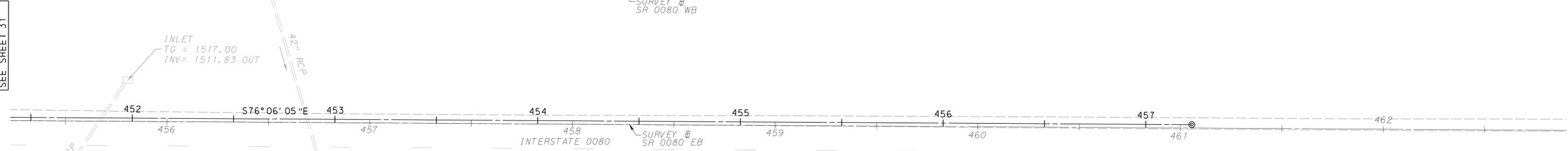
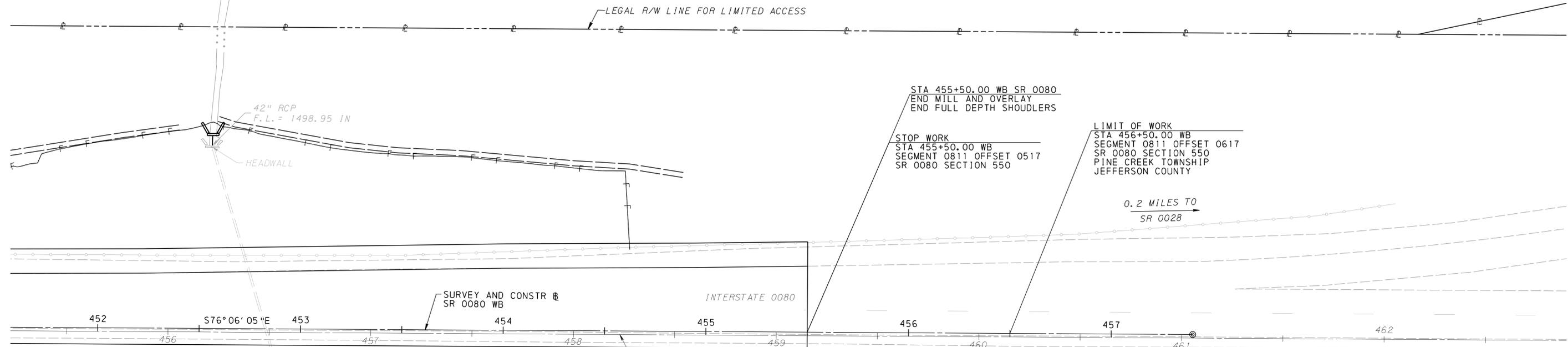
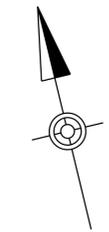
74
JEFFERSON COUNTY

76
JEFFERSON COUNTY
E. M. S., INC., A NON-PROFIT CORPORATION

77
OUTDOORSMAN INVENTORY, LLC

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	32 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY

79
 JAMES E. WINNER, JR., AND
 DONNA C. WINNER,
 HIS WIFE



LEGEND
 — PROPOSED DRAINAGE PIPE



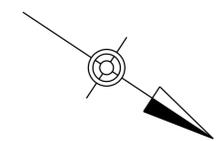
78
 DICKINSON CENTER, INC.,
 A PENNSYLVANIA NON-PROFIT

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEET 71

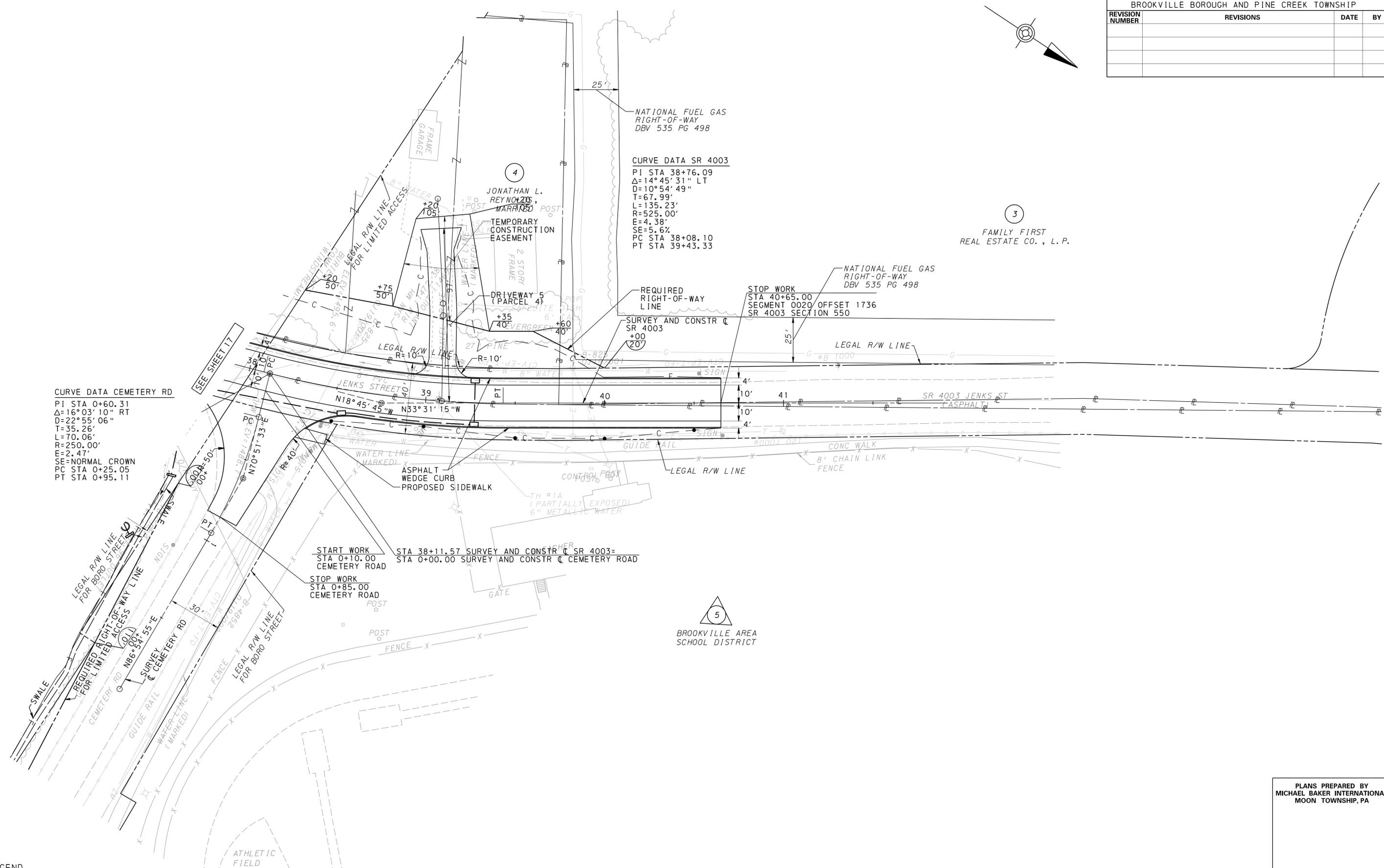
PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

9/28/2021 12:00:29 PM
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 SEE SHEET 31
 DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	33 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



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CURVE DATA CEMETERY RD
 PI STA 0+60.31
 $\Delta = 16^\circ 03' 10''$ RT
 $D = 22^\circ 55' 06''$
 $T = 35.26'$
 $L = 70.06'$
 $R = 250.00'$
 $E = 2.47'$
 SE = NORMAL CROWN
 PC STA 0+25.05
 PT STA 0+95.11

CURVE DATA SR 4003
 PI STA 38+76.09
 $\Delta = 14^\circ 45' 31''$ LT
 $D = 10^\circ 54' 49''$
 $T = 67.99'$
 $L = 135.23'$
 $R = 525.00'$
 $E = 4.38'$
 $SE = 5.6\%$
 PC STA 38+08.10
 PT STA 39+43.33

START WORK
 STA 0+10.00 CEMETERY ROAD
 STA 38+11.57 SURVEY AND CONSTR C SR 4003=
 STA 0+00.00 SURVEY AND CONSTR C CEMETERY ROAD

STOP WORK
 STA 0+85.00 CEMETERY ROAD

LEGEND
 ——— PROPOSED DRAINAGE
 P PIPE



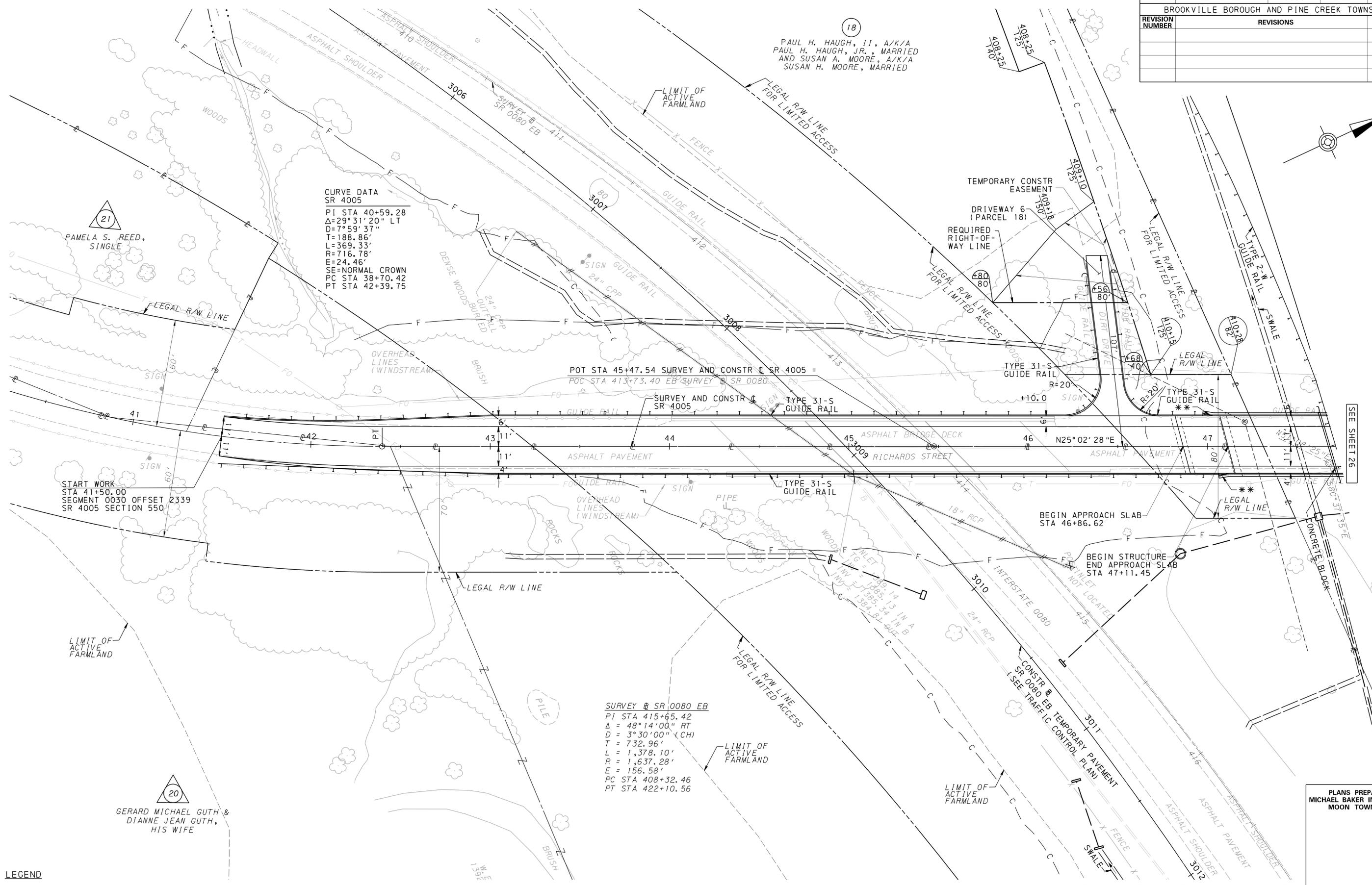
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 72, 75 & 76

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	34 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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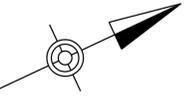
CURVE DATA
SR 4005
 PI STA 40+59.28
 $\Delta = 29^\circ 31' 20''$ LT
 $D = 7^\circ 59' 37''$
 $T = 188.86'$
 $L = 369.33'$
 $R = 716.78'$
 $E = 24.46'$
 SE = NORMAL CROWN
 PC STA 38+70.42
 PT STA 42+39.75

SURVEY # SR 0080 EB
 PI STA 415+65.42
 $\Delta = 48^\circ 14' 00''$ RT
 $D = 3^\circ 30' 00''$ (CH)
 $T = 732.96'$
 $L = 1,378.10'$
 $R = 1,637.28'$
 $E = 156.58'$
 PC STA 408+32.46
 PT STA 422+10.56

START WORK
 STA 41+50.00
 SEGMENT 0030 OFFSET 2339
 SR 4005 SECTION 550

GERARD MICHAEL GUTH &
 DIANNE JEAN GUTH,
 HIS WIFE

(18)
 PAUL H. HAUGH, II, A/K/A
 PAUL H. HAUGH, JR., MARRIED
 AND SUSAN A. MOORE, A/K/A
 SUSAN H. MOORE, MARRIED



SEE SHEET 26

LEGEND
 ——— PROPOSED DRAINAGE PIPE



** THRIE-BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION WITHOUT CURB

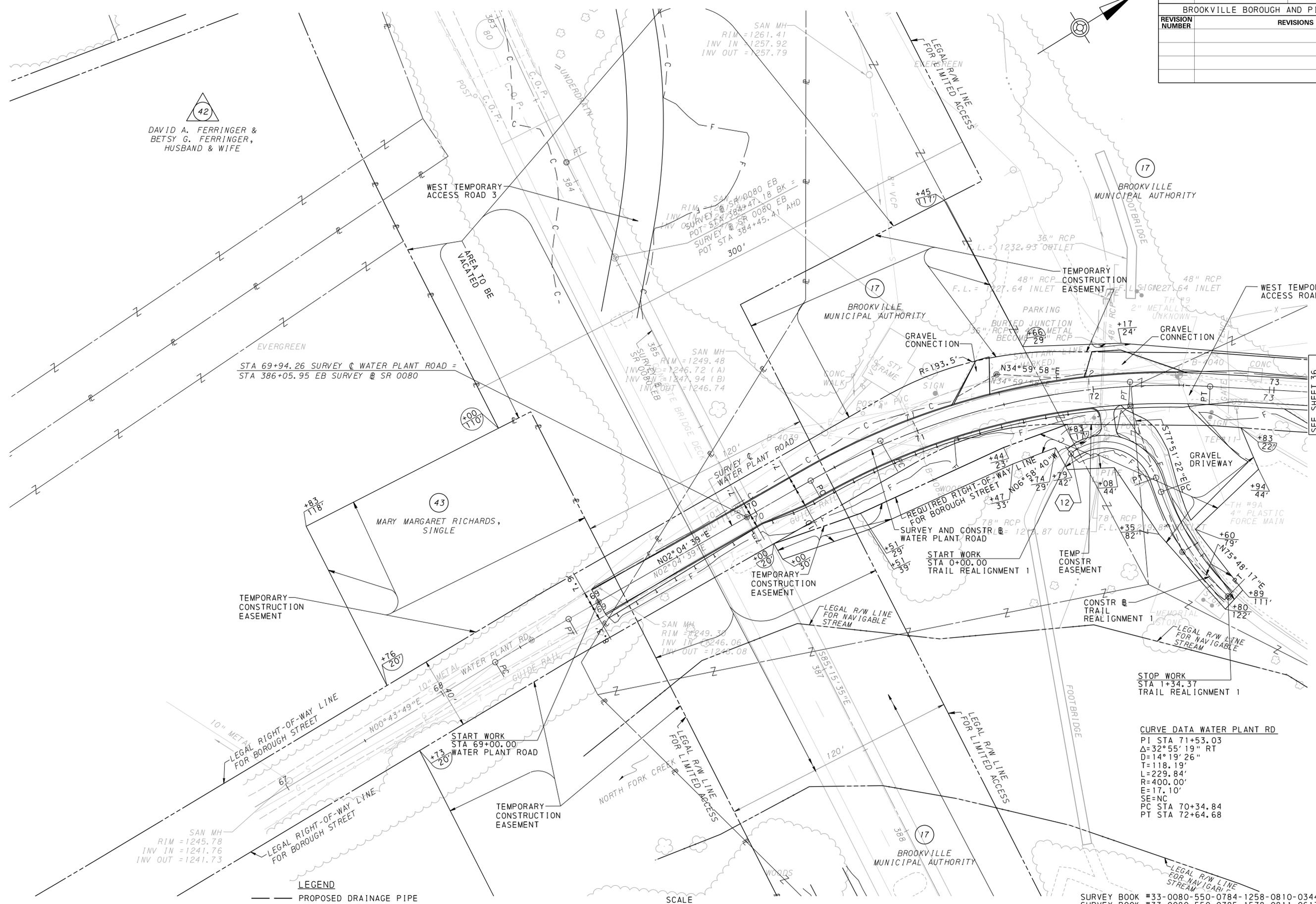
SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 73 & 76

DES: TJM DWG: BSP CKD: JMB

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	35 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

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42
DAVID A. FERRINGER &
BETSY G. FERRINGER,
HUSBAND & WIFE

STA 69+94.26 SURVEY & CONSTR @ WATER PLANT ROAD =
STA 386+05.95 EB SURVEY @ SR 0080

SAN MH
RIM = 1249.48
INV IN = 1246.72 (A)
INV ON = 1247.94 (B)
INV OUT = 1246.74

SAN MH
RIM = 1261.41
INV IN = 1257.92
INV OUT = 1257.79

SAN MH
RIM = 1249.48
INV IN = 1246.72 (A)
INV ON = 1247.94 (B)
INV OUT = 1246.74

SAN MH
RIM = 1249.30
INV IN = 1246.06
INV OUT = 1245.08

SAN MH
RIM = 1245.78
INV IN = 1241.76
INV OUT = 1241.73

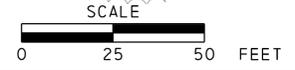
CURVE DATA WATER PLANT RD
PI STA 71+53.03
Δ=32°55'19" RT
D=14°19'26"
T=118.19'
L=229.84'
R=400.00'
E=17.10'
SE=NC
PC STA 70+34.84
PT STA 72+64.68

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
SURVEY BOOK #33-0080-550-0785-1578-0811-0617
FOR PROFILE, SEE SHEETS 77 & 78

LEGEND

— PROPOSED DRAINAGE PIPE

12 PC STA 0+00.00 SURVEY @ TRAIL REALIGNMENT 1 =
STA 71+82.56 SURVEY AND CONSTR @ WATER PLANT ROAD 33.5' RT



PLANS PREPARED BY
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MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	36 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

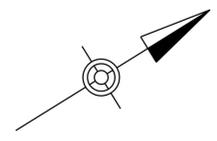
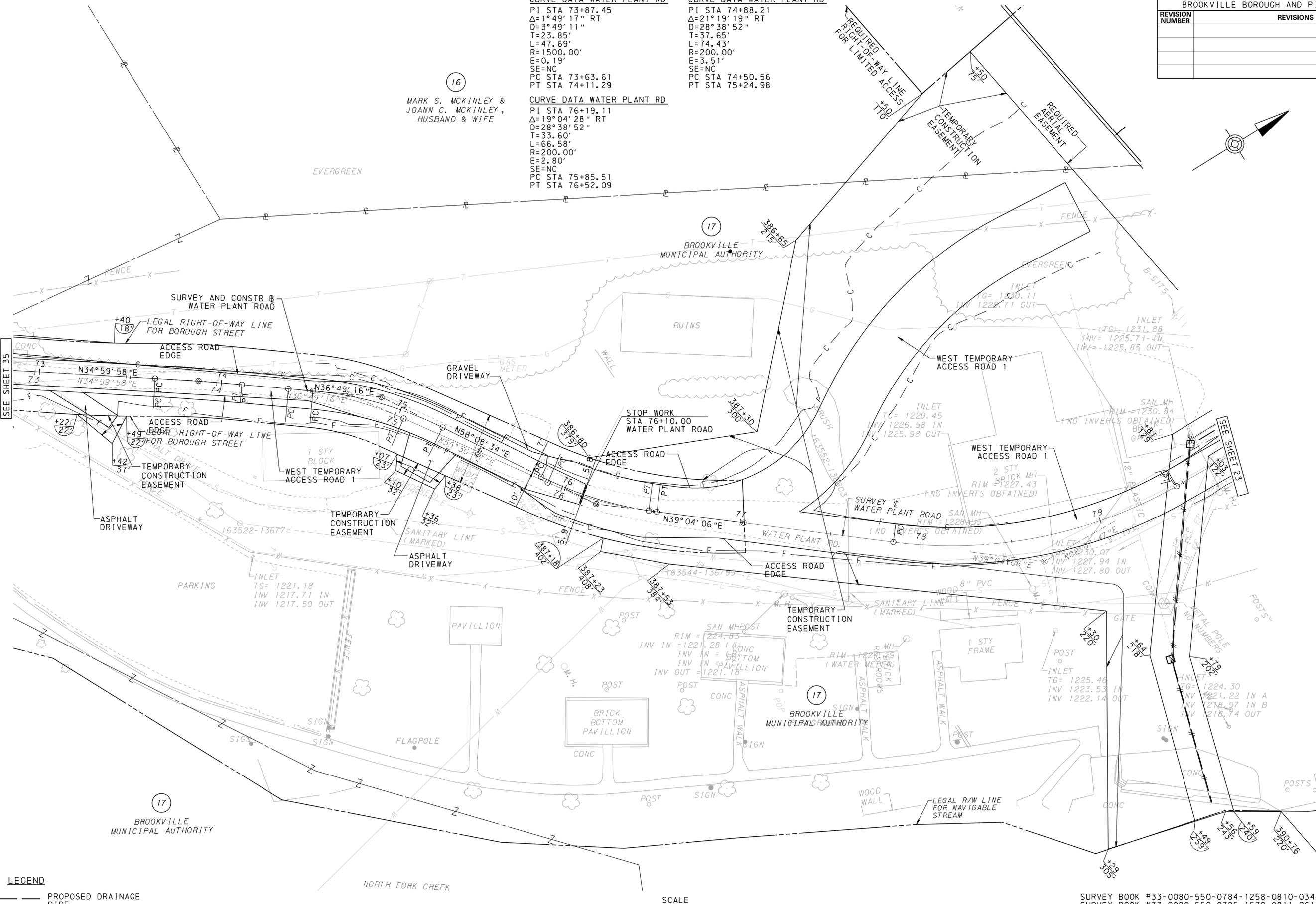
CURVE DATA WATER PLANT RD
 PI STA 73+87.45
 $\Delta=1^{\circ}49'17''$ RT
 $D=3^{\circ}49'11''$
 $T=23.85'$
 $L=47.69'$
 $R=1500.00'$
 $E=0.19'$
 $SE=NC$
 PC STA 73+63.61
 PT STA 74+11.29

