

# PRELIMINARY ENGINEERING NOISE SCREENING

*For The*

BRISTOL ROAD EXTENSION

SR 2025, SECTION 002

Chalfont Borough, New Britain Borough, and New Britain Township

Bucks County, PA

MPMS#12923

Prepared For



**PENNDOT District 6-0**

7000 Geerdes Boulevard

King of Prussia, PA 19406

Prepared By



**Traffic Planning and Design, Inc.**

2500 East High Street Suite 650

Pottstown, PA 19464

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## **EXECUTIVE SUMMARY**

The proposed project involves the construction of the Bristol Road Extension, a new roadway connecting Butler Avenue (SR 4042) and Park Avenue (SR 1006) near Chalfont in Bucks County, PA.

This report documents the existing and future noise levels associated with the Bristol Road Extension Project. There are three main components to the project: Bristol Road Extension (extending Bristol from current termini at Butler Ave to Park Ave), Main Street and Park Avenue intersection improvements (addition of turn lane), and Park Avenue/Callowhill Road/Ferry Road intersection improvements (intersection upgrade converting an existing three way stop condition to a signalized intersection). As the three portions of the project are not contiguous, three separate noise models were developed to analyze potential impacts.

Based on results of the analysis, design year (2042) build noise levels are predicted to approach the Noise Abatement Criteria Levels (NAC) at two receptor locations, exceed the NAC at two receptor locations, and have a substantial increase at an additional receptor. All three models have impacted receptors. These receptors include residences and a cemetery. All the impacted receptor locations have access driveways onto adjacent existing roadways.

Barriers were determined warranted at one location in NSA 7 in the proposed build condition for the Bristol Road Extension portion of the project, near the intersection of Forrest Park Drive and the Bristol Road Extension. Barriers at these locations are not considered feasible as they could not be constructed without introducing safety concerns at the intersection or impacts to driveway and pedestrian access. Barriers were found to be warranted at two locations, one in NSA 1 and one in NSA 4 at the Main Street and Park Avenue intersection in downtown Chalfont. Barriers at these locations are not considered feasible as they could not be constructed without introducing safety concerns at the intersection, or without introducing additional Section 4(f) impacts, and impacts to driveway and pedestrian access due to the improvement being located within a downtown setting. Barriers were found to be warranted at two locations, one in NSA 13 and one in NSA 15 at the Park Avenue, Callowhill Road, and Ferry Road intersection. Barriers at these locations are not considered feasible as they could not be constructed without introducing safety concerns at the intersection or impacts to driveway access.

## 1.0 INTRODUCTION

The following Preliminary Engineering Noise Screening was completed by Traffic Planning and Design, Inc. (TPD) in accordance with *PennDOT Publication 24: Project Level Highway Traffic Noise Handbook* (May 2019). This study was completed on behalf of PennDOT, District 6-0. Refer to Appendix A for a project location map (Figure 1) and plan sheets from preliminary engineering plans (Appendix D). The project is considered a Type 1 Project, highway on a new alignment. This project will involve the extension of Bristol Road (SR 2025) from the existing terminus at Butler Avenue (SR 4202) to the new proposed terminus at Park Avenue (SR 1006). Intersection and approach roadway improvements outside the extension area are necessary to improve the overall local traffic network. There are three main components to the project as follows:

### ***Bristol Road Extension (new roadway segment, proposed bridge, intersection with Park Avenue and intersection with Butler Avenue) - Referred to as BRE in the remainder of this report***

The Bristol Road Extension new roadway segment will extend Bristol Road from the existing terminus at Butler Avenue to Park Avenue. This section of proposed new roadway will consist of two lanes and shoulders. The proposed new roadway will be widened from two lanes to three lanes at the Bristol Road/Butler Avenue intersection and the Bristol Road/Park Avenue intersection to allow for left turn lanes. Within this new roadway segment, a new multi-span bridge carrying Bristol Road over Pine Run will be constructed. Span configuration, superstructure type, and substructure type will be determined during preliminary engineering. The proposed roadway extension between Butler Avenue and Park Avenue will be designed to minimize impacts to existing open space, wetlands, and floodplain to maximize efforts to comply with the associated NPDES permit. Additionally, a SEPTA rail line crosses the proposed Bristol/Butler intersection.

Lane configuration for the proposed new roadway and intersecting portion of Park Avenue are outlined below:

- Proposed Bristol Road Extension: two (2) 11' travel lanes and associated turn lanes, 43' total width.
- Reconstructed Park Ave at the intersection with the proposed Bristol Road Extension: two (2) 11' travel lanes, 44' total width.

Intersection upgrades at the Bristol Road Extension/Park Avenue intersection will include left and right turn lanes at each approach and the addition of a traffic signal. A multi-use trail will be constructed parallel to the new roadway.

Improvements at the Bristol Road and Butler Avenue intersection will include the following:

- Improvements at this intersection will incorporate the addition of the Bristol Road Extension and other associated improvements such as channelized vehicle movements, additional safety gates, pedestrian accommodations, and improvements to the railroad warning system and railroad signalization. The proposed improvements at Butler Avenue include left turn lane additions or extensions approaching the intersection (the improvements will extend approximately 800 feet in the southbound direction and approximately 1,300 feet in the northbound direction).
- The left turn lane along Bristol Road will extend approximately 200 feet to the east and west of Butler Avenue.

Lane configuration for each roadway section are outlined below:

- Reconstructed Bristol Road east of the Butler Avenue intersection: two (2) 11' travel lanes, width varies from 32 feet to 57 feet total width.
- Reconstructed Butler Avenue 1300 feet northbound to 800 feet southbound of the Butler Avenue intersection: two (2) 12' travel lanes and associated turning lanes, width varies from 36 feet to 54 feet.

***Park Avenue and Main Street Intersection Improvements – Referred to as MP in the remainder of this report***

The proposed project will include the widening of Park Avenue to add a left turn lane. Lane configuration for each roadway segment are outlined below:

- Reconstructed Park Avenue at Main Street Intersection: three (3) travel lanes, 32' total width.

***Park Avenue, Ferry Road, and Callowhill Road Intersection Improvements – Referred to as PFC in the remainder of this report***

Intersection improvements will involve widening of Ferry Road to accommodate a right turn lane, widening of Callowhill Road to accommodate a left turn lane/taper, and widening of Park Avenue to accommodate a lane taper. A new traffic signal will be installed at the intersection. Lane configuration for each roadway segment are outlined below:

- Reconstructed portion of Park Avenue at this intersection: three (3) 11' travel lanes, 10' median, two (2) 4' shoulders, 51' total width.
- Reconstructed portion of Ferry Road at this intersection: three (3) 11' travel lanes, two (2) 4' shoulders, 41' total width.
- Reconstructed portion of Callowhill Road at this intersection: three (3) 11' travel lanes, one 4' shoulder, one 6' shoulder, 45' total width.

## **2.0 METHODOLOGY**

This noise screening document used PennDOT's Noise Abatement Criteria to evaluate noise impacts. Predicted noise levels were determined using Version 2.5 of the FHWA Traffic Noise Model (TNM). The proposed project involves the construction of a new roadway and associated intersection improvements. The project involves receptors that include existing/proposed residential development, daycare, restaurants, cemetery, open space, Section 4(f) properties, utilities, emergency services, retail, light industrial, and offices (Land Use Activity Category B, C, and E). Noise Abatement Criteria (NAC) for each land use categories are listed in Table 1. The NAC's are based on criteria in 23 CFR 772 and guidelines from *PennDOT Pub 24*.

The Federal Highway Administration (FHWA)'s procedures require the State to define the level(s) that "approach" the abatement criteria. PennDOT considers the NAC to be approached if the traffic noise levels are within one decibel of the values shown in Table 1.

TABLE 1  
 NOISE ABATEMENT CRITERIA

ACTIVITY CATEGORY	Leq(h) <sup>1</sup>	DESCRIPTION OF LAND USE ACTIVITY CATEGORY
<b>A</b>	57 (exterior)	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
<b>B<sup>2</sup></b>	67 (exterior)	Residential
<b>C<sup>2</sup></b>	67 (exterior)	Active sport areas, amphitheaters, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails, and trail crossings.
<b>D</b>	52 (interior)	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
<b>E<sup>2</sup></b>	72 (exterior)	Hotels, motels, offices, restaurants/bars, and other undeveloped lands, properties or activities not included in A-D or F.
<b>F</b>	--	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.

1 Impact thresholds should not be used as design standards for noise abatement purposes.

2 Includes undeveloped lands permitted for this activity category.

If the criteria is approached or exceeded at any receptor, noise abatement is considered warranted (Phase 1). Noise barrier Feasibility (Phase 2) deals specifically with acoustical and engineering considerations such as:

- Ability of barrier to reduce noise levels by at least 5 dBA at the majority of impacted receptor units.
- Ability of barrier to be physically constructed at the proposed location.
- Ability of barrier to be constructed without causing safety issues.
- Ability of barrier to be constructed without pedestrian or vehicular access restrictions.
- Ability of barrier to be accessed for maintenance.
- Allowance for utilities and drainage to adequately function.
- Other noise sources in the area.

Noise impacts are also considered when the design year noise levels substantially exceed the existing noise levels (substantial noise increase). PennDOT has developed substantial noise increase criteria for all activity categories (Land Use Activity Categories A, B, C, D, and E) where the future noise level increases by 10 (dB(A) or more above the existing level. Receptor units that satisfy these criteria warrant further consideration of noise abatement.

These factors can affect the feasibility determination for particular areas. The proximity of a local street and/or a private driveway can greatly reduce the effectiveness of noise barriers because the existence of one requires the termination of the noise barrier to allow for access. These potential "breaks" in the barrier can reduce its effectiveness by allowing noise to propagate around the barrier's terminus points.

Essentially, the “breaks” in the noise barrier (for access reasons) often make achieving the required 5 dBA reductions (which are necessary to be found feasible) unlikely. If noise barriers are found to be Warranted and Feasible, the Reasonableness (Phase 3) of the barriers are then analyzed. A Reasonableness Analysis involves the examination of the following factors:

- PennDOT’s noise barrier cost reasonableness value (2,000 ft<sup>2</sup> per benefited receptor) with all receptors achieving a noise reduction of 5 dBA or more being counted toward the total number of benefited receptors
- Noise reduction design criteria and goals
- Consideration of viewpoints of all benefited receptors

Traffic counts using the standard TNM categories were conducted on December 12, 2019 to determine the AM and PM peak hourly volumes at several locations within the project area via Miovision. The PM data was determined to be the most representative of field conditions (worst-case peak hour data) and was used in the models. A third component to the project was added at the Park/Ferry/Callowhill Road intersection subsequent to the initial traffic data collection. Miovision traffic data was collected on April 21, 2022. The traffic information was then entered into TNM Version 2.5.

The existing traffic data was projected to the design year (2042). These traffic data projections included redistribution of area traffic to account for the new local travel patterns from the introduction of a new roadway. Appendix C contains information pertaining to the traffic counts and calculation of the future traffic volumes used in this noise analysis, completed by the traffic engineers for the project (*Bristol Road Ext. Traffic Report 03-24-2019*).

Existing year (2019 and 2022), design year (2042) build, and design year (2042) no-build conditions were modeled and compared as part of this noise study. Traffic speeds used in the model correspond to the posted roadway speeds for the existing year (2019 & 2022) and design year (2042) no-build models. The design year (2042) build model used the design speed.

For simplification purposes, the model has been broken into three separate models as the project area is not contiguous between all the components described in Section 1.0. These individual models will be referred to as the following in this report:

- Bristol Road Extension (BRE)
- Main/Park (MP)
- Park/Ferry/Callowhill (PFC).

### **3.0 EXISTING HIGHWAY TRAFFIC NOISE ENVIRONMENT**

#### **3.1 Existing Site Conditions**

Topography within the project is rolling. Elevation ranges from approximately 288 feet above mean sea level (ASL) to 242 feet ASL in the BRE area, 284 ASL to 270 ASL in the MP area, and 368 to 320 ASL in the PFC area. Land use in the BRE area consists of residential, railroad, commercial, offices, restaurants, and township open space. It was determined by FHWA that this area is not considered park and therefore does not qualify as a Section 4(f) property. There is one partially constructed/planned residential development at the southwestern corner of Bristol Road and Butler Avenue. This development is accounted for in the models. Primary land use in the MP area is commercial, apartments, residential, inactive church (currently vacant and advertised for commercial rental), a cemetery, an ice cream shop, and railroad.

This area of the project is located within a National Register Eligible Historic District which has contributing elements adjacent to the improvements. Coordination regarding Section 106 clearance is finished and resulted in needing a Section 4(f) evaluation. A Section 4(f) De Minimis No Adverse Use Form was completed and approved for the Chalfont Historic District. Land use in the PFC area consists of residential and a music studio/school. One property is listed on the National Register. Section 106 coordination is finished which required a Section 4(f) evaluation. A Section 4(f) De Minimis No Adverse Use Form was completed and approved for the Morgan James Homestead. A Level 2 CEE is anticipated for the project.

### 3.2 Existing Noise Levels

For a preliminary noise screening per the guidance in PennDOT Pub 24, model validation and noise monitoring are not required. Points representing all sensitive receptors within the project area were entered into the existing model of the local roadways. Receptors within the project area included existing/proposed residential development, schools, restaurants/bars, open space, emergency services, retail, and offices. Sixteen (16) NSA's are located within the project area. The location and characteristics of the NSA are listed in Table 2 below. NSA locations are shown on Figure 2 in Appendix A.

TABLE 2A  
 NSA SUMMARY PARK/MAIN

NSA NUMBER	LOCATION	RECEPTOR NUMBERS	NAC TYPES
1	West of Main Street, north of SEPTA in Chalfont Borough	317, 329-342	Residential (B)
2	West of Main Street, south of SEPTA in Chalfont Borough	298, 299	Residential (B)
3	East of Main Street, north of Park Avenue	313-316, 318-328, 344, 346, 348-362, 368	Residential (B)/Office (E)
4	East of Main Street, south of between SEPTA and Park Avenue	303-312, 364, 369, 370	Residential, restaurant, and cemetery (B/C/E)
5	East of Main Street, south of SEPTA	300, 301	Residential (B)

TABLE 2B  
 NSA SUMMARY BRISTOL ROAD EXTENSION

NSA NUMBER	LOCATION	RECEPTOR NUMBERS	NAC TYPES
6	West of Park Avenue and Bristol Road Ext intersection	225-227	Residential (B)
7	South of Park Avenue and east of Bristol Road	244-277	Residential (B)
7A	North of Butler Avenue and west of Bristol Road	300, 319	Residential, restaurant (B/C)



NSA NUMBER	LOCATION	RECEPTOR NUMBERS	NAC TYPES
8	South of Butler Avenue and west of Bristol Road	69-129, 137-139, 140-146, 301-316, 318, 320, 321	Residential, restaurants, and daycare (B/C/E)
9	South of Butler Avenue and east of Bristol Road	164	Residential and commercial (B/E)
10	East of Bristol Road Extension and north of Butler Avenue	171-175, 188-224	Residential (B)
11	South of Butler Avenue and north of SEPTA	165	Daycare and commercial (B/E)

TABLE 2C  
 NSA SUMMARY PARK/FERRY/CALLOWHILL

NSA NUMBER	LOCATION	RECEPTOR NUMBERS	NAC TYPES
12	North of Callowhill Road and west of Ferry Road	3-6, 18-20, 30-33	Residential (B)
13	South of Callowhill Road and west of Peace Valley Road	1, 16	Residential (B)
14	South of Callowhill Road and east of Peace Valley Road	7, 11	Residential (B)
15	North of Callowhill Road and east of Ferry Road	8-10, 21-23, 25, 27-28	Residential/music studio (B/C)

One area located in the central portion of the project area is municipal owned open space. The nature of this open space in relation to its qualification as a Section 4(f) resource was determined by FHWA as not qualifying, as it is not used for active recreation. TNM runs are located in Appendix B. The location of receptors and their numbers are shown on Figure 3 – Receptor Location Plan in Appendix A.

An active rail line is located either adjacent to or within NSA’s 1, 2, 4, and 5 for Park/Main and NSA’s 7A, 8, 9, 10, and 11 for the Bristol Road Extension sections of the project. The Chalfont Train Station is located southwest of the Park/Main intersection. Trains stop at the station approximately twice an hour. The rail line is located adjacent to Butler Avenue on the eastern side of the Bristol Road Extension portion of the project and crosses through the Butler Ave and Bristol Road intersection. The railroad is considered an existing noise factor within the project area.

#### 4.0 FUTURE HIGHWAY TRAFFIC NOISE ENVIRONMENT

Two scenarios for future highway noise were modeled for the 2042 design year: No-build and Full Build. The no-build would be the existing traffic alignment/configuration with natural traffic growth (increases). The future build would be the 2042 full design. The comparison between the existing modeled levels and the future conditions is shown in Table 3. It was determined that the PM peak traffic hour was likely the worst case scenario based upon the existing use data collected for the traffic engineering report.

TABLE 3A  
 PARK AVE/MAIN STREET  
 MODELED RESULTS COMPARISON

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
<b>NSA 1</b>					
329	B	66	62	63	63
330	B	66	62	63	63
331	B	66	62	63	62
332	B	66	63	63	63
333	B	66	65	64	64
334	B	66	65	65	65
335	B	66	<b>68</b>	<b>69</b>	<b>68</b>
336	B	66	60	61	59
337	B	66	57	58	57
338	B	66	57	58	57
339	B	66	53	54	53
340	B	66	54	55	54
341	B	66	56	57	57
342	B	66	56	57	57
317	B	66	63	64	63
<b>NSA 2</b>					
298	B	66	61	61	61
299	B	66	63	64	63
<b>NSA 3</b>					
313	B	66	59	59	61
314	B	66	59	59	61
315	B	66	59	59	61
316	B	66	60	61	61
318	B	66	52	53	53
319	B	66	51	52	52
320	B	66	52	53	53
321	B	66	53	53	53
322	B	66	54	54	54
323	B	66	60	60	60
324	B	66	65	<b>66</b>	65
325	B	66	63	64	64
327	B	66	64	65	65
328	B	66	62	63	63
344	B	66	57	58	60
346	B	66	49	49	49
348	B	66	56	56	58
349	B	66	52	52	53
350	B	66	48	49	50
351	B	66	47	48	48
352	B	66	46	47	47
353	B	66	43	44	44
354	B	66	43	44	44
355	B	66	44	44	45
356	B	66	42	42	43

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
357	B	66	47	48	47
358	B	66	48	48	48
359	B	66	48	50	48
361	B	66	47	48	48
360	B	66	49	50	50
362	B	66	48	49	50
368	B	66	51	52	52
<b>NSA 4</b>					
303	C	66	<b>66</b>	<b>66</b>	<b>66</b>
304	B	66	47	48	47
305	B	66	48	49	48
306	B	66	51	52	52
307	B	66	54	55	55
308	B	66	54	55	56
309	B	66	50	50	51
310	B	66	46	46	46
311	B	66	45	45	45
312	B	66	60	61	62
364	B	66	48	49	49
369	C	66	64	65	64
370	B	66	49	49	50
<b>NSA 5</b>					
300	B	66	62	63	62
301	B	66	65	65	65

Two (2) receivers will be impacted in the design year (2042) no-build and design year (2042) build conditions. Receiver 335 (single family home) is located in NSA 1. This receiver exceeds the NAC in the existing, design year (2042) no-build, and the design year (2042) build condition. Receiver 303 (cemetery) is located in NSA 4. This receiver approaches the NAC in the existing, design year (2042) no-build, and the design year (2042) build condition. One (1) receiver in NSA 3 will be impacted in the design year (2042) no-build condition (approaches) but levels at this receiver decrease in the future condition due to traffic redistribution. These receivers are all located within 40 feet of the edge of the existing road.

TABLE 3B  
 BRISTOL ROAD EXTENSION  
 MODELED RESULTS COMPARISON

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
<b>NSA 6</b>					
225	B	66	50	50	55
226	B	66	57	57	62
227	B	66	59	60	61
<b>NSA 7</b>					
244	B	66	59	59	59
245	B	66	55	55	55
246	B	66	52	53	52

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
247	B	66	49	50	50
248	B	66	50	51	51
249	B	66	52	52	53
250	B	66	52	52	53
251	B	66	54	55	56
252	B	66	55	55	56
253	B	66	55	56	57
254	B	66	56	57	57
255	B	66	55	56	57
256	B	66	55	55	56
257	B	66	55	56	57
258	B	66	55	56	57
259	B	66	54	55	56
260	B	66	53	54	56
261	B	66	50	51	55
262	B	66	49	50	56
263	B	66	44	45	<b>55</b>
264	B	66	44	45	52
265	B	66	44	45	51
266	B	66	43	43	47
267	B	66	44	44	47
268	B	66	45	45	48
269	B	66	47	47	50
270	B	66	48	48	51
271	B	66	48	48	50
272	B	66	48	48	50
273	B	66	47	48	49
274	B	66	47	47	49
275	B	66	46	47	48
276	B	66	45	46	47
277	B	66	44	45	46
<b>NSA 7A</b>					
319	B	66	61	61	59
300	B	66	62	63	62
<b>NSA 8</b>					
69	B	66	48	48	49
70	B	66	48	49	50
71	B	66	49	49	50
72	B	66	49	49	50
73	B	66	49	50	51
74	B	66	51	51	52
75	B	66	51	52	53
76	B	66	52	53	53
77	B	66	53	54	54
78	B	66	55	56	55
79	B	66	57	57	56
80	B	66	58	58	57
81	B	66	60	61	59
82	B	66	48	49	50
83	B	66	48	49	50

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Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
84	B	66	48	49	50
85	B	66	49	49	50
86	B	66	49	49	50
87	B	66	49	49	50
88	B	66	49	49	50
89	B	66	49	49	50
90	B	66	49	50	50
91	B	66	49	50	50
92	B	66	59	59	58
93	B	66	57	57	56
94	B	66	55	56	55
95	B	66	54	55	55
96	B	66	53	54	54
97	B	66	52	53	54
98	B	66	51	52	53
99	B	66	51	52	53
100	B	66	51	51	53
101	B	66	51	51	53
102	B	66	51	51	53
103	B	66	51	51	52
104	B	66	51	51	52
105	B	66	51	52	53
106	B	66	52	52	53
107	B	66	50	51	51
108	B	66	51	51	51
109	B	66	52	52	51
110	B	66	52	53	52
111	B	66	53	53	52
112	B	66	53	53	53
113	B	66	53	54	54
114	B	66	53	54	55
115	B	66	54	54	55
116	B	66	54	55	56
117	B	66	56	56	57
118	B	66	61	62	60
119	B	66	61	61	59
120	B	66	61	61	59
121	B	66	61	62	60
122	B	66	61	61	60
123	B	66	61	62	60
124	B	66	61	62	62
125	B	66	60	61	61
126	B	66	60	61	60
127	B	66	60	60	60
128	B	66	60	60	59
129	B	66	59	60	59
137	B	66	60	60	58
138	B	66	60	61	57
139	B	66	61	62	59
140	B	66	51	52	52

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
141	B	66	55	56	55
142	B	66	62	62	62
143	B	66	61	61	61
144	B	66	60	61	61
145	B	66	62	63	63
146	B	66	64	64	64
301	B	66	48	49	49
302	B	66	48	49	49
303	B	66	49	49	49
304	B	66	50	51	50
305	B	66	52	53	52
306	B	66	49	50	50
307	B	66	48	49	49
308	B	66	49	49	49
309	B	66	47	47	47
310	B	66	57	58	57
311	B	66	50	51	51
312	B	66	50	51	51
313	B	66	51	51	51
314	B	66	49	50	50
315	B	66	50	50	51
316	B	66	51	52	52
318	B	66	50	50	51
320	B	66	53	54	53
321	B	66	52	53	53
<b>NSA 9</b>					
164	B	66	<b>66</b>	<b>66</b>	65
<b>NSA 10</b>					
171	B	66	49	50	50
172	B	66	48	48	48
173	B	66	47	47	48
174	B	66	45	46	46
175	B	66	44	45	46
188	B	66	43	44	47
189	B	66	43	44	47
190	B	66	44	44	48
191	B	66	45	46	49
192	B	66	45	46	49
193	B	66	46	47	50
194	B	66	48	48	51
195	B	66	49	49	52
196	B	66	50	51	54
197	B	66	48	48	50
198	B	66	47	47	49
199	B	66	45	45	48
200	B	66	44	45	47
201	B	66	43	43	46
202	B	66	43	43	46
203	B	66	43	44	46
204	B	66	43	44	46

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2019) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
205	B	66	44	44	46
206	B	66	45	45	46
207	B	66	45	46	47
208	B	66	46	47	47
209	B	66	55	56	57
210	B	66	53	53	56
211	B	66	51	52	55
212	B	66	47	48	52
213	B	66	46	47	51
214	B	66	44	44	48
215	B	66	44	44	48
216	B	66	44	44	49
217	B	66	45	46	51
218	B	66	46	47	52
219	B	66	47	48	53
220	B	66	48	49	54
221	B	66	50	50	56
222	B	66	50	50	55
223	B	66	57	58	61
224	B	66	58	58	61
<b>NSA 11</b>					
165	C	66	62	62	60

One receiver is impacted under the existing (2019) and design year (2042) no-build condition. This receiver is located in NSA 9 and approaches the NAC. The existing receiver is located within 25 feet of the edge of road and 80 feet from an existing rail line. Noise levels at this receiver decrease in the future condition due to traffic redistribution. More traffic will divert to the new roadway and Butler Avenue will experience less traffic in the build scenario.

One receiver is impacted under the design year (2042) build condition. This receiver is located in NSA 7 and experiences a 10 plus DBA change in noise volume. This receiver is considered to have a substantial increase in noise levels. The existing receiver is located within 50 feet of the existing section of the Bristol Road Extension. Once the new section of Bristol Road is constructed the roadway will be moved a little further away from the receptor and a new sidewalk will be constructed between the road and the house.

TABLE 3C  
 PARK/FERRY/CALLOWHILL  
 MODELED RESULTS COMPARISON

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2022) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
<b>NSA 12</b>					
3	B	66	54	55	56
4	B	66	55	556	57
5	B	66	58	59	60
6	B	66	62	63	63
18	B	66	61	61	61
19	B	66	58	59	59

Receiver	Land Use Activity Category	Criteria Level	Existing Year (2022) PM	Design Year (2042) No-Build PM	Design Year (2042) Build PM
20	B	66	62	63	62
30	B	66	54	54	54
31	B	66	52	52	53
32	B	66	51	52	52
33	B	66	52	53	53
<b>NSA 13</b>					
1	B	66	50	51	51
16	B	66	<b>66</b>	<b>66</b>	<b>66</b>
<b>NSA 14</b>					
7	B	66	48	48	49
11	B	66	56	57	57
<b>NSA 15</b>					
8	B	66	<b>68</b>	<b>69</b>	<b>68</b>
9	C	66	59	60	60
10	B	66	58	59	60
21	B	66	61	61	61
22	B	66	61	61	61
23	B	66	62	63	63
25	B	66	46	46	46
27	B	66	47	47	47
28	B	66	58	59	59

Two receivers are impacted in the existing year (2022), design year (2042) no-build, and design year (2042) build scenarios. Receiver 16 is located in NSA 13 and Receiver 8 is located in NSA 15. The receiver in NSA 13 approaches the NAC in the existing, design year (2042) no-build, and the design year (2042) build condition. The receiver in NSA 15 exceeds the NAC in the existing, design year (2042) no-build, and the design year (2042) build condition. These two receivers are located within 20 feet of the existing edge of road.