CURVE DATA WATER PLANT RD
 PI STA 74+88.21
 $\Delta=21^{\circ}19'19''$ RT
 $D=28^{\circ}38'52''$
 $T=37.65'$
 $L=74.43'$
 $R=200.00'$
 $E=3.51'$
 $SE=NC$
 PC STA 74+50.56
 PT STA 75+24.98

CURVE DATA WATER PLANT RD
 PI STA 76+19.11
 $\Delta=19^{\circ}04'28''$ RT
 $D=28^{\circ}38'52''$
 $T=33.60'$
 $L=66.58'$
 $R=200.00'$
 $E=2.80'$
 $SE=NC$
 PC STA 75+85.51
 PT STA 76+52.09

16
 MARK S. MCKINLEY &
 JOANN C. MCKINLEY,
 HUSBAND & WIFE

17
 BROOKVILLE
 MUNICIPAL AUTHORITY



LEGEND
 ——— PROPOSED DRAINAGE PIPE

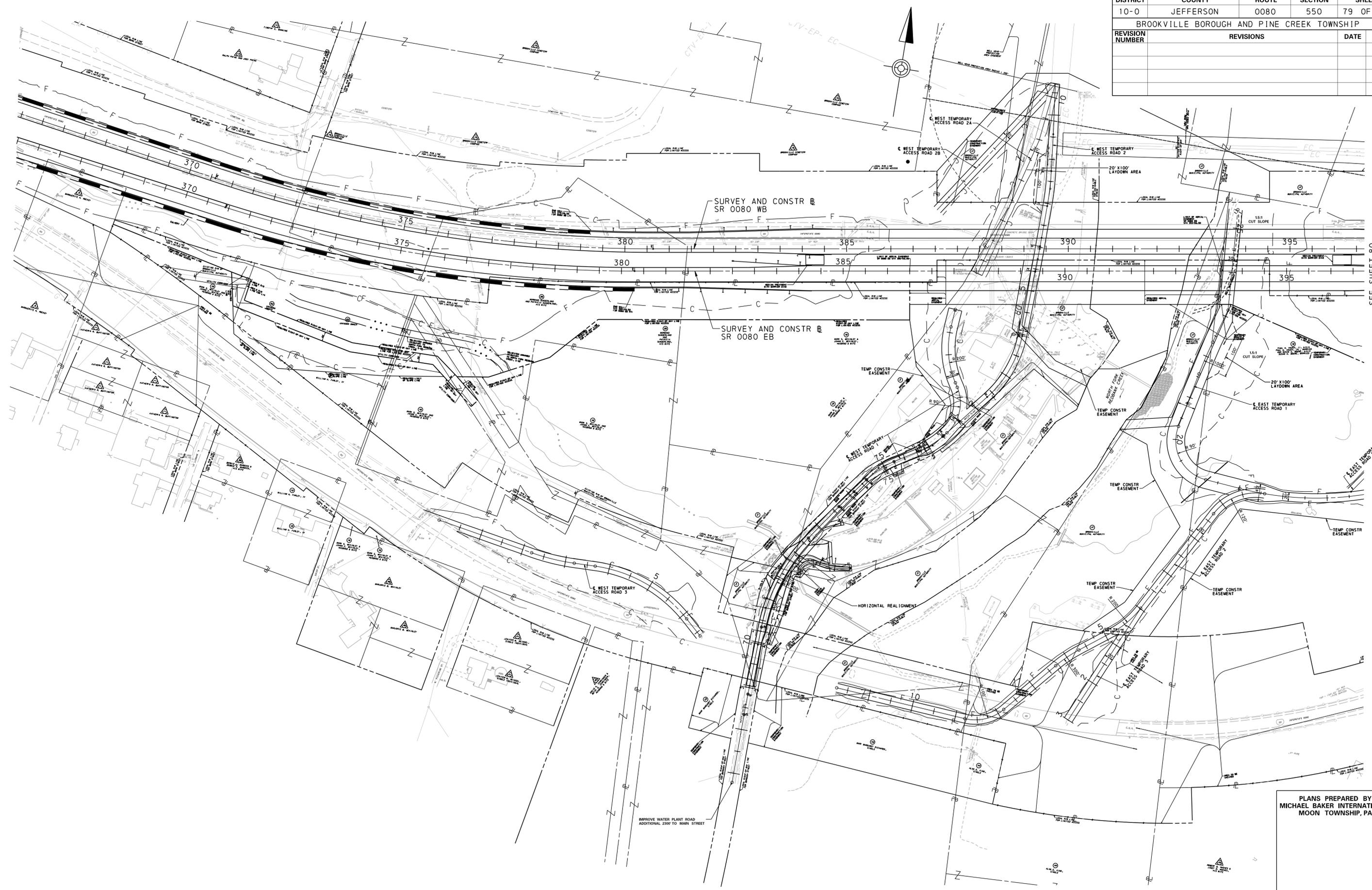


SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEET 77

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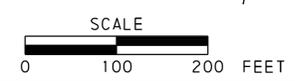
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
10-0	JEFFERSON	0080	550	79 OF 86
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE BY



9/28/2021 12:01:40 PM
 I-80_Nor.Th._Fork._AccessRoad_P.lans.dgn

SEE SHEET 80



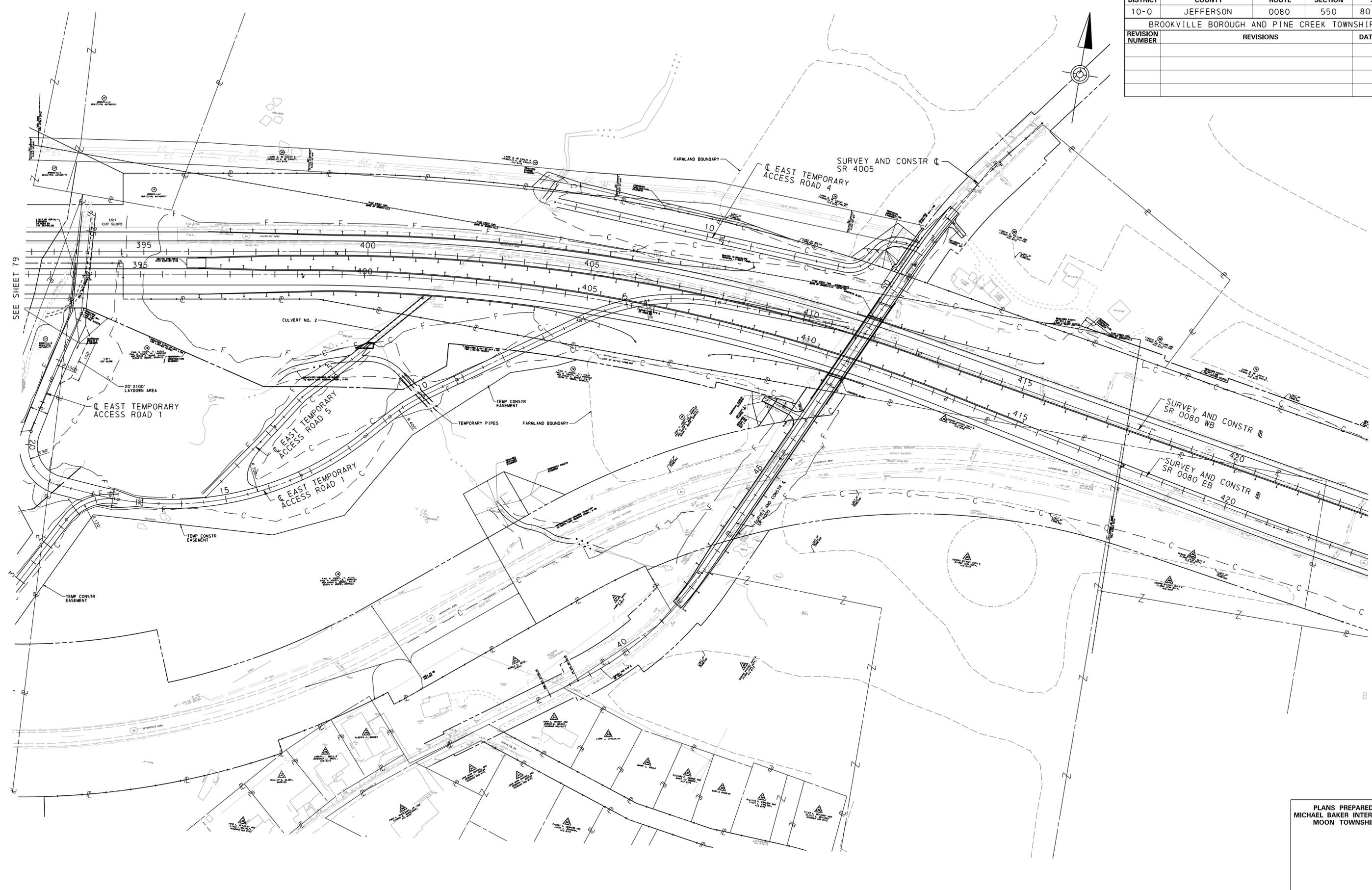
CONCEPTUAL TEMPORARY ACCESS ROADS

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 81 TO 86

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 MOON TOWNSHIP, PA

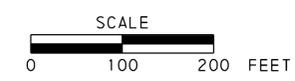
DES: TJM DWG: BSP CKD: JMB

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
10-0	JEFFERSON	0080	550	80 OF 86	
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



9/28/2021 12:01:42 PM
I-80_Nor th_Fork_AccessRoad_P lams.dgn

SEE SHEET 79



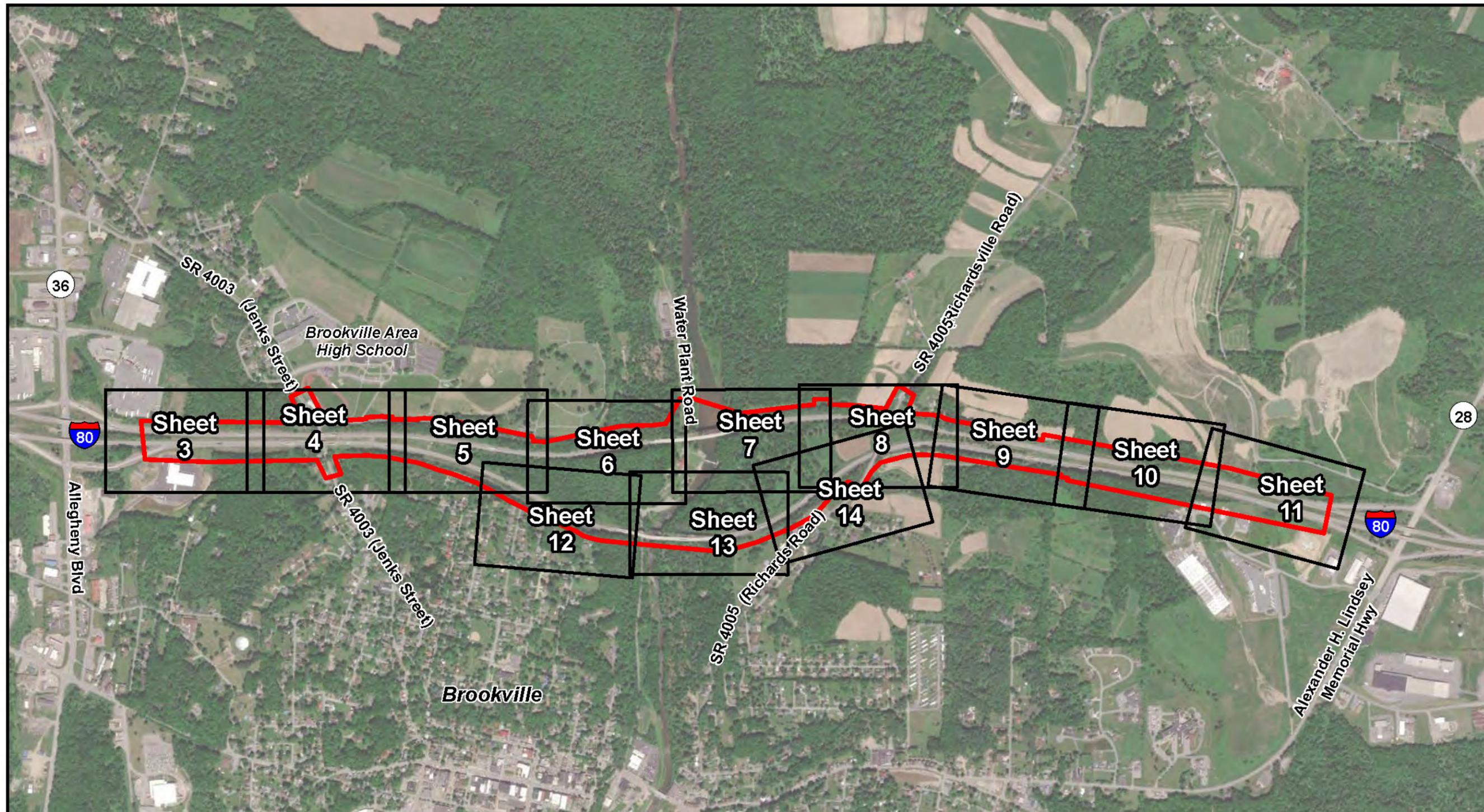
CONCEPTUAL TEMPORARY ACCESS ROADS

SURVEY BOOK #33-0080-550-0784-1258-0810-0344
 SURVEY BOOK #33-0080-550-0785-1578-0811-0617
 FOR PROFILE, SEE SHEETS 81 TO 86

PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

DES: TJM DWG: BSP CKD: JMB

Appendix C
Environmental Constraints Map



Legend

 Study Area



Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 1 of 14

Watercourse Impacts			
Watercourse ID	Flow Regime ¹	Temporary Impact (linear feet)	Permanent Impact (linear feet)
S-01	INT	0	530
S-02	PER	27	1,300
S-03	INT	0	0
S-04	EPH	0	0
S-05	EPH	42	24
S-06	EPH	0	493
S-07	EPH	0	29
S-08	PER	0	656
S-09	PER	861	1,352
S-10	PER	53	0
S-11	PER	75	142
S-12	INT	0	0
S-12A	EPH	0	0
S-13	EPH	0	0
S-14	PER	0	0
S-15	EPH	0	35
S-16	PER	14	107
S-17	INT	0	194
S-18	PER	12	84
S-19	PER	47	11
S-20	INT	0	153
S-21	PER	0	200
North Fork Redbank Creek	PER	655	68
Totals		1,786	5,378

¹ Flow regimes are either ephemeral (EPH), intermittent (INT), or perennial (PER)

Wetland Impacts				
Wetland ID	Cowardin Classification	Total Size (acres)	Temporary Impact (acres)	Permanent Impact (acres)
W-01	PEM	0.079	0.000	0.000
W-02	PEM	0.009	0.000	0.009
W-03	PEM	0.016	0.000	0.016
W-04	PEM	0.211	0.000	0.000
W-05	PEM	0.003	0.003	0.000
W-06	PEM	0.025	0.006	0.000
W-07	PEM	0.009	0.000	0.000
W-08	PEM	0.242	0.000	0.000
W-09	PEM	0.007	0.000	0.000
W-10	PEM	0.004	0.000	0.004
W-11	PEM	0.063	0.000	0.063
W-12	PEM	0.071	0.000	0.071
W-13	PSS	1.35	0.000	0.000
Totals		2.089	0.009	0.163

Floodplain Impacts			
Resource Name	Temporary or Permanent Impact	Impact Length (linear feet)	Impact Area (square feet)
North Fork Redbank Creek	Temporary	620	35472.9
North Fork Redbank Creek	Permanent	71	1382.2
Totals		691	36855.1

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550



See Sheet 4 of 14

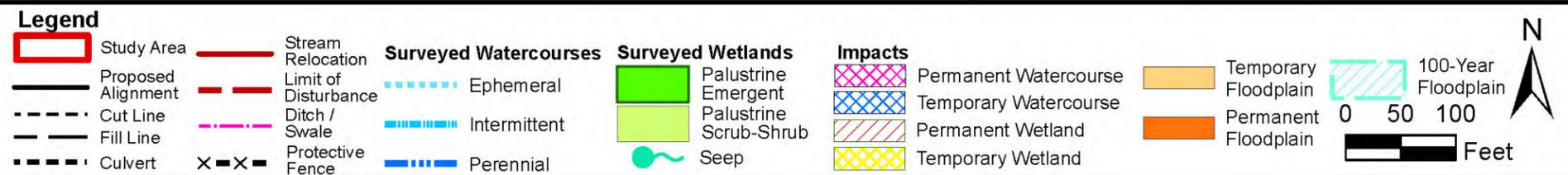


Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 3 of 14

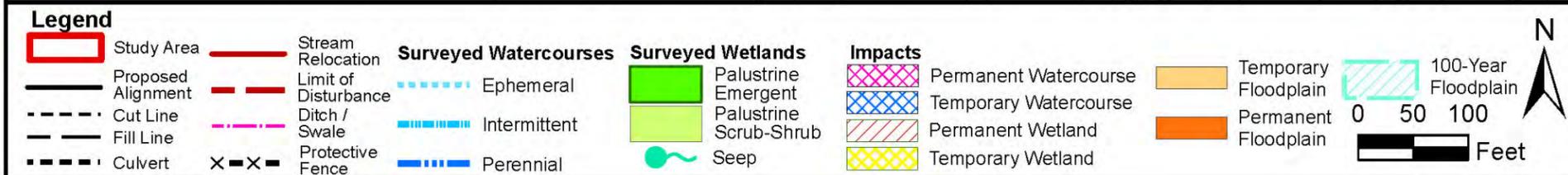
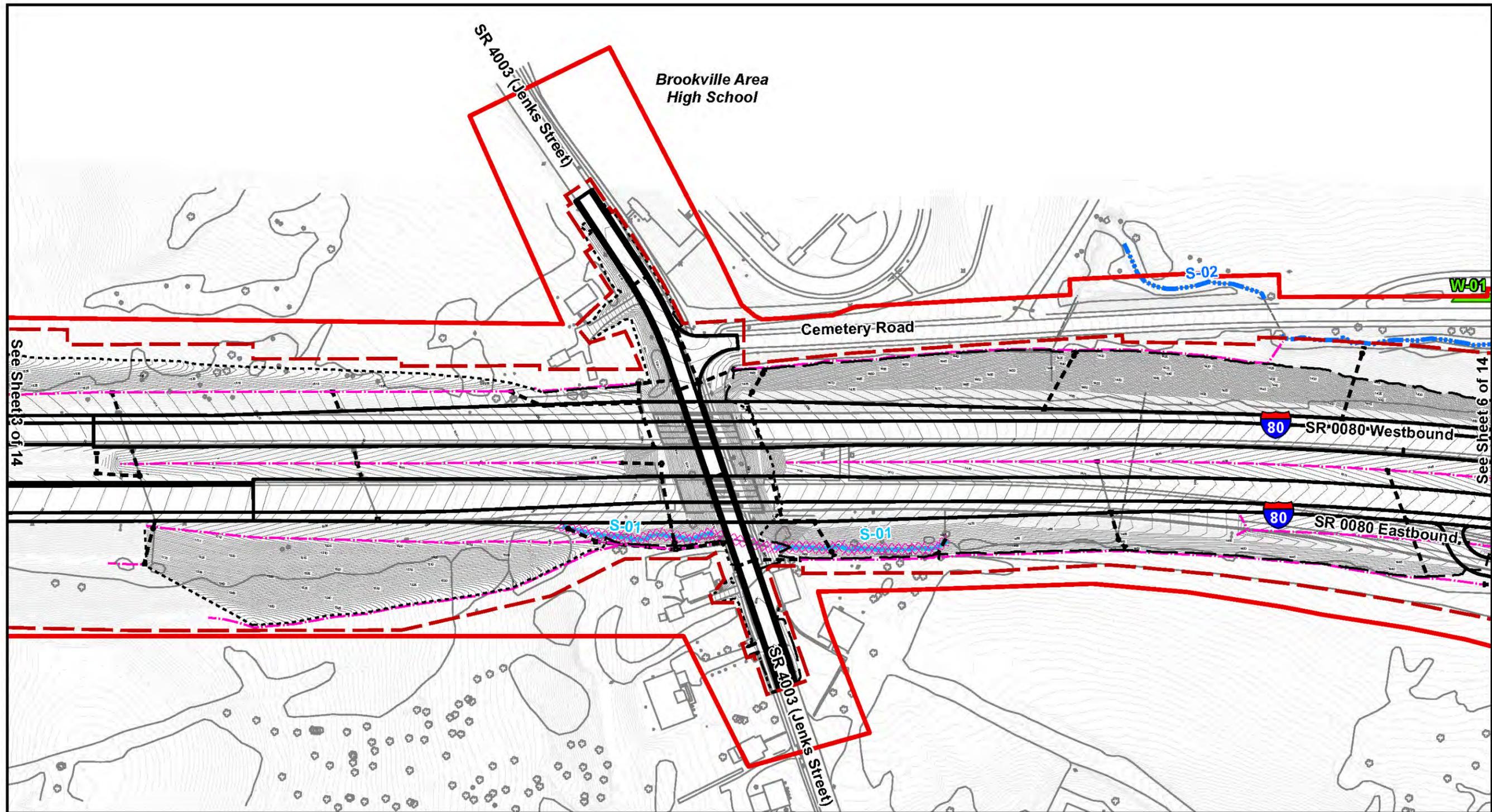


Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 4 of 14

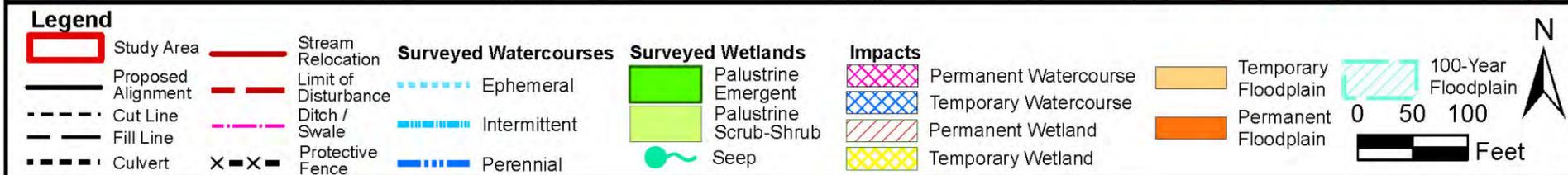
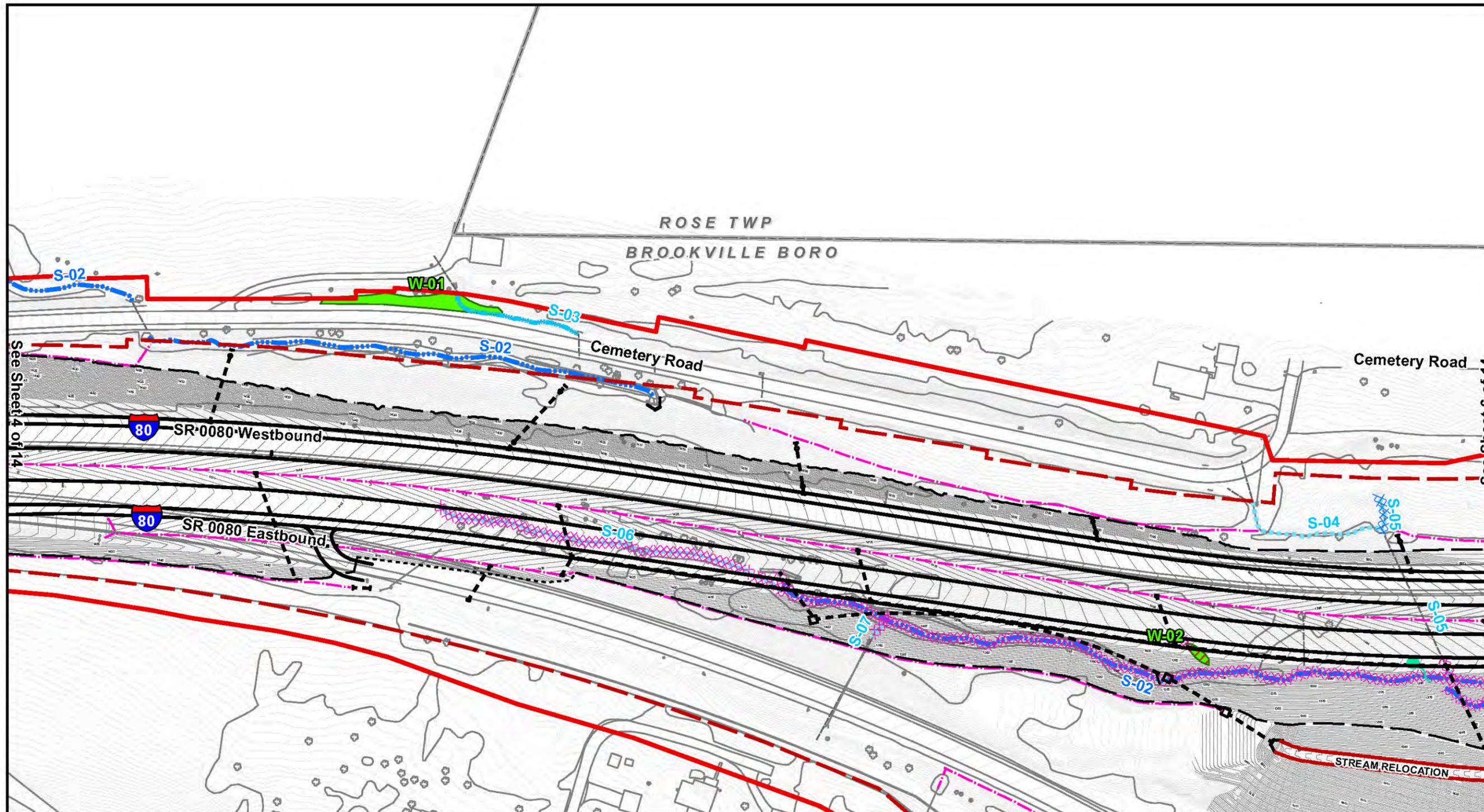


Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 5 of 14

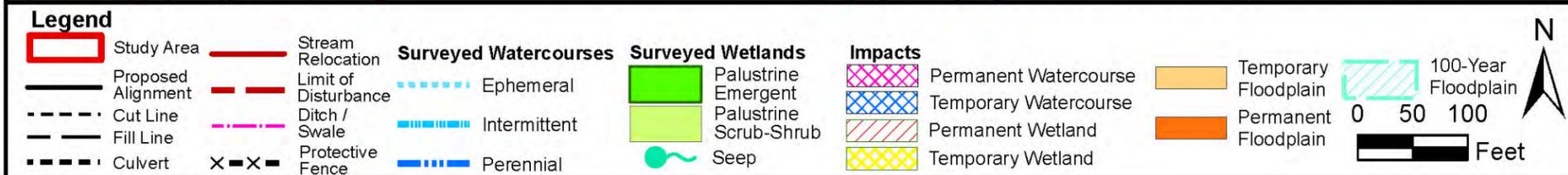
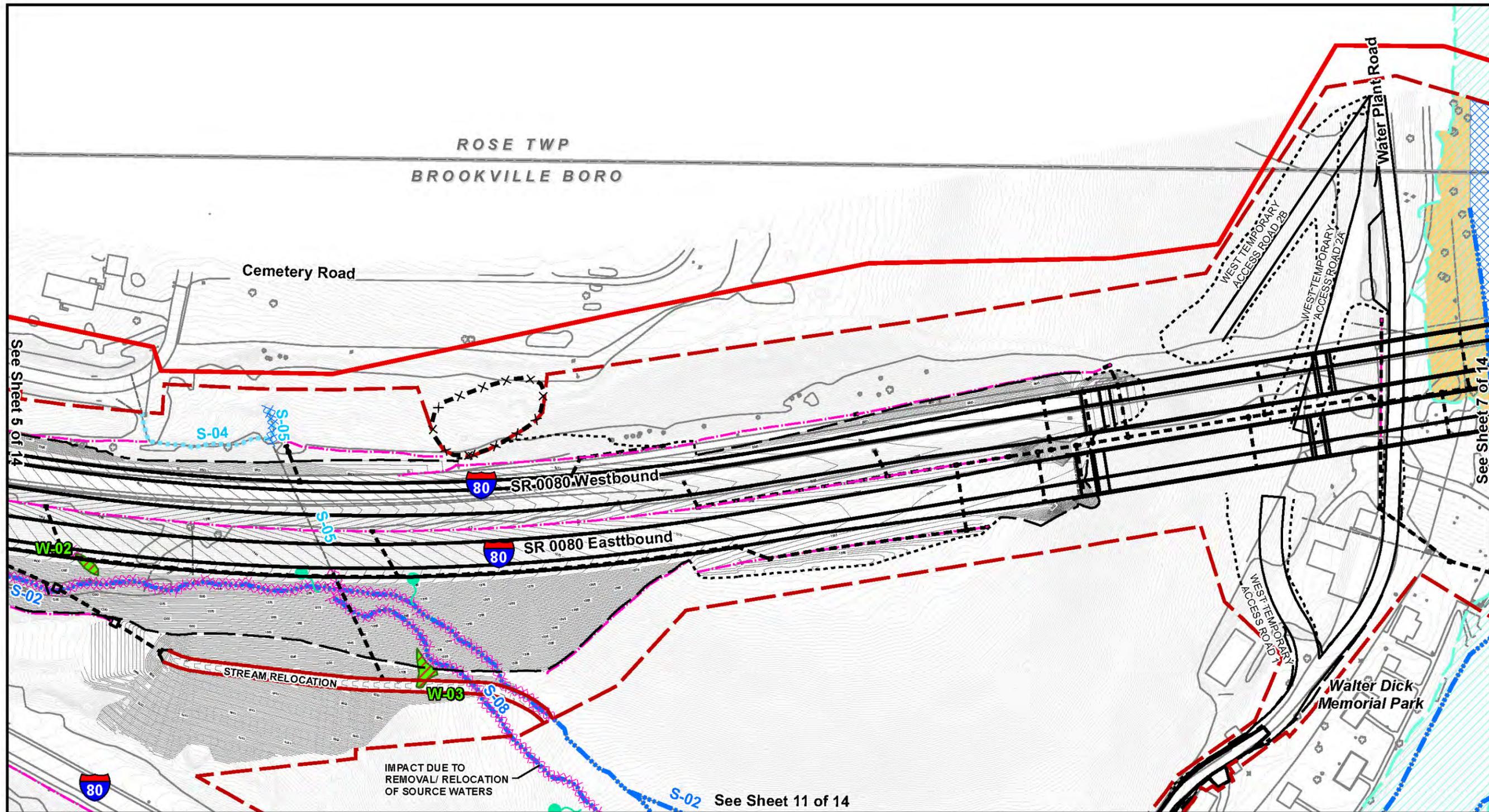
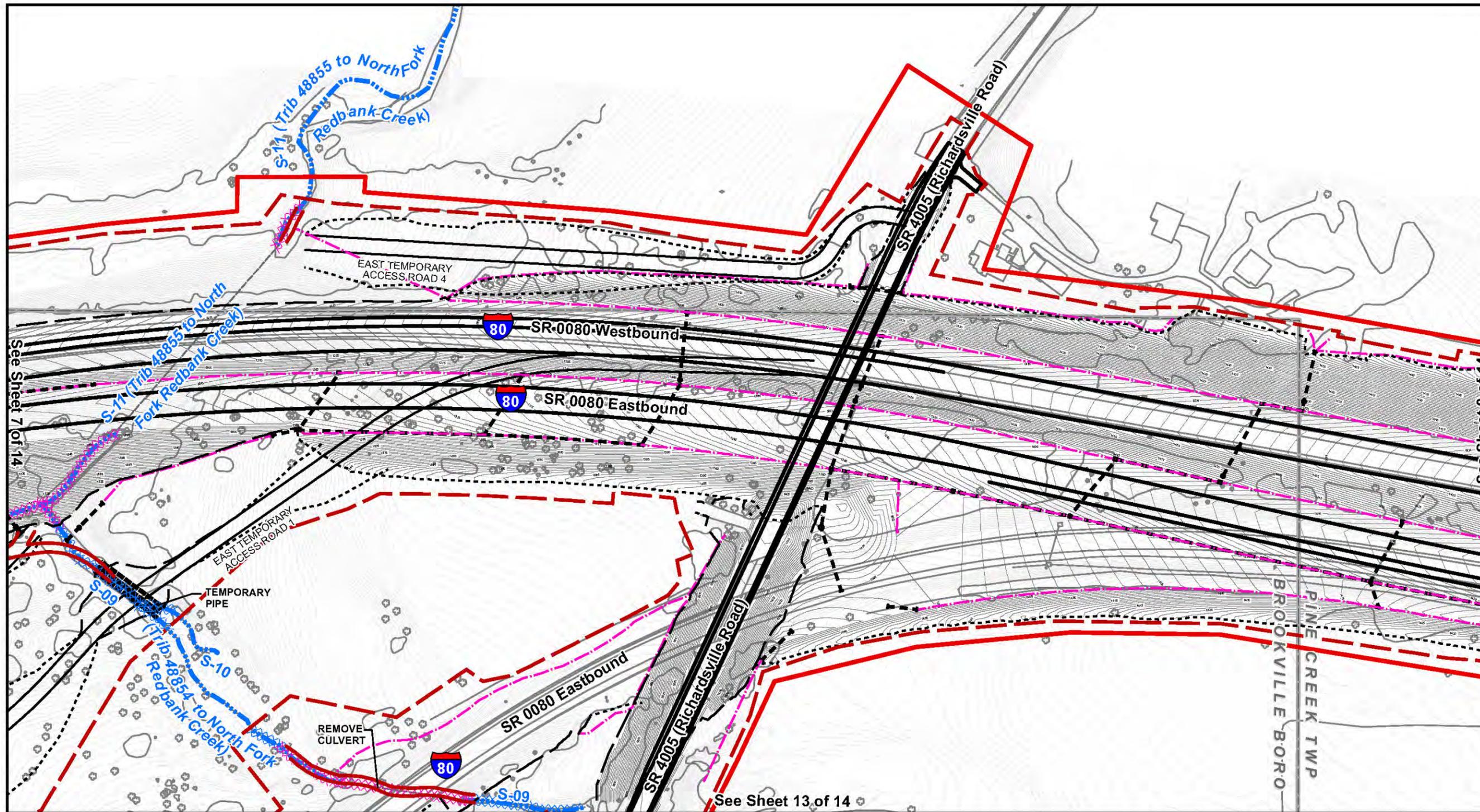
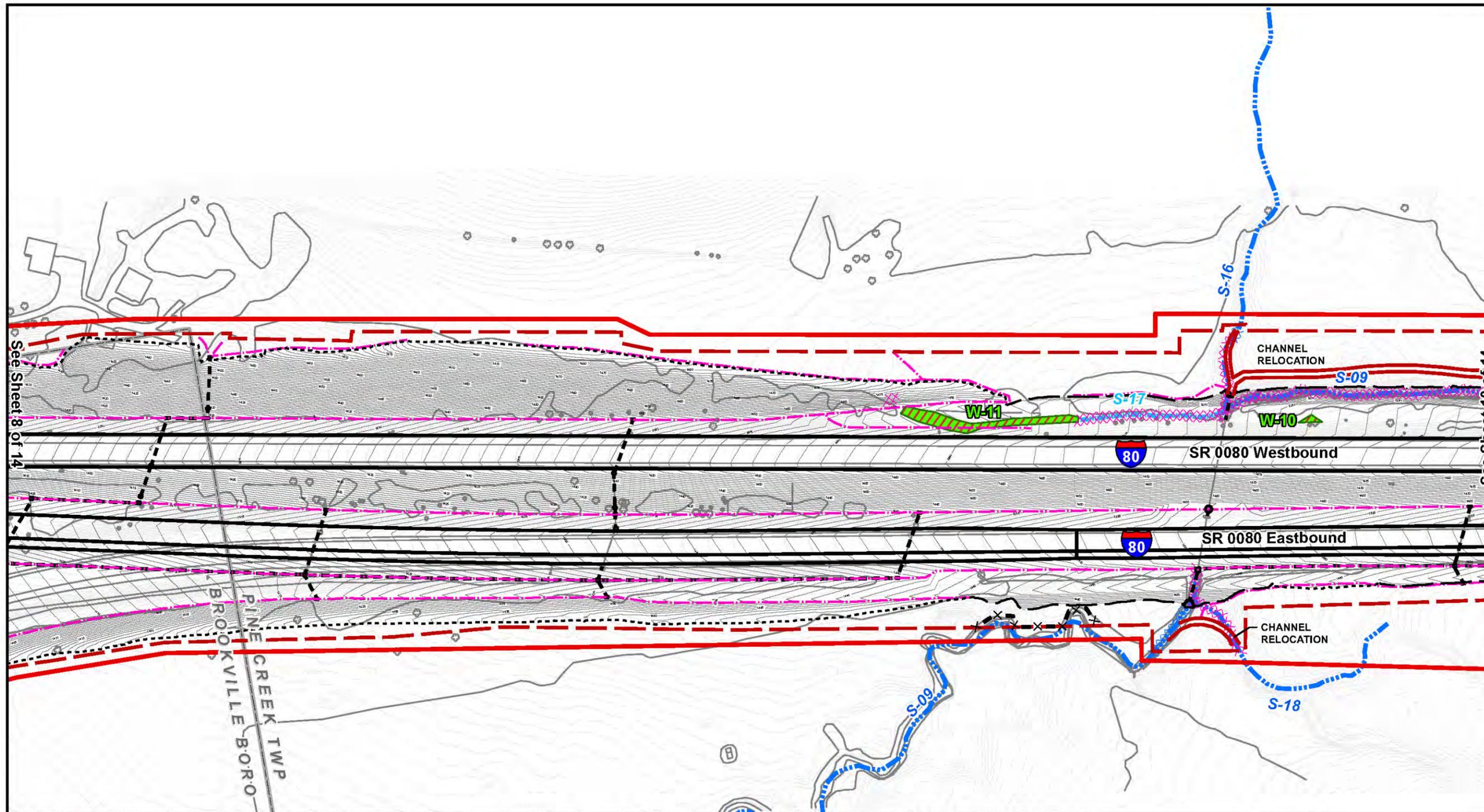


Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 6 of 14



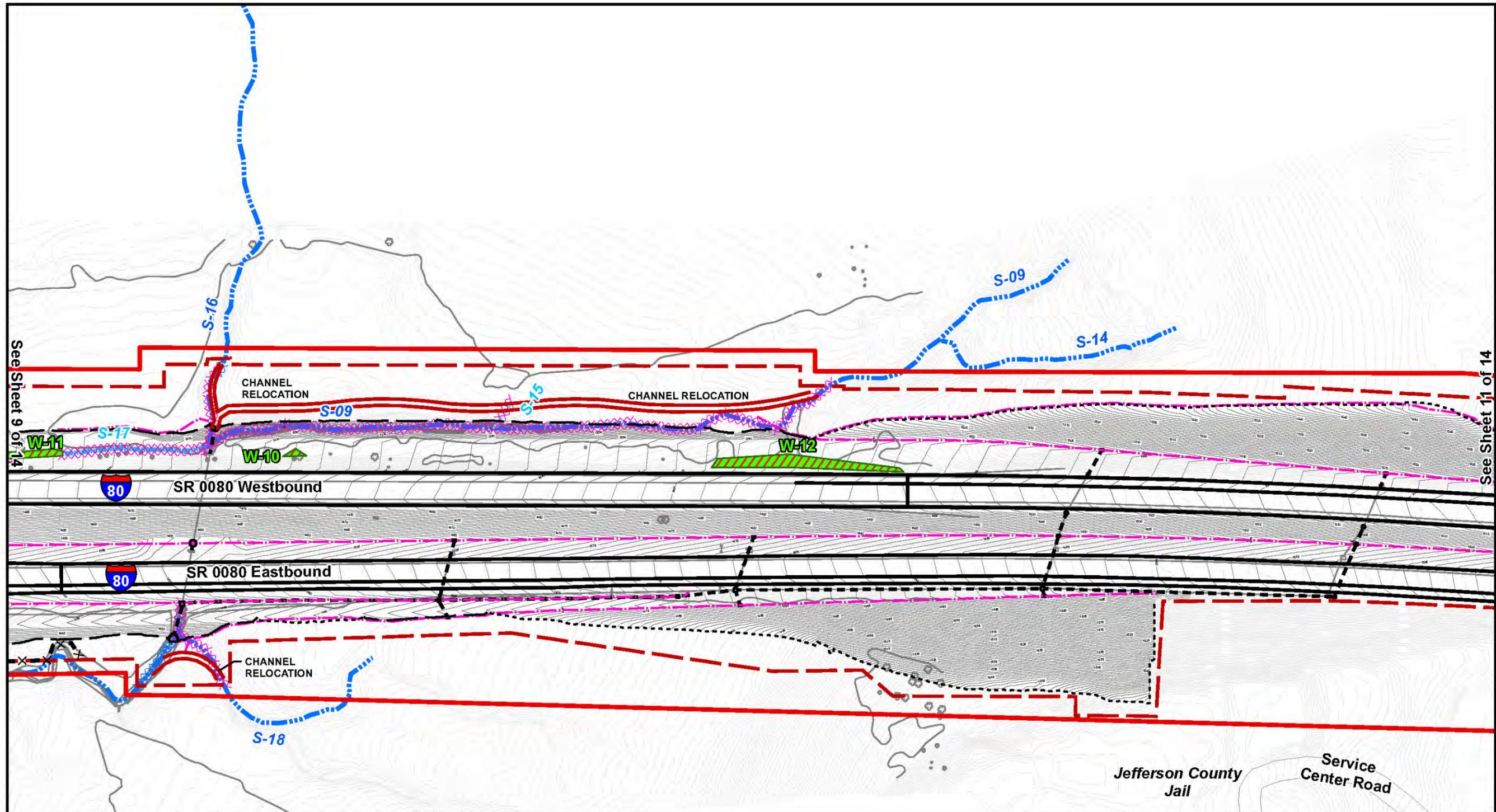
Legend		Surveyed Watercourses		Surveyed Wetlands		Impacts		Floodplains	
	Study Area		Stream Relocation		Palustrine Emergent		Permanent Watercourse		Temporary Floodplain
	Proposed Alignment		Limit of Disturbance		Palustrine Scrub-Shrub		Temporary Watercourse		Permanent Floodplain
	Cut Line		Ditch / Swale		Seep		Permanent Wetland		100-Year Floodplain
	Fill Line		Protective Fence		Perennial		Temporary Wetland		0 50 100 Feet
	Culvert								

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 8 of 14



Legend		Surveyed Watercourses		Surveyed Wetlands		Impacts		Floodplains	
	Study Area		Stream Relocation		Palustrine Emergent		Permanent Watercourse		Temporary Floodplain
	Proposed Alignment		Limit of Disturbance		Palustrine Scrub-Shrub		Temporary Watercourse		Permanent Floodplain
	Cut Line		Ditch / Swale		Seep		Permanent Wetland		100-Year Floodplain
	Fill Line		Protective Fence				Temporary Wetland		0 50 100 Feet
	Culvert								