A summary by NSA from the three design year models are found in Table 4 below:

TABLE 4  
 FUTURE NOISE SUMMARY BY NSA

NSA NUMBER	RECEPTOR NUMBERS	RANGE OF DESIGN YEAR (2042) BUILD NOISE LEVELS	BARRIER WARRANTED
<b>1</b>	317, 329-342	53-68 dBA	Yes
<b>2</b>	298, 299	61-63	No
<b>3</b>	313-316, 318-328, 344, 346, 348-362, 368	43-65 dBA	No
<b>4</b>	303-312, 364, 369 370	45-66 dBA	Yes
<b>5</b>	300, 301	62-65 dBA	No



NSA NUMBER	RECEPTOR NUMBERS	RANGE OF DESIGN YEAR (2042) BUILD NOISE LEVELS	BARRIER WARRANTED
6	225-227	55-62 dBA	No
7	244-277	46-59 dBA	Yes
7A	300, 319	59-62 dBA	No
8	69-129, 136-146, 301-309, 311-316, 318	47-64 dBA	No
9	164	65 dBA	No
10	171-175, 188-224	46-61 dBA	No
11	165	60 dBA	No
12	3-6, 18-20, 30-33	52-63 dBA	No
13	1, 16	51-66 dBA	Yes
14	7, 11	49-57 dBA	No
15	8-10, 21-23, 25, 27-28	46-68 dBA	Yes

Barriers are warranted for the future design year (2042) build condition at the MP intersection at two locations: one residence to the west of the MP intersection (Receiver 335 (single family)) and Receiver 303 (St. James Lutheran Church cemetery). Berms would require too much right-of-way and relocation of grave sites within a National Register Historic District. Barriers are warranted in the design year (2042) build condition for one residence within BRE portion of the project, at the Forest Park Drive and Bristol Road intersection. Two receivers (8 and 16) warrant barriers at the PFC intersection.

Coordination with the organization in charge of the cemetery indicated that there was no information available on visitors per day and would be very low so it was assumed that it was equivalent to one residential unit to be conservative. During multiple field views over a number of years very few visitors were observed. The church building is currently vacant and has been advertised for rent for commercial use for a number of years. A barrier or berm at this location would not be feasible as impacts would be required to grave sites in order to maintain pedestrian access and not impact safety/capacity along the roadways and intersections while still meeting project needs. Additionally, the church property is a contributing element to the Chalfont Historic District. Section 106 clearance has been received for the project and a Section 4(f) evaluation was required. A Section 4(f) Deminimis Use form was submitted and approved for the historic district.

A barrier to mitigate impacts at Receiver 335 would not be feasible as there are driveways and sidewalks for this residence and others along Main Street. The barrier length required would exceed 4X the distance from the receiver and eliminate access to the building itself.

A barrier would not be feasible at the BRE intersection of Bristol Road and Forest Park Drive for Receiver 263. The receiver is located close to the adjacent intersection. A barrier to mitigate impacts to receptor 263 would not be feasible as there are driveways and sidewalks on Forest Park Drive and proposed sidewalks on the new section of Bristol Road, and the barrier could not be constructed without affecting sight distance at the intersection. Additionally, the barrier length required would exceed 4X the distance from the receiver and eliminate access to this property and adjacent properties.

A barrier would not be feasible at the PFC intersection for Receiver 8 and 16, both receivers are located very close to the adjacent intersections. The barrier length required would exceed 4X the distance from the receiver and eliminate access to the properties. The Morgan James Homestead which is listed on the National Register of Historic Places, is located to the east of the intersection. Shifting the roadway improvements away from Receiver 8 (single family residence) in order to construct a barrier while meeting design criteria would result in additional Section 4(f) impacts to this property. Receiver 16 (single family residence) is located at the intersection of East Peace Valley Road and Callowhill Road. A barrier could not be constructed at this location without introducing safety concerns consisting mainly of site distance issues while maintaining the existing roadway alignment. The proposed roadway is being shifted to avoid the need for acquisition of the property.

## **5.0 HIGHWAY TRAFFIC NOISE CONSIDERATION AND ABATEMENT ALTERNATIVES**

Noise abatement is warranted within NSA's 1, 4, 7, 13, and 15 within the project area. Barriers in the area of these receptors are not feasible due to safety concerns, pedestrian access, driveway access, grave relocations, and additional Section 106 impacts. Earthen berms were considered but due to narrow right of way, existing Section 106 considerations, the existing Chalfont Historic District, the existing railroad, driveways, and pedestrian access; they were considered not feasible. The intersection improvements included in this project will provide alternative routes for traffic to reduce congestion in the overall traffic network in this area. Several Traffic Management techniques will be provided with the implementation of the intersection improvements. Several new signals will be constructed with the intersection improvements proposed for this project. Timing of the signals will be coordinated where possible to achieve smooth traffic flow and reduce the need for frequent acceleration and deceleration. The speed limit on Butler Avenue in the east bound direction past the railroad tracks will be reduced by 5 mph. The speed limit on Bristol Road approaching the Butler/Bristol intersection will be reduced by 10 mph. Separating noisier vehicles from other vehicles and placing them in a lane further away from receiver, is not possible for this intersection improvement due to downtown like setting and lack of additional space for additional lanes.

## **6.0 CONSTRUCTION NOISE CONSIDERATION AND ABATEMENT OPPORTUNITIES**

Highway construction is inherently noisy due to the use of heavy equipment. In order to mitigate highway construction noise several techniques can be employed:

- Use of less noisy equipment with mufflers
- Limit construction to day-time hours only
- Limit particularly noisy operations to specific time periods or durations through contract specifications and special provisions
- Locate storage areas and haul roads away from sensitive receptors
- Use alternative pile driving methods
- Use of electric compressors vs. diesel or gasoline compressors
- Use of shields and dampeners

A reasonable combination of these techniques to limit noise during construction will be incorporated in this project.

## 7.0 PUBLIC INVOLVEMENT PROCESS

Stake holder meetings were held in 2017 and early 2018 for local groups and governments within the project limits. An initial public meeting was held on June 5, 2018, at Unami Middle School to discuss the project alternatives. A second meeting was held with the Forest Park Neighbors Coalition on August 23, 2018 to further discuss the Bristol Road Extension Study Alternative Analysis. Residents asked PennDOT and the design team questions about the project, noise issues were not discussed at this meeting. A second meeting was held with the Forest Park Neighbors Coalition on January 30, 2020 to discuss the final preferred Bristol Road Extension Alternative and safety concerns for this neighborhood. Noise was not a discussion at this meeting. A second meeting was held on February 26, 2020 with the Carousel Point HOA Board to discuss the selected Bristol Road Extension Alternative and impacts to the housing community. Noise impacts were briefly discussed at this meeting but the proximity to the rail line was noted as being an existing noise factor for the community. An additional local stakeholder meeting was held in June of 2023 to update the local officials on the project status. A second public meeting will be held virtually as the project moves from preliminary engineering to final design.

## 8.0 LIST OF QUALIFICATIONS

Name:	Tracy Stroschein
Title:	Environmental Services Manager
Education:	B.S. - Environmental Resource Management, Minor – Environmental Engineering
Years of Professional Experience:	26 years
Responsibility:	TNM modeling, Report preparation

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Name:	Jaimie E. Ruane
Title:	Senior Environmental Scientist
Education:	B.S. – Biology, 2001, Minor – Geography
Years of Professional Experience:	23 years
Responsibility:	TNM modeling, Report review/preparation/edits

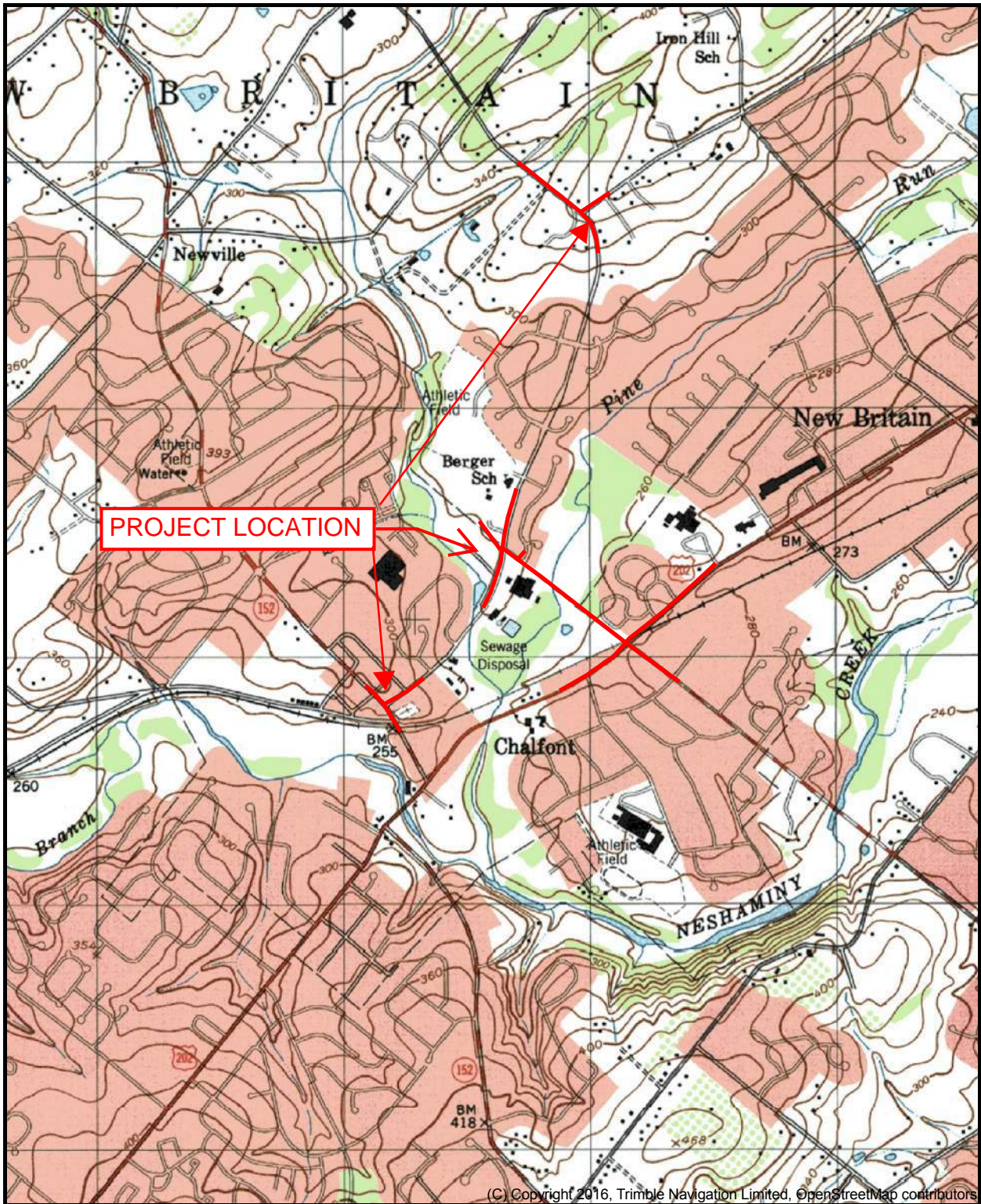
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Name:	Benjamin Guthrie, PE
Title:	Project Manager
Education:	B.S. – Civil and Environmental Engineering M.S. – Transportation Engineering
Years of Professional Experience:	16 years
Responsibility:	Traffic data projections

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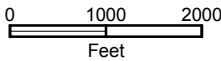
Name:	Brett Grove, PE
Title:	Senior Project Manager
Education:	B.S. – Civil Engineering
Years of Professional Experience:	29 years
Responsibility:	Project Management, Report QA/QC

# **APPENDIX A: FIGURES**



(C) Copyright 2016, Trimble Navigation Limited, OpenStreetMap contributors



Source: Doylestown USGS 7.5 Minute  
 Quadrangle  
 MPMS#: 12923  
 Prepared: Dec. 9, 2019

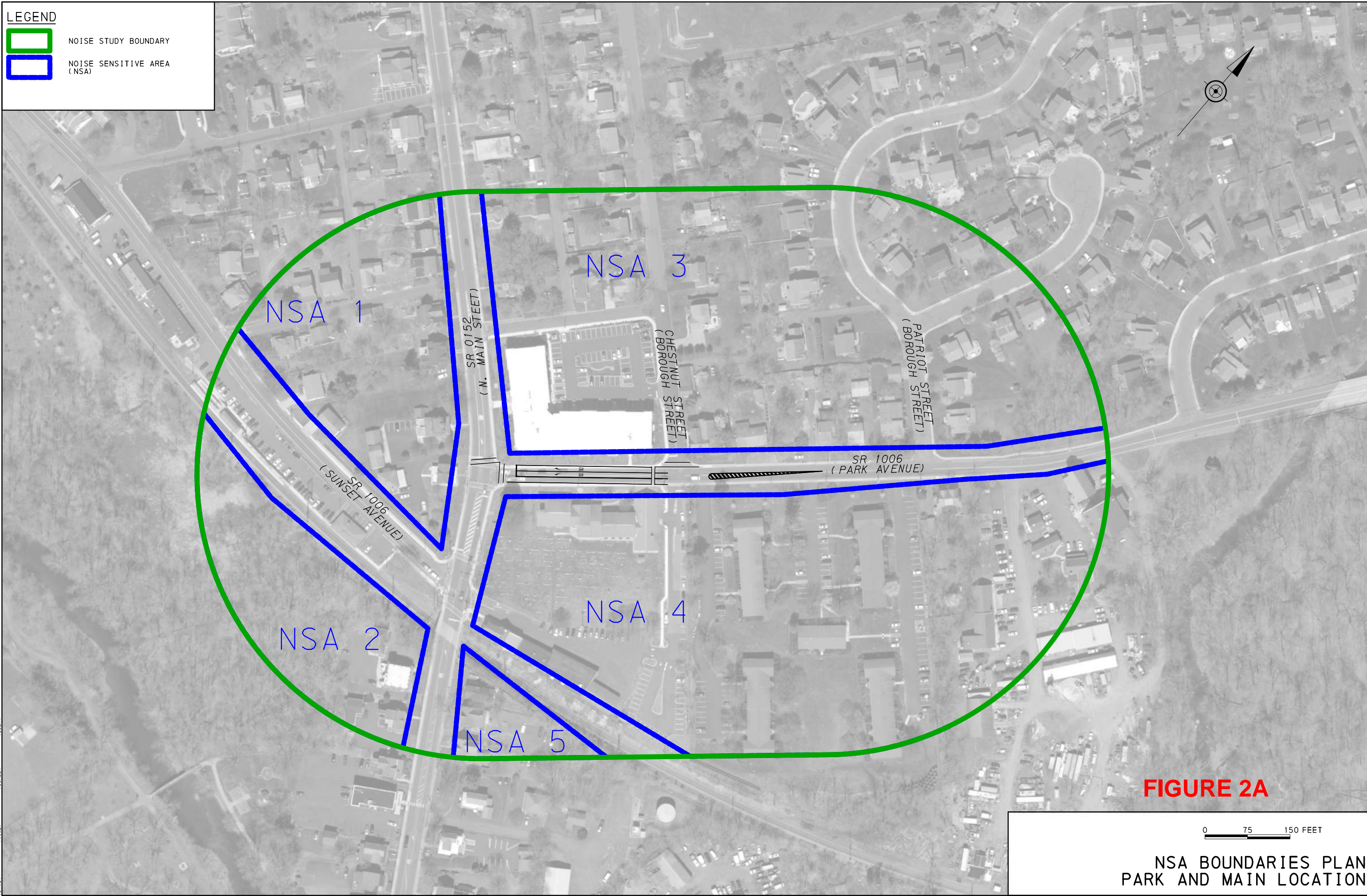


**PROJECT LOCATION MAP**  
 BRISTOL ROAD EXTENSION  
 NEW BRITAIN TOWNSHIP,  
 CHALFONT BOROUGH, AND NEW  
 BRITAIN BOROUGH,  
 BUCKS COUNTY, PA

**FIGURE 1**

LEGEND

-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)





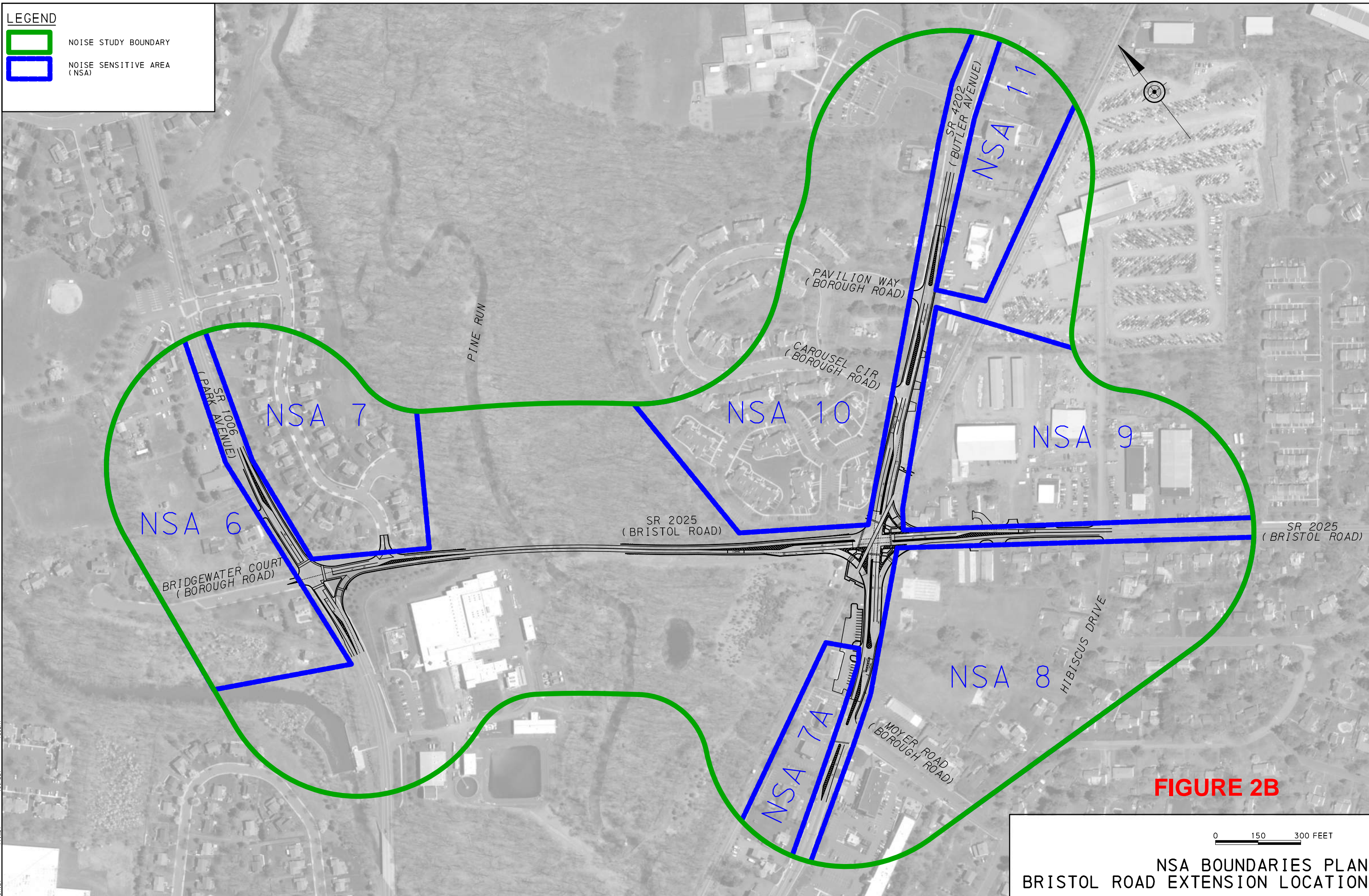
**FIGURE 2A**

0 75 150 FEET  
NSA BOUNDARIES PLAN  
PARK AND MAIN LOCATION

\$FILE\$  
\$DATE\$  
\$TIME\$  
\$SCALE\$  
\$USER\$

LEGEND

-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)





**FIGURE 2B**

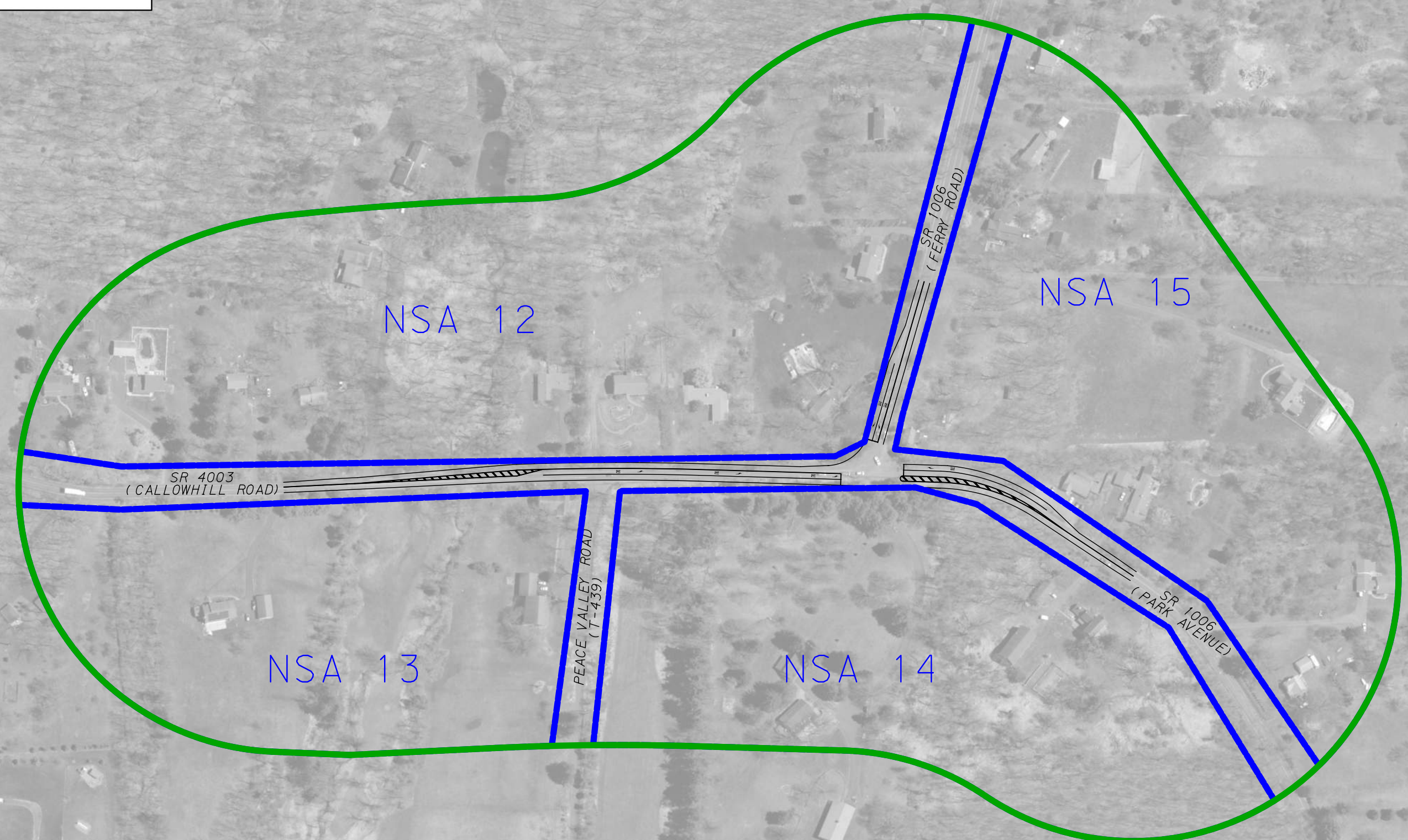
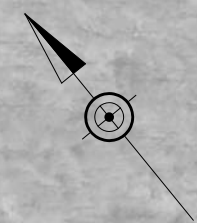
0 150 300 FEET

NSA BOUNDARIES PLAN  
BRISTOL ROAD EXTENSION LOCATION

DATE: 11/15/2011  
TIME: 10:00 AM  
SCALE: 1" = 100'  
USER: JLS

LEGEND

-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)



**FIGURE 2C**





0 100 200 FEET

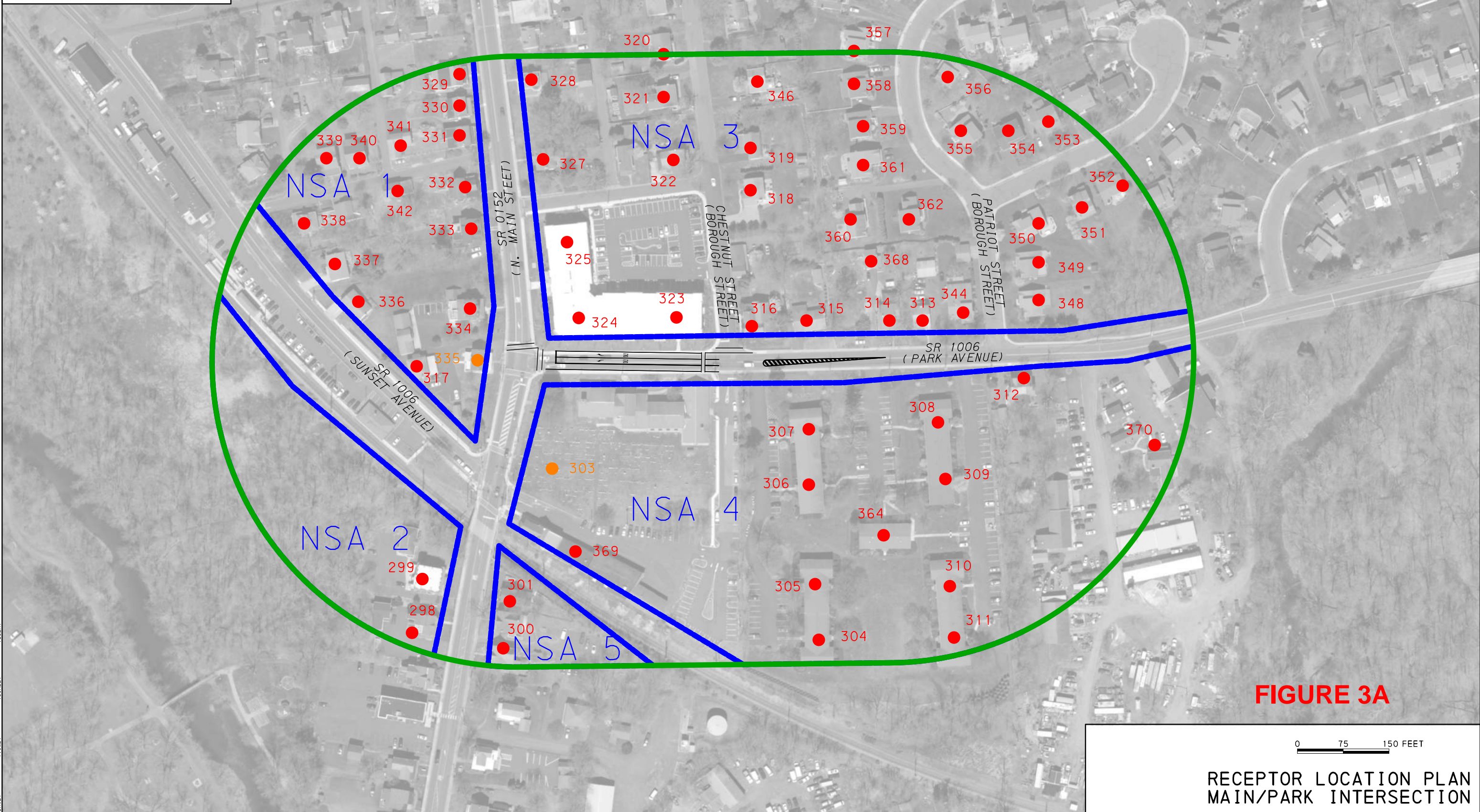
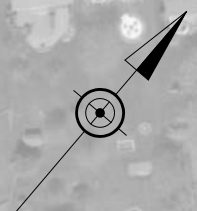
NSA BOUNDARIES PLAN  
PARK/FERRY/CALLOWHILL LOCATION