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 9 of 14

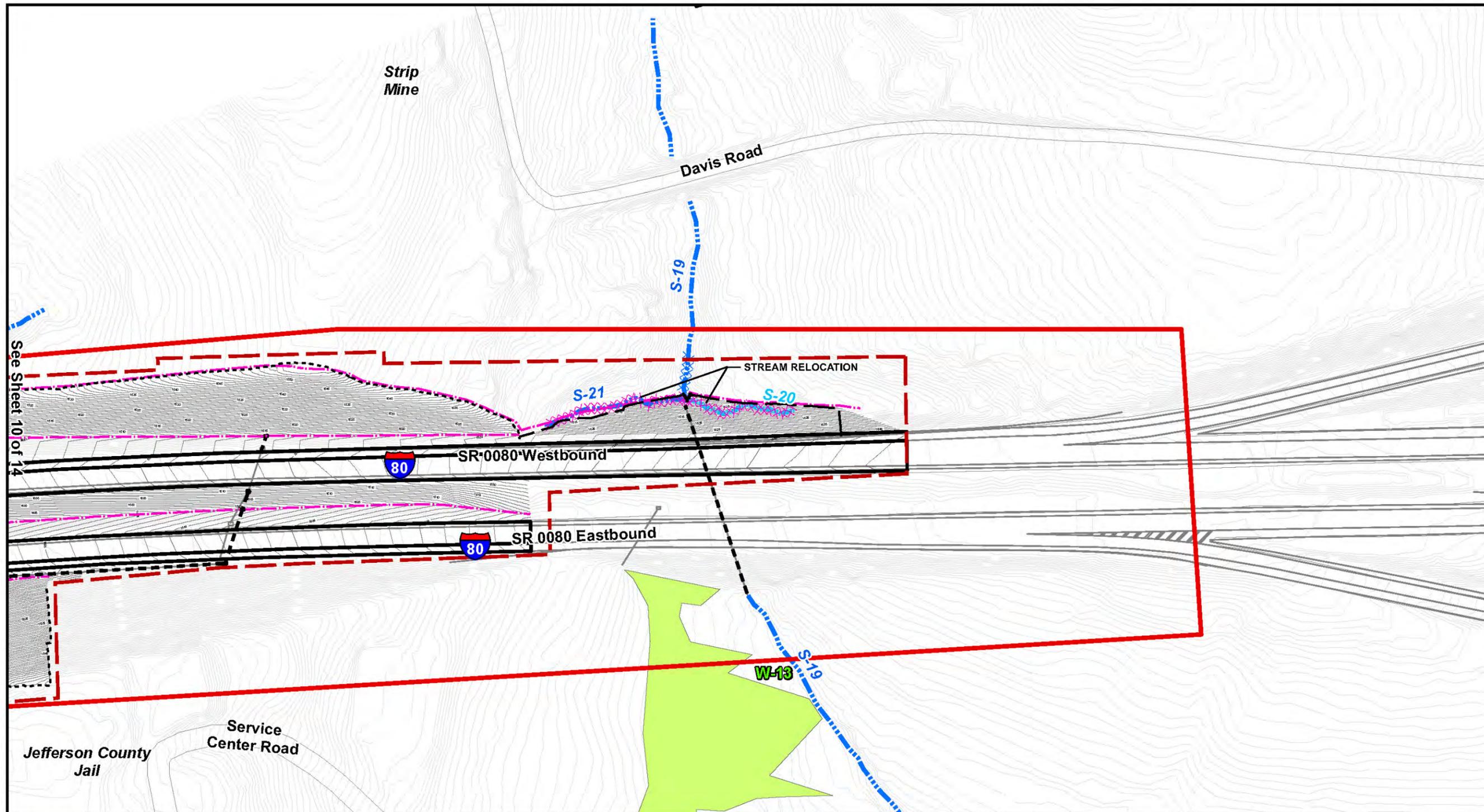


See Sheet 9 of 14

See Sheet 11 of 14

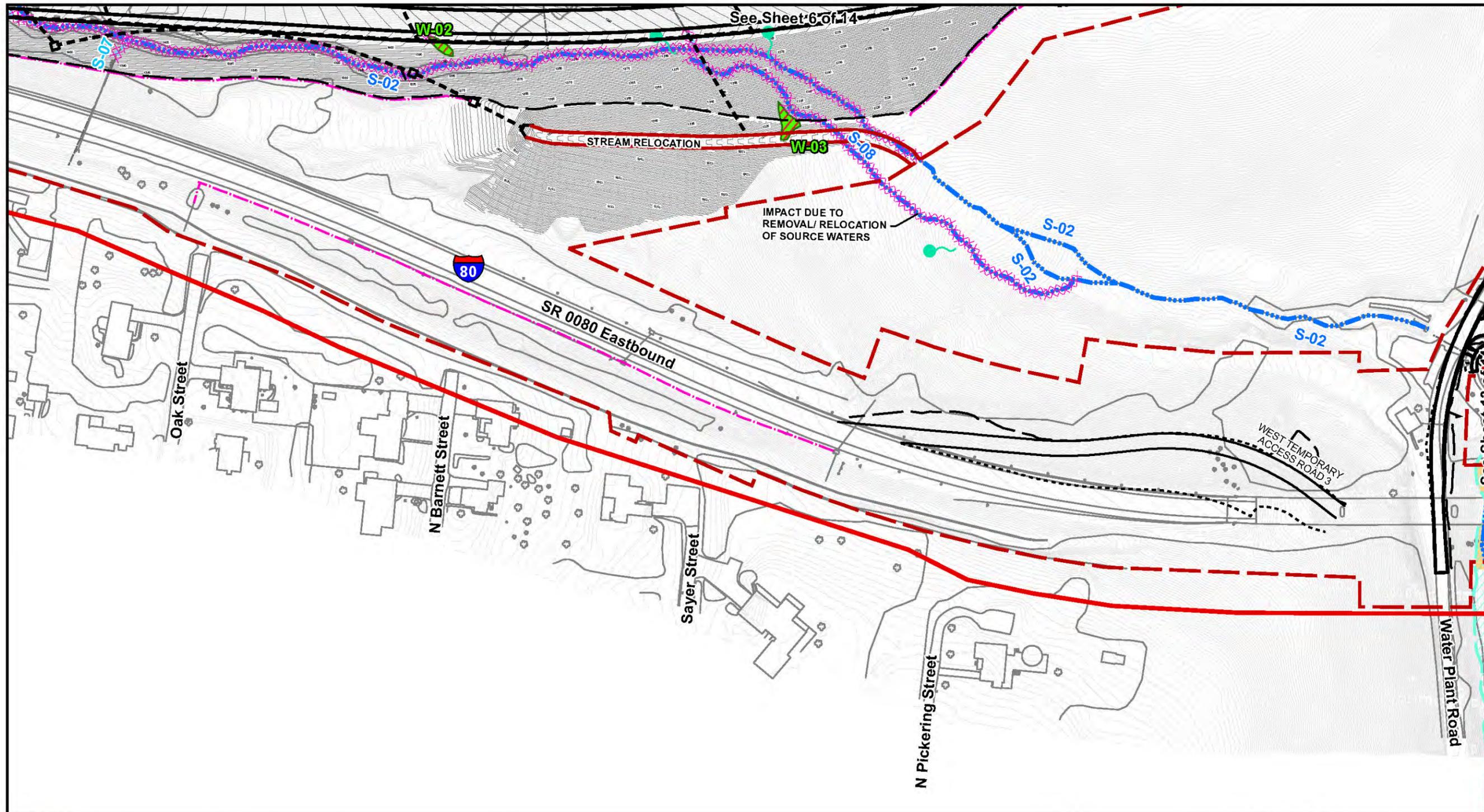
Legend	Study Area	Stream Relocation	Limit of Disturbance	Surveyed Watercourses	Palustrine Emergent	Impacts	Temporary Floodplain	100-Year Floodplain
Proposed Alignment	Ditch / Swale	Ephemeral	Palustrine Scrub-Shrub	Intermittent	Permanent Watercourse	Temporary Watercourse	Permanent Floodplain	0 50 100
Cut Line	Protective Fence	Perennial	Seep	Permanent Wetland	Temporary Wetland			Feet
Fill Line								
Culvert								

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 10 of 14



Legend		Surveyed Watercourses		Surveyed Wetlands		Impacts		Floodplains	
	Study Area		Stream Relocation		Palustrine Emergent		Permanent Watercourse		Temporary Floodplain
	Proposed Alignment		Limit of Disturbance		Palustrine Scrub-Shrub		Temporary Watercourse		Permanent Floodplain
	Cut Line		Ditch / Swale		Seep		Permanent Wetland		100-Year Floodplain
	Fill Line		Protective Fence				Temporary Wetland		Feet
	Culvert								

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 11 of 14



Legend

Study Area	Stream Relocation	Surveyed Watercourses	Surveyed Wetlands	Impacts	Temporary Floodplain	100-Year Floodplain
Proposed Alignment	Limit of Disturbance					
Cut Line	Ditch / Swale	Intermittent	Palustrine Scrub-Shrub	Temporary Watercourse	0	N
Fill Line	Protective Fence	Perennial	Seep	Permanent Wetland	50	Feet
Culvert				Temporary Wetland	100	

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania Sheet 12 of 14

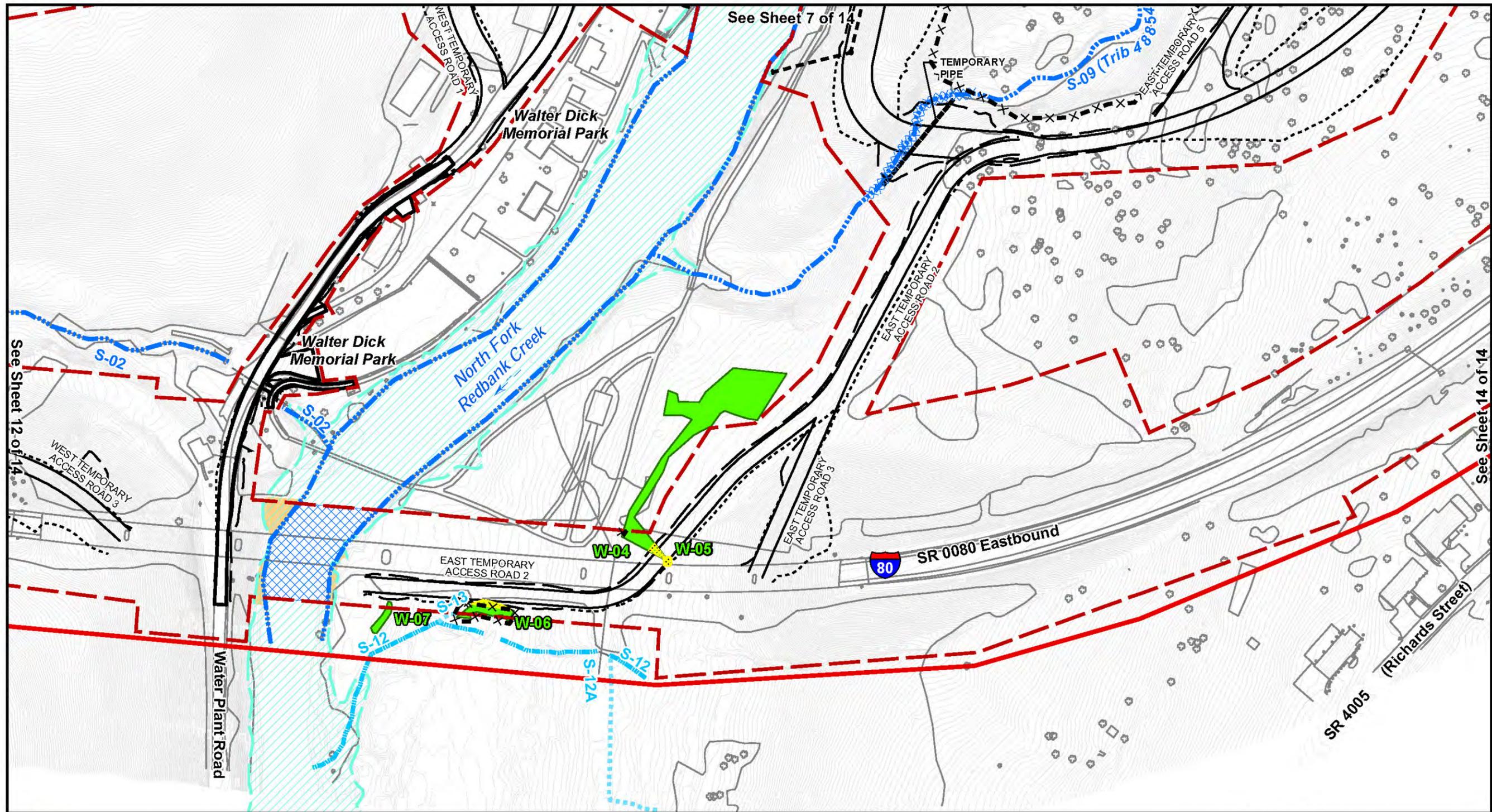
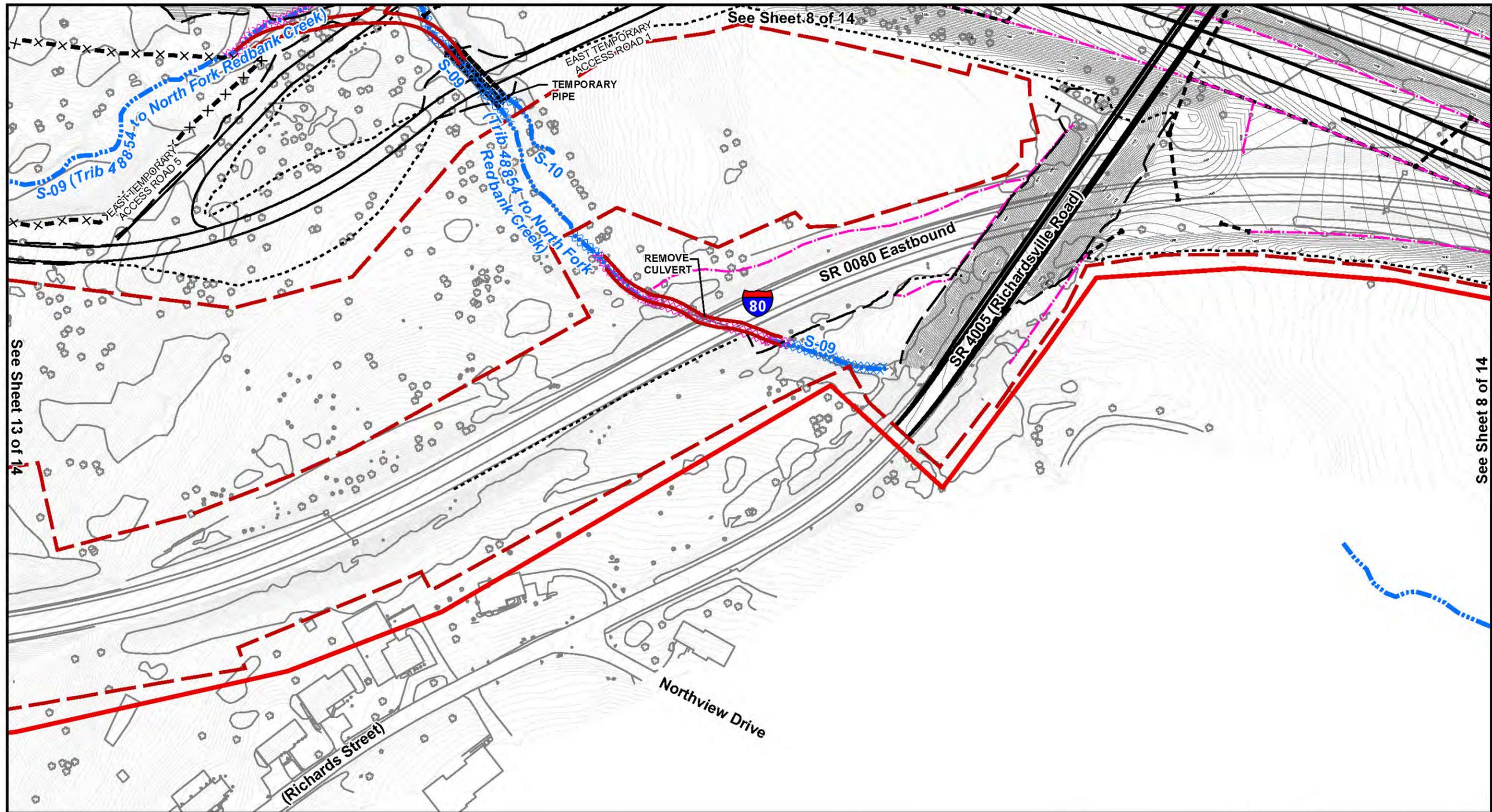


Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania **Sheet 13 of 14**



Legend

Study Area	Stream Relocation	Surveyed Watercourses	Palustrine Emergent	Impacts	Permanent Watercourse	Temporary Floodplain	100-Year Floodplain
Proposed Alignment	Limit of Disturbance						
Cut Line	Ditch / Swale	Intermittent	Seep	Permanent Wetland	Temporary Wetland	Feet	
Fill Line	Protective Fence	Perennial		Temporary Wetland			
Culvert							

Figure 2
Environmental Constraints
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania **Sheet 14 of 14**

Appendix D
Threatened and Endangered
Species

1. PROJECT INFORMATION

Project Name: **PennDOT Pathways Program - I-80 North Fork Project**

Date of Review: **12/7/2021 04:46:29 PM**

Project Category: **Transportation, Structures and Bridges, New Bridge construction on new alignment**

Project Area: **247.05 acres**

County(s): **Jefferson**

Township/Municipality(s): **BROOKVILLE; PINE CREEK TOWNSHIP; ROSE TOWNSHIP**

ZIP Code:

Quadrangle Name(s): **BROOKVILLE**

Watersheds HUC 8: **Middle Allegheny-Redbank**

Watersheds HUC 12: **Mill Creek; Pekin Run-North Fork Redbank Creek; Upper Redbank Creek**

Decimal Degrees: **41.169727, -79.069329**

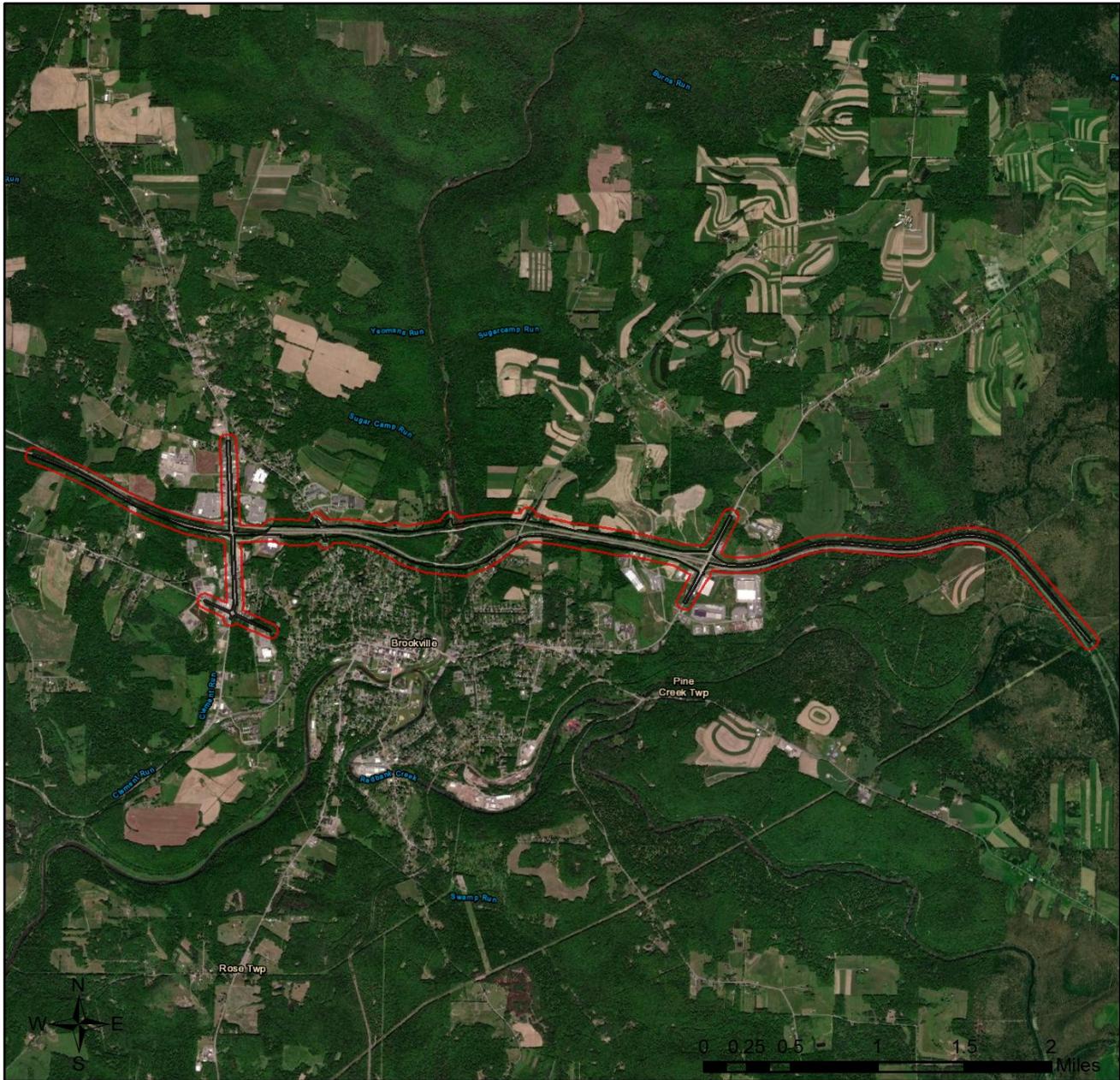
Degrees Minutes Seconds: **41° 10' 11.172" N, 79° 4' 9.5861" W**

2. SEARCH RESULTS

Agency	Results	Response
PA Game Commission	No Known Impact	No Further Review Required
PA Department of Conservation and Natural Resources	No Known Impact	No Further Review Required
PA Fish and Boat Commission	No Known Impact	No Further Review Required
U.S. Fish and Wildlife Service	No Known Impact	No Further Review Required

As summarized above, Pennsylvania Natural Diversity Inventory (PNDI) records indicate no known impacts to threatened and endangered species and/or special concern species and resources within the project area. Therefore, based on the information you provided, no further coordination is required with the jurisdictional agencies. This response does not reflect potential agency concerns regarding impacts to other ecological resources, such as wetlands.

PennDOT Pathways Program - I-80 North Fork Project

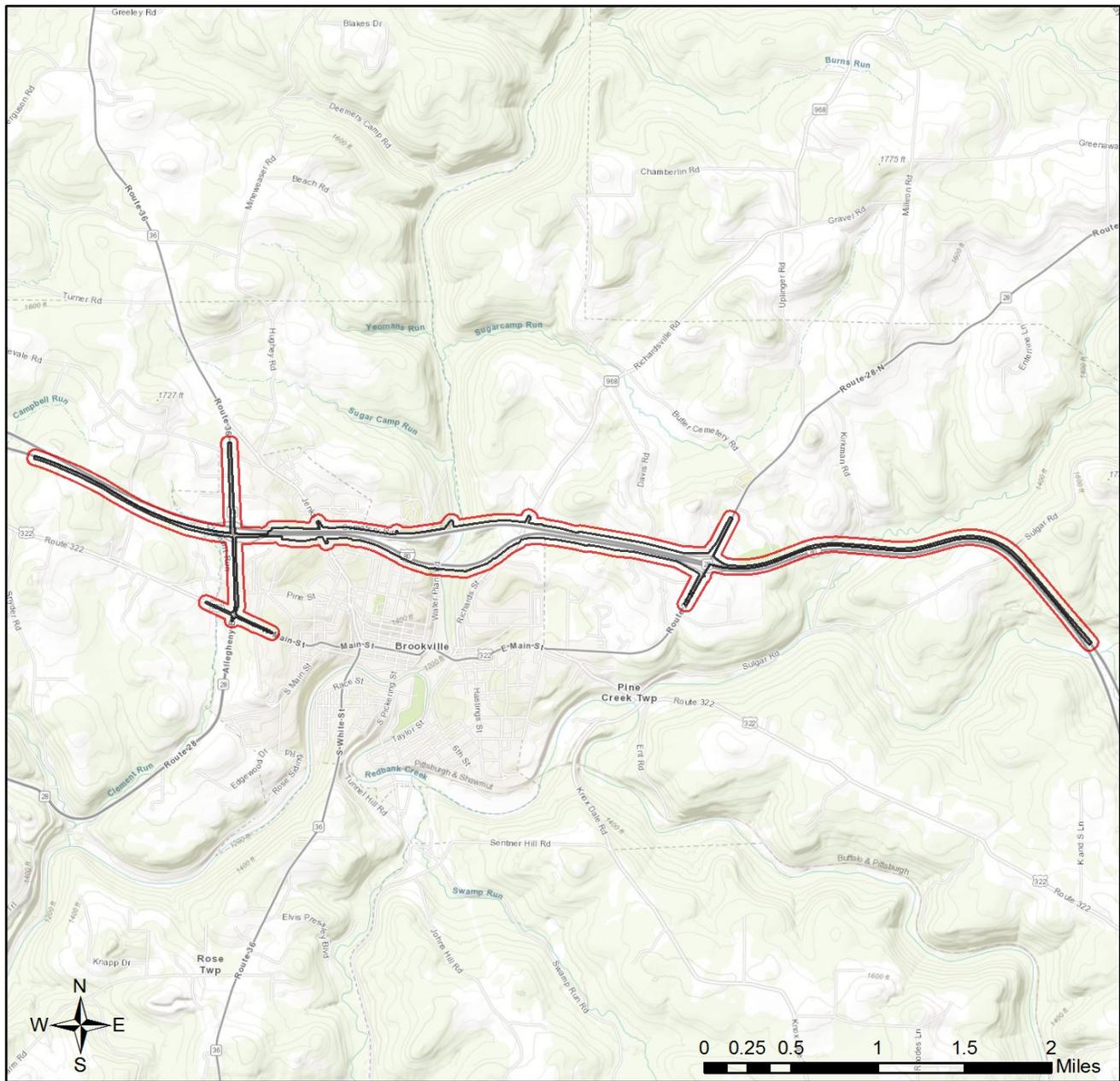


-  Project Boundary
-  Buffered Project Boundary



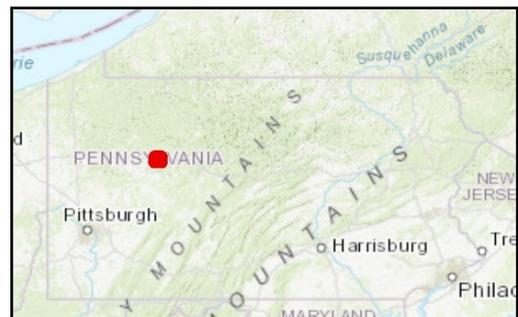
Service Layer Credits: Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community
Esri, HERE, Garmin, (c) OpenStreetMap contributors, and the GIS user community
Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China

PennDOT Pathways Program - I-80 North Fork Project



- Project Boundary
- Buffered Project Boundary

Service Layer Credits: Sources: Esri, HERE, Garmin, Intemap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), (c) OpenStreetMap contributors, and the GIS User Community



3. AGENCY COMMENTS

Regardless of whether a DEP permit is necessary for this proposed project, any potential impacts to threatened and endangered species and/or special concern species and resources must be resolved with the appropriate jurisdictional agency. In some cases, a permit or authorization from the jurisdictional agency may be needed if adverse impacts to these species and habitats cannot be avoided.

These agency determinations and responses are **valid for two years** (from the date of the review), and are based on the project information that was provided, including the exact project location; the project type, description, and features; and any responses to questions that were generated during this search. If any of the following change: 1) project location, 2) project size or configuration, 3) project type, or 4) responses to the questions that were asked during the online review, the results of this review are not valid, and the review must be searched again via the PNDI Environmental Review Tool and resubmitted to the jurisdictional agencies. The PNDI tool is a primary screening tool, and a desktop review may reveal more or fewer impacts than what is listed on this PNDI receipt. The jurisdictional agencies **strongly advise against** conducting surveys for the species listed on the receipt prior to consultation with the agencies.

PA Game Commission

RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

PA Department of Conservation and Natural Resources

RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

PA Fish and Boat Commission

RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

U.S. Fish and Wildlife Service

RESPONSE:

No impacts to **federally** listed or proposed species are anticipated. Therefore, no further consultation/coordination under the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq. is required. Because no take of federally listed species is anticipated, none is authorized. This response does not reflect potential Fish and Wildlife Service concerns under the Fish and Wildlife Coordination Act or other authorities.

4. DEP INFORMATION

The Pa Department of Environmental Protection (DEP) requires that a signed copy of this receipt, along with any required documentation from jurisdictional agencies concerning resolution of potential impacts, be submitted with applications for permits requiring PNDI review. Two review options are available to permit applicants for handling PNDI coordination in conjunction with DEP's permit review process involving either T&E Species or species of special concern. Under sequential review, the permit applicant performs a PNDI screening and completes all coordination with the appropriate jurisdictional agencies prior to submitting the permit application. The applicant will include with its application, both a PNDI receipt and/or a clearance letter from the jurisdictional agency if the PNDI Receipt shows a Potential Impact to a species or the applicant chooses to obtain letters directly from the jurisdictional agencies. Under concurrent review, DEP, where feasible, will allow technical review of the permit to occur concurrently with the T&E species consultation with the jurisdictional agency. The applicant must still supply a copy of the PNDI Receipt with its permit application. The PNDI Receipt should also be submitted to the appropriate agency according to directions on the PNDI Receipt. The applicant and the jurisdictional agency will work together to resolve the potential impact(s). See the DEP PNDI policy at <https://conservationexplorer.dcnr.pa.gov/content/resources>.

5. ADDITIONAL INFORMATION

The PNDI environmental review website is a preliminary screening tool. There are often delays in updating species status classifications. Because the proposed status represents the best available information regarding the conservation status of the species, state jurisdictional agency staff give the proposed statuses at least the same consideration as the current legal status. If surveys or further information reveal that a threatened and endangered and/or special concern species and resources exist in your project area, contact the appropriate jurisdictional agency/agencies immediately to identify and resolve any impacts.

For a list of species known to occur in the county where your project is located, please see the species lists by county found on the PA Natural Heritage Program (PNHP) home page (www.naturalheritage.state.pa.us). Also note that the PNDI Environmental Review Tool only contains information about species occurrences that have actually been reported to the PNHP.

6. AGENCY CONTACT INFORMATION

PA Department of Conservation and Natural Resources

Bureau of Forestry, Ecological Services Section
400 Market Street, PO Box 8552
Harrisburg, PA 17105-8552
Email: RA-HeritageReview@pa.gov

PA Fish and Boat Commission

Division of Environmental Services
595 E. Rolling Ridge Dr., Bellefonte, PA 16823
Email: RA-FBPACENOTIFY@pa.gov

U.S. Fish and Wildlife Service

Pennsylvania Field Office
Endangered Species Section
110 Radnor Rd; Suite 101
State College, PA 16801
Email: IR1_ESPenn@fws.gov
NO Faxes Please

PA Game Commission

Bureau of Wildlife Habitat Management
Division of Environmental Planning and Habitat Protection
2001 Elmerton Avenue, Harrisburg, PA 17110-9797
Email: RA-PGC_PNDI@pa.gov
NO Faxes Please

7. PROJECT CONTACT INFORMATION

Name: Kathleen I. Krommes
Company/Business Name: HDR
Address: 4900 Ritter Rd, Suite 101
City, State, Zip: Mechanicsburg, PA 17055
Phone: (717) 516-3158 Fax: ()
Email: Kathy.Krommes@hdrinc.com

8. CERTIFICATION

I certify that ALL of the project information contained in this receipt (including project location, project size/configuration, project type, answers to questions) is true, accurate and complete. In addition, if the project type, location, size or configuration changes, or if the answers to any questions that were asked during this online review change, I agree to re-do the online environmental review.

Kath I Krommes
applicant/project proponent signature

12/7/2021
date

Appendix E
Section 4(f) De minimis Use Form



On Behalf of the Federal Highway
Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
Wildlife and/or Waterfowl Refuges,
State Forest Land, and State Game Land
May 2014 Version

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

SELECT ONE: EIS EA CE EER ED

PROJECT DESCRIPTION:

(Provide a concise but thorough description of the proposed action.)

The Pennsylvania Department of Transportation (PennDOT), in cooperation with the Federal Highway Administration (FHWA), is advancing the replacement of two bridges carrying SR 0080, Section 550 Westbound (WB) and Eastbound (EB) over North Fork Redbank Creek and Water Plant Road in Brookville Borough and Pine Creek Township, Jefferson County, Pennsylvania (**Attachment A - Figure 1**). The proposed project will consist of an off-alignment replacement of two bridges that carry the WB and EB lanes of SR 0080 over North Fork Redbank Creek and Water Plant Road, realigning the WB and EB lanes of SR 0080, an on-alignment replacement of the SR 0080 EB and WB bridges over SR 4003 (Jenks Street), and an on-alignment replacement of the SR 4005 (Richardsville Road) bridge over SR 0080 WB and EB.

IDENTIFICATION OF SECTION 4(f)/SECTION 2002 PROPERTY:

(List the property and provide a description of the property as per Chapter 6 of the *Section 4(f)/Section 2002 Handbook*. Attach a map, photo(s), etc. as appropriate.)

Walter Dick Memorial Park is situated below the SR 0080 WB and EB bridges (**Attachment A – Figure 2**). It is owned and maintained by Brookville Borough and is located on Brookville Municipal Authority property. Park amenities include the North Fork Environmental Education Center, four picnic shelters, a restroom facility, playground, concession building, swimming area, nature trails, and a suspension footbridge that connects both sides of North Fork Redbank Creek (**Attachment B – Photographs**). Vehicular access is from Water Plant Road. Walter Dick Memorial Park received a Land and Water Conservation Fund (LWCF) grant and is therefore considered both a Section 4(f) and a Section 6(f) resource.

FOR PARKS, IDENTIFY KEY COMPONENTS OF ANY EXISTING MANAGEMENT PLAN (if it exists):

N/A



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
Wildlife and/or Waterfowl Refuges,
State Forest Land, and State Game Land
May 2014 Version**

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

OFFICIAL WITH JURISDICTION OVER SECTION 4(f)/SECTION 2002 PROPERTY:

1. Identify agency with jurisdiction:
Brookville Borough
2. Name and title of contact person at agency:
Dana Rooney, Brookville Borough Manager

APPLICABILITY DETERMINATION:

1. Provide the total acreage of the property: 16.5 acres

Describe the use of land from the property (identify amount of the property to be used, including temporary and permanent acquisition):

Walter Dick Memorial Park will be impacted due to the permanent right-of-way needed for the new pier locations. No active recreation areas will be permanently impacted; however, a portion of a nature trail will need to be relocated on the eastern side of North Fork Redbank Creek (see **Attachment A – Figure 2**). Additionally, the project will require temporary access roads and temporary easements for access during construction. Approximately 1.2 acres of park property will be permanently impacted and approximately 5.5 acres will be temporarily impacted. Since the temporary construction easements (TCEs) will be needed for approximately three years, the project will result in a permanent Section 6(f) conversion and replacement land will need to be acquired in accordance with LWCF requirements.

2. The project **does not** adversely affect the activities, features, and attributes of the resource that qualify it for protection under Section 4(f) or Section 2002. **(If this statement cannot be verified as true, de minimis/no adverse use does not apply.)** YES

Describe the effect to the qualities, activities, features, or attributes of the resource that qualify it for protection. Include a description of any mitigation included when making the determination regarding effects to the resource:

The activities, features, and attributes that qualify the park for protection under Section 4(f) will not be adversely affected by the project. Permanent impacts will occur in undeveloped portions of Walter Dick Memorial Park and some steep hillsides greater than 15% in slope. A portion of a nature trail will be impacted by the placement of a bridge pier, but it will be



On Behalf of the Federal Highway
Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
Wildlife and/or Waterfowl Refuges,
State Forest Land, and State Game Land
May 2014 Version

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

relocated around the pier. Coordination with Brookville Borough regarding the proposed TCEs will continue throughout construction to ensure the least disruption to park activities as possible.

3. The public was afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the resource. YES

Identify the opportunity(ies) for public comment and describe the input received (provide attachments as appropriate to document the public involvement activity):

A virtual public meeting was held between March 1 and March 22, 2021

([https://www.penndot.gov/RegionalOffices/district-10/PublicMeetings/JeffersonCounty/Pages/I-](https://www.penndot.gov/RegionalOffices/district-10/PublicMeetings/JeffersonCounty/Pages/I-80%20North%20Fork%20Bridges%20Online%20Meeting.aspx)

[80% 20North% 20Fork% 20Bridges% 20Online% 20Meeting.aspx](https://www.penndot.gov/RegionalOffices/district-10/PublicMeetings/JeffersonCounty/Pages/I-80%20North%20Fork%20Bridges%20Online%20Meeting.aspx)). The public was afforded the opportunity to comment on the proposed impacts to the park. Only one comment was received on the park impacts and it was regarding the potential viewshed being impacted by removing trees (see **Attachment C**). Impacts to mature trees will be minimized as much practicable. In addition, a flyer was posted in Walter Dick Memorial Park between August 11 and August 31, 2021, to gather input on the proposed impacts to the park (see **Attachment C**). No additional comments were received.

4. The official with jurisdiction over the property was informed of FHWA's and/or PennDOT's intent to make a *de minimis*/no adverse use finding. YES

Identify the method used to notify the official with jurisdiction, and attach appropriate correspondence.

Email

Correspondence documenting notification of the official with jurisdiction is included in the following Attachment: **Attachment C**



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
Wildlife and/or Waterfowl Refuges,
State Forest Land, and State Game Land
May 2014 Version

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

5. The official with jurisdiction over the property concurred in writing with FHWA's and/or PennDOT's determination that the project will not adversely affect the property. (NOTE: Public input must be received and considered prior to the official with jurisdiction making a final determination.) YES

Identify the official with jurisdiction and date of concurrence and attach written concurrence:

Dana Rooney on 9/8/2021

Written concurrence from the official with jurisdiction is included in the following Attachment:
Attachment C

6. If the Section 4(f)/Section 2002 use involves State Game Land, verify that the use is considered *de minimis* in accordance with the *Cooperative Interagency Agreement for Interdepartmental Land Transfer of State Game Lands*. (Describe and obtain PA Game Commission concurrence signature below for use of a State Game Land Bank and/or Interdepartmental Land Transfer.) YES

State Game Land Bank

Debiting (acres)

From SGL bank

PGC Signature:

Date:

Interdepartmental Land Transfer

PGC Signature:

Date:

7. Have Federal or State funds [LWCF 6(f)/Project 70/Project 500/other recreation grants] been used in the acquisition of, or for any improvements to, the Section 4(f) property? YES
 NO

If Yes, the appropriate Federal agency has been coordinated with and is in agreement with the land conversion or transfer. YES

Provide more information regarding the Section 6(f)/Project 70/Project 500/other recreation grants coordination:

The Pennsylvania Department of Conservation and Natural Resources (DCNR) has coordinated with the National Park Service (NPS) regarding two potential replacement land



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
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State Forest Land, and State Game Land
May 2014 Version

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

options located on either side of the park and is awaiting its approval. These options have been approved by Brookville Borough. Staff representing Brookville Borough have indicated that the current trail system within Walter Dick Memorial Park could be expanded on the replacement land since it is directly adjacent to the park. The replacement land will be selected and appraised during final design and the LWCF Compliance and Stewardship (C&S) Form, including the Conversion Summary & Appraisal/Waiver Valuation Certification, will be submitted to the NPS during that time.

8. The project does not involve any uses that would require an individual Section 4(f) evaluation. (It is acceptable if there are other Section 4(f) uses that are covered by one of the nationwide programmatic Section 4(f) evaluations or meet temporary occupancy criteria.) YES

If there are other Section 4(f) properties used, list them here, briefly describe the use, and identify which form(s) will be completed to address the use:

N/A

In accordance with PA Act 120 Section 2002 requirements, briefly summarize the impacts to other Section 2002 areas of concern that would occur if the use of the public park, recreation area, or wildlife or waterfowl refuge was avoided. Other Section 2002 areas of concern to be discussed could include the following:

(1) residential and neighborhood character and location, (2) conservation including air, erosion, sedimentation, wildlife and general ecology of area, (3) noise, and air and water pollution, (4) multiple use of space, (5) replacement housing, (6) displacement of families and business, (7) aesthetics, (8) public health and safety, (9) fast, safe and efficient transportation, (10) civil defenses, (11) economic activity, (12) employment, (13) fire protection, (14) public utilities, (15) religious institutions, (16) conduct and financing of government including the effect on the local tax base and social service costs, (17) property values, (18) education, including the disruption of school district operations, (19) engineering, right-of-way and construction costs of the project and related facilities, (20) maintenance and operating costs of the project and related facilities, and (21) operation and use of existing transportation routes and programs during construction and after completion.

Since Walter Dick Memorial Park is located between the SR 0080 WB and EB bridges, any alternative to totally avoid the park would have to be located to the north or south of the park boundary. An alternative to the north of the WB bridge would result in impacts to the Brookville High School District facilities and would severely disrupt school district operations. In addition, this alternative would cause the potential displacement of the Brookville Cemetery. Several grave sites located within the cemetery would likely have to



On Behalf of the Federal Highway
Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
Public Parks, Recreation Areas,
Wildlife and/or Waterfowl Refuges,
State Forest Land, and State Game Land
May 2014 Version

County:	Jefferson	State Route:	0080	Section:	550
Project Name:	I-80 North Fork Bridges Project	FPN:	TBD	MPMS:	106275

be re-interred in another location. An alternative to the south of the EB bridge would result in several displacements, affect property values, and result in the loss of community cohesion of the residential neighborhood to the south of the bridge. It would also have greater potential for noise impacts than the preferred alternative since it would be moving the bridge closer to area residences.

Include any additional information related to the park impact that is relevant to the determination of *de minimis*/no adverse use:

N/A

SUMMARY AND DETERMINATION:

The project involves a *de minimis*/no adverse use on the Section 4(f)/Section 2002 property as evidenced through the minimization of harm to a public park, recreation land, or wildlife and waterfowl refuge as a result of mitigation to or avoidance of impacts to the qualifying characteristics and/or the functions of the resource. Based on the scope of the undertaking; the fact that the undertaking does not adversely affect the function/qualities of the Section 4(f)/Section 2002 property on a permanent or temporary basis; and with agreement from the official with jurisdiction, the proposed action constitutes a *de minimis*/no adverse use; and therefore, no analysis of avoidance alternatives is required.

Name and Organization of Preparer: Amy Pinizzotto, Michael Baker International Date: 9/9/2021

Project Manager: Jason E. Layman  Digitally signed by Jason E. Layman
Date: 2021.09.10 08:15:59-04'00' Date:

Environmental Manager: Jessica Schrecengost 2021.09.10 08:24:51 -04'00' Date:

PennDOT, BOPD: Raymond DeArmitt  Digitally signed by Raymond DeArmitt
Date: 2021.09.13 14:44:33 -04'00' Date:

FHWA: JONATHAN P CRUM  Digitally signed by JONATHAN P CRUM
Date: 2021.09.14 07:10:59 -04'00' Date:



On Behalf of the Federal Highway
Administration—Pennsylvania Division Office

Determination of Section 4(f) De Minimis Use
Section 2002 No Adverse Use
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List Section 4(f) mitigation measures associated with this use that are part of this project:

Coordination with Brookville Borough regarding the proposed TCEs will continue throughout construction.