\$FILE\$  
\$DATE\$  
\$TIME\$  
\$SCALE\$  
\$USER\$



LEGEND





-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)
-  NON-IMPACTED RECEPTOR LOCATION
-  IMPACTED RECEPTOR LOCATION

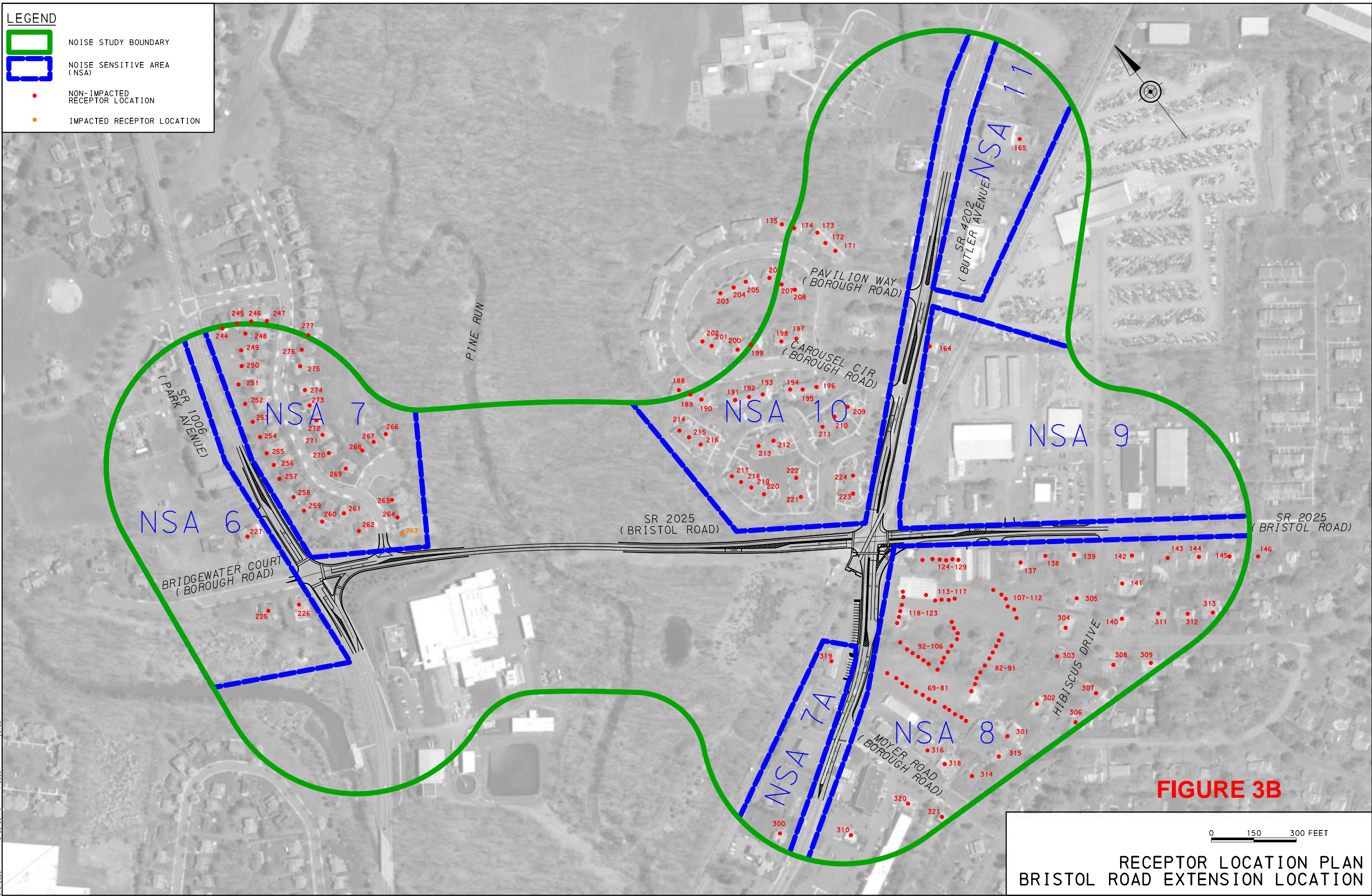


**FIGURE 3A**

0 75 150 FEET  
RECEPTOR LOCATION PLAN  
MAIN/PARK INTERSECTION

**LEGEND**

-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)
-  NON-IMPACTED RECEPTOR LOCATION
-  IMPACTED RECEPTOR LOCATION







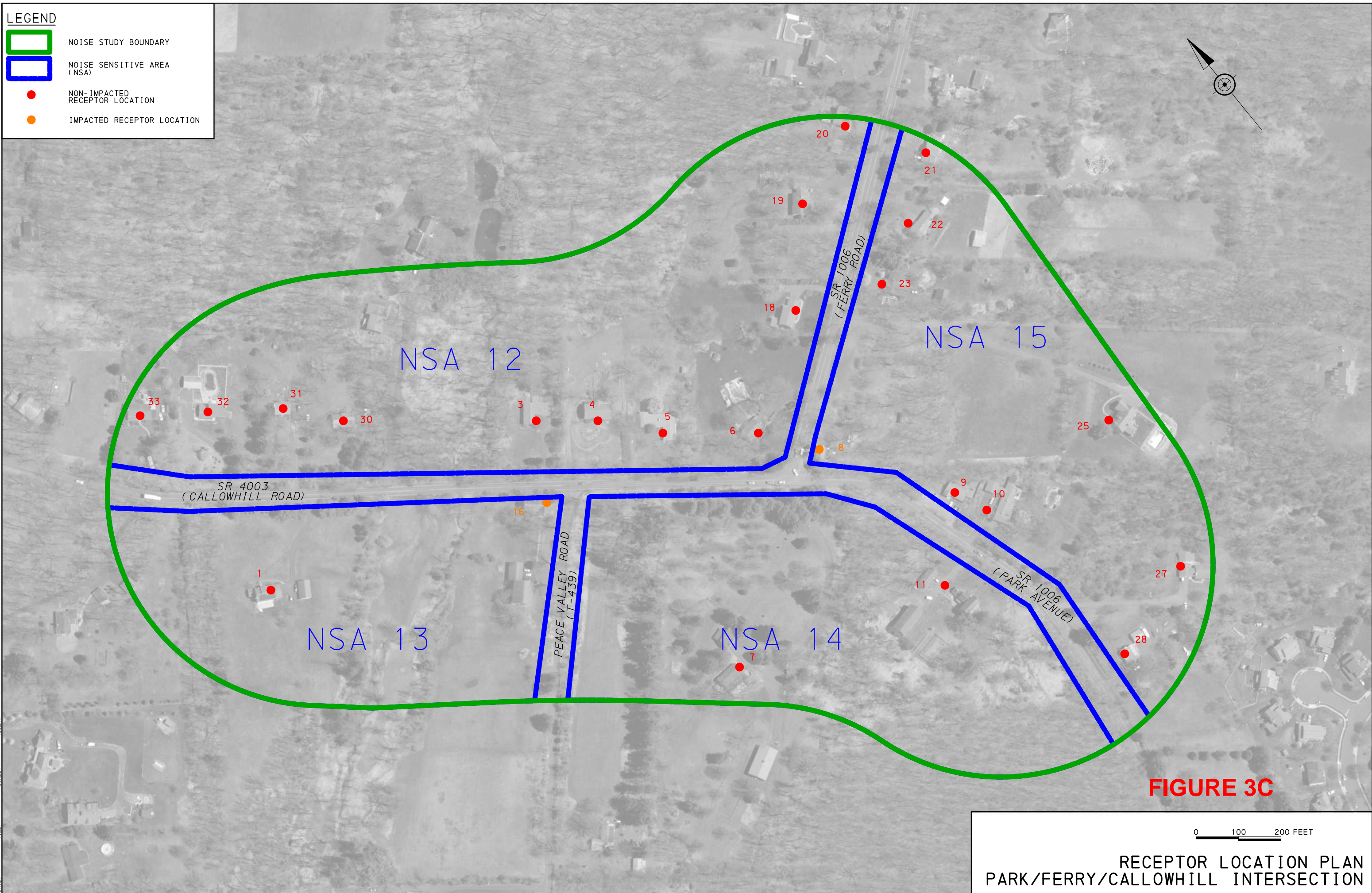
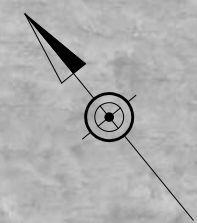
**FIGURE 3B**

0 150 300 FEET

**RECEPTOR LOCATION PLAN  
BRISTOL ROAD EXTENSION LOCATION**

LEGEND

-  NOISE STUDY BOUNDARY
-  NOISE SENSITIVE AREA (NSA)
-  NON-IMPACTED RECEPTOR LOCATION
-  IMPACTED RECEPTOR LOCATION



**FIGURE 3C**

0 100 200 FEET

RECEPTOR LOCATION PLAN  
PARK/FERRY/CALLOWHILL INTERSECTION

\$FILE\$  
\$DATE\$  
\$TIME\$  
\$SCALE\$  
\$USER\$

# **APPENDIX B: TNM RUNS**

Three models were created in TNM for this project

#### Bristol Road Extension – BRE

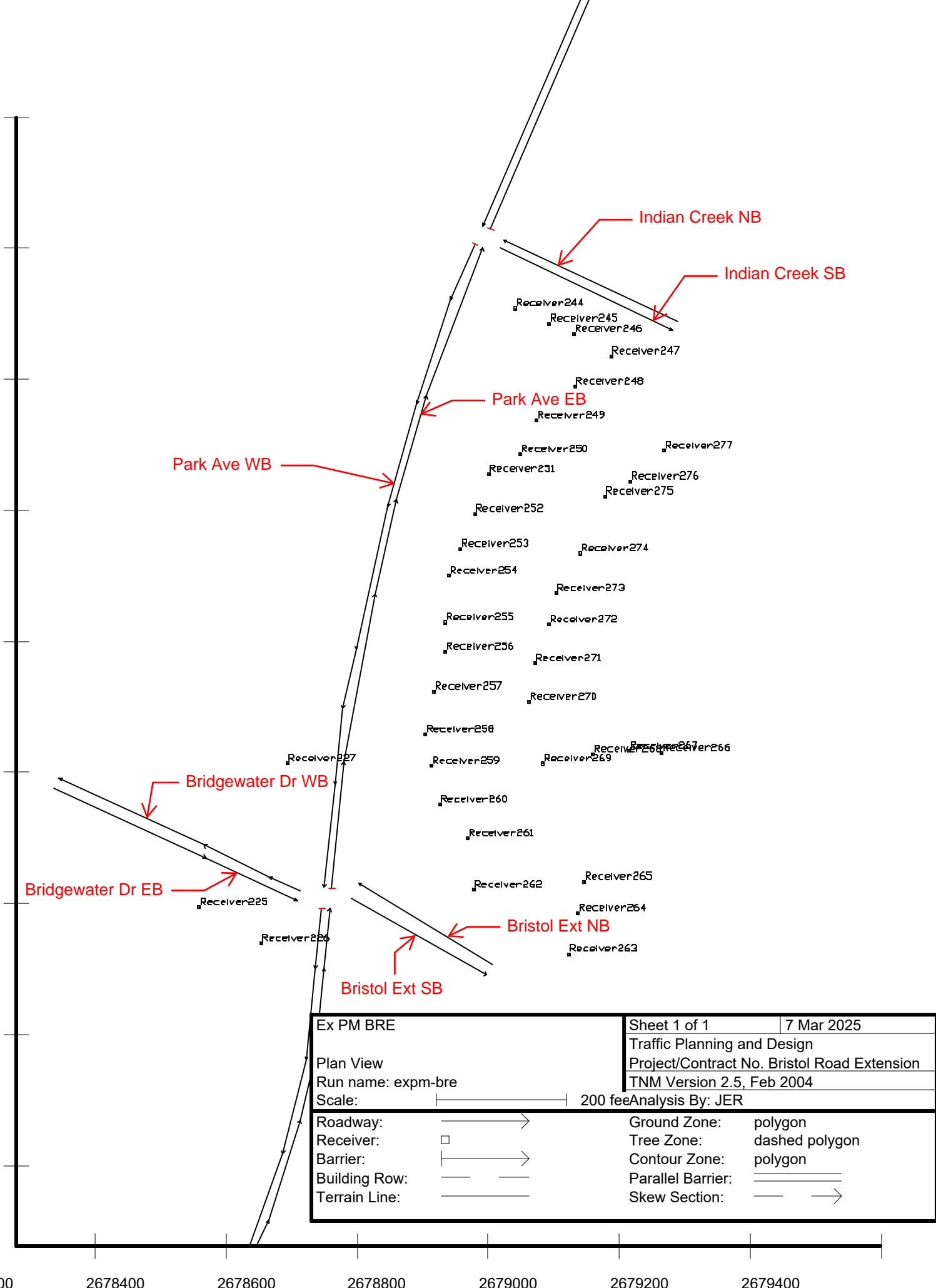
This includes the intersection of Butler Avenue and Bristol Road with the side roads Moyer Avenue (intersects with Butler Ave) and Hibiscus Drive (intersects with Bristol Rd). This model also includes the roads on the other end of the proposed extension of Bristol Road. The Intersection of Park Avenue with Bristol Road Extension and Bridgwater Court with the sideroad Indian Creek Way (intersects with Park Ave). The receivers in this model are residences, daycares, or restaurants (with outdoor seating).

#### Main Street

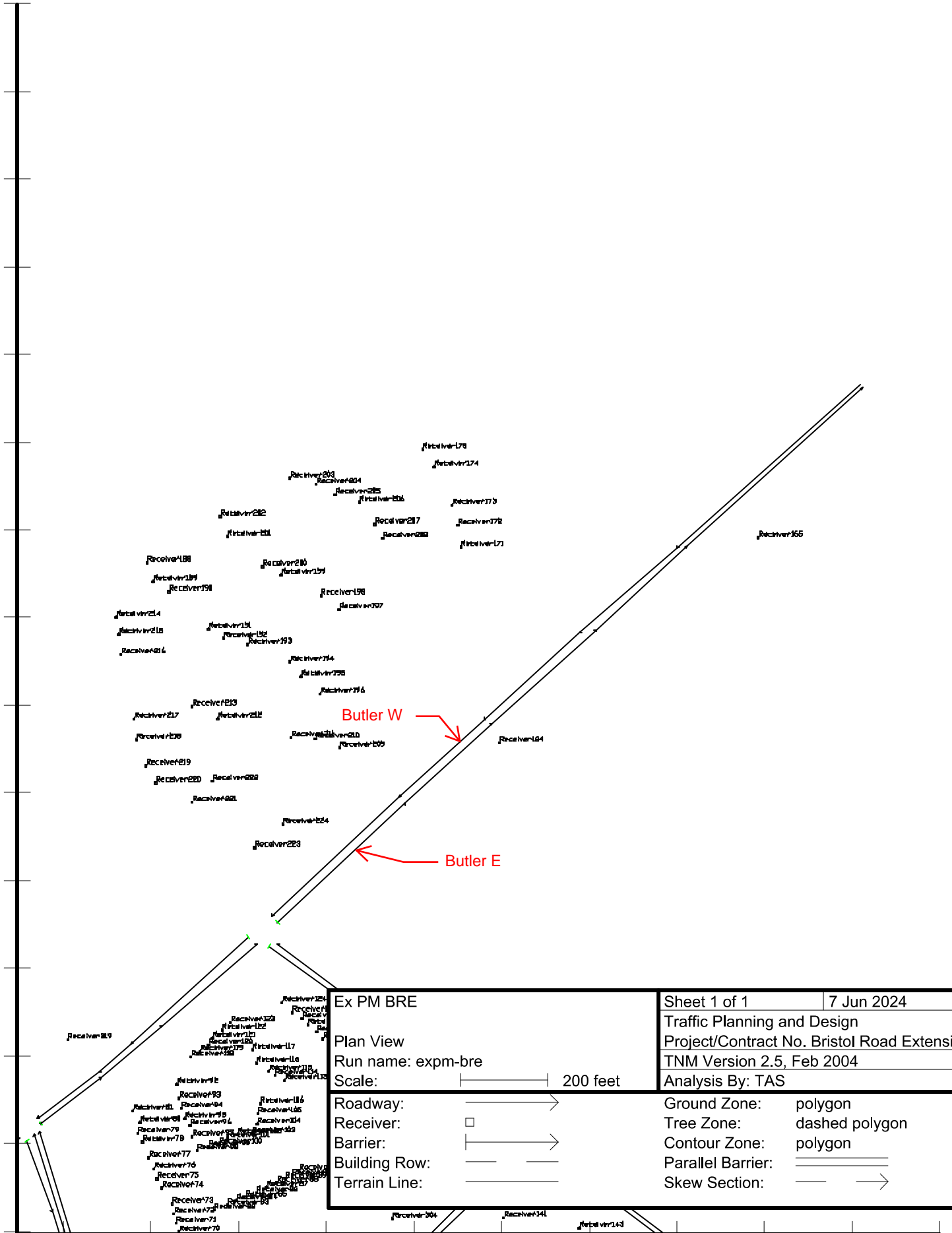
This includes the intersection of Main Street and Park Avenue with the side roads Chestnut Street (intersection with Park Ave) and Sunset Avenue (intersects with Main ST). The receivers in the model are residences, a graveyard, and a creamery (with outdoor seating).









#### Park, Ferry, Callowhill – PFC

This includes the intersection of Park Avenue, Ferry Road, and Callowhill Road with the sideroad Peace Valley Road (intersects with Callowhill). The receivers in this model are residences and a music studio.

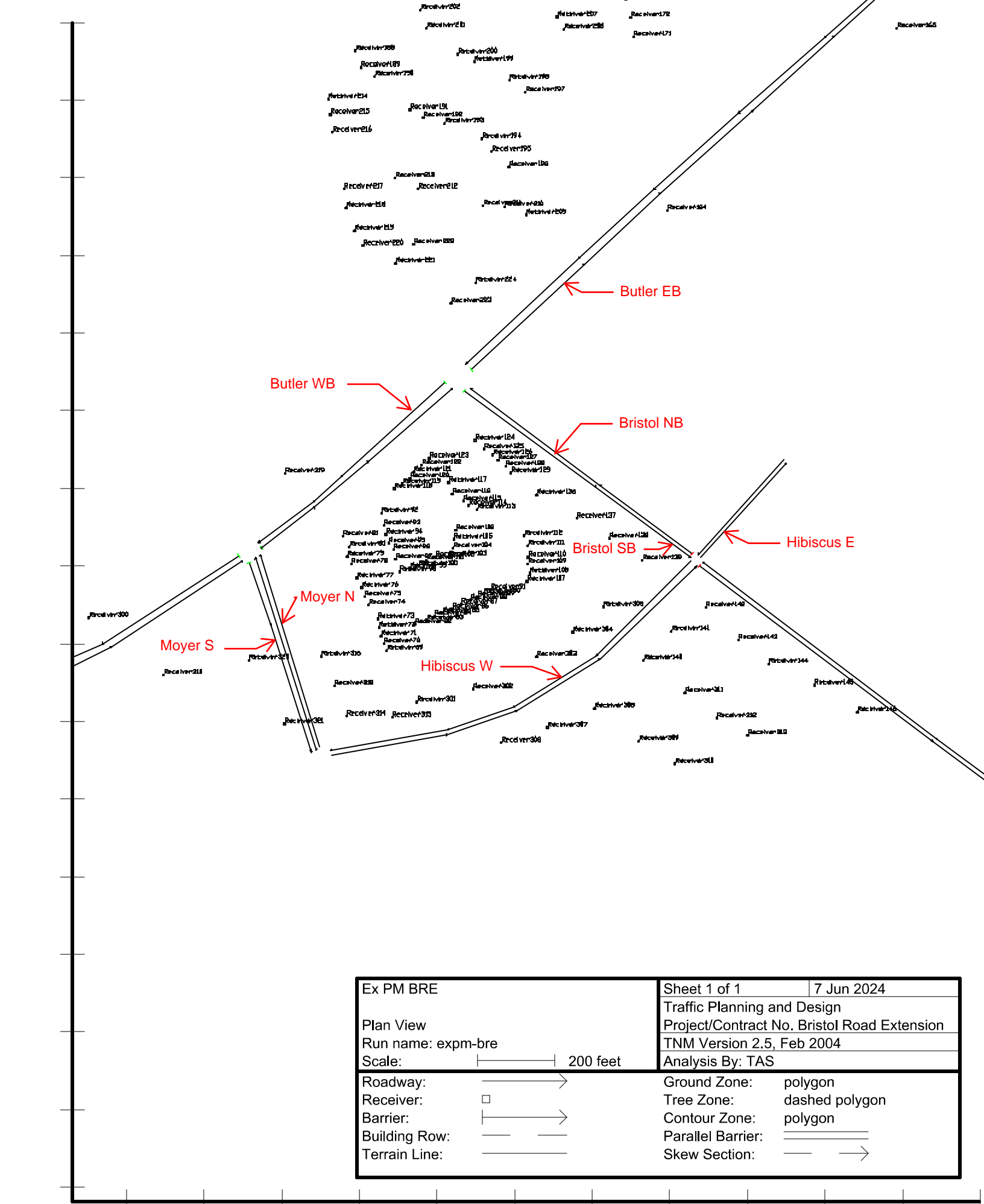










2678200      2678400      2678600      2678800      2679000      2679200      2679400



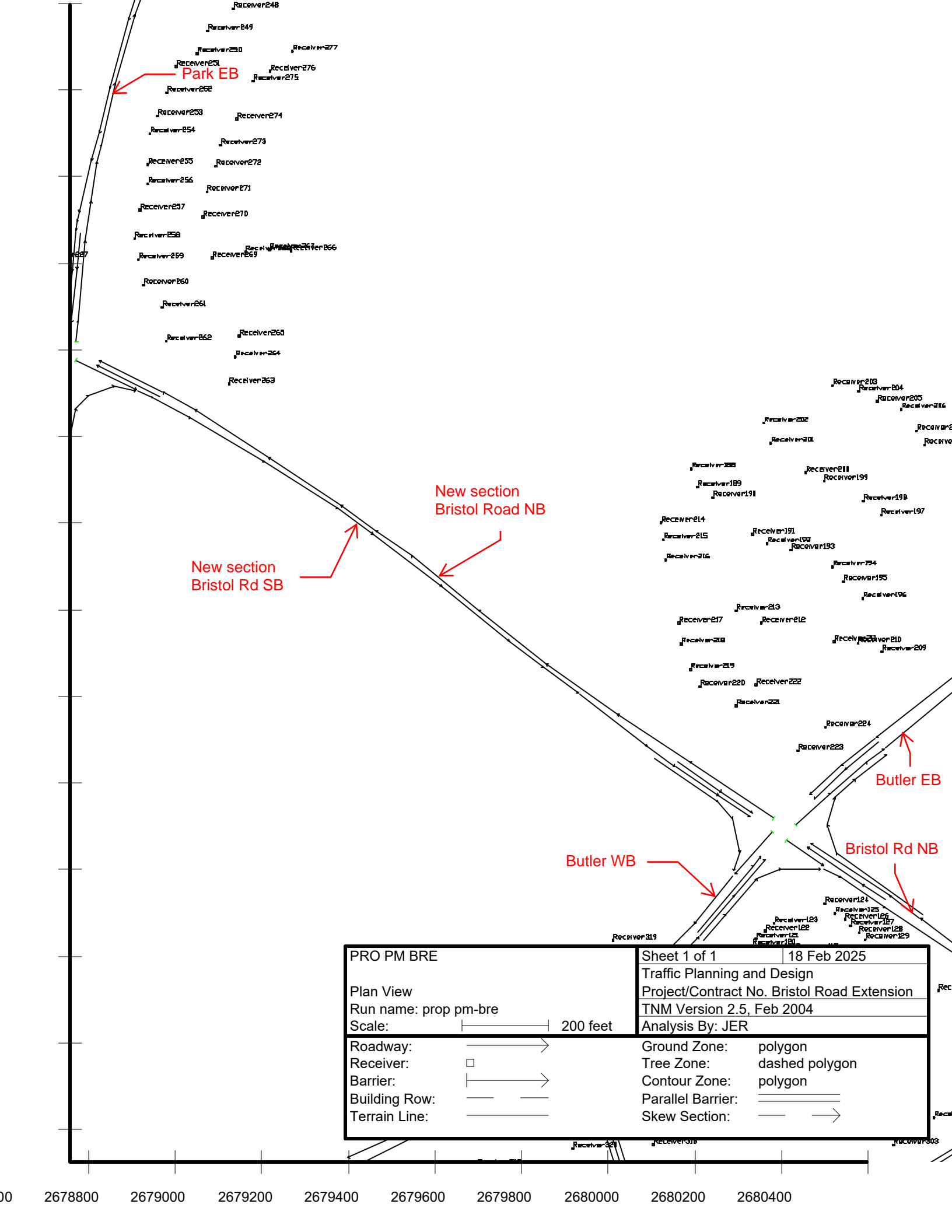
Ex PM BRE		Sheet 1 of 1	7 Jun 2024
Plan View		Traffic Planning and Design	
Run name: expm-bre		Project/Contract No. Bristol Road Extension	
Scale: 		TNM Version 2.5, Feb 2004	
Roadway: 		Ground Zone:	polygon
Receiver: 	Barrier: 	Tree Zone:	dashed polygon
Building Row: 	Terrain Line: 	Contour Zone:	polygon
		Parallel Barrier:	
		Skew Section:	