Impacts to mature trees will be minimized as much practicable. Any other vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.

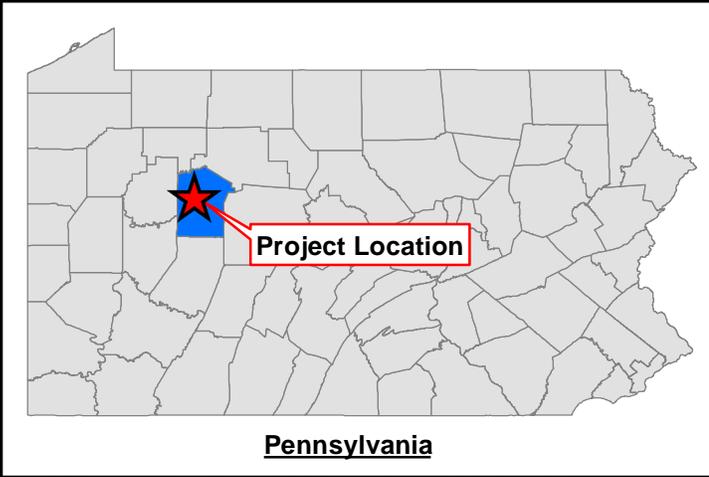
The portion of the nature trail that is to be impacted will be relocated.

Replacement property for the permanent and temporary impacts to park property will be acquired in accordance with LWCF requirements.

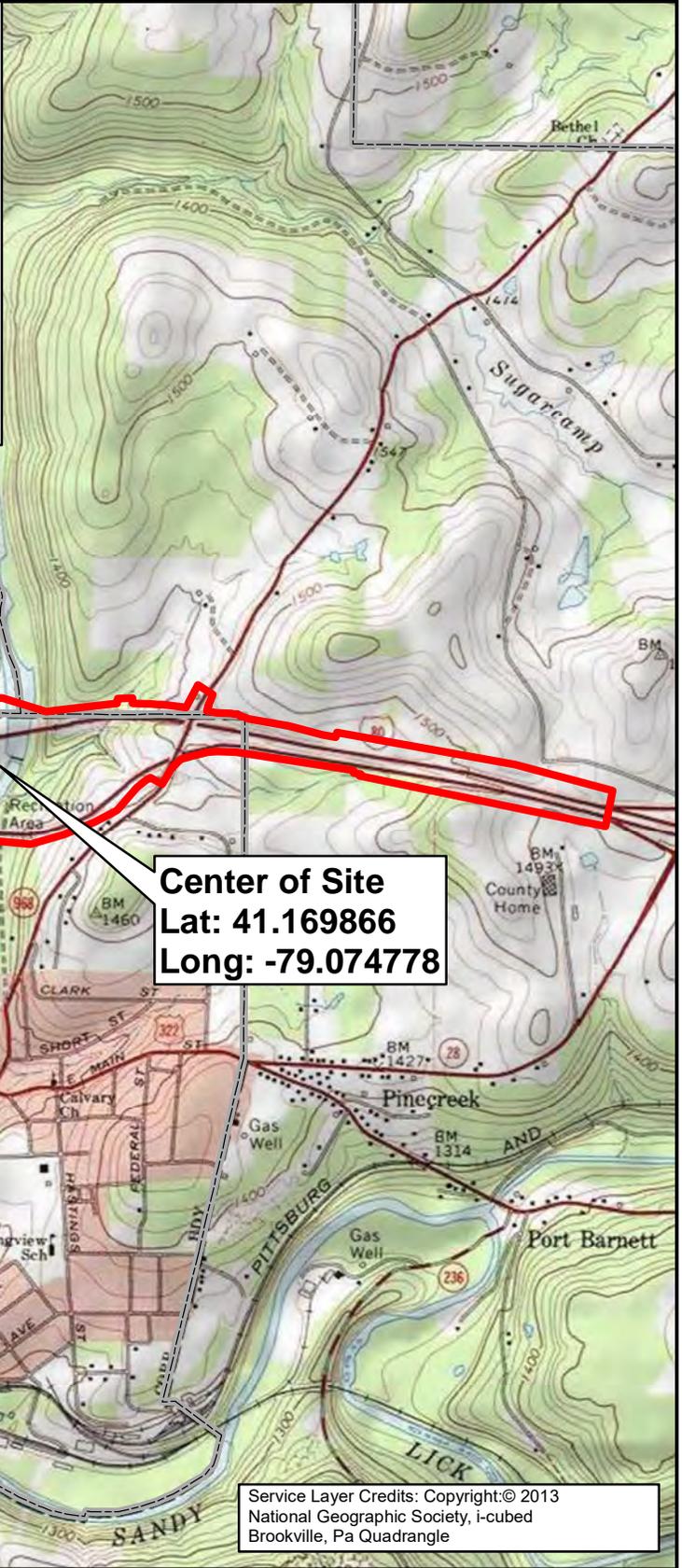
A Notice-to-Contractor will be developed during final design to inform the contractor of restrictions and related mitigation.

Typical attachments for this form include, but are not limited to:

- Project location map
- Map of affected Section 4(f) property and other Section 4(f) property(ies) in the project vicinity
- Photographs of the Section 4(f) property
- Project plan sheet to show impacts
- Correspondence with the official with jurisdiction
- Public involvement information



Pennsylvania



Center of Site
 Lat: 41.169866
 Long: -79.074778

Service Layer Credits: Copyright:© 2013
 National Geographic Society, i-cubed
 Brookville, Pa Quadrangle

Legend

 Project Area

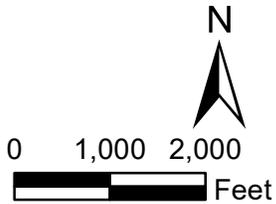


Figure 1
Project Location Map
I-80 North Fork Bridges Replacement Project
SR 0080 Section 550
 Brookville Borough and Pine Creek Township
 Jefferson County, Pennsylvania

RESOURCE	TEMPORARY IMPACTS (TEMPORARY CONSTRUCTION EASEMENTS)	PERMANENT IMPACTS (REQUIRED RIGHT-OF-WAY FOR TOWNSHIP ROAD AND AERIAL EASEMENTS)
WALTER DICK MEMORIAL PARK	5.489 ACRES	1.157 ACRES

 Walter Dick Memorial Park	 Temporary Access Roads
 Limited Access Right-Of-Way for I-80	 Temporary Impacts
 Proposed I-80 Bridges	 Permanent Impacts
 North Fork Redbank Creek	

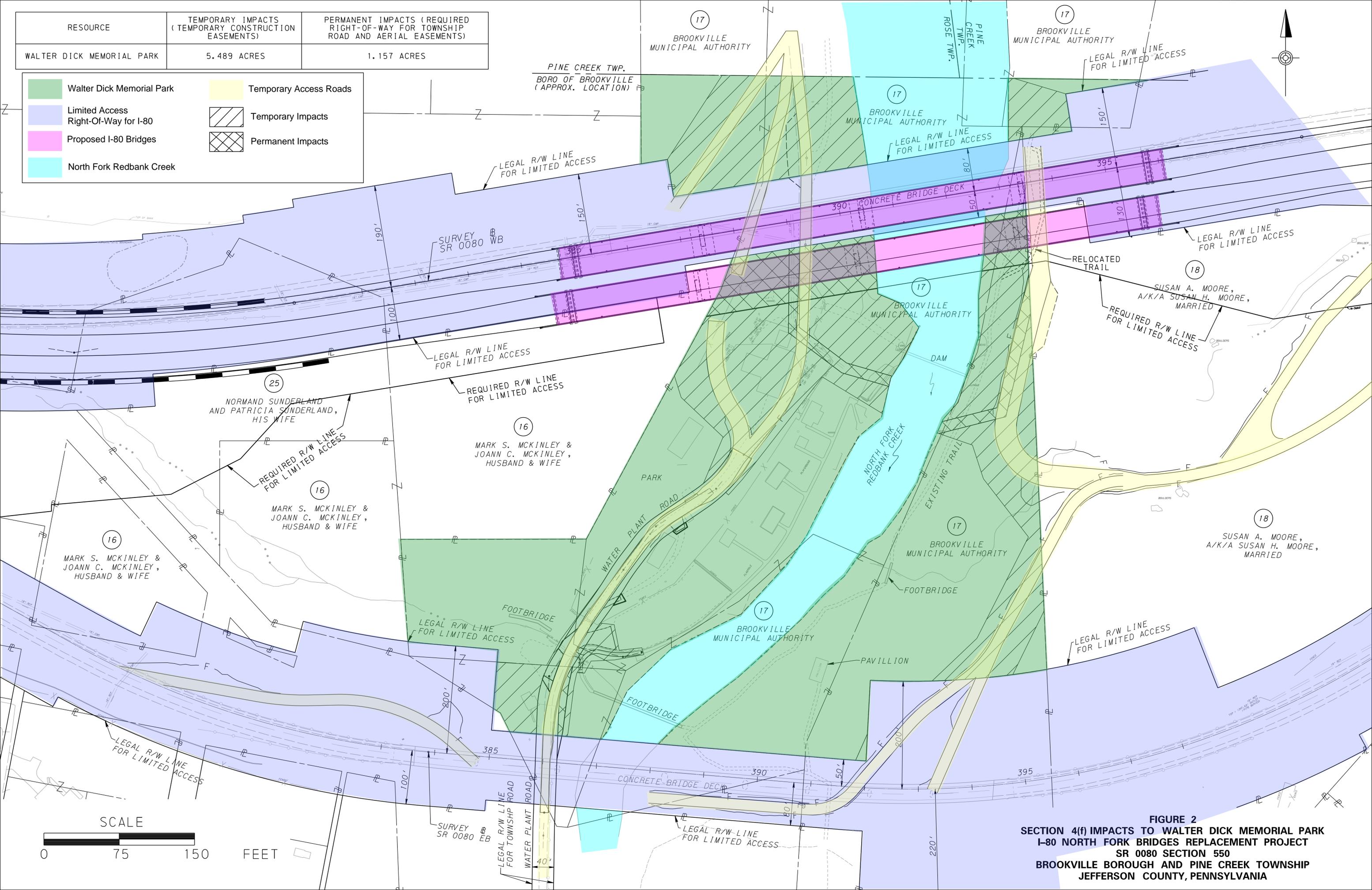


FIGURE 2
SECTION 4(f) IMPACTS TO WALTER DICK MEMORIAL PARK
I-80 NORTH FORK BRIDGES REPLACEMENT PROJECT
SR 0080 SECTION 550
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP
JEFFERSON COUNTY, PENNSYLVANIA

*I-80 NORTH FORK BRIDGES PROJECT
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP
JEFFERSON COUNTY, PENNSYLVANIA*



Photograph 1 – Overview of buildings and swimming area in Walter Dick Memorial Park

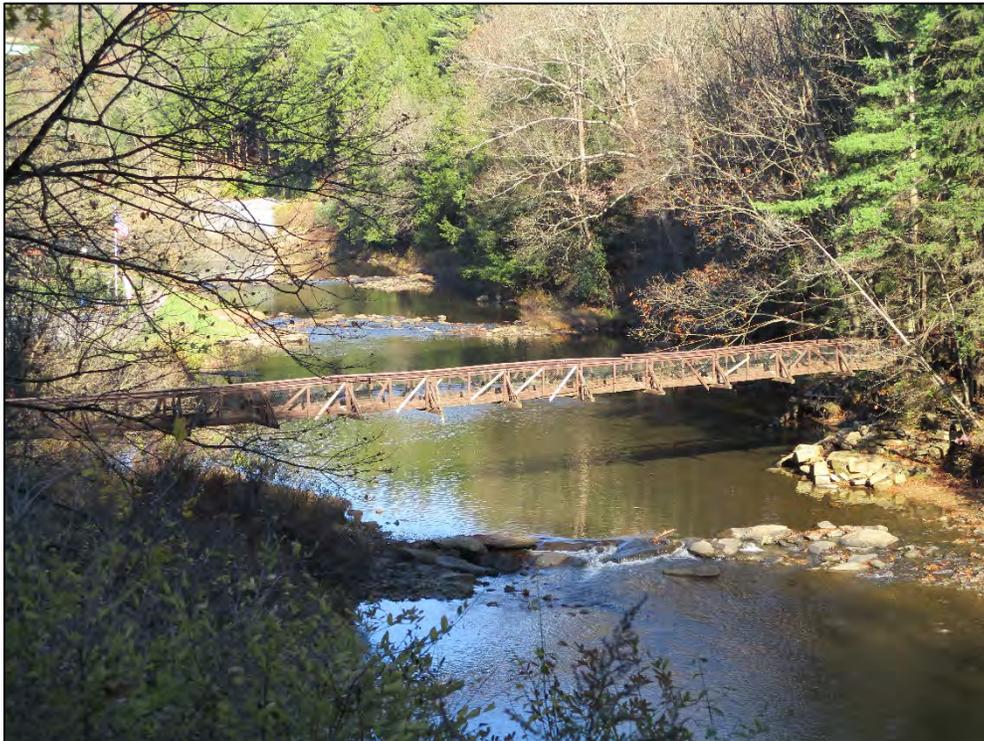


Photograph 2 – Pavilion and playground area within Walter Dick Memorial Park

*I-80 NORTH FORK BRIDGES PROJECT
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP
JEFFERSON COUNTY, PENNSYLVANIA*



Photograph 3 – Nature trail within Walter Dick Memorial Park



Photograph 4 – Footbridge crossing North Fork Redbank Creek

Public Involvement/Coordination

Comment Number	Comment Date	Comment Origir	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Associator	Online Meeting Submisio	Status
468639200001738003	03/01/2021	web comment	I am opposed to the addition of a toll on I-80, it places a significant burden on my household. As a single parent I calculated spending \$2400 on gasoline costs alone last year.	negative	Opposed to Tolls; Financial Concerns		Michele	McDonald				I-80 North Fork		closed
468639200001770816	03/02/2021	email	Jason: I received a letter inviting me to view a virtual Public Meeting and Plans Display for Route 0080 North Fork Bridges replacement project. I received this letter as the director of the Brookville Area Chamber of Commerce. I left the Chamber of Commerce in 2014 and at that time the office moved to another location on Main Street in Brookville. I do not know the current director's name nor the street number but you may want to send letter out to them. These are the sorts of issues I enjoyed working on when I was there. I do miss working with county a regional people.	neutral	Information Request	Hello, Ms. Darrin: Thank you for alerting us to the error in terms of your status with the Brookville Chamber of Commerce and its new address. We will update our records. Again, thank you and we appreciate your continued interest in this project.	Melanie	Darrin		melaniekdarrin@gmail.com		I-80 North Fork		closed
468639200001770853	03/02/2021	email	Almost \$.51 a gallon gas tax (one of the highest in the United States) 12 million vehicle registrations, 10.5 million drivers license and ID's WHERE IS ALL THAT MONEY GOING? YOUR DEPARTMENT AND THE GOVENOR WANT MORE AND MORE. WHEN ARE YOU PEOPLE GOING TO STOP? You have over 11,375 employees, of that 4,175 are administration. I don't think this makes any common sense. You need to start cutting the pork in your administrative departments. You have too many executives and I would assume you could eliminate 25% of your useless workforce to start balancing your budget instead of asking more money from the already over taxed people of the state of Pennsylvania. Private companies and industry cut their workforce to function within their budgets. Why can't the state? Penn Dot wants to use taxpayers money for smart, environmentally sustainable transportation which is largely in cities, but you forget about rural area. We need roads repaired and resurfaced but that never seems to happen in rural areas as much as in big cities. Any time of the year I love you watch your employees wasting their time and taxpayers money surveying roads, bridges, and anything they can survey. HOW MUCH DO ROADS, BRIDGES AND ANYTHING MADE OF CONCRETE AND CEMENT MOVE? What will this toll do to small rural towns, and companies? Some of these areas would shrink up and die. People could not pay tolls everyday to go to work, pay their bills and buy extra items in these small towns. There is only a small amount of extra money in their budgets. Why can Chuck Schumer get enough money to fund a bridge to Canada and you can't get any extra money for the people of Pennsylvania. Ask Senators Casey and Toomey to get extra money it's about time both of them stood up for the Pennsylvania taxpayers not just for themselves.	negative	General Dissatisfaction; Other Suggested Means of Raising Money		Deborah	Cenni		dcenni@comcast.net	814-265-0182	I-80 North Fork		closed
468639200001782064	03/02/2021	web comment	No More taxes! Learn to prioritize	neutral	Against Tax Increases		Paula	Herman		pherman@clarion.edu	814-226-6766	I-80 North Fork		closed
468639200001787014	03/02/2021	hotline	Hello, this is Steven Zeak, I'm a resident of Lucerne County. I am Sandra, Lee opposed, to any towing of inter-state bridges on either I 81 or I a D I feel that the tax structure in Pennsylvania is burdensome, begin with high gasoline taxes and higher tolls on other road 10 dot administers. Please do what you can too. torpedo this idea of tolling inter-state bridges on 81 and I 80. I do not support it. Thank you. Bye."	negative	Financial; General Dissatisfaction; Opposed to Tolls	General Response	Steven	Zeak		stevejfrompa007@gmail.com	(570) 814-1571	I-80 Canoe Creek; I-80 Nescopeck; I-80 North Fork; I-80 White Haven; I-81 Susquehanna		closed
468639200001792003	03/02/2021	public meeting	When will information be released about the acquisition of real estate for right-of-way purposes for the 80 North Fork bridge replacements? How and when will property owners affected by this acquisition be contacted?	neutral	Concerns about displacement		Norman	Sunderland		norm@sunderlandforestry.com		I-80 North Fork	North Fork	closed
468639200001794003	03/02/2021	public meeting	When will information be released about the acquisition of real estate for right-of-way purposes for the 80 North Fork bridge replacements? How and when will property owners affected by the acquisition be notified?	neutral	Concerns about displacement		Norman	Sunderland		norm@sunderlandforestry.com		I-80 North Fork	North Fork	closed
468639200001807005	03/02/2021	public meeting	east bound bridge replacement long over due. I can see the eastbound bridge from my house on 1 Penn Ave Since I built this house in 1987 I have seen numerous accidents especially in the dip and am surprised there have not been more deaths. Please try to limit the noise barriers since the current one eastbound was totally unnecessary use the money more effectively.	neutral	Concerns about Bridge Safety; Concerns about noise		Tom	Maloney		tmaloney5002@gmail.com	(814) 849-1941	I-80 North Fork	North Fork	closed
468639200001832568	03/02/2021	email	Hello Jason, I am reviewing the project information on the PennDot website and wanted to reach out. I have a concern about the right-of-way line through the back yard of my property located at 68 Jenks Street Brookville. This property has a septic tank that I am planning to mark with flags but wanted you to be aware of this. I would prefer to not have an issue with the system being compromised due to heavy equipment driving/sitting in that area. If you would like to discuss further please do not hesitate to call or email me.	neutral	Property takes or business displacement; Construction Concerns	Custom? Add a Code: "Design Team Alert" This comment should be sent to the attention of the PennDOT PM to inform the design team. Suggest it be flagged or sent separately from the total dump of comments. Could also provide a "custom" response to that effect.	Molly	McNutt		molly.mcnutt@yahoo.com	814-715-1891	I-80 North Fork		Escalated
468639200001885014	03/03/2021	email	Good Afternoon. I received a letter regarding the virtual public meeting and plans display for the I-80 North Fork Bridge Project. I went to the webpage to access the link and it indicated that I was not allowed access. I'm not sure if the link is down or something is being changed, but I could not access it.	neutral	Technical Issues	Hello Ms. Zents, Thank you for bringing the accessibility issue on the I-80 North Fork online meeting to our attention. Our web team has identified and resolved the issue, so you should now be able to access the online meeting, which can be found at the link. Please let us know if you are still unable to view the meeting. Sincerely, I-80 North Fork Bridges Project team	Tracy	William Zents		tzents@jeffersoncountypa.com	814-849-3185	I-80 North Fork		closed
468639200001888054	03/04/2021	hotline	Hello. This is Nancy Duffalo. My address is Post Office Box 1082. That's Post Office Box 1082, Dubois, Pennsylvania, 15801. My comment is that we should definitely have a toll. I have been driving back and forth to New York, where I work during summer, spring, and fall for 13 years, up and back, so that makes 26 trips. I see where a lot of the trucks do not leave or stop anywhere. And at 101, I have had the same trucks go by me and when I get to go to Scranton, it is still the same trucks, so you know they didn't stop to leave even money for a cup of coffee. I think we should even have the entire I-80 in Pennsylvania tolled. I am all for it. And also, when they get on, if they are going up towards Binghamton on I-81, when I cross over the lines, I still see some of the same trucks and I've been keeping an eye on this for 13 years now. I am definitely for tolling. Thank you very much. My name is Nancy Duffalo. Post Office Box 1082, Dubois, Pennsylvania 15801. Thank you very much. Bye	positive	Supportive of Tolls; Supportive of User Fees		Nancy	Duffalo			814-771-0741	I-80 North Fork		closed
468639200001961075	03/04/2021	email	Good morning! After reviewing the information regarding the North Fork Bridge project (including the bridges over Jenks Street and the Richardsville Road bridge) and talking to another member of our office, I have one main question. When the construction on the Jenks Street overpass bridge is taking place, will Jenks Street be closed to all traffic and detoured or will there still be one lane of traffic open with traffic lights controlling traffic? The other member of our office staff had been told that there would be traffic lights being utilized during this time. I did not see that indicated anywhere in the release, so I just wanted to clarify what the plan was for that area. Thanks so much for your time. Have a good day! Andrea	neutral	Construction Traffic Detours; Construction Concerns	Custom response needed	Andrea	Blair		ablair@basd.us	814-849-1106	I-80 North Fork		response drafted