79800 2680000 2680200 2680400 2680600 2680800 2681000 2681200 2681400 2681600 2681800



Ex PM BRE		Sheet 1 of 1	7 Jun 2024
Plan View		Traffic Planning and Design	
Run name: expm-bre		Project/Contract No. Bristol Road Extension	
Scale: 		TNM Version 2.5, Feb 2004	
Roadway: 		Ground Zone:	polygon
Receiver:		Tree Zone:	dashed polygon
Barrier:		Contour Zone:	polygon
Building Row:		Parallel Barrier:	
Terrain Line:		Skew Section:	













New section  
Bristol Rd SB

New section  
Bristol Road NB

Butler EB

Butler WB

Bristol Rd NB

PRO PM BRE		Sheet 1 of 1		18 Feb 2025	
Plan View		Traffic Planning and Design			
Run name: prop pm-bre		Project/Contract No. Bristol Road Extension			
Scale:  200 feet		TNM Version 2.5, Feb 2004			
		Analysis By: JER			
Roadway:		Ground Zone:	polygon		
Receiver:		Tree Zone:	dashed polygon		
Barrier:		Contour Zone:	polygon		
Building Row:		Parallel Barrier:			
Terrain Line:		Skew Section:			

2678800 2679000 2679200 2679400 2679600 2679800 2680000 2680200 2680400

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Traffic Planning and Design												
JER												
7 February 2025												
TNM 2.5												
Calculated with TNM 2.5												
<b>RESULTS: SOUND LEVELS</b>												
<b>PROJECT/CONTRACT:</b>												
Bristol Road Extension												
<b>RUN:</b>												
Ex PM BRE												
<b>BARRIER DESIGN:</b>												
INPUT HEIGHTS												
Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.												
<b>ATMOSPHERICS:</b>												
68 deg F, 50% RH												
<b>Receiver</b>												
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>				<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>		<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>	<b>Noise Reduction</b>		
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>
							<b>Sub'l Inc</b>					<b>minus</b>
												<b>Goal</b>
			<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>		<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>
Receiver69	68	1	0.0	47.7	66	47.7	10	----	47.7	0.0	5	-5.0
Receiver70	69	1	0.0	48.2	66	48.2	10	----	48.2	0.0	5	-5.0
Receiver71	70	1	0.0	48.6	66	48.6	10	----	48.6	0.0	5	-5.0
Receiver72	71	1	0.0	48.9	66	48.9	10	----	48.9	0.0	5	-5.0
Receiver73	72	1	0.0	49.4	66	49.4	10	----	49.4	0.0	5	-5.0
Receiver74	73	1	0.0	50.5	66	50.5	10	----	50.5	0.0	5	-5.0
Receiver75	74	1	0.0	51.2	66	51.2	10	----	51.2	0.0	5	-5.0
Receiver76	75	1	0.0	52.0	66	52.0	10	----	52.0	0.0	5	-5.0
Receiver77	76	1	0.0	53.2	66	53.2	10	----	53.2	0.0	5	-5.0
Receiver78	77	1	0.0	55.2	66	55.2	10	----	55.2	0.0	5	-5.0
Receiver79	78	1	0.0	56.5	66	56.5	10	----	56.5	0.0	5	-5.0
Receiver80	79	1	0.0	57.6	66	57.6	10	----	57.6	0.0	5	-5.0
Receiver81	80	1	0.0	60.4	66	60.4	10	----	60.4	0.0	5	-5.0
Receiver82	81	1	0.0	48.4	66	48.4	10	----	48.4	0.0	5	-5.0
Receiver83	82	1	0.0	48.2	66	48.2	10	----	48.2	0.0	5	-5.0
Receiver84	83	1	0.0	48.4	66	48.4	10	----	48.4	0.0	5	-5.0
Receiver85	84	1	0.0	48.6	66	48.6	10	----	48.6	0.0	5	-5.0
Receiver86	85	1	0.0	48.6	66	48.6	10	----	48.6	0.0	5	-5.0
Receiver87	86	1	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
Receiver88	87	1	0.0	48.6	66	48.6	10	----	48.6	0.0	5	-5.0
Receiver89	88	1	0.0	48.7	66	48.7	10	----	48.7	0.0	5	-5.0
Receiver90	89	1	0.0	48.9	66	48.9	10	----	48.9	0.0	5	-5.0
Receiver91	90	1	0.0	49.2	66	49.2	10	----	49.2	0.0	5	-5.0
Receiver92	91	1	0.0	58.7	66	58.7	10	----	58.7	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver93	92	1	0.0	56.6	66	56.6	10	----	56.6	0.0	5	-5.0
Receiver94	93	1	0.0	55.3	66	55.3	10	----	55.3	0.0	5	-5.0
Receiver95	94	1	0.0	54.1	66	54.1	10	----	54.1	0.0	5	-5.0
Receiver96	95	1	0.0	53.3	66	53.3	10	----	53.3	0.0	5	-5.0
Receiver97	96	1	0.0	52.3	66	52.3	10	----	52.3	0.0	5	-5.0
Receiver98	97	1	0.0	51.3	66	51.3	10	----	51.3	0.0	5	-5.0
Receiver99	98	1	0.0	51.0	66	51.0	10	----	51.0	0.0	5	-5.0
Receiver100	99	1	0.0	50.7	66	50.7	10	----	50.7	0.0	5	-5.0
Receiver101	100	1	0.0	50.9	66	50.9	10	----	50.9	0.0	5	-5.0
Receiver102	101	1	0.0	50.8	66	50.8	10	----	50.8	0.0	5	-5.0
Receiver103	102	1	0.0	50.5	66	50.5	10	----	50.5	0.0	5	-5.0
Receiver104	103	1	0.0	50.8	66	50.8	10	----	50.8	0.0	5	-5.0
Receiver105	104	1	0.0	51.2	66	51.2	10	----	51.2	0.0	5	-5.0
Receiver106	105	1	0.0	51.9	66	51.9	10	----	51.9	0.0	5	-5.0
Receiver107	106	1	0.0	50.3	66	50.3	10	----	50.3	0.0	5	-5.0
Receiver108	107	1	0.0	50.8	66	50.8	10	----	50.8	0.0	5	-5.0
Receiver109	108	1	0.0	51.5	66	51.5	10	----	51.5	0.0	5	-5.0
Receiver110	109	1	0.0	51.9	66	51.9	10	----	51.9	0.0	5	-5.0
Receiver111	110	1	0.0	52.5	66	52.5	10	----	52.5	0.0	5	-5.0
Receiver112	111	1	0.0	52.6	66	52.6	10	----	52.6	0.0	5	-5.0
Receiver113	112	1	0.0	53.0	66	53.0	10	----	53.0	0.0	5	-5.0
Receiver114	113	1	0.0	53.2	66	53.2	10	----	53.2	0.0	5	-5.0
Receiver115	114	1	0.0	53.6	66	53.6	10	----	53.6	0.0	5	-5.0
Receiver116	115	1	0.0	54.4	66	54.4	10	----	54.4	0.0	5	-5.0
Receiver117	116	1	0.0	55.7	66	55.7	10	----	55.7	0.0	5	-5.0
Receiver118	117	1	0.0	61.0	66	61.0	10	----	61.0	0.0	5	-5.0
Receiver119	118	1	0.0	60.7	66	60.7	10	----	60.7	0.0	5	-5.0
Receiver120	119	1	0.0	60.5	66	60.5	10	----	60.5	0.0	5	-5.0
Receiver121	120	1	0.0	61.0	66	61.0	10	----	61.0	0.0	5	-5.0
Receiver122	121	1	0.0	60.9	66	60.9	10	----	60.9	0.0	5	-5.0
Receiver123	122	1	0.0	61.0	66	61.0	10	----	61.0	0.0	5	-5.0
Receiver124	123	1	0.0	61.1	66	61.1	10	----	61.1	0.0	5	-5.0
Receiver125	124	1	0.0	60.2	66	60.2	10	----	60.2	0.0	5	-5.0
Receiver126	125	1	0.0	60.3	66	60.3	10	----	60.3	0.0	5	-5.0
Receiver127	126	1	0.0	59.8	66	59.8	10	----	59.8	0.0	5	-5.0
Receiver128	127	1	0.0	59.6	66	59.6	10	----	59.6	0.0	5	-5.0
Receiver129	128	1	0.0	59.2	66	59.2	10	----	59.2	0.0	5	-5.0
Receiver137	136	1	0.0	59.5	66	59.5	10	----	59.5	0.0	5	-5.0
Receiver138	137	1	0.0	60.2	66	60.2	10	----	60.2	0.0	5	-5.0
Receiver139	138	1	0.0	61.0	66	61.0	10	----	61.0	0.0	5	-5.0
Receiver140	139	1	0.0	51.4	66	51.4	10	----	51.4	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver141	140	1	0.0	55.2	66	55.2	10	----	55.2	0.0	5	-5.0
Receiver142	141	1	0.0	61.9	66	61.9	10	----	61.9	0.0	5	-5.0
Receiver143	142	1	0.0	60.6	66	60.6	10	----	60.6	0.0	5	-5.0
Receiver144	143	1	0.0	60.0	66	60.0	10	----	60.0	0.0	5	-5.0
Receiver145	144	1	0.0	62.0	66	62.0	10	----	62.0	0.0	5	-5.0
Receiver146	145	1	0.0	63.5	66	63.5	10	----	63.5	0.0	5	-5.0
Receiver164	146	1	0.0	65.8	66	65.8	10	----	65.8	0.0	5	-5.0
Receiver165	147	1	0.0	61.8	66	61.8	10	----	61.8	0.0	5	-5.0
Receiver171	150	2	0.0	49.0	66	49.0	10	----	49.0	0.0	5	-5.0
Receiver172	151	2	0.0	47.9	66	47.9	10	----	47.9	0.0	5	-5.0
Receiver173	152	2	0.0	46.9	66	46.9	10	----	46.9	0.0	5	-5.0
Receiver174	153	2	0.0	45.1	66	45.1	10	----	45.1	0.0	5	-5.0
Receiver175	154	2	0.0	44.3	66	44.3	10	----	44.3	0.0	5	-5.0
Receiver188	155	2	0.0	42.9	66	42.9	10	----	42.9	0.0	5	-5.0
Receiver189	156	2	0.0	43.3	66	43.3	10	----	43.3	0.0	5	-5.0
Receiver190	157	2	0.0	43.5	66	43.5	10	----	43.5	0.0	5	-5.0
Receiver191	158	2	0.0	44.9	66	44.9	10	----	44.9	0.0	5	-5.0
Receiver192	159	2	0.0	45.4	66	45.4	10	----	45.4	0.0	5	-5.0
Receiver193	160	2	0.0	46.1	66	46.1	10	----	46.1	0.0	5	-5.0
Receiver194	161	2	0.0	47.9	66	47.9	10	----	47.9	0.0	5	-5.0
Receiver195	162	2	0.0	48.8	66	48.8	10	----	48.8	0.0	5	-5.0
Receiver196	163	2	0.0	50.4	66	50.4	10	----	50.4	0.0	5	-5.0
Receiver197	164	2	0.0	47.5	66	47.5	10	----	47.5	0.0	5	-5.0
Receiver198	165	2	0.0	46.6	66	46.6	10	----	46.6	0.0	5	-5.0
Receiver199	166	2	0.0	44.8	66	44.8	10	----	44.8	0.0	5	-5.0
Receiver200	169	2	0.0	44.1	66	44.1	10	----	44.1	0.0	5	-5.0
Receiver201	170	2	0.0	42.9	66	42.9	10	----	42.9	0.0	5	-5.0
Receiver202	171	2	0.0	42.6	66	42.6	10	----	42.6	0.0	5	-5.0
Receiver203	172	2	0.0	43.1	66	43.1	10	----	43.1	0.0	5	-5.0
Receiver204	173	2	0.0	43.4	66	43.4	10	----	43.4	0.0	5	-5.0
Receiver205	174	2	0.0	43.9	66	43.9	10	----	43.9	0.0	5	-5.0
Receiver206	175	2	0.0	44.5	66	44.5	10	----	44.5	0.0	5	-5.0
Receiver207	176	2	0.0	45.4	66	45.4	10	----	45.4	0.0	5	-5.0
Receiver208	177	2	0.0	46.1	66	46.1	10	----	46.1	0.0	5	-5.0
Receiver209	178	3	0.0	55.3	66	55.3	10	----	55.3	0.0	5	-5.0
Receiver210	179	3	0.0	52.7	66	52.7	10	----	52.7	0.0	5	-5.0
Receiver211	180	3	0.0	51.2	66	51.2	10	----	51.2	0.0	5	-5.0
Receiver212	181	6	0.0	47.3	66	47.3	10	----	47.3	0.0	5	-5.0
Receiver213	182	6	0.0	46.0	66	46.0	10	----	46.0	0.0	5	-5.0
Receiver214	183	4	0.0	43.5	66	43.5	10	----	43.5	0.0	5	-5.0
Receiver215	184	4	0.0	43.6	66	43.6	10	----	43.6	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver216	185	4	0.0	43.5	66	43.5	10	----	43.5	0.0	5	-5.0
Receiver217	186	4	0.0	45.3	66	45.3	10	----	45.3	0.0	5	-5.0
Receiver218	187	4	0.0	46.2	66	46.2	10	----	46.2	0.0	5	-5.0
Receiver219	188	4	0.0	47.2	66	47.2	10	----	47.2	0.0	5	-5.0
Receiver220	189	4	0.0	48.0	66	48.0	10	----	48.0	0.0	5	-5.0
Receiver221	190	4	0.0	49.8	66	49.8	10	----	49.8	0.0	5	-5.0
Receiver222	191	4	0.0	49.6	66	49.6	10	----	49.6	0.0	5	-5.0
Receiver223	192	4	0.0	57.2	66	57.2	10	----	57.2	0.0	5	-5.0
Receiver224	192	4	0.0	57.7	66	57.7	10	----	57.7	0.0	5	-5.0
Receiver225	193	4	0.0	49.9	66	49.9	10	----	49.9	0.0	5	-5.0
Receiver226	194	1	0.0	56.9	66	56.9	10	----	56.9	0.0	5	-5.0
Receiver244	195	1	0.0	58.7	66	58.7	10	----	58.7	0.0	5	-5.0
Receiver245	196	1	0.0	54.6	66	54.6	10	----	54.6	0.0	5	-5.0
Receiver246	197	1	0.0	52.2	66	52.2	10	----	52.2	0.0	5	-5.0
Receiver247	198	1	0.0	48.9	66	48.9	10	----	48.9	0.0	5	-5.0
Receiver248	199	1	0.0	50.1	66	50.1	10	----	50.1	0.0	5	-5.0
Receiver249	200	1	0.0	51.8	66	51.8	10	----	51.8	0.0	5	-5.0
Receiver250	201	1	0.0	51.9	66	51.9	10	----	51.9	0.0	5	-5.0
Receiver251	202	1	0.0	54.2	66	54.2	10	----	54.2	0.0	5	-5.0
Receiver252	203	1	0.0	54.7	66	54.7	10	----	54.7	0.0	5	-5.0
Receiver253	204	1	0.0	55.4	66	55.4	10	----	55.4	0.0	5	-5.0
Receiver254	205	1	0.0	56.1	66	56.1	10	----	56.1	0.0	5	-5.0
Receiver255	206	1	0.0	55.4	66	55.4	10	----	55.4	0.0	5	-5.0
Receiver256	207	1	0.0	54.7	66	54.7	10	----	54.7	0.0	5	-5.0
Receiver257	208	1	0.0	55.2	66	55.2	10	----	55.2	0.0	5	-5.0
Receiver258	209	1	0.0	55.4	66	55.4	10	----	55.4	0.0	5	-5.0
Receiver259	210	1	0.0	54.1	66	54.1	10	----	54.1	0.0	5	-5.0
Receiver260	211	1	0.0	53.0	66	53.0	10	----	53.0	0.0	5	-5.0
Receiver261	212	1	0.0	50.4	66	50.4	10	----	50.4	0.0	5	-5.0
Receiver262	213	1	0.0	49.4	66	49.4	10	----	49.4	0.0	5	-5.0
Receiver263	214	1	0.0	44.2	66	44.2	10	----	44.2	0.0	5	-5.0
Receiver264	215	1	0.0	44.2	66	44.2	10	----	44.2	0.0	5	-5.0
Receiver265	216	1	0.0	44.1	66	44.1	10	----	44.1	0.0	5	-5.0
Receiver266	217	1	0.0	42.7	66	42.7	10	----	42.7	0.0	5	-5.0
Receiver267	218	1	0.0	43.6	66	43.6	10	----	43.6	0.0	5	-5.0
Receiver268	219	1	0.0	44.7	66	44.7	10	----	44.7	0.0	5	-5.0
Receiver269	220	1	0.0	46.6	66	46.6	10	----	46.6	0.0	5	-5.0
Receiver270	221	1	0.0	47.7	66	47.7	10	----	47.7	0.0	5	-5.0
Receiver271	222	1	0.0	47.7	66	47.7	10	----	47.7	0.0	5	-5.0
Receiver272	223	1	0.0	47.6	66	47.6	10	----	47.6	0.0	5	-5.0
Receiver273	224	1	0.0	47.4	66	47.4	10	----	47.4	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver274	225	1	0.0	46.6	66	46.6	10	----	46.6	0.0	5	-5.0
Receiver275	226	1	0.0	46.2	66	46.2	10	----	46.2	0.0	5	-5.0
Receiver276	227	1	0.0	45.2	66	45.2	10	----	45.2	0.0	5	-5.0
Receiver277	228	1	0.0	44.4	66	44.4	10	----	44.4	0.0	5	-5.0
Receiver300	237	1	0.0	62.4	66	62.4	10	----	62.4	0.0	5	-5.0
Receiver301	238	1	0.0	48.1	66	48.1	10	----	48.1	0.0	5	-5.0
Receiver302	239	1	0.0	48.3	66	48.3	10	----	48.3	0.0	5	-5.0
Receiver303	240	1	0.0	48.9	66	48.9	10	----	48.9	0.0	5	-5.0
Receiver304	241	1	0.0	50.0	66	50.0	10	----	50.0	0.0	5	-5.0
Receiver305	242	1	0.0	52.4	66	52.4	10	----	52.4	0.0	5	-5.0
Receiver306	243	1	0.0	48.9	66	48.9	10	----	48.9	0.0	5	-5.0
Receiver307	244	1	0.0	48.2	66	48.2	10	----	48.2	0.0	5	-5.0
Receiver308	245	1	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
Receiver309	246	1	0.0	46.8	66	46.8	10	----	46.8	0.0	5	-5.0
Receiver311	248	1	0.0	50.2	66	50.2	10	----	50.2	0.0	5	-5.0
Receiver312	249	1	0.0	50.2	66	50.2	10	----	50.2	0.0	5	-5.0
Receiver313	250	1	0.0	50.8	66	50.8	10	----	50.8	0.0	5	-5.0
Receiver314	251	1	0.0	49.4	66	49.4	10	----	49.4	0.0	5	-5.0
Receiver315	299	1	0.0	49.8	66	49.8	10	----	49.8	0.0	5	-5.0
Receiver319	301	1	0.0	60.7	66	60.7	10	----	60.7	0.0	5	-5.0
Receiver310	303	1	0.0	57.0	66	57.0	10	----	57.0	0.0	5	-5.0
Receiver320	304	1	0.0	52.9	66	52.9	10	----	52.9	0.0	5	-5.0
Receiver321	305	1	0.0	52.2	66	52.2	10	----	52.2	0.0	5	-5.0
Receiver318	307	1	0.0	49.8	66	49.8	10	----	49.8	0.0	5	-5.0
Receiver316	308	1	0.0	51.4	66	51.4	10	----	51.4	0.0	5	-5.0
Receiver227	310	2	0.0	59.3	66	59.3	10	----	59.3	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									
			<b>Min</b>	<b>Avg</b>	<b>Max</b>							
			<b>dB</b>	<b>dB</b>	<b>dB</b>							
All Selected		252	0.0	0.0	0.0							
All Impacted		0	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Traffic Planning and Design													
JER													
7 February 2025													
TNM 2.5													
Calculated with TNM 2.5													
<b>RESULTS: SOUND LEVELS</b>													
<b>PROJECT/CONTRACT:</b>													
Bristol Road Extension													
<b>RUN:</b>													
No Build PM-BRE													
<b>BARRIER DESIGN:</b>													
INPUT HEIGHTS													
Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.													
<b>ATMOSPHERICS:</b>													
68 deg F, 50% RH													
<b>Receiver</b>													
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>					<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>	<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>	<b>Noise Reduction</b>				
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>	
							<b>Sub'l Inc</b>					<b>minus</b>	
												<b>Goal</b>	
			<b>dBA</b>	<b>dBA</b>	<b>dBA</b>	<b>dB</b>	<b>dB</b>		<b>dBA</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>	
Receiver69	68	1	47.7	48.2	66	0.5	10	----	48.2	0.0	5	-5.0	
Receiver70	69	1	48.2	48.7	66	0.5	10	----	48.7	0.0	5	-5.0	
Receiver71	70	1	48.6	49.1	66	0.5	10	----	49.1	0.0	5	-5.0	
Receiver72	71	1	48.9	49.4	66	0.5	10	----	49.4	0.0	5	-5.0	
Receiver73	72	1	49.4	49.8	66	0.4	10	----	49.8	0.0	5	-5.0	
Receiver74	73	1	50.5	51.0	66	0.5	10	----	51.0	0.0	5	-5.0	
Receiver75	74	1	51.2	51.7	66	0.5	10	----	51.7	0.0	5	-5.0	
Receiver76	75	1	52.0	52.5	66	0.5	10	----	52.5	0.0	5	-5.0	
Receiver77	76	1	53.2	53.7	66	0.5	10	----	53.7	0.0	5	-5.0	
Receiver78	77	1	55.2	55.6	66	0.4	10	----	55.6	0.0	5	-5.0	
Receiver79	78	1	56.5	57.0	66	0.5	10	----	57.0	0.0	5	-5.0	
Receiver80	79	1	57.6	58.1	66	0.5	10	----	58.1	0.0	5	-5.0	
Receiver81	80	1	60.4	60.9	66	0.5	10	----	60.9	0.0	5	-5.0	
Receiver82	81	1	48.4	48.9	66	0.5	10	----	48.9	0.0	5	-5.0	
Receiver83	82	1	48.2	48.7	66	0.5	10	----	48.7	0.0	5	-5.0	
Receiver84	83	1	48.4	48.9	66	0.5	10	----	48.9	0.0	5	-5.0	
Receiver85	84	1	48.6	49.1	66	0.5	10	----	49.1	0.0	5	-5.0	
Receiver86	85	1	48.6	49.1	66	0.5	10	----	49.1	0.0	5	-5.0	
Receiver87	86	1	48.5	49.0	66	0.5	10	----	49.0	0.0	5	-5.0	
Receiver88	87	1	48.6	49.1	66	0.5	10	----	49.1	0.0	5	-5.0	
Receiver89	88	1	48.7	49.2	66	0.5	10	----	49.2	0.0	5	-5.0	
Receiver90	89	1	48.9	49.5	66	0.6	10	----	49.5	0.0	5	-5.0	
Receiver91	90	1	49.2	49.7	66	0.5	10	----	49.7	0.0	5	-5.0	
Receiver92	91	1	58.7	59.1	66	0.4	10	----	59.1	0.0	5	-5.0	

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver93	92	1	56.6	57.0	66	0.4	10	----	57.0	0.0	5	-5.0
Receiver94	93	1	55.3	55.7	66	0.4	10	----	55.7	0.0	5	-5.0
Receiver95	94	1	54.1	54.5	66	0.4	10	----	54.5	0.0	5	-5.0
Receiver96	95	1	53.3	53.7	66	0.4	10	----	53.7	0.0	5	-5.0
Receiver97	96	1	52.3	52.8	66	0.5	10	----	52.8	0.0	5	-5.0
Receiver98	97	1	51.3	51.7	66	0.4	10	----	51.7	0.0	5	-5.0
Receiver99	98	1	51.0	51.5	66	0.5	10	----	51.5	0.0	5	-5.0
Receiver100	99	1	50.7	51.2	66	0.5	10	----	51.2	0.0	5	-5.0
Receiver101	100	1	50.9	51.3	66	0.4	10	----	51.3	0.0	5	-5.0
Receiver102	101	1	50.8	51.3	66	0.5	10	----	51.3	0.0	5	-5.0
Receiver103	102	1	50.5	51.0	66	0.5	10	----	51.0	0.0	5	-5.0
Receiver104	103	1	50.8	51.3	66	0.5	10	----	51.3	0.0	5	-5.0
Receiver105	104	1	51.2	51.7	66	0.5	10	----	51.7	0.0	5	-5.0
Receiver106	105	1	51.9	52.4	66	0.5	10	----	52.4	0.0	5	-5.0
Receiver107	106	1	50.3	50.9	66	0.6	10	----	50.9	0.0	5	-5.0
Receiver108	107	1	50.8	51.3	66	0.5	10	----	51.3	0.0	5	-5.0
Receiver109	108	1	51.5	52.0	66	0.5	10	----	52.0	0.0	5	-5.0
Receiver110	109	1	51.9	52.5	66	0.6	10	----	52.5	0.0	5	-5.0
Receiver111	110	1	52.5	53.1	66	0.6	10	----	53.1	0.0	5	-5.0
Receiver112	111	1	52.6	53.1	66	0.5	10	----	53.1	0.0	5	-5.0
Receiver113	112	1	53.0	53.6	66	0.6	10	----	53.6	0.0	5	-5.0
Receiver114	113	1	53.2	53.7	66	0.5	10	----	53.7	0.0	5	-5.0
Receiver115	114	1	53.6	54.1	66	0.5	10	----	54.1	0.0	5	-5.0
Receiver116	115	1	54.4	54.9	66	0.5	10	----	54.9	0.0	5	-5.0
Receiver117	116	1	55.7	56.1	66	0.4	10	----	56.1	0.0	5	-5.0
Receiver118	117	1	61.0	61.5	66	0.5	10	----	61.5	0.0	5	-5.0
Receiver119	118	1	60.7	61.1	66	0.4	10	----	61.1	0.0	5	-5.0
Receiver120	119	1	60.5	60.9	66	0.4	10	----	60.9	0.0	5	-5.0
Receiver121	120	1	61.0	61.5	66	0.5	10	----	61.5	0.0	5	-5.0
Receiver122	121	1	60.9	61.4	66	0.5	10	----	61.4	0.0	5	-5.0
Receiver123	122	1	61.0	61.5	66	0.5	10	----	61.5	0.0	5	-5.0
Receiver124	123	1	61.1	61.6	66	0.5	10	----	61.6	0.0	5	-5.0
Receiver125	124	1	60.2	60.8	66	0.6	10	----	60.8	0.0	5	-5.0
Receiver126	125	1	60.3	60.8	66	0.5	10	----	60.8	0.0	5	-5.0
Receiver127	126	1	59.8	60.4	66	0.6	10	----	60.4	0.0	5	-5.0
Receiver128	127	1	59.6	60.2	66	0.6	10	----	60.2	0.0	5	-5.0
Receiver129	128	1	59.2	59.7	66	0.5	10	----	59.7	0.0	5	-5.0
Receiver137	136	1	59.5	60.1	66	0.6	10	----	60.1	0.0	5	-5.0
Receiver138	137	1	60.2	60.8	66	0.6	10	----	60.8	0.0	5	-5.0
Receiver139	138	1	61.0	61.5	66	0.5	10	----	61.5	0.0	5	-5.0
Receiver140	139	1	51.4	52.0	66	0.6	10	----	52.0	0.0	5	-5.0



**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver141	140	1	55.2	55.7	66	0.5	10	----	55.7	0.0	5	-5.0
Receiver142	141	1	61.9	62.4	66	0.5	10	----	62.4	0.0	5	-5.0
Receiver143	142	1	60.6	61.2	66	0.6	10	----	61.2	0.0	5	-5.0
Receiver144	143	1	60.0	60.6	66	0.6	10	----	60.6	0.0	5	-5.0
Receiver145	144	1	62.0	62.6	66	0.6	10	----	62.6	0.0	5	-5.0
Receiver146	145	1	63.5	64.1	66	0.6	10	----	64.1	0.0	5	-5.0
Receiver164	146	1	65.8	66.4	66	0.6	10	Snd Lvl	66.4	0.0	5	-5.0
Receiver165	147	1	61.8	62.3	66	0.5	10	----	62.3	0.0	5	-5.0
Receiver171	150	2	49.0	49.5	66	0.5	10	----	49.5	0.0	5	-5.0
Receiver172	151	2	47.9	48.4	66	0.5	10	----	48.4	0.0	5	-5.0
Receiver173	152	2	46.9	47.4	66	0.5	10	----	47.4	0.0	5	-5.0
Receiver174	153	2	45.1	45.6	66	0.5	10	----	45.6	0.0	5	-5.0
Receiver175	154	2	44.3	44.8	66	0.5	10	----	44.8	0.0	5	-5.0
Receiver188	155	2	42.9	43.5	66	0.6	10	----	43.5	0.0	5	-5.0
Receiver189	156	2	43.3	43.9	66	0.6	10	----	43.9	0.0	5	-5.0
Receiver190	157	2	43.5	44.0	66	0.5	10	----	44.0	0.0	5	-5.0
Receiver191	158	2	44.9	45.5	66	0.6	10	----	45.5	0.0	5	-5.0
Receiver192	159	2	45.4	45.9	66	0.5	10	----	45.9	0.0	5	-5.0
Receiver193	160	2	46.1	46.7	66	0.6	10	----	46.7	0.0	5	-5.0
Receiver194	161	2	47.9	48.4	66	0.5	10	----	48.4	0.0	5	-5.0
Receiver195	162	2	48.8	49.3	66	0.5	10	----	49.3	0.0	5	-5.0
Receiver196	163	2	50.4	50.9	66	0.5	10	----	50.9	0.0	5	-5.0
Receiver197	164	2	47.5	48.0	66	0.5	10	----	48.0	0.0	5	-5.0
Receiver198	165	2	46.6	47.1	66	0.5	10	----	47.1	0.0	5	-5.0
Receiver199	166	2	44.8	45.3	66	0.5	10	----	45.3	0.0	5	-5.0
Receiver200	169	2	44.1	44.7	66	0.6	10	----	44.7	0.0	5	-5.0
Receiver201	170	2	42.9	43.4	66	0.5	10	----	43.4	0.0	5	-5.0
Receiver202	171	2	42.6	43.2	66	0.6	10	----	43.2	0.0	5	-5.0
Receiver203	172	2	43.1	43.6	66	0.5	10	----	43.6	0.0	5	-5.0
Receiver204	173	2	43.4	44.0	66	0.6	10	----	44.0	0.0	5	-5.0
Receiver205	174	2	43.9	44.4	66	0.5	10	----	44.4	0.0	5	-5.0
Receiver206	175	2	44.5	45.0	66	0.5	10	----	45.0	0.0	5	-5.0
Receiver207	176	2	45.4	46.0	66	0.6	10	----	46.0	0.0	5	-5.0
Receiver208	177	2	46.1	46.6	66	0.5	10	----	46.6	0.0	5	-5.0
Receiver209	178	3	55.3	55.8	66	0.5	10	----	55.8	0.0	5	-5.0
Receiver210	179	3	52.7	53.3	66	0.6	10	----	53.3	0.0	5	-5.0
Receiver211	180	3	51.2	51.7	66	0.5	10	----	51.7	0.0	5	-5.0
Receiver212	181	6	47.3	47.8	66	0.5	10	----	47.8	0.0	5	-5.0
Receiver213	182	6	46.0	46.5	66	0.5	10	----	46.5	0.0	5	-5.0
Receiver214	183	4	43.5	44.0	66	0.5	10	----	44.0	0.0	5	-5.0
Receiver215	184	4	43.6	44.1	66	0.5	10	----	44.1	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver216	185	4	43.5	44.0	66	0.5	10	----	44.0	0.0	5	-5.0
Receiver217	186	4	45.3	45.8	66	0.5	10	----	45.8	0.0	5	-5.0
Receiver218	187	4	46.2	46.7	66	0.5	10	----	46.7	0.0	5	-5.0
Receiver219	188	4	47.2	47.7	66	0.5	10	----	47.7	0.0	5	-5.0
Receiver220	189	4	48.0	48.5	66	0.5	10	----	48.5	0.0	5	-5.0
Receiver221	190	4	49.8	50.3	66	0.5	10	----	50.3	0.0	5	-5.0
Receiver222	191	4	49.6	50.1	66	0.5	10	----	50.1	0.0	5	-5.0
Receiver223	192	4	57.2	57.7	66	0.5	10	----	57.7	0.0	5	-5.0
Receiver224	193	4	57.7	58.2	66	0.5	10	----	58.2	0.0	5	-5.0
Receiver225	194	1	49.9	50.4	66	0.5	10	----	50.4	0.0	5	-5.0
Receiver226	195	1	56.9	57.4	66	0.5	10	----	57.4	0.0	5	-5.0
Receiver244	196	1	58.7	59.2	66	0.5	10	----	59.2	0.0	5	-5.0
Receiver245	197	1	54.6	55.2	66	0.6	10	----	55.2	0.0	5	-5.0
Receiver246	198	1	52.2	52.8	66	0.6	10	----	52.8	0.0	5	-5.0
Receiver247	199	1	48.9	49.5	66	0.6	10	----	49.5	0.0	5	-5.0
Receiver248	200	1	50.1	50.7	66	0.6	10	----	50.7	0.0	5	-5.0
Receiver249	201	1	51.8	52.3	66	0.5	10	----	52.3	0.0	5	-5.0
Receiver250	202	1	51.9	52.4	66	0.5	10	----	52.4	0.0	5	-5.0
Receiver251	203	1	54.2	54.8	66	0.6	10	----	54.8	0.0	5	-5.0
Receiver252	204	1	54.7	55.2	66	0.5	10	----	55.2	0.0	5	-5.0
Receiver253	205	1	55.4	56.0	66	0.6	10	----	56.0	0.0	5	-5.0
Receiver254	206	1	56.1	56.7	66	0.6	10	----	56.7	0.0	5	-5.0
Receiver255	207	1	55.4	55.9	66	0.5	10	----	55.9	0.0	5	-5.0
Receiver256	208	1	54.7	55.2	66	0.5	10	----	55.2	0.0	5	-5.0
Receiver257	209	1	55.2	55.8	66	0.6	10	----	55.8	0.0	5	-5.0
Receiver258	210	1	55.4	55.9	66	0.5	10	----	55.9	0.0	5	-5.0
Receiver259	211	1	54.1	54.6	66	0.5	10	----	54.6	0.0	5	-5.0
Receiver260	212	1	53.0	53.5	66	0.5	10	----	53.5	0.0	5	-5.0
Receiver261	213	1	50.4	50.9	66	0.5	10	----	50.9	0.0	5	-5.0
Receiver262	214	1	49.4	49.8	66	0.4	10	----	49.8	0.0	5	-5.0
Receiver263	215	1	44.2	44.5	66	0.3	10	----	44.5	0.0	5	-5.0
Receiver264	216	1	44.2	44.6	66	0.4	10	----	44.6	0.0	5	-5.0
Receiver265	217	1	44.1	44.6	66	0.5	10	----	44.6	0.0	5	-5.0
Receiver266	218	1	42.7	43.2	66	0.5	10	----	43.2	0.0	5	-5.0
Receiver267	219	1	43.6	44.1	66	0.5	10	----	44.1	0.0	5	-5.0
Receiver268	220	1	44.7	45.2	66	0.5	10	----	45.2	0.0	5	-5.0
Receiver269	221	1	46.6	47.1	66	0.5	10	----	47.1	0.0	5	-5.0
Receiver270	222	1	47.7	48.2	66	0.5	10	----	48.2	0.0	5	-5.0
Receiver271	223	1	47.7	48.3	66	0.6	10	----	48.3	0.0	5	-5.0
Receiver272	224	1	47.6	48.1	66	0.5	10	----	48.1	0.0	5	-5.0
Receiver273	225	1	47.4	47.9	66	0.5	10	----	47.9	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver274	226	1	46.6	47.1	66	0.5	10	----	47.1	0.0	5	-5.0
Receiver275	227	1	46.2	46.8	66	0.6	10	----	46.8	0.0	5	-5.0
Receiver276	228	1	45.2	45.8	66	0.6	10	----	45.8	0.0	5	-5.0
Receiver277	229	1	44.4	45.0	66	0.6	10	----	45.0	0.0	5	-5.0
Receiver300	238	1	62.4	62.9	66	0.5	10	----	62.9	0.0	5	-5.0
Receiver301	239	1	48.1	48.7	66	0.6	10	----	48.7	0.0	5	-5.0
Receiver302	240	1	48.3	48.8	66	0.5	10	----	48.8	0.0	5	-5.0
Receiver303	241	1	48.9	49.4	66	0.5	10	----	49.4	0.0	5	-5.0
Receiver304	242	1	50.0	50.6	66	0.6	10	----	50.6	0.0	5	-5.0
Receiver305	243	1	52.4	52.9	66	0.5	10	----	52.9	0.0	5	-5.0
Receiver306	244	1	48.9	49.5	66	0.6	10	----	49.5	0.0	5	-5.0
Receiver307	245	1	48.2	48.7	66	0.5	10	----	48.7	0.0	5	-5.0
Receiver308	246	1	48.5	49.0	66	0.5	10	----	49.0	0.0	5	-5.0
Receiver309	247	1	46.8	47.4	66	0.6	10	----	47.4	0.0	5	-5.0
Receiver311	249	1	50.2	50.8	66	0.6	10	----	50.8	0.0	5	-5.0
Receiver312	250	1	50.2	50.7	66	0.5	10	----	50.7	0.0	5	-5.0
Receiver313	251	1	50.8	51.4	66	0.6	10	----	51.4	0.0	5	-5.0
Receiver314	298	1	49.4	49.9	66	0.5	10	----	49.9	0.0	5	-5.0
Receiver315	299	1	49.8	50.3	66	0.5	10	----	50.3	0.0	5	-5.0
Receiver319	301	1	0.0	61.2	66	61.2	10	----	61.2	0.0	5	-5.0
Receiver310	302	1	0.0	57.6	66	57.6	10	----	57.6	0.0	5	-5.0
Receiver320	303	1	0.0	53.5	66	53.5	10	----	53.5	0.0	5	-5.0
Receiver321	304	1	0.0	52.7	66	52.7	10	----	52.7	0.0	5	-5.0
Receiver318	305	1	0.0	50.3	66	50.3	10	----	50.3	0.0	5	-5.0
Receiver316	306	1	0.0	51.9	66	51.9	10	----	51.9	0.0	5	-5.0
Receiver227	308	2	0.0	59.9	66	59.9	10	----	59.9	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									
			<b>Min</b>	<b>Avg</b>	<b>Max</b>							
			<b>dB</b>	<b>dB</b>	<b>dB</b>							
All Selected		249	0.0	0.0	0.0							
All Impacted		1	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Traffic Planning and Design													
JER													
7 February 2025													
TNM 2.5													
Calculated with TNM 2.5													
<b>RESULTS: SOUND LEVELS</b>													
<b>PROJECT/CONTRACT:</b>													
Bristol Road Extension													
<b>RUN:</b>													
PRO PM BRE													
<b>BARRIER DESIGN:</b>													
INPUT HEIGHTS													
Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.													
<b>ATMOSPHERICS:</b>													
68 deg F, 50% RH													
<b>Receiver</b>													
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>					<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>	<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>	<b>Noise Reduction</b>				
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>	
							<b>Sub'l Inc</b>					<b>minus</b>	
												<b>Goal</b>	
			<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>		<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>	
Receiver69	68	1	47.7	49.2	66	1.5	10	----	49.2	0.0	5	-5.0	
Receiver70	69	1	48.2	49.6	66	1.4	10	----	49.6	0.0	5	-5.0	
Receiver71	70	1	48.6	50.1	66	1.5	10	----	50.1	0.0	5	-5.0	
Receiver72	71	1	48.9	50.4	66	1.5	10	----	50.4	0.0	5	-5.0	
Receiver73	72	1	49.4	50.9	66	1.5	10	----	50.9	0.0	5	-5.0	
Receiver74	73	1	50.5	51.8	66	1.3	10	----	51.8	0.0	5	-5.0	
Receiver75	74	1	51.2	52.5	66	1.3	10	----	52.5	0.0	5	-5.0	
Receiver76	75	1	52.0	53.1	66	1.1	10	----	53.1	0.0	5	-5.0	
Receiver77	76	1	53.2	53.8	66	0.6	10	----	53.8	0.0	5	-5.0	
Receiver78	77	1	55.2	55.1	66	-0.1	10	----	55.1	0.0	5	-5.0	
Receiver79	78	1	56.5	56.0	66	-0.5	10	----	56.0	0.0	5	-5.0	
Receiver80	79	1	57.6	56.9	66	-0.7	10	----	56.9	0.0	5	-5.0	
Receiver81	80	1	60.4	59.1	66	-1.3	10	----	59.1	0.0	5	-5.0	
Receiver82	81	1	48.4	49.8	66	1.4	10	----	49.8	0.0	5	-5.0	
Receiver83	82	1	48.2	49.7	66	1.5	10	----	49.7	0.0	5	-5.0	
Receiver84	83	1	48.4	49.7	66	1.3	10	----	49.7	0.0	5	-5.0	
Receiver85	84	1	48.6	49.8	66	1.2	10	----	49.8	0.0	5	-5.0	
Receiver86	85	1	48.6	49.8	66	1.2	10	----	49.8	0.0	5	-5.0	
Receiver87	86	1	48.5	49.6	66	1.1	10	----	49.6	0.0	5	-5.0	
Receiver88	87	1	48.6	49.7	66	1.1	10	----	49.7	0.0	5	-5.0	
Receiver89	88	1	48.7	49.8	66	1.1	10	----	49.8	0.0	5	-5.0	
Receiver90	89	1	48.9	50.0	66	1.1	10	----	50.0	0.0	5	-5.0	
Receiver91	90	1	49.2	50.1	66	0.9	10	----	50.1	0.0	5	-5.0	
Receiver92	91	1	58.7	57.8	66	-0.9	10	----	57.8	0.0	5	-5.0	

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver93	92	1	56.6	56.3	66	-0.3	10	----	56.3	0.0	5	-5.0
Receiver94	93	1	55.3	55.4	66	0.1	10	----	55.4	0.0	5	-5.0
Receiver95	94	1	54.1	54.7	66	0.6	10	----	54.7	0.0	5	-5.0
Receiver96	95	1	53.3	54.2	66	0.9	10	----	54.2	0.0	5	-5.0
Receiver97	96	1	52.3	53.6	66	1.3	10	----	53.6	0.0	5	-5.0
Receiver98	97	1	51.3	52.8	66	1.5	10	----	52.8	0.0	5	-5.0
Receiver99	98	1	51.0	52.6	66	1.6	10	----	52.6	0.0	5	-5.0
Receiver100	99	1	50.7	52.5	66	1.8	10	----	52.5	0.0	5	-5.0
Receiver101	100	1	50.9	52.6	66	1.7	10	----	52.6	0.0	5	-5.0
Receiver102	101	1	50.8	52.5	66	1.7	10	----	52.5	0.0	5	-5.0
Receiver103	102	1	50.5	52.0	66	1.5	10	----	52.0	0.0	5	-5.0
Receiver104	103	1	50.8	52.3	66	1.5	10	----	52.3	0.0	5	-5.0
Receiver105	104	1	51.2	52.9	66	1.7	10	----	52.9	0.0	5	-5.0
Receiver106	105	1	51.9	53.3	66	1.4	10	----	53.3	0.0	5	-5.0
Receiver107	106	1	50.3	50.6	66	0.3	10	----	50.6	0.0	5	-5.0
Receiver108	107	1	50.8	50.9	66	0.1	10	----	50.9	0.0	5	-5.0
Receiver109	108	1	51.5	51.3	66	-0.2	10	----	51.3	0.0	5	-5.0
Receiver110	109	1	51.9	51.8	66	-0.1	10	----	51.8	0.0	5	-5.0
Receiver111	110	1	52.5	52.4	66	-0.1	10	----	52.4	0.0	5	-5.0
Receiver112	111	1	52.6	53.0	66	0.4	10	----	53.0	0.0	5	-5.0
Receiver113	112	1	53.0	54.3	66	1.3	10	----	54.3	0.0	5	-5.0
Receiver114	113	1	53.2	54.9	66	1.7	10	----	54.9	0.0	5	-5.0
Receiver115	114	1	53.6	55.2	66	1.6	10	----	55.2	0.0	5	-5.0
Receiver116	115	1	54.4	55.8	66	1.4	10	----	55.8	0.0	5	-5.0
Receiver117	116	1	55.7	56.6	66	0.9	10	----	56.6	0.0	5	-5.0
Receiver118	117	1	61.0	59.7	66	-1.3	10	----	59.7	0.0	5	-5.0
Receiver119	118	1	60.7	59.4	66	-1.3	10	----	59.4	0.0	5	-5.0
Receiver120	119	1	60.5	59.3	66	-1.2	10	----	59.3	0.0	5	-5.0
Receiver121	120	1	61.0	59.7	66	-1.3	10	----	59.7	0.0	5	-5.0
Receiver122	121	1	60.9	59.8	66	-1.1	10	----	59.8	0.0	5	-5.0
Receiver123	122	1	61.0	59.9	66	-1.1	10	----	59.9	0.0	5	-5.0
Receiver124	123	1	61.1	61.5	66	0.4	10	----	61.5	0.0	5	-5.0
Receiver125	124	1	60.2	60.5	66	0.3	10	----	60.5	0.0	5	-5.0
Receiver126	125	1	60.3	60.2	66	-0.1	10	----	60.2	0.0	5	-5.0
Receiver127	126	1	59.8	59.7	66	-0.1	10	----	59.7	0.0	5	-5.0
Receiver128	127	1	59.6	59.2	66	-0.4	10	----	59.2	0.0	5	-5.0
Receiver129	128	1	59.2	58.6	66	-0.6	10	----	58.6	0.0	5	-5.0
Receiver137	132	1	59.5	57.5	66	-2.0	10	----	57.5	0.0	5	-5.0
Receiver138	133	1	60.2	57.4	66	-2.8	10	----	57.4	0.0	5	-5.0
Receiver139	134	1	61.0	58.5	66	-2.5	10	----	58.5	0.0	5	-5.0
Receiver140	135	1	51.4	51.6	66	0.2	10	----	51.6	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver141	136	1	55.2	55.4	66	0.2	10	----	55.4	0.0	5	-5.0
Receiver142	137	1	61.9	62.0	66	0.1	10	----	62.0	0.0	5	-5.0
Receiver143	138	1	60.6	61.1	66	0.5	10	----	61.1	0.0	5	-5.0
Receiver144	139	1	60.0	60.5	66	0.5	10	----	60.5	0.0	5	-5.0
Receiver145	140	1	62.0	62.6	66	0.6	10	----	62.6	0.0	5	-5.0
Receiver146	141	1	63.5	64.1	66	0.6	10	----	64.1	0.0	5	-5.0
Receiver164	142	1	65.8	64.5	66	-1.3	10	----	64.5	0.0	5	-5.0
Receiver165	143	1	61.8	60.1	66	-1.7	10	----	60.1	0.0	5	-5.0
Receiver171	144	2	49.0	49.6	66	0.6	10	----	49.6	0.0	5	-5.0
Receiver172	145	2	47.9	48.4	66	0.5	10	----	48.4	0.0	5	-5.0
Receiver173	146	2	46.9	47.5	66	0.6	10	----	47.5	0.0	5	-5.0
Receiver174	147	2	45.1	46.0	66	0.9	10	----	46.0	0.0	5	-5.0
Receiver175	150	2	44.3	45.5	66	1.2	10	----	45.5	0.0	5	-5.0
Receiver188	151	2	42.9	46.8	66	3.9	10	----	46.8	0.0	5	-5.0
Receiver189	152	2	43.3	47.3	66	4.0	10	----	47.3	0.0	5	-5.0
Receiver190	153	2	43.5	47.6	66	4.1	10	----	47.6	0.0	5	-5.0
Receiver191	154	2	44.9	48.9	66	4.0	10	----	48.9	0.0	5	-5.0
Receiver192	155	2	45.4	49.4	66	4.0	10	----	49.4	0.0	5	-5.0
Receiver193	156	2	46.1	49.8	66	3.7	10	----	49.8	0.0	5	-5.0
Receiver194	157	2	47.9	51.1	66	3.2	10	----	51.1	0.0	5	-5.0
Receiver195	158	2	48.8	52.0	66	3.2	10	----	52.0	0.0	5	-5.0
Receiver196	159	2	50.4	53.5	66	3.1	10	----	53.5	0.0	5	-5.0
Receiver197	160	2	47.5	49.5	66	2.0	10	----	49.5	0.0	5	-5.0
Receiver198	161	2	46.6	48.7	66	2.1	10	----	48.7	0.0	5	-5.0
Receiver199	162	2	44.8	47.7	66	2.9	10	----	47.7	0.0	5	-5.0
Receiver200	163	2	44.1	47.3	66	3.2	10	----	47.3	0.0	5	-5.0
Receiver201	164	2	42.9	46.3	66	3.4	10	----	46.3	0.0	5	-5.0
Receiver202	165	2	42.6	45.8	66	3.2	10	----	45.8	0.0	5	-5.0
Receiver203	166	2	43.1	45.5	66	2.4	10	----	45.5	0.0	5	-5.0
Receiver204	169	2	43.4	45.5	66	2.1	10	----	45.5	0.0	5	-5.0
Receiver205	170	2	43.9	45.9	66	2.0	10	----	45.9	0.0	5	-5.0
Receiver206	171	2	44.5	46.3	66	1.8	10	----	46.3	0.0	5	-5.0
Receiver207	172	2	45.4	46.9	66	1.5	10	----	46.9	0.0	5	-5.0
Receiver208	173	2	46.1	47.4	66	1.3	10	----	47.4	0.0	5	-5.0
Receiver209	174	3	55.3	57.2	66	1.9	10	----	57.2	0.0	5	-5.0
Receiver210	175	3	52.7	55.6	66	2.9	10	----	55.6	0.0	5	-5.0
Receiver211	176	3	51.2	55.0	66	3.8	10	----	55.0	0.0	5	-5.0
Receiver212	177	6	47.3	51.8	66	4.5	10	----	51.8	0.0	5	-5.0
Receiver213	178	6	46.0	50.9	66	4.9	10	----	50.9	0.0	5	-5.0
Receiver214	179	4	43.5	47.7	66	4.2	10	----	47.7	0.0	5	-5.0
Receiver215	180	4	43.6	48.1	66	4.5	10	----	48.1	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