Comment Number	Comment Date	Comment Origin	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Association	Online Meeting Submission	Status			
468639200001962004	03/05/2021	web comment	Tolling North Fork bridges will increase traffic on secondary roads. This will increase maintenance costs on these roads. There will also be serious safety concerns with more large trucks navigating the hills in and out of Brookville.	negative	Opposed to Tolls; Traffic Detours		Jerry	Chamberlin		jekkk_626@windstream.net	(814) 715-1182	I-80 North Fork		closed			
468639200001969066	03/06/2021	public meeting	I have only suggestions/comments regarding everything I read in this presentation. First would be that if PennDOT decides to toll the northfork bridges its only going to create HEAVY unnecessary truck traffic through downtown Brookville because truckers and even locals will anything to save a dime and go around to avoid the toll. Its common sense. it will take travelers an extra 5-10 minutes but it will save them a lot of money in the long run. Therefore tolling the northfork bridges seems like a giant waste of time in my personal opinion. Second, I live on Northview Drive with the current eastbound traffic in plain view from my backyard. I also have an inground pool in my backyard which I use a lot in the summer months and the noise from the eastbound traffic can be quite annoying at times. My point is for you to please consider noise barrier walls along the eastbound side near Northview Drive and Pennsylvania Ave if you are already also considering them near the schools. Thank You. -Tyler McKinney	neutral	Concerns about Congestion; Toll Avoidance		Tyler	McKinney		tylermckinney259@gmail.com	(814) 715-1188	I-80 North Fork	North Fork	closed			
468639200001993012	03/05/2021	public meeting	Bridge tolls would only result in a major traffic problem in Brookville, PA. Here's an inexpensive solution: Lower the speed limit in the area of the bridges and enforce it. The bridge location was poorly done 60 years ago. Why wasn't it done as per the present replacement proposal?	negative	Concerns about Congestion; Opposed to Tolls; Other Suggested Means of Raising Money		Eric	Armstrib		earmst@windstream.net		I-80 North Fork	North Fork	closed			
468639200002006003	03/05/2021	public meeting	Both my wife and I use these bridges to get to and from work. My wife traveling daily to Dubois, myself traveling to Brockway. If tolled we would literally be paying tolls to cross four times a day. To take alternate routes, we would have to use roads not typically maintained well during snow events, storms especially in the morning hours when it's necessary to be traveling to work. The economy in this area has changed so much in the last few years forcing us both into jobs with less yearly income, and driving further distances to work, with less pay. Please consider another option for those people who live, work and pay taxes in this community so as to not have to pay these tolls in addition. This, if enacted would be a financial hardship to many local people, especially if they rely on this route for daily work. The bulk of traffic seems to be out of state truck commerce, and this traffic has the most damaging effect on the road surface and structures, it seems more fitting to tax or toll this commerce to fund the bridge rather than a toll on local employees getting to and from work each day.	neutral	General Support; Mitigation/Toll Discount; Other Suggested Means of Raising Money; Pandemic/Financial Concerns		Victor	Zents		zentsfam@yahoo.com	(814) 849-0181	I-80 North Fork	North Fork	closed			
468639200002015003	03/06/2021	web comment	With the amount of fuel taxes, license fees, and vehicle registration fees that we Pennsylvanians pay, our streets and highways should be paved in gold and platinum. These bridge tolls are nothing more than robbery. Try sending some of that fuel tax money back to western Pennsylvania instead of pouring it into Philly and Harrisburg. All these tolls are going to do is divert traffic off of I-80 and onto route 322 thru the boro of Brookville to bypass those tolled bridges. This heavy traffic is going to strangle Brookville. When there is a wreck on I-80 and they have to close 80 and divert traffic through Brookville it completely shuts Brookville down. The residents can't get anywhere. Find another way to fund your project. God knows we taxpayers have had about enough of the tax and spend idiots in Harrisburg.	negative	Against Tax Increases; Concerns about Congestion; Opposed to Tolls; Political Dissatisfaction; Route Diversion/Alternate Route; Toll Avoidance		TBD	TBD				I-80 North Fork		closed			
468639200002146003	03/12/2021	web comment	NO TO TOLLS ON I-80	negative	Opposed to Tolls	General Response	Emily	McClintic		antielemonhead@gmail.com	(814) 229-1590	I-80 North Fork		closed			
468639200002152031	03/12/2021	email	Please call me. I have a question about the "detour" anticipated for Jenks Street during the North Fork Bridges Project. I really need an answer before discussing the issue with our school board. Please call me on my cell at (814) 404-8292. Thanks,	neutral	Construction Traffic Detours; Information Request	Jason Layman (the PennDOT PM on the project) called Mr. May this morning and spoke to him about construction detours and their impact on the school district. Below is the summary he provided us. You can log this as the comment response and close the comment. I discussed the Jenks Street detour with Mr. May this morning. He understands the need for the detour and would like to ensure we keep in communication with the school district in regards to when the detour is anticipated. As typical, he is concerned with emergency vehicles accessing the school if there is an issue since the posted detour is 3.1 miles and will add time to get there. Another concern is there is quite a bit of pedestrian use from students who walk to school, and what these students will do now since it will be closed. A temporary bus stop will be added south of the I-80 bridges over Jenks Street to transport students in the area to school during construction. We anticipate that this detour will last for one construction season, or 7 months from April to October.	neutral	Construction Traffic Detours; Information Request		Erich	May		emay@basd.us	(814) 849-1100	I-80 North Fork		closed
468639200002180147	03/13/2021	hotline	My name is Fred Foster. I live in Clarion, Pennsylvania and I travel Interstate 80 daily. This is most craziest, hair-brained idea I have ever heard of in my life, tolling Interstate 80. They've tried it before. And remember, we have elections still United States and in Pennsylvania, although they don't mean a whole lot, but we still have elections. These fools need to understand that this is completely wrong! They will just absolutely trash Brookville Borough! People won't use these bridges and they'll go through town. What a safety hazard that is? Good grief! My address is 4 6 8 Brook Road, Clarion, Pennsylvania, 16, 2, 1, 4. Thank you.	negative	General Dissatisfaction; Opposed to Tolls; Toll Avoidance		Fred	Foster			(814) 745-2403	I-80 North Fork		closed			
468639200002295507	03/17/2021	outside source	GoDubois Comment: (1/26/2021) What concerns do you have about the project? none What would you like the project team to consider as part of this project? looks good Additional Comments: well needed	positive	General Support		Dave	Roman		daveroman@verizon.net	814-603-3278	I-80 North Fork		closed			
468639200002295541	03/17/2021	outside source	GoDubois Comment: (1/27/2021) What concerns do you have about the project? I hope that the aesthetics will be considered when designing the project. For example, I hope that a minimum of trees will be removed and the effect on the park and the view of the valley will be minimally impacted. What would you like the project team to consider as part of this project? Please be sure that the existing highway, sound wall, and original fence that are no longer needed will be removed. The original chain fence was never removed when the sound wall went up and it continues to be a safety hazard with children and pets. Additional Comments: I can't emphasize removal of obsolete construction enough.	neutral	Bridge Safety; Construction; Trails/Recreation/Parks		Mark S.	McKinley		mark@mckinleycpa.com	814 591 6704	I-80 North Fork		closed			

Comment Number	Comment Date	Comment Origir	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Associator	Online Meeting Submissio	Status
468639200002295568	03/17/2021	outside source	GoDubois Comment: (1/27/2021) What concerns do you have about the project? That it gets finished. The project is much needed and long overdue. It doesn't seem to address the chain link fence, sound barrier, and eastbound lane removal. Will the project include removal and disposal of the unused east bound lane on the bridge approaches? What would you like the project team to consider as part of this project? Restoration of Eastbound lanes, preserve the park, install new sound barriers Additional Comments: Make it safer and good looking	neutral	Construction; Noise; Trails/Recreation/Parks		John	Matson	Jefferson County Commissioner	jmatson@jeffersoncountypa.com	814-849-1653	I-80 North Fork		closed
468639200002295612	03/17/2021	outside source	GoDubois Comment: (1/27/2021) What concerns do you have about the project? Please remove the sound barrier, chain-link fence, and old section of highway when the project is complete. What would you like the project team to consider as part of this project? see above Additional Comments: none	neutral	Construction		Samuel	McKinley		smckinley@mckinleycpa.com	412-780-5501	I-80 North Fork		closed
468639200002295637	03/17/2021	outside source	GoDubois Comment: (2/10/2021) What concerns do you have about the project? Traffic backlog through main street Brookville, traffic accidents What would you like the project team to consider as part of this project? The existing sound wall does do anything in reducing the noise as it is, why would you replace the one wall, and ADD another one when the already existing one does NOTHING Additional Comments: Lets be honest, enough accidents have happened on those bridges over the years, including the one last year where the truck went over the side. The bridges are in "questionable" shape, just get the funding and start the replacement of them now.	negative	Bridge Safety; Noise; Traffic Congestion		Greg	Burkett			814-648-1921	I-80 North Fork		closed
468639200002295660	03/17/2021	outside source	GoDubois Comment: (3/3/2021) What concerns do you have about the project? Several Concerns - First, in the overview of the project, it lists that the speed limit of I-80 through this area is 70mph and that the roads were not designed for this speed, especially east bound. Many years ago, I addressed this with PennDOT and PSP in order to get speeds reduced on these bridges to 55mph and was told that it could not be done. It appears in other parts of the state along the Interstate corridors, that this is in fact a false statement. What ever happens to this project, I would like it looked at to reduce the speeds in this area because it is a residential section of the county. I am in favor of the realigning of the bridge as there have been way to many accidents in the area. Again, speed is a factor coming down on the bridge. There has to be emphasis put into more signage as well. Reduction of speeds will help reduce accidents and the potential for a hazardous material spill within a residential area. The other area of concern is the tolling aspect. Understand that the bridges are in dire need of replacement, however, there should be other avenues looked at to reduce overall state spending that would allow for this project to continue. If the bridges are tolled, it is anticipated that traffic will exit the Interstate and come through the historic section of Brookville. This is not good when dealing with emergency vehicle traffic and will destroy these roads and the East Main Street Bridge as well. What would you like the project team to consider as part of this project? Reduction of speeds coming into the Brookville Area is a priority even if the project was to proceed. Additional Comments: See above	neutral	Bridge Safety; Construction; Opposed to Tolls; Toll Avoidance; Traffic Congestion		Tracy	William Zents	Jefferson County Department of Emergency Services	tzents@jeffersoncountypa.com	814-849-3185	I-80 North Fork		closed
468639200002342004	03/19/2021	public meeting	As the long time custodial proprietor of the David E Siar estate. I am concerneed about the proposed construction site for the I-80 bridge project. considering the path along the fencing, Caldwell st extension, Richard lane, Jane st and the main Jenks st are heavy travel routes for children going to school and home from the Hickory Grove elementary and Brookville high schools. Any reconsideration for this site will be greatly appreciated	neutral	Bridge Safety; Construction; Traffic Congestion	General Response	John Richard	Barrett		sho-ryd@comcast.net	(814) 849-8802	I-80 North Fork	North Fork	closed
468639200002352076	03/21/2021	public meeting	As the owner of property that will once again be impacted by I-80 with the proposed North Fork Bridge project, I write to express my concerns. My property involved stretches from Jenks Street to the existing I-80 right-of-way, and includes the house located at 58 Jenks Street. That original right-of-way for I-80 diagonally bifurcated the same parcel of land and left a useless corner of the property near Cemetery Road. The land had already been subdivided for residential development by my family. The construction of I-80 destroyed its potential for single family housing, the intended use of the subdivision. As a result, the value of the property not taken by the Commonwealth was greatly diminished. My family was not fairly compensated for that taking or the diminution of value of the remaining property. The current proposal again seems to impact the remainder of my property. However, I have yet to be notified by PennDOT of the nature of that impact and the maps provided online are not sufficiently clear to properly apprise me of that impact. I request that I be furnished an exact description of the impact on my property. In addition to the economic impact, I am advised that there is considerable foot traffic on my property between Jenks Street and the existing I-80 right-of-way. Most of the pedestrians are high school students, who travel on the subdivision roads and rights-of-way and then cross under I-80 at the Jenks Street overpass to get to and from school. The proposed construction impacts not only those children, but the many who travel by school bus on Jenks Street. The current plans seem inadequate to ensure that students are protected during all phases of the construction. Lastly, the possibility of making North Fork Bridge a toll bridge raises many issues, not least of which involves the potential location of toll booths and the necessary widening of the I-80 right-of-way for those toll booths. The current plans do not address the location of those toll booths or the impact to adjacent property. PennDOT cannot assume (as it seems to do now) that all vehicles will participate in the E-ZPass toll program, particularly since many vehicles that travel I-80 are not registered in Pennsylvania. That will necessitate several manned or electronic toll lanes in each direction. Somewhere at each end of the North Fork Bridge, a significant amount of additional property might need to be acquired if the bridge becomes a toll bridge. I have strong objections to making any portion of any freeway a toll road. It is understandable that tolls might need to be collected to reimburse government's costs of construction of certain highways or highway structures, but only until those costs have been recovered. However, there is something inherently wrong about allowing private enterprise to profit forever from the operation of a toll bridge. That is particularly true since the only other major East-West artery through the Commonwealth is the Pennsylvania Turnpike, which has been a toll road since its construction. Thank you for your consideration of my concerns. David E. Siar 1420 58th Avenue North	negative	General Dissatisfaction; Information Request; Opposed to Tolls; Property takes or business displacement	Custom - Non-Urgent	David	Siar		dsiar@hotmail.com	727-522-4922	I-80 North Fork	North Fork	response drafted
468639200002395281	03/21/2021	email	Dear Mr. Layman, I am an owner of a parcel of land that, according to you online map, will be impacted by the North Fork Bridge Project. My property involved stretches from Jenks Street to the existing I-80 right-of-way, and includes the house located at 58 Jenks Street. That original right-of-way for I-80 diagonally bifurcated the same parcel of land and left a useless corner of the property near Cemetery Road. Because neither the map included with your 2/26/2021 correspondence nor the online map on the PennDOT website are sufficiently detailed or clear, it is impossible for me to determine the nature of the impact on my property. Obviously, I would like to better understand that impact. Although my comment in your online Public Meeting requests additional information about the impact to my property, I thought it best to contact you directly with my request. Please provide me an exact written description of the impact on my property as a result of the proposed project, along with any more detailed maps regarding my property that are available to you, it is convenient for you to do so, providing the description and any maps by email would be satisfactory at this time. Thank you for your assistance with this matter.	neutral	Information Request; Property takes or business displacement	Custom - Non-Urgent	David	Siar		dsiar@hotmail.com	727-522-4922	I-80 North Fork		response drafted

NOTICE

The Pennsylvania Department of Transportation (PennDOT), in cooperation with the Federal Highway Administration (FHWA), is advancing the replacement of the two bridges carrying SR 0080, Section 550 Westbound (WB) and Eastbound (EB) over North Fork Redbank Creek and Water Plant Road (see Figure 1). The proposed project will consist of an off-alignment replacement of two bridges that carry the WB and EB lanes of SR 0080 over North Fork Redbank Creek and Water Plant Road, realigning the WB and EB lanes of SR 0080, an on-alignment replacement of the SR 0080 EB and WB bridges over SR 4003 (Jenks Street), and an on-alignment replacement of the SR 4005 (Richardsville Road) bridge over SR 0080 WB and EB.



Figure 1 – Aerial view of the proposed I-80 North Fork Bridges replacement project (looking east). Walter Dick Memorial Park is on the right.

Walter Dick Memorial Park, a Section 4(f) resource, will be impacted due to the permanent right-of-way needed for the new pier locations. No active recreation areas will be permanently impacted; however, a portion of a nature trail will be relocated. Additionally, the project will require temporary roads and easements for access during construction (see Figure 2).

PennDOT is providing the public the opportunity to review and provide comment on the effect the I-80 North Fork Bridges replacement project will have on the activities, features, and attributes of Walter Dick Memorial Park. PennDOT intends to make a *de minimis* use, or no adverse impact, finding for the proposed project.

Any questions or comments should be sent by **August 31, 2021** to:

Jason E. Layman, Project Manager
PA Department of Transportation, District 10-0
2550 Oakland Avenue
Indiana, PA 15701-3388
Phone: 724-357-7614
Email: jlayman@pa.gov

RESOURCE	TEMPORARY IMPACTS (TEMPORARY CONSTRUCTION EASEMENTS)	PERMANENT IMPACTS (REQUIRED RIGHT-OF-WAY FOR TOWNSHIP ROAD AND AERIAL EASEMENTS)
WALTER DICK MEMORIAL PARK	5,489 ACRES	1,157 ACRES

 Walter Dick Memorial Park	 Temporary Access Roads
 Limited Access Right-Of-Way for I-80	 Temporary Impacts
 Proposed I-80 Bridges	 Permanent Impacts
 North Fork Redbank Creek	

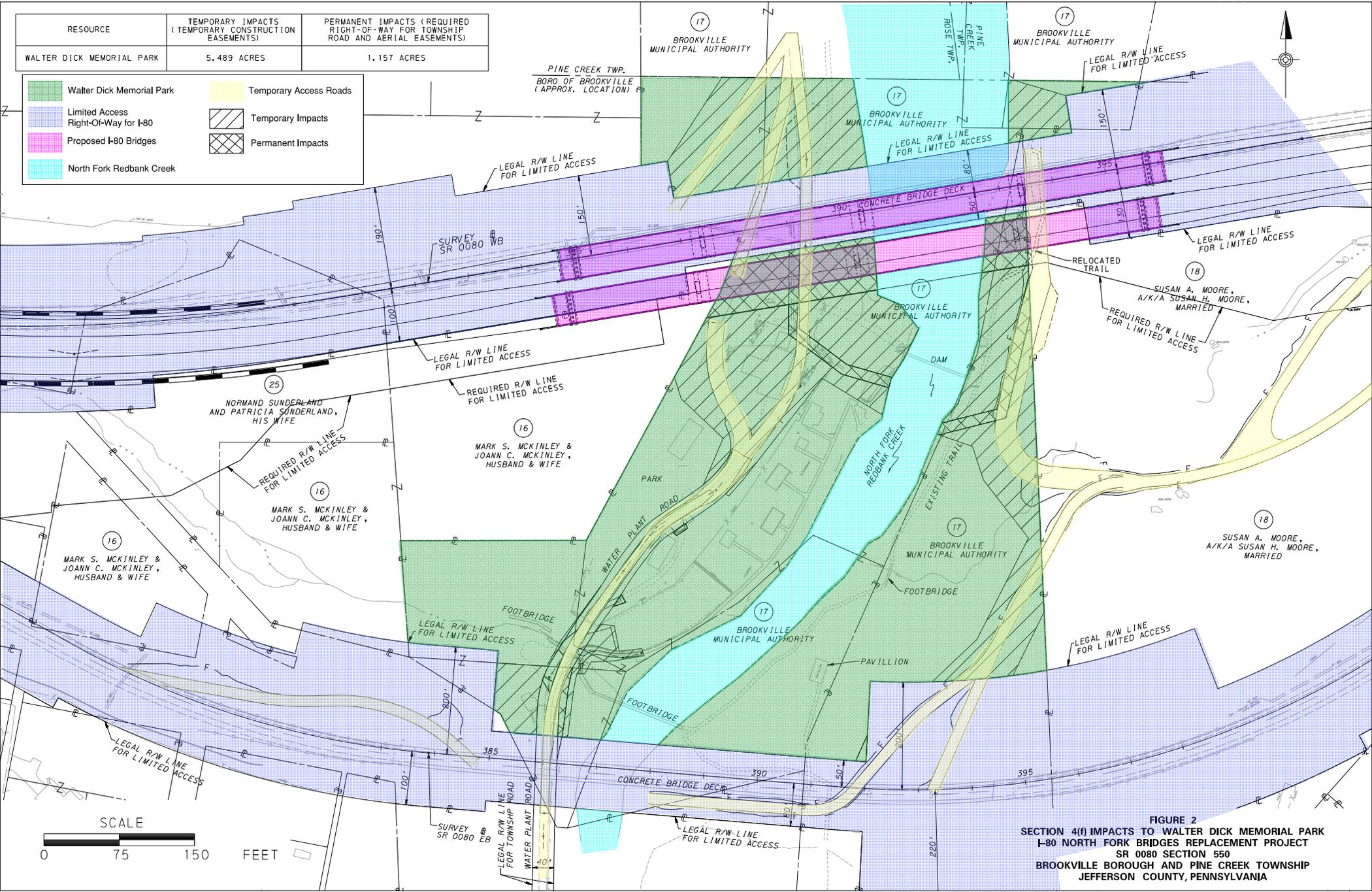


FIGURE 2
SECTION 4(f) IMPACTS TO WALTER DICK MEMORIAL PARK
I-80 NORTH FORK BRIDGES REPLACEMENT PROJECT
SR 0080 SECTION 550
BROOKVILLE BOROUGH AND PINE CREEK TOWNSHIP
JEFFERSON COUNTY, PENNSYLVANIA

Pinizzotto, Amy

From: Dana Rooney <manager@brookvilleborough.org>
Sent: Wednesday, September 8, 2021 3:33 PM
To: Pinizzotto, Amy
Cc: Hart, Sean; Layman, Jason E; Schrecengost, Jessica
Subject: Re: EXTERNAL: Re: I-80 North Fork Bridges Project | Section 4(f) Impacts

Amy,

I concur that the I-80 North Fork Bridge Project will have no adverse impacts on Walter Dick Memorial Park.

Thank you,

Dana D. Rooney, CBO
Brookville Borough Manager
Administrative Manager - Municipal Authority
18 Western Avenue, Suite A
Brookville, PA 15825
814-849-5320
814-849-5321

On Tue, Sep 7, 2021 at 3:38 PM Pinizzotto, Amy <Amy.Pinizzotto@mbakerintl.com> wrote:

Dana,

I am reaching out to you to make a final determination that the I-80 North Fork Bridges Project will not adversely affect Walter Dick Memorial Park after consideration of all public input. Only one comment was received on the potential park impacts and it was regarding the viewshed being impacted by removing trees. Please note that impacts to mature trees will be minimized when possible and this minimization measure has been incorporated into the project.

PennDOT is requesting that you concur in writing by responding to this email that the I-80 North Fork Bridges Project will have no adverse impacts on, and will result in a de minimis use of, Walter Dick Memorial Park.

Thank you!