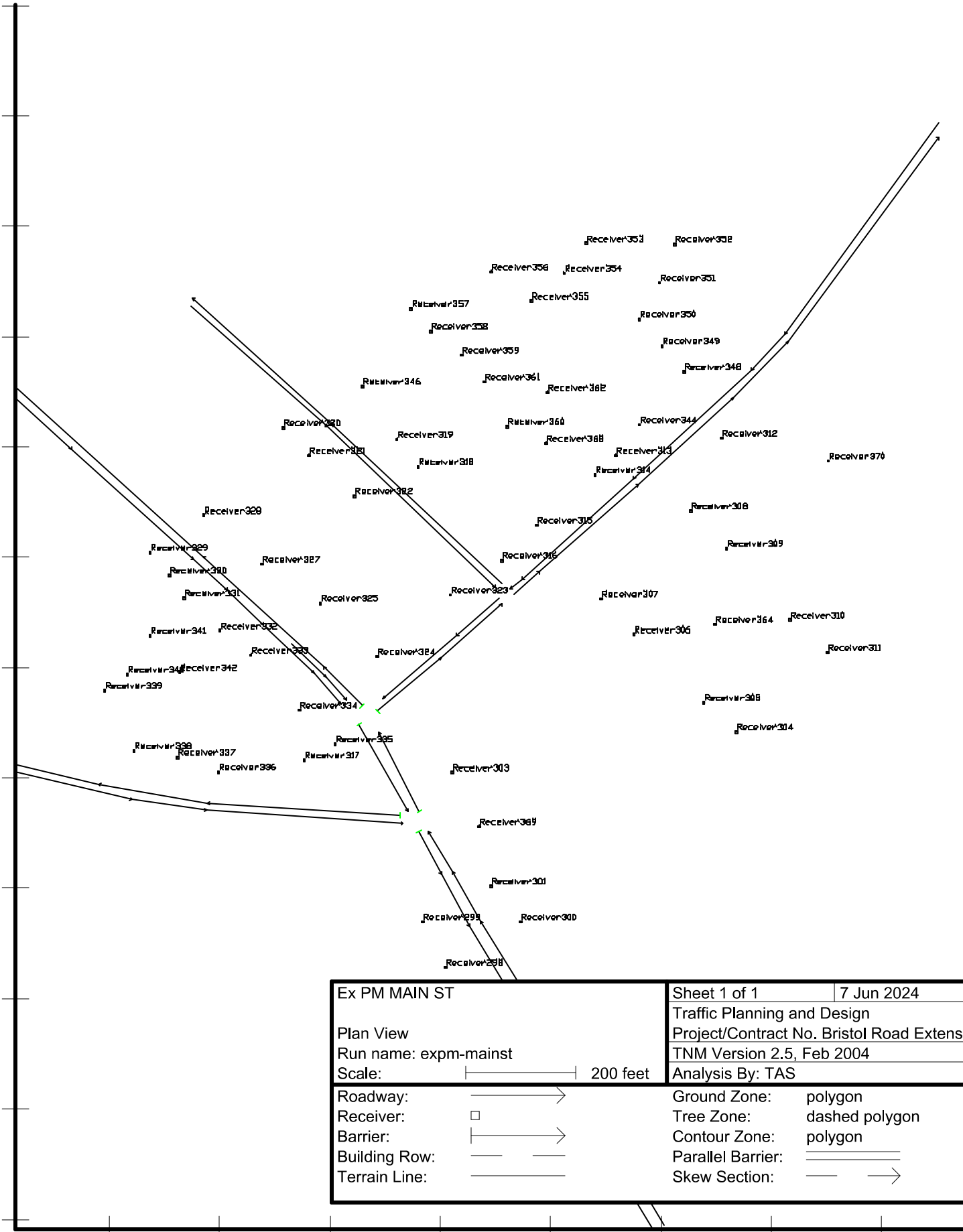
Receiver216	181	4	43.5	48.7	66	5.2	10	----	48.7	0.0	5	-5.0
Receiver217	182	4	45.3	50.7	66	5.4	10	----	50.7	0.0	5	-5.0
Receiver218	183	4	46.2	51.9	66	5.7	10	----	51.9	0.0	5	-5.0
Receiver219	184	4	47.2	53.3	66	6.1	10	----	53.3	0.0	5	-5.0
Receiver220	185	4	48.0	54.2	66	6.2	10	----	54.2	0.0	5	-5.0
Receiver221	186	4	49.8	55.8	66	6.0	10	----	55.8	0.0	5	-5.0
Receiver222	187	4	49.6	55.0	66	5.4	10	----	55.0	0.0	5	-5.0
Receiver223	188	4	57.2	60.8	66	3.6	10	----	60.8	0.0	5	-5.0
Receiver224	189	4	57.7	60.5	66	2.8	10	----	60.5	0.0	5	-5.0
Receiver225	190	1	49.9	54.7	66	4.8	10	----	54.7	0.0	5	-5.0
Receiver226	191	1	56.9	61.5	66	4.6	10	----	61.5	0.0	5	-5.0
Receiver244	192	1	58.7	59.1	66	0.4	10	----	59.1	0.0	5	-5.0
Receiver245	193	1	54.6	54.7	66	0.1	10	----	54.7	0.0	5	-5.0
Receiver246	194	1	52.2	52.3	66	0.1	10	----	52.3	0.0	5	-5.0
Receiver247	195	1	48.9	49.5	66	0.6	10	----	49.5	0.0	5	-5.0
Receiver248	196	1	50.1	50.6	66	0.5	10	----	50.6	0.0	5	-5.0
Receiver249	197	1	51.8	52.5	66	0.7	10	----	52.5	0.0	5	-5.0
Receiver250	198	1	51.9	53.1	66	1.2	10	----	53.1	0.0	5	-5.0
Receiver251	199	1	54.2	55.6	66	1.4	10	----	55.6	0.0	5	-5.0
Receiver252	200	1	54.7	56.0	66	1.3	10	----	56.0	0.0	5	-5.0
Receiver253	201	1	55.4	56.8	66	1.4	10	----	56.8	0.0	5	-5.0
Receiver254	202	1	56.1	57.4	66	1.3	10	----	57.4	0.0	5	-5.0
Receiver255	203	1	55.4	56.6	66	1.2	10	----	56.6	0.0	5	-5.0
Receiver256	204	1	54.7	56.1	66	1.4	10	----	56.1	0.0	5	-5.0
Receiver257	205	1	55.2	56.7	66	1.5	10	----	56.7	0.0	5	-5.0
Receiver258	206	1	55.4	57.1	66	1.7	10	----	57.1	0.0	5	-5.0
Receiver259	207	1	54.1	56.4	66	2.3	10	----	56.4	0.0	5	-5.0
Receiver260	208	1	53.0	55.8	66	2.8	10	----	55.8	0.0	5	-5.0
Receiver261	209	1	50.4	54.5	66	4.1	10	----	54.5	0.0	5	-5.0
Receiver262	210	1	49.4	55.8	66	6.4	10	----	55.8	0.0	5	-5.0
Receiver263	211	1	44.2	55.0	66	10.8	10	Sub'l Inc	55.0	0.0	5	-5.0
Receiver264	212	1	44.2	52.1	66	7.9	10	----	52.1	0.0	5	-5.0
Receiver265	213	1	44.1	50.7	66	6.6	10	----	50.7	0.0	5	-5.0
Receiver266	214	1	42.7	46.5	66	3.8	10	----	46.5	0.0	5	-5.0
Receiver267	215	1	43.6	47.4	66	3.8	10	----	47.4	0.0	5	-5.0
Receiver268	216	1	44.7	48.4	66	3.7	10	----	48.4	0.0	5	-5.0
Receiver269	217	1	46.6	50.0	66	3.4	10	----	50.0	0.0	5	-5.0
Receiver270	218	1	47.7	50.5	66	2.8	10	----	50.5	0.0	5	-5.0
Receiver271	219	1	47.7	50.2	66	2.5	10	----	50.2	0.0	5	-5.0
Receiver272	220	1	47.6	49.7	66	2.1	10	----	49.7	0.0	5	-5.0
Receiver273	221	1	47.4	49.4	66	2.0	10	----	49.4	0.0	5	-5.0









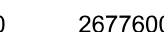


**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

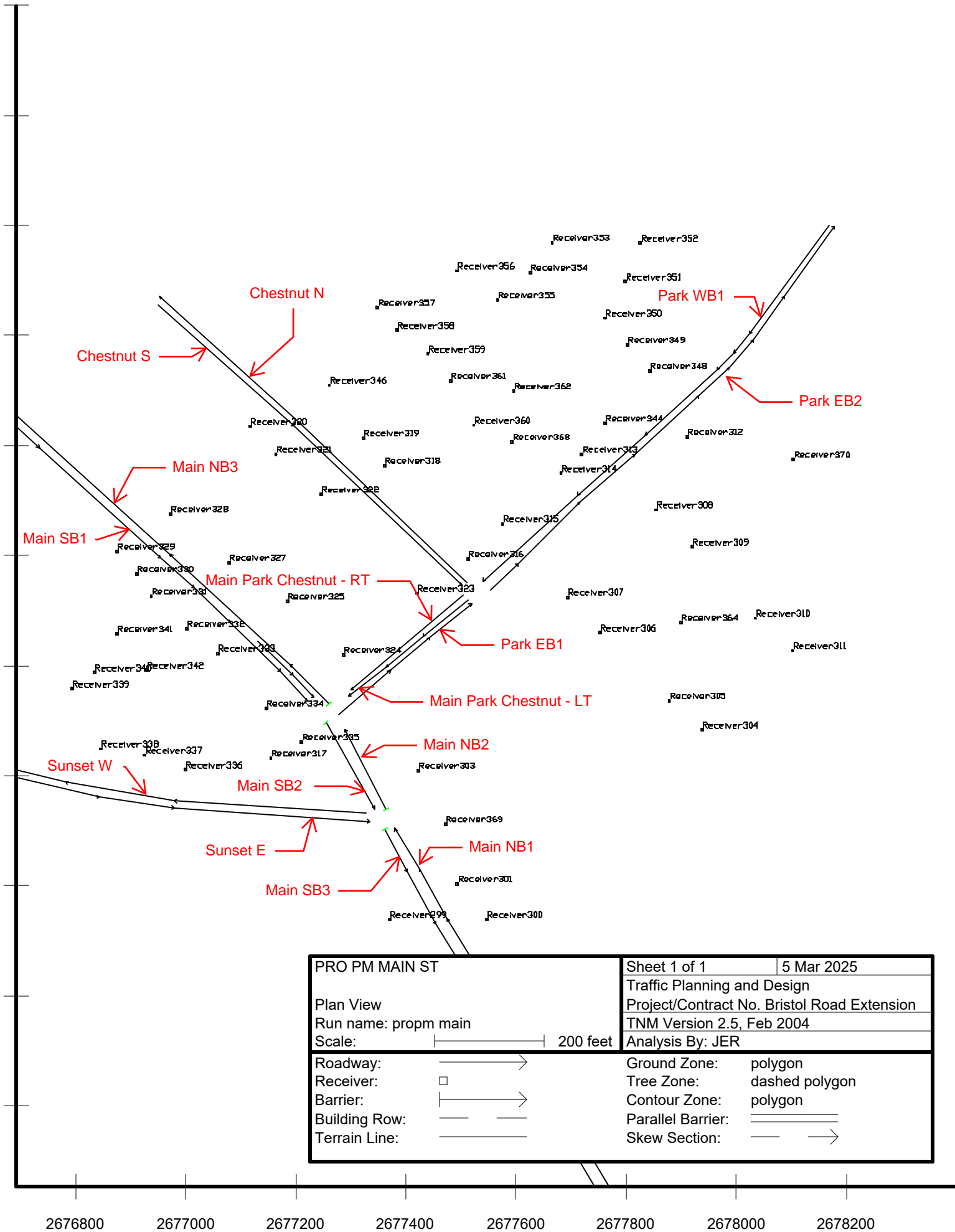
Receiver274	222	1	46.6	48.5	66	1.9	10	----	48.5	0.0	5	-5.0
Receiver275	223	1	46.2	47.9	66	1.7	10	----	47.9	0.0	5	-5.0
Receiver276	224	1	45.2	47.0	66	1.8	10	----	47.0	0.0	5	-5.0
Receiver277	225	1	44.4	46.1	66	1.7	10	----	46.1	0.0	5	-5.0
Receiver300	234	1	62.4	61.8	66	-0.6	10	----	61.8	0.0	5	-5.0
Receiver301	235	1	48.1	49.0	66	0.9	10	----	49.0	0.0	5	-5.0
Receiver302	236	1	48.3	49.1	66	0.8	10	----	49.1	0.0	5	-5.0
Receiver303	237	1	48.9	49.4	66	0.5	10	----	49.4	0.0	5	-5.0
Receiver304	238	1	50.0	50.4	66	0.4	10	----	50.4	0.0	5	-5.0
Receiver305	239	1	52.4	52.4	66	0.0	10	----	52.4	0.0	5	-5.0
Receiver306	240	1	48.9	49.6	66	0.7	10	----	49.6	0.0	5	-5.0
Receiver307	241	1	48.2	48.8	66	0.6	10	----	48.8	0.0	5	-5.0
Receiver308	242	1	48.5	48.5	66	0.0	10	----	48.5	0.0	5	-5.0
Receiver309	243	1	46.8	46.8	66	0.0	10	----	46.8	0.0	5	-5.0
Receiver311	245	1	50.2	50.7	66	0.5	10	----	50.7	0.0	5	-5.0
Receiver312	246	1	50.2	50.5	66	0.3	10	----	50.5	0.0	5	-5.0
Receiver313	247	1	50.8	50.8	66	0.0	10	----	50.8	0.0	5	-5.0
Receiver314	298	1	49.4	50.2	66	0.8	10	----	50.2	0.0	5	-5.0
Receiver315	299	1	49.8	50.6	66	0.8	10	----	50.6	0.0	5	-5.0
Receiver319	301	1	0.0	59.4	66	59.4	10	----	59.4	0.0	5	-5.0
Receiver310	302	1	0.0	56.5	66	56.5	10	----	56.5	0.0	5	-5.0
Receiver320	303	1	0.0	53.0	66	53.0	10	----	53.0	0.0	5	-5.0
Receiver321	304	1	0.0	52.8	66	52.8	10	----	52.8	0.0	5	-5.0
Receiver318	305	1	0.0	50.8	66	50.8	10	----	50.8	0.0	5	-5.0
Receiver316	306	1	0.0	52.0	66	52.0	10	----	52.0	0.0	5	-5.0
Receiver227	308	2	0.0	60.7	66	60.7	10	----	60.7	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									
			<b>Min</b>	<b>Avg</b>	<b>Max</b>							
			<b>dB</b>	<b>dB</b>	<b>dB</b>							
All Selected		249	0.0	0.0	0.0							
All Impacted		1	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							













Ex PM MAIN ST		Sheet 1 of 1	7 Jun 2024
Plan View		Traffic Planning and Design	
Run name: expm-mainst		Project/Contract No. Bristol Road Extension	
Scale: 		TNM Version 2.5, Feb 2004	
Roadway: 		Analysis By: TAS	
Receiver: 	Ground Zone: 	Tree Zone: 	Contour Zone: 
Barrier: 	Building Row: 	Parallel Barrier: 	Skew Section: 
Terrain Line: 			

2676600    2676800    2677000    2677200    2677400    2677600    2677800    2678000    2678200



<b>PRO PM MAIN ST</b>		Sheet 1 of 1	5 Mar 2025
Plan View		Traffic Planning and Design	
Run name: propm main		Project/Contract No. Bristol Road Extension	
Scale: 		TNM Version 2.5, Feb 2004	
		Analysis By: JER	
Roadway:		Ground Zone:	polygon
Receiver:		Tree Zone:	dashed polygon
Barrier:		Contour Zone:	polygon
Building Row:		Parallel Barrier:	
Terrain Line:		Skew Section:	

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Traffic Planning and Design												
TAS												
29 January 2025												
TNM 2.5												
Calculated with TNM 2.5												
RESULTS: SOUND LEVELS												
PROJECT/CONTRACT: Bristol Road Extension												
RUN: Ex PM MAIN ST												
BARRIER DESIGN: INPUT HEIGHTS												
Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.												
ATMOSPHERICS: 68 deg F, 50% RH												
Receiver												
Name	No.	#DUs	Existing	No Barrier		Increase over existing		Type	With Barrier		Noise Reduction	
			LAeq1h	LAeq1h	Crit'n	Calculated	Crit'n		Calculated	LAeq1h	Calculated	Goal
				Calculated	Crit'n	Calculated	Crit'n	Impact				
			dB	dB	dB	dB	dB		dB	dB	dB	dB
Receiver298	298	1	0.0	61.0	66	61.0	10	----	61.0	0.0	5	-5.0
Receiver299	300	1	0.0	63.2	66	63.2	10	----	63.2	0.0	5	-5.0
Receiver300	301	1	0.0	62.4	66	62.4	10	----	62.4	0.0	5	-5.0
Receiver301	302	1	0.0	65.2	66	65.2	10	----	65.2	0.0	5	-5.0
Receiver369	303	1	0.0	64.3	72	64.3	10	----	64.3	0.0	5	-5.0
Receiver303	304	1	0.0	65.5	66	65.5	10	----	65.5	0.0	5	-5.0
Receiver304	305	8	0.0	47.2	66	47.2	10	----	47.2	0.0	5	-5.0
Receiver305	306	8	0.0	48.0	66	48.0	10	----	48.0	0.0	5	-5.0
Receiver306	307	8	0.0	51.0	66	51.0	10	----	51.0	0.0	5	-5.0
Receiver307	308	8	0.0	54.1	66	54.1	10	----	54.1	0.0	5	-5.0
Receiver308	309	8	0.0	54.3	66	54.3	10	----	54.3	0.0	5	-5.0
Receiver309	311	8	0.0	49.6	66	49.6	10	----	49.6	0.0	5	-5.0
Receiver310	312	8	0.0	45.7	66	45.7	10	----	45.7	0.0	5	-5.0
Receiver311	313	8	0.0	44.6	66	44.6	10	----	44.6	0.0	5	-5.0
Receiver312	314	2	0.0	60.2	66	60.2	10	----	60.2	0.0	5	-5.0
Receiver313	315	1	0.0	58.6	66	58.6	10	----	58.6	0.0	5	-5.0
Receiver314	316	1	0.0	58.7	66	58.7	10	----	58.7	0.0	5	-5.0
Receiver315	317	1	0.0	58.5	66	58.5	10	----	58.5	0.0	5	-5.0
Receiver316	318	1	0.0	60.3	66	60.3	10	----	60.3	0.0	5	-5.0
Receiver318	320	1	0.0	52.4	66	52.4	10	----	52.4	0.0	5	-5.0
Receiver319	321	1	0.0	51.4	66	51.4	10	----	51.4	0.0	5	-5.0
Receiver320	322	1	0.0	52.3	66	52.3	10	----	52.3	0.0	5	-5.0
Receiver321	323	1	0.0	52.8	66	52.8	10	----	52.8	0.0	5	-5.0
Receiver322	324	1	0.0	53.5	66	53.5	10	----	53.5	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver323	325	23	0.0	59.5	66	59.5	10	----	59.5	0.0	5	-5.0
Receiver324	326	23	0.0	65.4	66	65.4	10	----	65.4	0.0	5	-5.0
Receiver325	327	23	0.0	63.2	66	63.2	10	----	63.2	0.0	5	-5.0
Receiver317	328	1	0.0	63.1	66	63.1	10	----	63.1	0.0	5	-5.0
Receiver327	329	1	0.0	64.3	66	64.3	10	----	64.3	0.0	5	-5.0
Receiver328	330	1	0.0	62.0	66	62.0	10	----	62.0	0.0	5	-5.0
Receiver329	331	1	0.0	62.0	66	62.0	10	----	62.0	0.0	5	-5.0
Receiver330	332	2	0.0	62.4	66	62.4	10	----	62.4	0.0	5	-5.0
Receiver331	333	1	0.0	61.9	66	61.9	10	----	61.9	0.0	5	-5.0
Receiver332	334	1	0.0	62.8	66	62.8	10	----	62.8	0.0	5	-5.0
Receiver333	335	1	0.0	63.7	66	63.7	10	----	63.7	0.0	5	-5.0
Receiver334	336	6	0.0	64.6	66	64.6	10	----	64.6	0.0	5	-5.0
Receiver335	337	1	0.0	68.0	66	68.0	10	Snd Lvl	68.0	0.0	5	-5.0
Receiver336	338	1	0.0	60.1	66	60.1	10	----	60.1	0.0	5	-5.0
Receiver337	339	1	0.0	57.3	66	57.3	10	----	57.3	0.0	5	-5.0
Receiver338	340	1	0.0	57.0	66	57.0	10	----	57.0	0.0	5	-5.0
Receiver339	341	1	0.0	53.0	66	53.0	10	----	53.0	0.0	5	-5.0
Receiver340	342	1	0.0	53.9	66	53.9	10	----	53.9	0.0	5	-5.0
Receiver341	343	1	0.0	56.2	66	56.2	10	----	56.2	0.0	5	-5.0
Receiver342	344	1	0.0	56.3	66	56.3	10	----	56.3	0.0	5	-5.0
Receiver344	345	1	0.0	57.1	66	57.1	10	----	57.1	0.0	5	-5.0
Receiver346	346	1	0.0	48.8	66	48.8	10	----	48.8	0.0	5	-5.0
Receiver348	347	1	0.0	55.6	66	55.6	10	----	55.6	0.0	5	-5.0
Receiver349	348	1	0.0	51.9	66	51.9	10	----	51.9	0.0	5	-5.0
Receiver350	349	1	0.0	48.3	66	48.3	10	----	48.3	0.0	5	-5.0
Receiver351	350	1	0.0	47.3	66	47.3	10	----	47.3	0.0	5	-5.0
Receiver352	351	1	0.0	46.4	66	46.4	10	----	46.4	0.0	5	-5.0
Receiver353	352	1	0.0	43.0	66	43.0	10	----	43.0	0.0	5	-5.0
Receiver354	353	1	0.0	43.2	66	43.2	10	----	43.2	0.0	5	-5.0
Receiver355	354	1	0.0	43.6	66	43.6	10	----	43.6	0.0	5	-5.0
Receiver356	355	1	0.0	41.8	66	41.8	10	----	41.8	0.0	5	-5.0
Receiver357	356	1	0.0	47.0	66	47.0	10	----	47.0	0.0	5	-5.0
Receiver358	357	1	0.0	47.5	66	47.5	10	----	47.5	0.0	5	-5.0
Receiver359	358	1	0.0	48.0	66	48.0	10	----	48.0	0.0	5	-5.0
Receiver361	359	1	0.0	47.2	66	47.2	10	----	47.2	0.0	5	-5.0
Receiver360	360	1	0.0	49.2	66	49.2	10	----	49.2	0.0	5	-5.0
Receiver362	361	1	0.0	48.0	66	48.0	10	----	48.0	0.0	5	-5.0
Receiver364	362	8	0.0	47.9	66	47.9	10	----	47.9	0.0	5	-5.0
Receiver368	364	1	0.0	51.1	66	51.1	10	----	51.1	0.0	5	-5.0
Receiver370	366	1	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									

**RESULTS: SOUND LEVELS****Bristol Road Extension**

			Min	Avg	Max							
			dB	dB	dB							
All Selected		200	0.0	0.0	0.0							
All Impacted		1	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

<b>Traffic Planning and Design</b>												
<b>JER</b>												
<b>4 February 2025</b>												
<b>TNM 2.5</b>												
<b>Calculated with TNM 2.5</b>												
<b>RESULTS: SOUND LEVELS</b>												
<b>PROJECT/CONTRACT:</b>			<b>Bristol Road Extension</b>									
<b>RUN:</b>			<b>NO BUILD PM MAIN ST</b>									
<b>BARRIER DESIGN:</b>			<b>INPUT HEIGHTS</b>									
<b>Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.</b>												
<b>ATMOSPHERICS:</b>			<b>68 deg F, 50% RH</b>									
<b>Receiver</b>												
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>				<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>		<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>		<b>Noise Reduction</b>	
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>
							<b>Sub'l Inc</b>					<b>minus</b>
												<b>Goal</b>
			<b>dBA</b>	<b>dBA</b>	<b>dBA</b>	<b>dB</b>	<b>dB</b>		<b>dBA</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>
Receiver298	1	1	0.0	61.4	66	61.4	10	----	61.4	0.0	5	-5.0
Receiver299	2	1	0.0	63.6	66	63.6	10	----	63.6	0.0	5	-5.0
Receiver300	3	1	0.0	62.7	66	62.7	10	----	62.7	0.0	5	-5.0
Receiver301	4	1	0.0	65.4	66	65.4	10	----	65.4	0.0	5	-5.0
Receiver369	6	1	0.0	64.9	72	64.9	10	----	64.9	0.0	5	-5.0
Receiver303	7	1	0.0	66.3	66	66.3	10	Snd Lvl	66.3	0.0	5	-5.0
Receiver304	8	8	0.0	47.8	66	47.8	10	----	47.8	0.0	5	-5.0
Receiver305	9	8	0.0	48.6	66	48.6	10	----	48.6	0.0	5	-5.0
Receiver306	10	8	0.0	51.6	66	51.6	10	----	51.6	0.0	5	-5.0
Receiver307	11	8	0.0	54.6	66	54.6	10	----	54.6	0.0	5	-5.0
Receiver308	12	8	0.0	54.8	66	54.8	10	----	54.8	0.0	5	-5.0
Receiver309	13	8	0.0	50.1	66	50.1	10	----	50.1	0.0	5	-5.0
Receiver310	14	8	0.0	46.3	66	46.3	10	----	46.3	0.0	5	-5.0
Receiver311	15	8	0.0	45.2	66	45.2	10	----	45.2	0.0	5	-5.0
Receiver312	16	2	0.0	60.8	66	60.8	10	----	60.8	0.0	5	-5.0
Receiver313	17	1	0.0	59.1	66	59.1	10	----	59.1	0.0	5	-5.0
Receiver314	18	1	0.0	59.2	66	59.2	10	----	59.2	0.0	5	-5.0
Receiver315	19	1	0.0	59.0	66	59.0	10	----	59.0	0.0	5	-5.0
Receiver316	20	1	0.0	60.7	66	60.7	10	----	60.7	0.0	5	-5.0
Receiver318	21	1	0.0	52.8	66	52.8	10	----	52.8	0.0	5	-5.0
Receiver319	22	1	0.0	51.8	66	51.8	10	----	51.8	0.0	5	-5.0
Receiver320	23	1	0.0	52.8	66	52.8	10	----	52.8	0.0	5	-5.0
Receiver321	24	1	0.0	53.3	66	53.3	10	----	53.3	0.0	5	-5.0
Receiver322	25	1	0.0	54.1	66	54.1	10	----	54.1	0.0	5	-5.0

**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Receiver323	26	23	0.0	59.7	66	59.7	10	----	59.7	0.0	5	-5.0
Receiver324	27	23	0.0	66.0	66	66.0	10	Snd Lvl	66.0	0.0	5	-5.0
Receiver325	28	23	0.0	63.8	66	63.8	10	----	63.8	0.0	5	-5.0
Receiver317	30	1	0.0	63.7	66	63.7	10	----	63.7	0.0	5	-5.0
Receiver327	31	1	0.0	64.9	66	64.9	10	----	64.9	0.0	5	-5.0
Receiver328	32	1	0.0	62.6	66	62.6	10	----	62.6	0.0	5	-5.0
Receiver329	33	1	0.0	62.5	66	62.5	10	----	62.5	0.0	5	-5.0
Receiver330	34	2	0.0	62.9	66	62.9	10	----	62.9	0.0	5	-5.0
Receiver331	35	1	0.0	62.5	66	62.5	10	----	62.5	0.0	5	-5.0
Receiver332	36	1	0.0	63.4	66	63.4	10	----	63.4	0.0	5	-5.0
Receiver333	37	1	0.0	64.3	66	64.3	10	----	64.3	0.0	5	-5.0
Receiver334	38	6	0.0	65.2	66	65.2	10	----	65.2	0.0	5	-5.0
Receiver335	39	1	0.0	68.6	66	68.6	10	Snd Lvl	68.6	0.0	5	-5.0
Receiver336	40	1	0.0	60.7	66	60.7	10	----	60.7	0.0	5	-5.0
Receiver337	41	1	0.0	57.9	66	57.9	10	----	57.9	0.0	5	-5.0
Receiver338	42	1	0.0	57.6	66	57.6	10	----	57.6	0.0	5	-5.0
Receiver339	43	1	0.0	53.6	66	53.6	10	----	53.6	0.0	5	-5.0
Receiver340	44	1	0.0	54.5	66	54.5	10	----	54.5	0.0	5	-5.0
Receiver341	45	1	0.0	56.8	66	56.8	10	----	56.8	0.0	5	-5.0
Receiver342	46	1	0.0	56.9	66	56.9	10	----	56.9	0.0	5	-5.0
Receiver344	47	1	0.0	57.7	66	57.7	10	----	57.7	0.0	5	-5.0
Receiver346	48	1	0.0	49.3	66	49.3	10	----	49.3	0.0	5	-5.0
Receiver348	49	1	0.0	56.1	66	56.1	10	----	56.1	0.0	5	-5.0
Receiver349	50	1	0.0	52.4	66	52.4	10	----	52.4	0.0	5	-5.0
Receiver351	51	1	0.0	47.8	66	47.8	10	----	47.8	0.0	5	-5.0
Receiver352	52	1	0.0	47.0	66	47.0	10	----	47.0	0.0	5	-5.0
Receiver353	53	1	0.0	43.6	66	43.6	10	----	43.6	0.0	5	-5.0
Receiver354	54	1	0.0	43.7	66	43.7	10	----	43.7	0.0	5	-5.0
Receiver355	55	1	0.0	44.1	66	44.1	10	----	44.1	0.0	5	-5.0
Receiver356	56	1	0.0	42.3	66	42.3	10	----	42.3	0.0	5	-5.0
Receiver357	57	1	0.0	47.6	66	47.6	10	----	47.6	0.0	5	-5.0
Receiver358	58	1	0.0	48.0	66	48.0	10	----	48.0	0.0	5	-5.0
Receiver359	59	1	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
Receiver361	60	1	0.0	47.7	66	47.7	10	----	47.7	0.0	5	-5.0
Receiver360	61	1	0.0	49.7	66	49.7	10	----	49.7	0.0	5	-5.0
Receiver362	62	1	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
Receiver364	63	8	0.0	48.5	66	48.5	10	----	48.5	0.0	5	-5.0
Receiver368	299	1	0.0	51.6	66	51.6	10	----	51.6	0.0	5	-5.0
Receiver370	301	1	0.0	49.1	66	49.1	10	----	49.1	0.0	5	-5.0
Receiver350	303	1	0.0	48.8	66	48.8	10	----	48.8	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									

**RESULTS: SOUND LEVELS****Bristol Road Extension**

			<b>Min</b>	<b>Avg</b>	<b>Max</b>							
			<b>dB</b>	<b>dB</b>	<b>dB</b>							
All Selected		200	0.0	0.0	0.0							
All Impacted		25	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							



**RESULTS: SOUND LEVELS**

**Bristol Road Extension**

Traffic Planning and Design												
JER												
14 February 2025												
TNM 2.5												
Calculated with TNM 2.5												
RESULTS: SOUND LEVELS												
PROJECT/CONTRACT: Bristol Road Extension												
RUN: PRO PM MAIN ST												
BARRIER DESIGN: INPUT HEIGHTS												
Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.												
ATMOSPHERICS: 68 deg F, 50% RH												
Receiver												
Name												
No.	#DUs	Existing LAeq1h	No Barrier LAeq1h	Increase over existing		Type	With Barrier	Noise Reduction				
		Calculated	Crit'n	Calculated	Crit'n	Impact	Calculated LAeq1h	Calculated	Goal	Calculated minus Goal		
		dBA	dBA	dBA	dB	dB	dBA	dB	dB	dB	dB	
Receiver298	1	1	61.0	60.5	66	-0.5	10	----	60.5	0.0	5	-5.0
Receiver299	2	1	63.2	62.8	66	-0.4	10	----	62.8	0.0	5	-5.0
Receiver300	3	1	62.4	62.0	66	-0.4	10	----	62.0	0.0	5	-5.0
Receiver301	4	1	65.2	64.9	66	-0.3	10	----	64.9	0.0	5	-5.0
Receiver303	6	1	65.5	65.6	66	0.1	10	----	65.6	0.0	5	-5.0
Receiver304	7	8	47.2	47.4	66	0.2	10	----	47.4	0.0	5	-5.0
Receiver305	8	8	48.0	48.2	66	0.2	10	----	48.2	0.0	5	-5.0
Receiver306	9	8	51.0	51.9	66	0.9	10	----	51.9	0.0	5	-5.0
Receiver307	10	8	54.1	55.3	66	1.2	10	----	55.3	0.0	5	-5.0
Receiver308	11	8	54.3	56.1	66	1.8	10	----	56.1	0.0	5	-5.0
Receiver309	12	8	49.6	50.9	66	1.3	10	----	50.9	0.0	5	-5.0
Receiver310	13	8	45.7	46.1	66	0.4	10	----	46.1	0.0	5	-5.0
Receiver311	14	8	44.6	44.7	66	0.1	10	----	44.7	0.0	5	-5.0
Receiver312	15	1	60.2	62.1	66	1.9	10	----	62.1	0.0	5	-5.0
Receiver313	16	1	58.6	61.1	71	2.5	10	----	61.1	0.0	5	-5.0
Receiver314	17	1	58.7	61.2	66	2.5	10	----	61.2	0.0	5	-5.0
Receiver315	18	1	58.5	60.7	66	2.2	10	----	60.7	0.0	5	-5.0
Receiver316	19	1	60.3	60.8	66	0.5	10	----	60.8	0.0	5	-5.0
Receiver317	20	1	63.1	62.8	66	-0.3	10	----	62.8	0.0	5	-5.0
Receiver318	21	1	52.4	52.7	66	0.3	10	----	52.7	0.0	5	-5.0
Receiver319	22	1	51.4	51.5	66	0.1	10	----	51.5	0.0	5	-5.0
Receiver320	23	1	52.3	52.5	66	0.2	10	----	52.5	0.0	5	-5.0
Receiver321	24	1	52.8	52.9	66	0.1	10	----	52.9	0.0	5	-5.0
Receiver322	25	1	53.5	53.7	66	0.2	10	----	53.7	0.0	5	-5.0

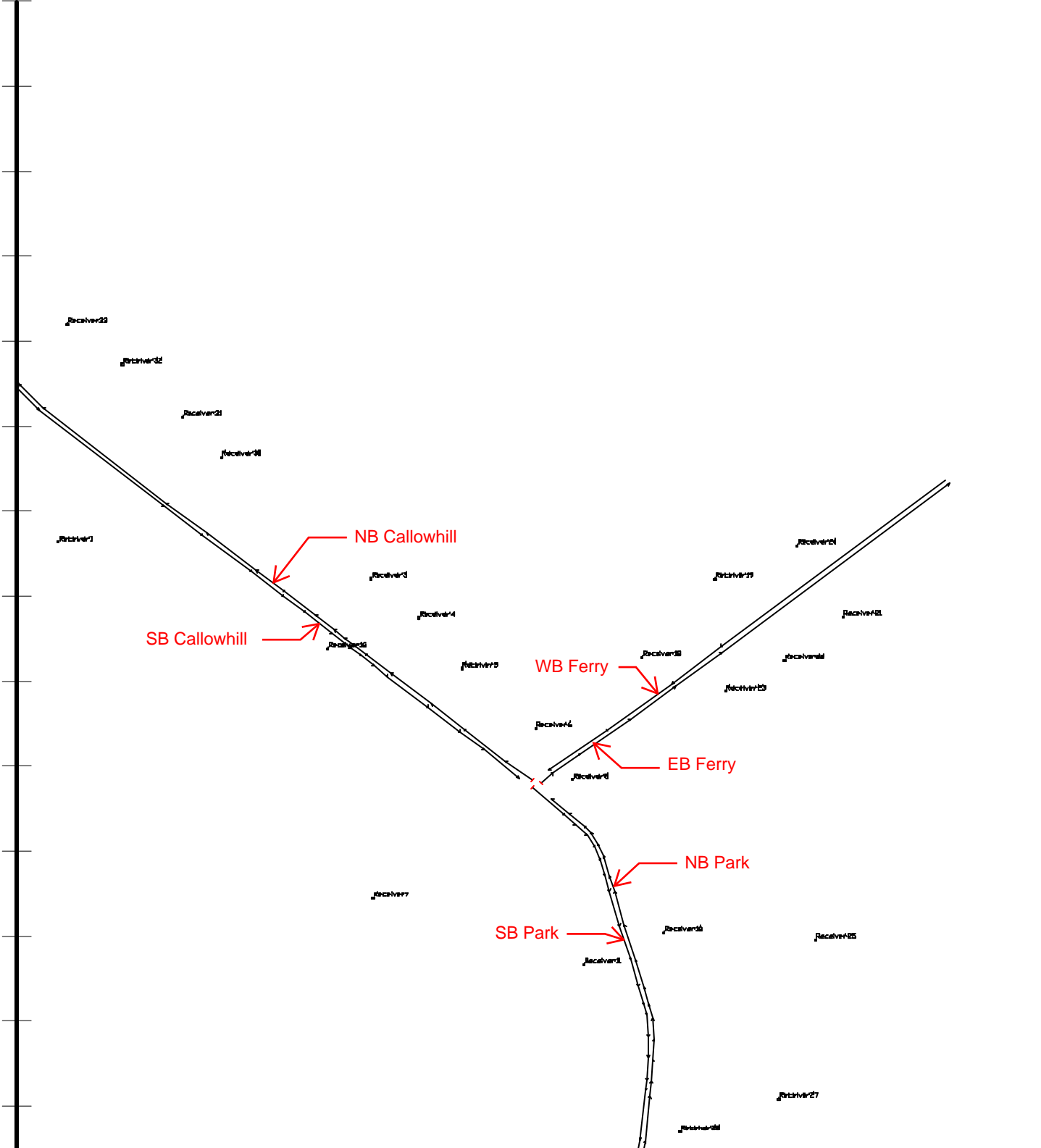
**RESULTS: SOUND LEVELS**









**Bristol Road Extension**

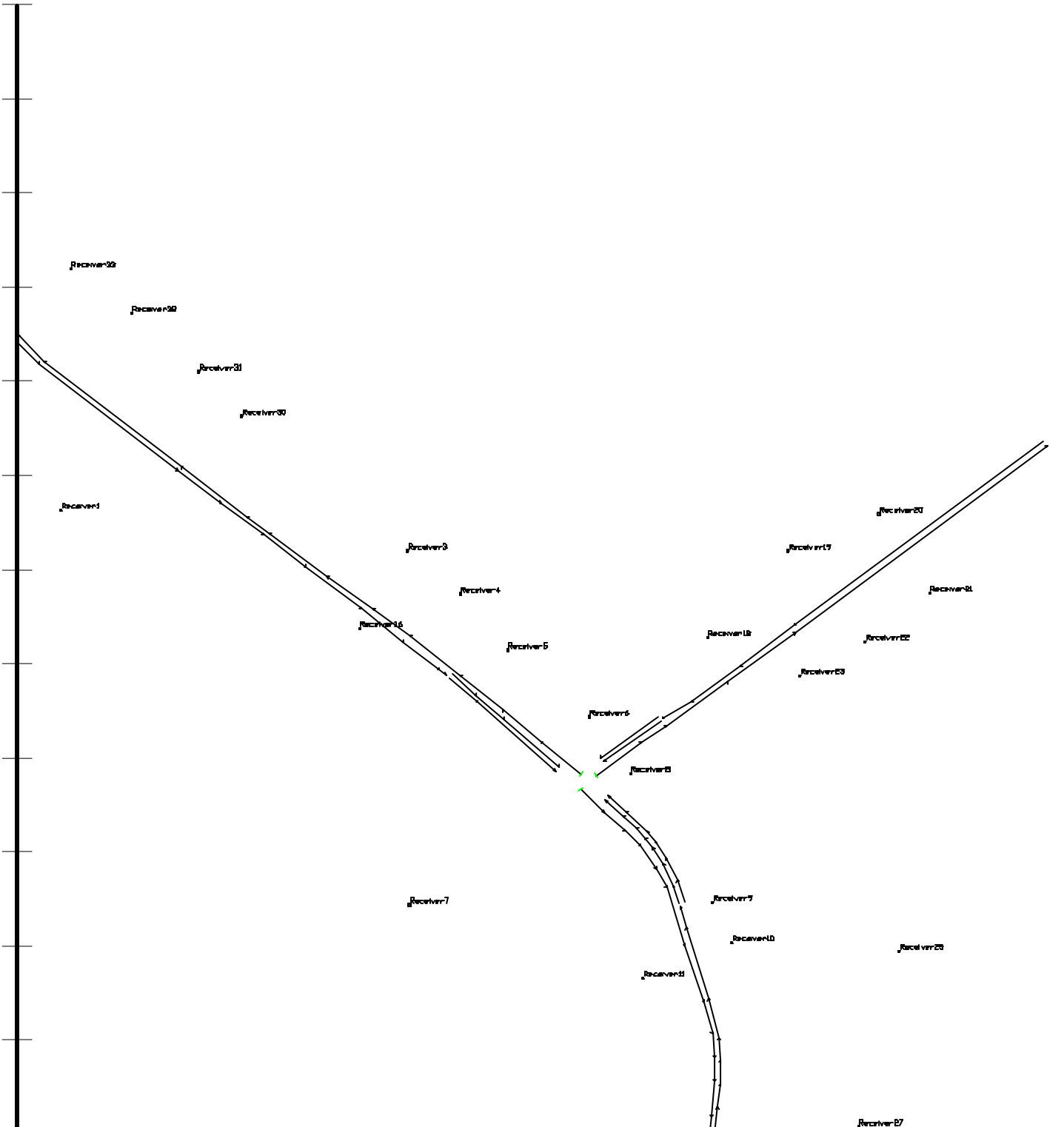
Receiver323	26	23	59.5	59.8	66	0.3	10	----	59.8	0.0	5	-5.0
Receiver324	27	23	65.4	65.4	66	0.0	10	----	65.4	0.0	5	-5.0
Receiver325	28	23	63.2	63.5	66	0.3	10	----	63.5	0.0	5	-5.0
Receiver327	30	1	64.3	64.8	66	0.5	10	----	64.8	0.0	5	-5.0
Receiver328	31	1	62.0	62.5	66	0.5	10	----	62.5	0.0	5	-5.0
Receiver329	32	1	62.0	62.5	66	0.5	10	----	62.5	0.0	5	-5.0
Receiver330	33	1	62.4	62.9	66	0.5	10	----	62.9	0.0	5	-5.0
Receiver331	34	1	61.9	62.4	66	0.5	10	----	62.4	0.0	5	-5.0
Receiver332	35	1	62.8	63.2	66	0.4	10	----	63.2	0.0	5	-5.0
Receiver333	36	1	63.7	64.1	66	0.4	10	----	64.1	0.0	5	-5.0
Receiver334	37	1	64.6	64.8	66	0.2	10	----	64.8	0.0	5	-5.0
Receiver335	38	1	68.0	67.8	66	-0.2	10	Snd Lvl	67.8	0.0	5	-5.0
Receiver336	39	1	60.1	59.4	66	-0.7	10	----	59.4	0.0	5	-5.0
Receiver337	40	1	57.3	57.2	66	-0.1	10	----	57.2	0.0	5	-5.0
Receiver338	41	1	57.0	57.2	66	0.2	10	----	57.2	0.0	5	-5.0
Receiver339	42	1	53.0	53.1	66	0.1	10	----	53.1	0.0	5	-5.0
Receiver340	43	1	53.9	54.1	66	0.2	10	----	54.1	0.0	5	-5.0
Receiver341	44	1	56.2	56.6	66	0.4	10	----	56.6	0.0	5	-5.0
Receiver342	45	1	56.3	56.5	66	0.2	10	----	56.5	0.0	5	-5.0
Receiver344	46	1	57.1	59.4	66	2.3	10	----	59.4	0.0	5	-5.0
Receiver346	47	1	48.8	49.3	66	0.5	10	----	49.3	0.0	5	-5.0
Receiver348	48	1	55.6	57.7	66	2.1	10	----	57.7	0.0	5	-5.0
Receiver349	49	1	51.9	53.4	66	1.5	10	----	53.4	0.0	5	-5.0
Receiver350	50	1	48.3	49.5	66	1.2	10	----	49.5	0.0	5	-5.0
Receiver351	51	1	47.3	48.2	66	0.9	10	----	48.2	0.0	5	-5.0
Receiver352	52	1	46.4	47.0	66	0.6	10	----	47.0	0.0	5	-5.0
Receiver353	53	1	43.0	44.0	66	1.0	10	----	44.0	0.0	5	-5.0
Receiver354	54	1	43.2	44.4	66	1.2	10	----	44.4	0.0	5	-5.0
Receiver355	55	1	43.6	44.6	66	1.0	10	----	44.6	0.0	5	-5.0
Receiver356	56	1	41.8	42.5	66	0.7	10	----	42.5	0.0	5	-5.0
Receiver357	57	1	47.0	47.1	66	0.1	10	----	47.1	0.0	5	-5.0
Receiver358	58	1	47.5	47.6	66	0.1	10	----	47.6	0.0	5	-5.0
Receiver359	59	1	48.0	48.0	66	0.0	10	----	48.0	0.0	5	-5.0
Receiver361	60	1	47.2	47.5	66	0.3	10	----	47.5	0.0	5	-5.0
Receiver360	61	1	49.2	50.1	66	0.9	10	----	50.1	0.0	5	-5.0
Receiver362	62	1	48.0	49.4	66	1.4	10	----	49.4	0.0	5	-5.0
Receiver364	63	8	47.9	48.6	66	0.7	10	----	48.6	0.0	5	-5.0
Receiver368	298	1	51.1	52.3	66	1.2	10	----	52.3	0.0	5	-5.0
Receiver369	301	1	64.3	64.1	66	-0.2	10	----	64.1	0.0	5	-5.0
Receiver370	303	1	48.5	49.8	66	1.3	10	----	49.8	0.0	5	-5.0
<b>Dwelling Units</b>		<b># DUs</b>	<b>Noise Reduction</b>									









**RESULTS: SOUND LEVELS****Bristol Road Extension**

			<b>Min</b>	<b>Avg</b>	<b>Max</b>							
			<b>dB</b>	<b>dB</b>	<b>dB</b>							
All Selected		193	0.0	0.0	0.0							
All Impacted		1	0.0	0.0	0.0							
All that meet NR Goal		0	0.0	0.0	0.0							



EXPM-Park Ferry		Sheet 1 of 1	7 Jun 2024
Plan View		Project/Contract No. Bristol Road	
Run name: expm-parkferry		TNM Version 2.5, Feb 2004	
Scale:		Analysis By:	
Roadway:		Ground Zone:	polygon
Receiver:		Tree Zone:	dashed polygon
Barrier:		Contour Zone:	polygon
Building Row:		Parallel Barrier:	
Terrain Line:		Skew Section:	



PRO PM PFC		Sheet 1 of 1	19 Feb 2025
Plan View		TRAFFIC PLANNING AND DESIGN	
Run name: PRO PM PFC		Project/Contract No. BRISTOL ROAD EXT	
Scale:  200 feet		TNM Version 2.5, Feb 2004	
Analysis By: JER			
Roadway:		Ground Zone:	polygon
Receiver:		Tree Zone:	dashed polygon
Barrier:		Contour Zone:	polygon
Building Row:		Parallel Barrier:	
Terrain Line:		Skew Section:	

**RESULTS: SOUND LEVELS**

**Bristol Road**

TPD													
JER													
<b>RESULTS: SOUND LEVELS</b>													
<b>PROJECT/CONTRACT:</b>			<b>Bristol Road</b>										
<b>RUN:</b>			<b>EXPM-Park Ferry</b>										
<b>BARRIER DESIGN:</b>			<b>INPUT HEIGHTS</b>							<b>Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.</b>			
<b>ATMOSPHERICS:</b>			<b>68 deg F, 50% RH</b>										
<b>Receiver</b>													
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>					<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>		<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>	<b>Noise Reduction</b>			
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>	<b>minus</b>
							<b>Sub'l Inc</b>					<b>Goal</b>	<b>Goal</b>
			<b>dBA</b>	<b>dBA</b>	<b>dBA</b>	<b>dB</b>	<b>dB</b>		<b>dBA</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>
Receiver1	15	1	0.0	50.2	66	50.2	10	----	50.2	0.0	8	-8.0	
Receiver3	16	1	0.0	54.3	66	54.3	10	----	54.3	0.0	8	-8.0	
Receiver4	17	1	0.0	55.1	66	55.1	10	----	55.1	0.0	8	-8.0	
Receiver5	18	1	0.0	58.2	66	58.2	10	----	58.2	0.0	8	-8.0	
Receiver6	19	1	0.0	62.1	66	62.1	10	----	62.1	0.0	8	-8.0	
Receiver7	20	1	0.0	47.5	66	47.5	10	----	47.5	0.0	8	-8.0	
Receiver8	21	1	0.0	68.1	66	68.1	10	Snd Lvl	68.1	0.0	8	-8.0	
Receiver10	22	1	0.0	58.0	66	58.0	10	----	58.0	0.0	8	-8.0	
Receiver11	23	1	0.0	56.0	66	56.0	10	----	56.0	0.0	8	-8.0	
Receiver16	26	1	0.0	65.6	66	65.6	10	----	65.6	0.0	8	-8.0	
Receiver18	27	1	0.0	60.5	66	60.5	10	----	60.5	0.0	8	-8.0	
Receiver19	28	1	0.0	58.4	66	58.4	10	----	58.4	0.0	8	-8.0	
Receiver20	29	1	0.0	62.2	66	62.2	10	----	62.2	0.0	8	-8.0	
Receiver21	30	1	0.0	60.8	66	60.8	10	----	60.8	0.0	8	-8.0	
Receiver22	31	1	0.0	60.6	66	60.6	10	----	60.6	0.0	8	-8.0	
Receiver23	32	1	0.0	62.0	66	62.0	10	----	62.0	0.0	8	-8.0	
Receiver25	33	1	0.0	45.5	66	45.5	10	----	45.5	0.0	8	-8.0	
Receiver27	34	1	0.0	46.8	66	46.8	10	----	46.8	0.0	8	-8.0	
Receiver28	35	1	0.0	58.2	66	58.2	10	----	58.2	0.0	8	-8.0	
Receiver30	36	1	0.0	53.0	66	53.0	10	----	53.0	0.0	8	-8.0	
Receiver31	37	1	0.0	51.9	66	51.9	10	----	51.9	0.0	8	-8.0	
Receiver32	38	1	0.0	51.3	66	51.3	10	----	51.3	0.0	8	-8.0	
Receiver33	39	1	0.0	52.3	66	52.3	10	----	52.3	0.0	8	-8.0	
Receiver9	41	1	0.0	59.4	66	59.4	10	----	59.4	0.0	8	-8.0	

**RESULTS: SOUND LEVELS**

**Bristol Road**

Dwelling Units	# DUs	Noise Reduction								
		Min	Avg	Max						
		dB	dB	dB						
All Selected	24	0.0	0.0	0.0						
All Impacted	1	0.0	0.0	0.0						
All that meet NR Goal	0	0.0	0.0	0.0						

**RESULTS: SOUND LEVELS**

**BRISTOL ROAD EXT**

<b>TRAFFIC PLANNING AND DESIGN</b>												
<b>JER</b>												
<b>7 February 2025</b>												
<b>TNM 2.5</b>												
<b>Calculated with TNM 2.5</b>												
<b>RESULTS: SOUND LEVELS</b>												
<b>PROJECT/CONTRACT:</b>			<b>BRISTOL ROAD EXT</b>									
<b>RUN:</b>			<b>NO BUILD PM PFC</b>									
<b>BARRIER DESIGN:</b>			<b>INPUT HEIGHTS</b>									
<b>Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.</b>												
<b>ATMOSPHERICS:</b>			<b>68 deg F, 50% RH</b>									
<b>Receiver</b>												
<b>Name</b>	<b>No.</b>	<b>#DUs</b>	<b>Existing</b>	<b>No Barrier</b>				<b>With Barrier</b>				
			<b>LAeq1h</b>	<b>LAeq1h</b>		<b>Increase over existing</b>		<b>Type</b>	<b>Calculated</b>		<b>Noise Reduction</b>	
				<b>Calculated</b>	<b>Crit'n</b>	<b>Calculated</b>	<b>Crit'n</b>	<b>Impact</b>	<b>LAeq1h</b>	<b>Calculated</b>	<b>Goal</b>	<b>Calculated</b>
							<b>Sub'l Inc</b>					<b>minus</b>
			<b>dBA</b>	<b>dBA</b>	<b>dBA</b>	<b>dB</b>	<b>dB</b>		<b>dBA</b>	<b>dB</b>	<b>dB</b>	<b>dB</b>
Receiver1	15	1	0.0	50.7	66	50.7	10	----	50.7	0.0	8	-8.0
Receiver3	16	1	0.0	54.9	66	54.9	10	----	54.9	0.0	8	-8.0
Receiver4	17	1	0.0	55.6	66	55.6	10	----	55.6	0.0	8	-8.0
Receiver5	18	1	0.0	58.8	66	58.8	10	----	58.8	0.0	8	-8.0
Receiver6	19	1	0.0	62.7	66	62.7	10	----	62.7	0.0	8	-8.0
Receiver7	20	1	0.0	48.1	66	48.1	10	----	48.1	0.0	8	-8.0
Receiver8	21	1	0.0	68.7	66	68.7	10	Snd Lvl	68.7	0.0	8	-8.0
Receiver10	22	1	0.0	58.6	66	58.6	10	----	58.6	0.0	8	-8.0
Receiver11	23	1	0.0	56.5	66	56.5	10	----	56.5	0.0	8	-8.0
Receiver16	26	1	0.0	66.1	66	66.1	10	Snd Lvl	66.1	0.0	8	-8.0
Receiver18	27	1	0.0	61.1	66	61.1	10	----	61.1	0.0	8	-8.0
Receiver19	28	1	0.0	59.0	66	59.0	10	----	59.0	0.0	8	-8.0
Receiver20	29	1	0.0	62.7	66	62.7	10	----	62.7	0.0	8	-8.0
Receiver21	30	1	0.0	61.4	66	61.4	10	----	61.4	0.0	8	-8.0
Receiver22	31	1	0.0	61.2	66	61.2	10	----	61.2	0.0	8	-8.0
Receiver23	32	1	0.0	62.6	66	62.6	10	----	62.6	0.0	8	-8.0
Receiver25	33	1	0.0	46.1	66	46.1	10	----	46.1	0.0	8	-8.0
Receiver27	34	1	0.0	47.4	66	47.4	10	----	47.4	0.0	8	-8.0
Receiver28	35	1	0.0	58.8	66	58.8	10	----	58.8	0.0	8	-8.0
Receiver30	36	1	0.0	53.5	66	53.5	10	----	53.5	0.0	8	-8.0
Receiver31	37	1	0.0	52.4	66	52.4	10	----	52.4	0.0	8	-8.0
Receiver32	38	1	0.0	51.8	66	51.8	10	----	51.8	0.0	8	-8.0
Receiver33	39	1	0.0	52.9	66	52.9	10	----	52.9	0.0	8	-8.0
Receiver9	41	1	0.0	60.0	66	60.0	10	----	60.0	0.0	8	-8.0



**RESULTS: SOUND LEVELS**

**BRISTOL ROAD EXT**

Dwelling Units	# DUs	Noise Reduction									
		Min	Avg	Max							
		dB	dB	dB							
All Selected	24	0.0	0.0	0.0							
All Impacted	2	0.0	0.0	0.0							
All that meet NR Goal	0	0.0	0.0	0.0							