Amy Pinizzotto | Sr. Transportation Planner
Michael Baker International | *We Make a Difference*
100 Airside Drive, Airside Business Park | Moon Township, PA 15108
[O] 412-269-6431 | [M] (724) 787-2528
amy.pinizzotto@mbakerintl.com | www.mbakerial.com

From: Dana Rooney <manager@brookvilleborough.org>
Sent: Thursday, June 24, 2021 2:24 PM
To: Pinizzotto, Amy <Amy.Pinizzotto@mbakerintl.com>
Cc: Layman, Jason E <jlayman@pa.gov>; Schrecengost, Jessica <jeschrecen@pa.gov>; Hart, Sean <SHart@mbakerintl.com>
Subject: EXTERNAL: Re: I-80 North Fork Bridges Project | Section 4(f) Impacts

Amy,

On behalf of Brookville Borough, I am concurring that the I-80 North Fork Bridges Project will have no long-term adverse impacts on Walter Dick Memorial Park.

Thank you,

Dana D. Rooney, CBO

Brookville Borough Manager

Administrative Manager - Municipal Authority

18 Western Avenue, Suite A

Brookville, PA 15825

814-849-5320

814-849-5321

Please note a new email address, please update your address book.

On Tue, Jun 22, 2021 at 3:10 PM Pinizzotto, Amy <Amy.Pinizzotto@mbakerintl.com> wrote:

Dana,

As you are aware, the Pennsylvania Department of Transportation (PennDOT), in cooperation with the Federal Highway Administration (FHWA), is advancing the replacement of two bridges carrying SR 0080, Section 550 Westbound (WB) and Eastbound (EB) over North Fork Redbank Creek and Water Plant Road in Brookville Borough and Pine Creek Township, Jefferson County, Pennsylvania. The proposed project will consist of an off-alignment replacement of two bridges that carry the WB and EB lanes of SR 0080 over North Fork Redbank Creek and Water Plant Road, realigning the WB and EB lanes of SR 0080, an on-alignment replacement of the SR 0080 EB and WB bridges over SR 4003 (Jenks Street), and an on-alignment replacement of the SR 4005 (Richardsville Road) bridge over SR 0080 WB and EB.

Walter Dick Memorial Park, a Section 4(f) resource, will be impacted due to the permanent right-of-way needed for the new pier locations as well as for aerial easements. No active recreation areas will be permanently impacted; however, a portion of a nature trail will be impacted and relocated (see attachment). Additionally, the project will require temporary roads and easements for access during construction. Approximately 1.2 acres of park property will be permanently impacted and approximately 5.5 acres will be temporarily impacted. The permanent impacts will occur in undeveloped portions of the park, mostly on steep hillsides. As discussed previously, any permanent or temporary impacts will result in a permanent Section 6(f) conversion and replacement land will be acquired in accordance with Land and Water Conservation Fund (LWCF) requirements. In addition, the following mitigation measures will be incorporated into the project:

- Coordination with Brookville Borough regarding the proposed temporary construction easements will continue throughout construction.
- Any vegetation removed or otherwise impacted by construction activities within Walter Dick Memorial Park will be restored to pre-construction conditions.
- The portion of the nature trail that is to be impacted will be relocated.
- A Notice-to-Contractor will be developed to inform the contractor of restrictions and related mitigation.

PennDOT is requesting that you concur in writing by responding to this email that the I-80 North Fork Bridges Project will have no adverse impacts on, and will result in a de minimis use of, the Walter Dick Memorial Park.

Please let me know if you have any questions or need additional information.

Thanks in advance for your consideration.

Amy Pinizzotto | Sr. Transportation Planner

100 Airside Drive, Airside Business Park | Moon Township, PA 15108 | [O] 412-269-6431 | [M] (724) 787-2528

amy.pinizzotto@mbakerintl.com | www.mbakertnl.com     



We Make a Difference

BUREAU OF RECREATION AND CONSERVATION

September 21, 2021

Nicole L. Auker
Environmental Planning Supervisor
PA Department of Transportation
400 North Street, 7th Floor
Harrisburg, PA 17120

RE: BRIDGE REPLACEMENT
I-80 Section 550
Brookville Borough and Pine Creek Township, Jefferson County
LWCF 42-00033, Walter Dick Memorial Park

Dear Ms. Auker:

This letter is being written based on PennDOT's need to proceed with the environmental process for the above referenced bridge replacement project. The Federal Highway Administration requires acknowledgement of coordination between agencies to allow the project to continue through the environmental process, to begin the Right-Of-Way process, engage with the landowner for potential acquisition, and complete the National Park Service (NPS) Compliance and Stewardship Form (C&S Form).

The bridge replacement will require approximately 6.65 acres of impact to the Land and Water Conservation Fund protected Walter Dick Memorial Park in Brookville Borough. This includes both permanent ROW of approximately 1.16 acres and temporary construction easement (lasting longer than 6 months) of approximately 5.49 acres.

Potential replacement property has been identified adjacent to Walter Dick Memorial Park. The proposed acreage is approximately 6.69 acres to the east of the Park. Brookville Borough, DCNR and PennDOT agree on the proposed replacement property to satisfy the value, location and recreational usefulness criteria under the Land and Water Conservation Fund Act.

The NPS C&S Form requests concurrence from the NPS on the conversion footprint and the replacement property. DCNR requested NPS concurrence on the proposed conversion and replacement property on the following dates: February 9, 2021, March 8, 2021, June 2, 2021, June 17, 2021, July 6, 2021, and July 19, 2021. To date, NPS has not responded to our inquiries.

DCNR recognizes PennDOT's need to proceed through the project planning and design phases and agrees with the proposed conversion footprint and replacement property while we await NPS concurrence. Please understand NPS has ultimate approval authority of the conversion package including replacement property.

If you have questions or concerns, please contact me at 717-783-4735 or Ashley Rebert at (717) 772-3322.

Sincerely,

Alex L. Tatanish

Alex L. Tatanish
ASLO, LWCF, Program Coordinator
Community Parks & Conservation Division

Krommes, Kathy

From: Krommes, Kathy
Sent: Monday, March 28, 2022 3:06 PM
To: Krommes, Kathy
Subject: FW: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

From: Morrison, Mary (Missy) <Mary_Morrison@nps.gov>
Sent: Monday, November 22, 2021 3:26 PM
To: Rebert, Ashley <arebert@pa.gov>
Cc: Imgrund, Lauren <limgrund@pa.gov>
Subject: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA_SPAM@pa.gov.

Good Afternoon Ashley-

The NPS, LWCF program has reviewed the proposed conversion footprint and replacement property for the SR0080 Section 550 conversion at LWCF 42-00033 Walter Dick Memorial Park and the I-80 White Haven Bridge, conversion replacement property materials (Impacts Lehigh Gorge State Park, Luzerne County).

Walter Dick Mem Park

The NPS, LWCF can concur that the proposed replacement property adjacent to Walter Dick Memorial Park satisfies the LWCF Act and Post Completion Compliance Regulations (36 CFR 59.3) for appropriate replacement property.

However, please confirm the final conversion footprint acreage: I had to go back and check through Jack's emails and a February email with attachment from you indicates the conversion footprint impact to this park is 7.6 acres; within the PennDot letter of August 31st 2021, PennDOT indicates a 5.5 acre footprint and approximately 6 acres replacement property.

An email from you with the updated/explanation of what the actual conversion footprint is meant to be will suffice. Also keep in mind that when the conversion package is submitted we will need maps indicating the correct acreage.

Lehigh Gorge SP conversion - I-80 White Haven Bridge, Impacts Lehigh Gorge State Park, Luzerne County

The NPS LWCF also concurs with the proposed replacement property at Unionville Road for the replacement of 7 acres of land converted from Recreation use at Lehigh Gorge SP meets the LWCF Act and regulations.

I sincerely apologize for the delay in reviewing the documentation for the above projects; thank you for your patience!

If you have any additional questions, please let me know.

Missy

Missy Morrison

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557

Appendix F
Bicycle and Pedestrian Checklist

Planning and Programming Checklist

Project _____
 SR _____ Segment _____ Offset _____
 Team Members _____
 _____ Date _____

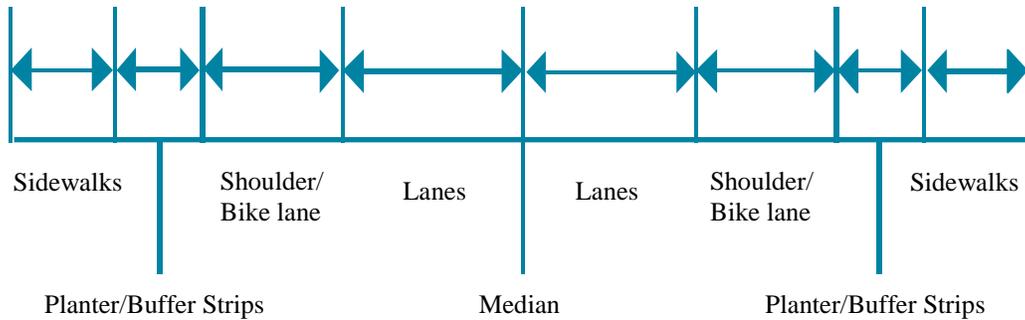
Item	Considerations	Check	Comments
1. Consistency with Bicycle/Pedestrian Planning Documents	Is the transportation facility included in or related to bicycle and pedestrian facilities identified in a master plan? <ul style="list-style-type: none"> • MPO/RPO bike/ped plan. • Local planning documents. • BicyclePA Routes. • Statewide Bicycle and Pedestrian Master Plan. 		
	Will the transportation facility provide continuity and linkages with existing or proposed bicycle/pedestrian facilities?		
	Is the transportation facility included in or related to a regional/local recreational plan? <ul style="list-style-type: none"> • Rails-to-Trails. • Greenways. • Local, State, National Parks. 		
2. Existing and Future Usage	Do bicycle/pedestrian groups regularly use the transportation facility? <ul style="list-style-type: none"> • Bike clubs. • Bicycle commuters. • Hiking, walking, or running clubs. • Skateboarding or rollerblading groups. • Bicycle touring groups. • General tourism/sightseeing. 		
	Does the existing transportation facility provide the only convenient transportation connection/linkage between land uses in the local area or region?		
	Could the transportation facility have favorable or unfavorable impacts upon the bike tourism/economy of an area/region? Consider: <ul style="list-style-type: none"> • Local businesses • Chamber of Commerce • Tourism Promotion Agencies. 		
	Are there physical or perceived impediments to bicycle or pedestrian use of the transportation facility?		
	Is there a higher than normal incidence of bicycle/pedestrian crashes in the area?		
3. Safety	Is the transportation facility in a high-density land use area that has pedestrian/bike/motor vehicle traffic?		

Item	Considerations	Check	Comments
3. Safety (continued)	Is there a high amount of crossing activity at intersections? <ul style="list-style-type: none"> • Midblock • Night crossing activity • Adequate lighting. 		
	Would the transportation facility (and all users) benefit from widened or improved shoulders or improved markings (shoulders, crosswalks)?		
4. Community and Land Use	Is the transportation facility in a city, town, or village?		
	Is the transportation facility within/near a community or neighborhood?		
	Is the transportation facility the "main street" in a community or town?		
	Could bicycle or pedestrian usage impact economic development?		
	Are sidewalks needed in the area? <ul style="list-style-type: none"> • Presence of worn paths along the facility. • Adjacent land uses generate pedestrian traffic. • Possible linkages/continuity with other pedestrian facilities. 		
	Is the transportation facility a link between complimentary land uses? <ul style="list-style-type: none"> • Residential and commercial. • Residential and business. 		
	Is the transportation facility in close proximity to hospitals, elderly care facilities, or the residences or businesses of persons with disabilities?		
	Is the transportation facility within or near educational buildings?		
5. Transit	Is the transportation facility on a transit route?		
	Is the transportation facility near park-and-ride lots?		
	Are there existing or proposed bicycle racks, shelters or parking available? Are there bike racks on buses?		
6. Traffic Calming	Is the community considering traffic calming as a possible solution to speeding and cut-through traffic?		

Scoping Checklist

Project _____
 SR _____ Segment _____ Offset _____
 Team Members _____
 _____ Date _____

Right-of-Way Needs Diagram



Element	Number Required	Width Required	Total Width
Sidewalks			
Buffer Strips			
Shoulders			
Lanes			
Median			
Total Right-of-Way Required			

Pedestrian Facilities

Item	Considerations	Check	Comments
1. Sidewalks	Appropriate width: <ul style="list-style-type: none"> • 1.5 m - 2.1 m (5' - 7') for residential, commercial, and industrial. • 2.5 m (8') minimum for high use areas/CBD. • 2.1 m (7') width for bridges. • 0.6 m (2') shy distance for vertical barriers. • 1.2 m - 2.1 m (5' - 7') barrier separating traffic from pedestrians on bridges. 		

Item	Considerations	Check	Comments					
Sidewalks (cont'd)	Applicability of planter or buffer strips.							
	Connectivity with other pedestrian facilities.							
	Proximity to transit bike/ped generators: <ul style="list-style-type: none"> • Transit stops. • Schools. • Park & rides. • Nursing homes. • Offices. • Business environments. • Athletic fields. • Recreation facilities. 							
	Observe pedestrian patterns for special needs such as: <ul style="list-style-type: none"> • Midblock crossings. • Islands and refuges. • Night crossing activity. 							
	ADA needs and concerns.							
2. Signalized Intersections	Crosswalks provided and marked.							
	Intersection bike/ped crash history reviewed.							
	Is there a dedicated pedestrian phase, if so how long?							
	Crossing distance is minimized.							
	Ped heads and ped pushbuttons provided.							
	ADA needs and concerns.	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">Retirement homes</td> <td style="width: 50%;"></td> </tr> <tr> <td>Schools</td> <td></td> </tr> <tr> <td>Medical facilities</td> <td></td> </tr> </table>	Retirement homes		Schools		Medical facilities	
Retirement homes								
Schools								
Medical facilities								
3. Traffic Calming	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?							

Bicycle Facilities

Item	Considerations	Check	Comments
1. Bikelanes/Paved Shoulders	Appropriate width of bike lane: <ul style="list-style-type: none"> • 1.5 m (5') adjacent to curb • 1.8 m (6') standard. 		
	Connectivity with other facilities. <ul style="list-style-type: none"> • Bike lanes • Shared use trails • Trail heads/parking areas. 		
	Maximize width of shoulders and provide appropriate markings as per <i>AASHTO Green Book</i> .		
	3 m (10') vertical clearance from fixed obstructions (excluding road signs).		
	Angle and smoothness of railroad crossings. Avoid angles of incidence of < 70° or re-design.		
	Bridge accesses provided/pinch points avoided.		
	Parking parallel or angled.		
2. Signalized intersections	Inventory existing bicycle facilities.		
	Intersection bike/ped crash history reviewed.		
	Crossing distance is minimized.		
	Considerations for bikes making turns.		
	Bike detection. Elevated push buttons.		
3. Traffic Calming	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?		

Final Design Checklist

Project _____
 SR _____ Segment _____ Offset _____
 Team Members _____
 _____ Date _____

Pedestrian Facilities

Item	Considerations	Check	Comments
1. Sidewalks and Signalized Intersections	Crosswalks are at least 3 m (10') wide.		
	Crosswalks are prominently marked using at least 6" line.		
	Pedestrian signals are provided.		
	Pushbuttons are provided and accessible.		
	Minimize crossing distance.		
	Maximize pedestrian visibility at crossings.		
	Coordination of turn phases with walk/don't walk signs.		
	Proper lighting type and placement.		
2. ADA Requirements	Pushbuttons accessible.		
	Pushbuttons height 1.0 m - 1.1 m (3.5' - 4.0').		
	Large pushbuttons used.		
	1.5 m (5') recommended passage (sidewalks).		
	5% maximum grade recommended (sidewalks).		
	2% cross-slope maximum.		
	Textured curb cuts.		
	2 curb cuts per corner at intersections.		
	Curb cuts flush with street surface 0.6 cm (1/4" tolerance).		
	Running slope of new curb cuts 1 in 12 max.		
	Longer signal cycles.		
	Audible crossing signals.		
	Level landings on perpendicular curb ramps.		
	Proper head/shoulder clearance for visually impaired.		
	Coordinate utilities with ADA requirements.		
	Proper lighting.		
	Analyze landscaping growth potential for future obstructions.		
Any conflicts with minimal distance that should be included in the project.			
Coordinate and minimize signage conflicts.			

Item	Considerations	Check	Comments
3. Traffic Calming	Consider traffic calming as a means to improve pedestrian and general traffic safety.		

Bicycle Facilities

Item	Considerations	Check	Comments
1. Bikelanes/Bikeways	Bicycle safe grates, RC-45M, Sheet 8 of 20.		
	Manhole covers flush with roadway surface.		
	Inlets flush with roadway surface.		
	Rumble strips type and placement.		
	Driveway aprons.		
	Conflicts eliminated with: <ul style="list-style-type: none"> • Turns at intersections. • Through movements. • Bicycle and pedestrian conflicts. • Parked cars, angled vs. parallel. • Driveway aprons. 		
2. Signage	3 m (10') vertical clearance from signs and structures.		
	"Share the Road Signs."		
	"Wrong Way Signs."		
	Lane stenciling.		
	Bike lane designation signs.		
	No parking signs.		
	Bike lane striped.		
	Transition from bike lane to bikeway.		
	Consistent width on roadways, bridges, and intersections.		
	Overlap bike lane/shoulder stripe over pavement joints.		
Meet or exceed AASHTO criteria.			
3. Traffic calming	Consider traffic calming as a means to improve pedestrian and general traffic safety.		

Appendix G
List of Preparers

Name	Organization	EA Role	Education	Years
Camille Otto Director of Planning, Environment, and Finance	FHWA PA Division	FHWA Approver	B.S. Biology	25
Jon Crum Senior Environmental Specialist	FHWA PA Division	FHWA Environmental Reviewer	B.S. Biology M.S. Environmental Science and Management	17
James Peratino, PE Transportation Engineer	FHWA PA Division	FHWA Approver	AAS Engineering	17
Jason E. Layman, P.E. Consultant Design Project Manager	PennDOT District 10-0	Engineering Reviewer	B.S. Civil Engineering Technology	14
Jessica Schrecengost Senior Civil Engineer Supervisor	PennDOT District 10-0	Environmental Reviewer	B.S. Civil Engineering/Minor Environmental Engineering	10
Drew Ames Environmental Planning Manager	PennDOT Central Office	Environmental Reviewer	B.H Communications M.S. Community and Regional Planning	26
Kenda Gardner Deputy Chief Counsel	PennDOT Office of Chief Counsel	Legal Review	B.S. Chemistry J.D.	28
Neal Brofee Environmental Counsel	PennDOT Office of Chief Counsel	Legal Review	B.A. Mathematics J.D.	24
David Anthony Historic Preservation Specialist	PennDOT Central Office	Above Ground Cultural Resources	M.S. Historic Preservation	24
Susanne Haney Archaeologist District 10-0 and PennDOT Highway Archaeological Survey Team	PennDOT Central Office	Archaeology	B.A Social Science Archaeology emphasis/M.A. Professional Growth (Applied Archaeology)	28
Jeff Bucher, PE Chief, Highway Design & Technology Division	PennDOT Central Office	Engineering Reviewer	B.S. Civil Engineer	33
Diane Nulton Environmental Project Manager	HDR	EA Project Manager	B.S. Biology/Ecology	35
Kathleen Krommes, ENV SP Environmental Project Manager	HDR	Environmental Lead, EA Technical Writer/Editor	B.S. Chemical Engineering	35
Katherine Markowitz Environmental Scientist	HDR	EA, Technical Writer/Editor	B.S. Marine and Environmental Biology and Policy	8
John McPherson, AICP Environmental Services Director	HDR	EA, Cumulative Impacts	B.A. Math/Economics; M.U.P.	30
Linda Smith Senior Environmental Planner	HDR	Author, Cumulative Impacts	M.S.E. Civil Engineering	17
Jenn Walsh, PE Traffic & Planning Section Manager	HDR	Traffic Diversion Analysis	B.S. Civil Engineering; M.S. Civil Engineering	28

Name	Organization	EA Role	Education	Years
Darryl Phillips, P.E., PTOE Senior Project Manager	HDR	Traffic Diversion Analysis	B.S. Civil Engineering M.S. Engineering	34
Audrey Heffernan Senior Environmental Planner	HDR	Environmental Justice	B.A. Math; M.A. Math; M.S. City & Regional Planning	28
Connie Eskin Administrative Coordinator	HDR	Technical Editor	Pennsylvania State University	25
Tina Adair Technical Editor	HDR	Technical Editor	B.S. Communications	35
Frank Brillhante GIS Manager	HDR	GIS Analysis	B.S. Engineering; M.S Environmental Engineering	28
Sean Hart, P.E., C.B.S.I. Project Manager	Baker	Bridge Replacement Project Manager	B.S. Civil Engineering; M.S. Engineering Management	21
Amy Pinizzotto Transportation Planner	Baker	Bridge Replacement NEPA and Section 4(f)/6(f) approval and coordination	B.S. Political Science	24
Crystal Roemer, P.W.S. Environmental Scientist	Baker	Bridge Replacement Environmental Scientist	B.S. Biology/ Environmental Science	8
Ben Reiman Sr. Project Manager	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.S. Environmental Science	24
Connor Sullivan Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	M.S. Environmental Science	9
Alyssa Hyziak Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.A. Anthropology	6
Terri Slack National Discipline Lead, Trans. Revenue Systems & Operations	CDM Smith	Traffic Forecasting	BA Economics; BA Political Science M.B.A Management	33
Tarannum Rima Travel Demand Modeler	CDM Smith	Traffic Forecasting	B.S. Civil Engineering M.S Transportation Engineering M.S. Computer Systems Engineering	16

Nathaniel Weinstock Air Quality and Acoustical Group Leader, Sr. Air Quality and Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis	B.S. Public Service	22
Kyle Brubaker Sr. Environmental Specialist, TD Environmental Task Leader	Navarro & Wright	Hazardous Materials	B.S. Environmental Science	13
Robert C. Kolmansberger Director of Environmental Services, Sr. Air Quality & Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis, QA/QC	B.A. Geography & Environmental Planning	30

Appendix H

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