**RESULTS: SOUND LEVELS**

**BRISTOL ROAD EXT**

TRAFFIC PLANNING AND DESIGN										14 February 2025			
JER										TNM 2.5			
										Calculated with TNM 2.5			
RESULTS: SOUND LEVELS													
PROJECT/CONTRACT:										BRISTOL ROAD EXT			
RUN:										PRO PM PFC			
BARRIER DESIGN:										INPUT HEIGHTS			
										Average pavement type shall be used unless a State highway agency substantiates the use of a different type with approval of FHWA.			
ATMOSPHERICS:										68 deg F, 50% RH			
Receiver													
Name		No.	#DUs	Existing LAeq1h	No Barrier LAeq1h	Increase over existing		Type	With Barrier	Noise Reduction			
				Calculated	Crit'n	Calculated	Crit'n	Impact	Calculated LAeq1h	Calculated	Goal	Calculated minus Goal	
							Sub'l Inc						
				dBA	dBA	dBA	dB		dBA	dB	dB	dB	
Receiver1	15	1	50.2	51.0	66	0.8	10	----	51.0	0.0	8	-8.0	
Receiver3	16	1	54.3	55.8	66	1.5	10	----	55.8	0.0	8	-8.0	
Receiver4	18	1	55.1	57.0	66	1.9	10	----	57.0	0.0	8	-8.0	
Receiver5	19	1	58.2	59.6	66	1.4	10	----	59.6	0.0	8	-8.0	
Receiver6	20	1	62.1	63.3	66	1.2	10	----	63.3	0.0	8	-8.0	
Receiver7	21	1	47.5	48.7	66	1.2	10	----	48.7	0.0	8	-8.0	
Receiver8	22	1	68.1	68.1	66	0.0	10	Snd Lvl	68.1	0.0	8	-8.0	
Receiver10	23	1	58.0	59.5	66	1.5	10	----	59.5	0.0	8	-8.0	
Receiver11	24	1	56.0	57.4	66	1.4	10	----	57.4	0.0	8	-8.0	
Receiver16	27	1	65.6	65.9	66	0.3	10	----	65.9	0.0	8	-8.0	
Receiver18	28	1	60.5	61.1	66	0.6	10	----	61.1	0.0	8	-8.0	
Receiver19	29	1	58.4	58.9	66	0.5	10	----	58.9	0.0	8	-8.0	
Receiver20	30	1	62.2	62.4	66	0.2	10	----	62.4	0.0	8	-8.0	
Receiver21	31	1	60.8	61.1	66	0.3	10	----	61.1	0.0	8	-8.0	
Receiver22	32	1	60.6	60.9	66	0.3	10	----	60.9	0.0	8	-8.0	
Receiver23	33	1	62.0	62.5	66	0.5	10	----	62.5	0.0	8	-8.0	
Receiver25	34	1	45.5	46.1	66	0.6	10	----	46.1	0.0	8	-8.0	
Receiver27	35	1	46.8	47.0	66	0.2	10	----	47.0	0.0	8	-8.0	
Receiver28	36	1	58.2	58.9	66	0.7	10	----	58.9	0.0	8	-8.0	
Receiver30	37	1	53.0	53.7	66	0.7	10	----	53.7	0.0	8	-8.0	
Receiver31	38	1	51.9	52.5	66	0.6	10	----	52.5	0.0	8	-8.0	
Receiver32	39	1	51.3	51.8	66	0.5	10	----	51.8	0.0	8	-8.0	
Receiver33	40	1	52.3	52.7	66	0.4	10	----	52.7	0.0	8	-8.0	
Receiver9	42	1	59.4	60.4	66	1.0	10	----	60.4	0.0	8	-8.0	

**RESULTS: SOUND LEVELS**

**BRISTOL ROAD EXT**

Dwelling Units	# DUs	Noise Reduction									
		Min	Avg	Max							
		dB	dB	dB							
All Selected	24	0.0	0.0	0.0							
All Impacted	1	0.0	0.0	0.0							
All that meet NR Goal	0	0.0	0.0	0.0							

# **APPENDIX C: TRAFFIC DATA**

## Existing Volumes - PM BRE

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

### Bristol and Hibiscus

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Bristol NB, thru	369	363	6	0	0	0
2. Bristol NB, right turn	7	3	0	4	0	0
3. Bristol NB, left turn	39	39	0	0	0	0
4. Bristol SB, thru	279	275	3	1	0	0
5. Bristol SB, right turn	7	7	0	0	0	0
6. Bristol SB, left turn	3	2	0	1	0	0
7. Hibiscus WB, right turn	5	3	0	1	1	0
8. Hibiscus WB, left turn	7	7	0	0	0	0
9. Hibiscus EB, right turn	43	42	1	0	0	0
10. Hibiscus EB, left turn	4	4	0	0	0	0
11. Hibiscus WB/EB thru	0	0	0	0	0	0
12. Hibiscus W	46	46	0	0	0	0
13. Hibiscus E	10	5	0	5	0	0
14. Bristol N	378	370	6	1	1	0
15. Bristol S	329	324	4	1	0	0

### Bristol and Butler

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Bristol NB, right turn	124	124	0	0	0	0
2. Bristol NB, left turn	249	245	4	0	0	0
3. Butler EB, thru	456	450	6	0	0	0
4. Butler EB, right turn	148	144	3	1	0	0
5. Butler WB, thru	453	446	4	3	0	0
6. Butler WB, left turn	127	127	0	0	0	0
7. Bristol S	275	271	3	1	0	0
8. Butler E	581	574	4	3	0	0
9. Butler W	702	691	8	3	0	0

### Butler and Moyer

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Moyer NB, right turn	36	35	0	0	1	0
2. Moyer NB, left turn	36	36	0	0	0	0
3. Butler EB, thru	552	539	11	2	0	0
4. Butler EB, right turn	57	57	0	0	0	0
5. Butler WB, thru	672	651	12	4	5	0
6. Butler WB, left turn	22	21	1	0	0	0
7. Moyer S	79	78	1	0	0	0
8. Butler E	592	578	11	2	1	0
9. Butler W	716	694	12	4	6	0

### Park and Bridgewater/Bristol Ext

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Park WB, right turn	1	1	0	0	0	0
2. Park WB, left turn	2	2	0	0	0	0
3. Park WB, thru	214	212	1	1	0	0
4. Park EB, right turn	11	11	0	0	0	0
5. Park EB, left turn	1	1	0	0	0	0
6. Park EB, thru	201	197	3	0	1	0
7. Bridgewater, left turn	0	0	0	0	0	0
8. Bridgewater, right turn	1	1	0	0	0	0
9. Bristol Ext, thru	0	0	0	0	0	0
10. Bristol Ext, right turn	2	2	0	0	0	0
11. Bristol Ext, left turn	10	10	0	0	0	0
12. Park E	203	199	3	0	1	0
13. Park W	225	223	1	1	0	0
14. Bridgewater N	2	2	0	0	0	0
15. Bristol S	13	13	0	0	0	0

### Park and Indian Creek

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Park WB, right turn	0	0	0	0	0	0
2. Park WB, left turn	5	5	0	0	0	0
3. Park WB, thru	207	207	0	0	0	0
4. Park EB, right turn	9	9	0	0	0	0
5. Park EB, left turn	1	1	0	0	0	0
6. Park EB, thru	183	181	2	0	0	0
7. Indian Creek, right turn	1	1	0	0	0	0
8. Indian Creek, left turn	6	6	0	0	0	0
9. Park E	185	183	2	0	0	0
10. Park W	215	215	0	0	0	0
11. Indian Creek	15	15	0	0	0	0

## No Build Volumes - PM BRE

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

### Bristol and Hibiscus

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1.	Bristol NB, thru	423	416	7	0	0	0
2.	Bristol NB, right turn	8	3	0	5	0	0
3.	Bristol NB, left turn	45	45	0	0	0	0
4.	Bristol SB, thru	320	316	3	1	0	0
5.	Bristol SB, right turn	8	8	0	0	0	0
6.	Bristol SB, left turn	3	2	0	1	0	0
7.	Hibiscus WB, right turn	6	4	0	1	1	0
8.	Hibiscus WB, left turn	8	8	0	0	0	0
9.	Hibiscus EB, right turn	49	48	1	0	0	0
10.	Hibiscus EB, left turn	5	5	0	0	0	0
11.	Hibiscus WB/EB thru	0	0	0	0	0	0
12	Hibiscus W	53	53	0	0	0	0
13	Hibiscus E	11	5	0	6	0	0
14	Bristol N	434	425	7	1	1	0
15	Bristol S	377	372	4	1	0	0

### Bristol and Butler

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Bristol NB, right turn	142	142	0	0	0	0
2	Bristol NB, left turn	286	281	5	0	0	0
3	Butler EB, thru	523	516	7	0	0	0
4	Butler EB, right turn	170	166	3	1	0	0
5	Butler WB, thru	520	512	5	3	0	0
6	Butler WB, left turn	146	146	0	0	0	0
7	Bristol S	316	312	3	1	0	0
8	Butler E	666	658	5	3	0	0
9	Butler W	806	793	10	3	0	0

### Butler and Moyer

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Moyer NB, right turn	41	40	0	0	1	0
2	Moyer NB, left turn	41	41	0	0	0	0
3	Butler EB, thru	633	618	13	2	0	0
4	Butler EB, right turn	65	65	0	0	0	0
5	Butler WB, thru	771	746	14	5	6	0
6	Butler WB, left turn	25	24	1	0	0	0
7	Moyer S	90	89	1	0	0	0
8	Butler E	674	658	13	2	1	0
9	Butler W	812	787	14	5	6	0

### Park and Bridgewater/Bristol Ext

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Park WB, right turn	1	1	0	0	0	0
2	Park WB, left turn	2	2	0	0	0	0
3	Park WB, thru	246	244	1	1	0	0
4	Park EB, right turn	13	13	0	0	0	0
5	Park EB, left turn	1	1	0	0	0	0
6	Park EB, thru	231	227	3	0	1	0
7	Bridgewater, left turn	0	0	0	0	0	0
8	Bridgewater, right turn	0	0	0	0	0	0
9	Bristol Ext, thru	1	1	0	0	0	0
10	Bristol Ext, right turn	0	0	0	0	0	0
11	Bristol Ext, left turn	2	2	0	0	0	0
12	Park E	231	227	3	0	1	0
13	Park W	248	246	1	1	0	0
14	Bridgewater N	3	3	0	0	0	0
15	Bristol S	15	15	0	0	0	0

### Park and Indian Creek

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Park WB, right turn	0	0	0	0	0	0
2	Park WB, left turn	6	6	0	0	0	0
3	Park WB, thru	238	238	0	0	0	0
4	Park EB, right turn	10	10	0	0	0	0
5	Park EB, left turn	1	1	0	0	0	0
6	Park EB, thru	210	208	2	0	0	0
7	Indian Creek, right turn	1	1	0	0	0	0
8	Indian Creek, left turn	7	7	0	0	0	0
9	Park E	212	210	2	0	0	0
10	Park W	247	247	0	0	0	0
11	Indian Creek	16	16	0	0	0	0

## Build Volumes - PM BRE

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

### Bristol and Hibiscus

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Bristol NB, thru	423	416	7	0	0	0
2. Bristol NB, right turn	8	3	0	5	0	0
3. Bristol NB, left turn	45	45	0	0	0	0
4. Bristol SB, thru	320	316	3	1	0	0
5. Bristol SB, right turn	8	8	0	0	0	0
6. Bristol SB, left turn	3	2	0	1	0	0
7. Hibiscus WB, right turn	6	4	0	1	1	0
8. Hibiscus WB, left turn	8	8	0	0	0	0
9. Hibiscus EB, right turn	49	48	1	0	0	0
10. Hibiscus EB, left turn	5	5	0	0	0	0
11. Hibiscus WB/EB thru	0	0	0	0	0	0
12. Hibiscus W	53	53	0	0	0	0
13. Hibiscus E	11	5	0	6	0	0
14. Bristol N	434	425	7	1	1	0
15. Bristol S	377	372	4	1	0	0

### Bristol and Butler

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Bristol NB, right turn	142	142	0	0	0	0
2. Bristol NB, left turn	170	167	3	0	0	0
Bristol NB, thru	116	114	2	0	0	0
Bristol SB, right turn	17	17	0	0	0	0
Bristol SB, left turn	58	58	0	0	0	0
Bristol SB, thru	70	70	0	0	0	0
3. Butler EB, thru	465	459	6	0	0	0
4. Butler EB, right turn	101	98	2	1	0	0
Butler EB, left turn	12	12	0	0	0	0
5. Butler WB, thru	403	397	4	2	0	0
Butler WB, right turn	116	114	1	1	0	0
6. Butler WB, left turn	146	146	0	0	0	0
7. Bristol S	317	314	2	1	0	0
Bristol N	244	240	3	1	0	0
8. Butler E	665	659	6	0	0	0
9. Butler W	590	581	7	2	0	0

### Butler and Moyer

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Moyer NB, right turn	41	40	0	0	1	0
2. Moyer NB, left turn	41	41	0	0	0	0
3. Butler EB, thru	518	505	11	2	0	0
4. Butler EB, right turn	65	65	0	0	0	0
5. Butler WB, thru	555	537	10	4	4	0
6. Butler WB, left turn	25	24	1	0	0	0
7. Moyer S	90	89	1	0	0	0
8. Butler E	559	545	11	2	1	0
9. Butler W	596	578	10	4	4	0

### Park and Bridgewater/Bristol Ext

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Park WB, right turn	1	1	0	0	0	0
2. Park WB, left turn	66	66	0	0	0	0
3. Park WB, thru	182	180	1	1	0	0
4. Park EB, right turn	94	94	0	0	0	0
5. Park EB, left turn	1	1	0	0	0	0
6. Park EB, thru	190	187	2	0	1	0
7. Bridgewater, left turn	0	0	0	0	0	0
8. Bridgewater, right turn	0	0	0	0	0	0
9. Bristol Ext, thru	1	1	0	0	0	0
10. Bristol Ext, right turn	41	40	1	0	0	0
11. Bristol Ext, left turn	205	202	2	1	0	0
12. Park E	231	227	3	0	1	0
13. Park W	387	382	3	2	0	0
14. Bridgewater N	3	3	0	0	0	0
15. Bristol S	160	160	0	0	0	0

### Park and Indian Creek

MANUAL COUNTS	Total	A	MT	HT	Buses	MC
1. Park WB, right turn	0	0	0	0	0	0
2. Park WB, left turn	6	6	0	0	0	0
3. Park WB, thru	238	238	0	0	0	0
4. Park EB, right turn	10	10	0	0	0	0
5. Park EB, left turn	1	1	0	0	0	0
6. Park EB, thru	210	208	2	0	0	0
7. Indian Creek, right turn	1	1	0	0	0	0
8. Indian Creek, left turn	7	7	0	0	0	0
9. Park E	212	210	2	0	0	0
10. Park W	247	247	0	0	0	0
11. Indian Creek	16	16	0	0	0	0

# PFC - PM

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

## EXISTING

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Park NB, right turn	137	137	0	0	0	0
2	Park NB, thru	51	51	0	0	0	0
3	Callowhill SB, thru	39	39	0	0	0	0
4	Callowhill SB, left-turn	226	224	1	0	1	0
5	Ferry WB, right turn	552	547	3	1	1	0
6	Ferry WB, left turn	208	206	1	0	1	0
7	Park S	247	245	1	0	1	0
8	Callowhill N	603	598	3	1	1	0
9	Fery E	363	361	1	0	1	0

## NO BUILD/BUILD

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Park NB, right turn	157	157	0	0	0	0
2	Park NB, thru	59	59	0	0	0	0
3	Callowhill SB, thru	45	45	0	0	0	0
4	Callowhill SB, left-turn	259	257	1	0	1	0
5	Ferry WB, right turn	633	628	3	1	1	0
6	Ferry WB, left turn	239	237	1	0	1	0
7	Park S	284	282	1	0	1	0
8	Callowhill N	692	687	3	1	1	0
9	Ferry E	416	414	1	0	1	0



# Existing Volumes PM - MP

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

## Main Street & Park Avenue

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Main NB, right turn	178	177	1	0	0	0
2	Main NB, thru	626	621	3	0	2	0
3	Main SB, thru	492	483	8	1	0	0
4	Main SB, left-turn	65	64	1	0	0	0
5	Park WB, right turn	17	17	0	0	0	0
6	Park WB, left turn	172	171	1	0	0	0
7	Main S	664	654	9	1	0	0
8	Main N	643	638	3	0	2	0
9	Park E	243	241	2	0	0	0

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1.	Main NB, thru	826	818	6	1	1	0
2.	Main NB, right turn	8	8	0	0	0	0
3.	Main NB, left turn	118	117	1	0	0	0
4.	Main SB, thru	557	543	11	2	1	0
5.	Main SB, right turn	39	38	1	0	0	0
6.	Main SB, left turn	1	1	0	0	0	0
7.	Sunset WB, right turn	4	4	0	0	0	0
8.	Sunset WB, left turn	7	7	0	0	0	0
9.	Sunset EB, right turn	118	116	1	0	1	0
10.	Sunset EB, left turn	51	51	0	0	0	0
11.	Sunset WB/EB thru	2	2	0	0	0	0
12	Sunset W	157	155	2	0	0	0
13	Sunset E	9	9	0	0	0	0
14	Main N	881	873	6	1	1	0
15	Main S	682	666	12	2	2	0

# No Build Volumes PM - MP

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

## Main Street & Park Avenue

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Main NB, right turn	204	203	1	0	0	0
2	Main NB, thru	718	713	3	0	2	0
3	Main SB, thru	565	555	9	1	0	0
4	Main SB, left-turn	75	74	1	0	0	0
5	Park WB, right turn	20	20	0	0	0	0
6	Park WB, left turn	197	196	1	0	0	0
7	Main S	762	751	10	1	0	0
8	Main N	738	733	3	0	2	0
9	Park E	279	277	2	0	0	0

## Main Street & Sunset Avenue

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1.	Main NB, thru	948	939	7	1	1	0
2.	Main NB, right turn	9	9	0	0	0	0
3.	Main NB, left turn	135	134	1	0	0	0
4.	Main SB, thru	639	623	13	2	1	0
5.	Main SB, right turn	45	44	1	0	0	0
6.	Main SB, left turn	1	1	0	0	0	0
7.	Sunset WB, right turn	5	5	0	0	0	0
8.	Sunset WB, left turn	8	8	0	0	0	0
9.	Sunset EB, right turn	135	133	1	0	1	0
10.	Sunset EB, left turn	59	59	0	0	0	0
11.	Sunset WB/EB thru	2	2	0	0	0	0
12	Sunset W	180	178	2	0	0	0
13	Sunset E	10	10	0	0	0	0
14	Main N	1012	1003	7	1	1	0
15	Main S	782	764	14	2	2	0

# Build Volumes PM - MP

A = Passenger Cars, MT = Medium Trucks, HT = Heavy Trucks, MC = Motorcycles.

## Main Street & Park Avenue

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1	Main NB, right turn	163	162	1	0	0	0
2	Main NB, thru	515	511	2	0	2	0
3	Main SB, thru	484	475	8	1	0	0
4	Main SB, left-turn	156	154	2	0	0	0
5	Park WB, right turn	223	223	0	0	0	0
6	Park WB, left turn	133	132	1	0	0	0
7	Main S	617	607	9	1	0	0
8	Main N	738	734	2	0	2	0
9	Park E	319	316	3	0	0	0

## Main Street & Sunset Avenue

MANUAL COUNTS		Total	A	MT	HT	Buses	MC
1.	Main NB, thru	705	698	5	1	1	0
2.	Main NB, right turn	9	9	0	0	0	0
3.	Main NB, left turn	135	134	1	0	0	0
4.	Main SB, thru	494	482	10	1	1	0
5.	Main SB, right turn	45	44	1	0	0	0
6.	Main SB, left turn	1	1	0	0	0	0
7.	Sunset WB, right turn	5	5	0	0	0	0
8.	Sunset WB, left turn	8	8	0	0	0	0
9.	Sunset EB, right turn	135	133	1	0	1	0
10.	Sunset EB, left turn	59	59	0	0	0	0
11.	Sunset WB/EB thru	2	2	0	0	0	0
12	Sunset W	180	178	2	0	0	0
13	Sunset E	10	10	0	0	0	0
14	Main N	769	762	5	1	1	0
15	Main S	637	623	11	1	2	0

# **APPENDIX D: PRELIMINARY ENGINEERING PLANS**

DISTRICT	COUNTY	TOWNSHIP	BOROUGH	ROUTE	SECTION	TOTAL SHEETS
6-0	BUCKS	NEW BRITAIN	CHALFONT NEW BRITAIN	2025	002	54
				4202		
				1006		
				4003		

ECMS NO. 12923

# COMMONWEALTH OF PENNSYLVANIA



## DEPARTMENT OF TRANSPORTATION

### DESIGN DESIGNATION - SR 2025

		TRAFFIC DATA	
HIGHWAY CLASSIFICATION - URBAN MINOR ARTERIAL		CURRENT A. D. T.	- 6,311 (2025)
DESIGN SPEED - 35 M.P.H.		DESIGN YEAR A. D. T.	- 6,537 (2047)
PAVEMENT WIDTH - VARIES 32' TO 55'		D. H. V.	- 654
SHOULDER WIDTH - VARIES 5' (LT) TO 5' (RT)		D	- 60%
		T	- 4%

### DESIGN DESIGNATION - SR 4202

		TRAFFIC DATA	
HIGHWAY CLASSIFICATION - URBAN PRINCIPAL ARTERIAL		CURRENT A. D. T.	- 14,298 (2025)
DESIGN SPEED - 35 M.P.H.		DESIGN YEAR A. D. T.	- 14,810 (2047)
PAVEMENT WIDTH - VARIES 34' TO 42'		D. H. V.	- 1,185
SHOULDER WIDTH - VARIES 4' TO 6' (LT) TO 6' (RT)		D	- 61%
		T	- 6%

### DESIGN DESIGNATION - SR 1006 SEG 0070 & 0080

		TRAFFIC DATA	
HIGHWAY CLASSIFICATION - URBAN MINOR ARTERIAL		CURRENT A. D. T.	- 4,670 (2025)
DESIGN SPEED - 35 & 45 M.P.H.		DESIGN YEAR A. D. T.	- 4,837 (2047)
PAVEMENT WIDTH - VARIES 32' TO 44'		D. H. V.	- 484
SHOULDER WIDTH - VARIES 0' TO 10'		D	- 53%
		T	- 7%

### DESIGN DESIGNATION - SR 1006 SEG 0090

		TRAFFIC DATA	
HIGHWAY CLASSIFICATION - URBAN MINOR ARTERIAL		CURRENT A. D. T.	- 7,359 (2025)
DESIGN SPEED - 40 M.P.H.		DESIGN YEAR A. D. T.	- 7,622 (2047)
PAVEMENT WIDTH - 25'		D. H. V.	- 762
SHOULDER WIDTH - VARIES 2' TO 3' (LT) TO 1' TO 2' (RT)		D	- 60%
		T	- 3%

### DESIGN DESIGNATION - SR 4003

		TRAFFIC DATA	
HIGHWAY CLASSIFICATION - URBAN MAJOR COLLECTOR		CURRENT A. D. T.	- 6,019 (2025)
DESIGN SPEED - 40 M.P.H.		DESIGN YEAR A. D. T.	- 6,245 (2047)
PAVEMENT WIDTH - VARIES 20' TO 22'		D. H. V.	- 624
SHOULDER WIDTH - VARIES 0' TO 1'		D	- 55%
		T	- 4%

## DRAWINGS FOR CONSTRUCTION OF

STATE ROUTE 2025 SECTION 002

IN BUCKS COUNTY

FROM STA. 101+80.36 TO STA. 130+50.00 LENGTH 2819.64 FT. 0.534 MI.

FROM SEG. N/A OFFSET N/A TO SEG. 0012 OFFSET 0835

AND

STATE ROUTE 4202 SECTION 002

FROM STA. 198+50.00 TO STA. 222+50.00 LENGTH 2266.75 FT. 0.429 MI.

FROM SEG. 0050 OFFSET 1150 TO SEG. 0060 OFFSET 1224

AND

STATE ROUTE 1006 SECTION 002

FROM STA. 0+00.00 TO STA. 810+00.00 LENGTH 2376.00 FT. 0.450 MI.

FROM SEG. 0070 OFFSET 0000 TO SEG. 0090 OFFSET 0500

AND

STATE ROUTE 4003 SECTION 002

FROM STA. 604+00.00 TO STA. 612+00.00 LENGTH 750.00 FT. 0.142 MI.

FROM SEG. 0010 OFFSET 0000 TO SEG. 0010 OFFSET 0800


ALSO INCLUDED:

TRAFFIC CONTROL PLAN	61	SHEETS
SIGNING AND PAVEMENT MARKING PLAN	18	SHEETS
EROSION AND SEDIMENT POLLUTION CONTROL PLAN	--	SHEETS
POST CONSTRUCTION STORMWATER MANAGEMENT PLAN	--	SHEETS
WETLAND MITIGATION PLAN	--	SHEETS
RAILROAD PLAN	--	SHEETS
TRAFFIC SIGNAL PLAN	--	SHEETS
STRUCTURE PLANS S - 41980	3	SHEETS
CROSS SECTIONS	--	SHEETS

### SCALE

HORIZONTAL 0 25 50 FEET

VERTICAL 0 5 10 FEET

PREPARED BY:  
  
TRAFFIC PLANNING AND DESIGN, INC.  
1025 ANDREW DRIVE  
SUITE 110  
WEST CHESTER, PA 19380

### SAFETY REVIEW SUBMISSION

RECOMMENDED DATE: \_\_\_\_\_

DISTRICT EXECUTIVE

RECOMMENDED DATE: \_\_\_\_\_

DEPUTY SECRETARY

APPROVED DATE: \_\_\_\_\_

REG. PROF. ENGINEER

SECRETARY OF TRANSPORTATION  
(ON BEHALF OF THE GOVERNOR  
AS WELL AS THE SECRETARY)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	2 OF 54
		4202		
		1006 4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

**LEGEND**

- PLAN
- PROFILE
- PARCEL IDENTIFICATION NUMBER  
(SEE SHEET 4 FOR PROPERTY OWNER NAMES)
- △ PARCEL IDENTIFICATION NUMBER - NO TAKE  
(SEE SHEET 4 FOR PROPERTY OWNER NAMES)

**TABULATION OF SEGMENT EQUALITIES**

- SR 2025**  
SEGMENT 0012, OFFSET 0000 = STATION 122+15.18
- SR 4202**  
SEGMENT 0050, OFFSET 2331 = STATION 210+25.64  
SEGMENT 0060, OFFSET 0000 = STATION 210+25.64
- SR 1006**  
SEGMENT 0070, OFFSET 0000 = STATION 00+00.00  
SEGMENT 0080, OFFSET 3797 = STATION 612+00.00  
SEGMENT 0090, OFFSET 0000 = STATION 805+00.00
- SR 4003**  
SEGMENT 0010, OFFSET 0000 = STATION 612+00.00

**LIST OF EQUALITIES**

- SR 2025**  
SR 2025 SURVEY & R/W @ STA 646+79.78 = SR 2025 CONSTRUCTION @ STA 131+00.00
- SR 4202**  
SR 4202 SURVEY & R/W @ STA 115+50.00 = SR 4202 CONSTRUCTION @ STA 198+00.00  
SR 4202 SURVEY & R/W @ STA 140+19.32 = SR 4202 CONSTRUCTION @ STA 222+73.29  
SR 4202 CONSTRUCTION @ STA 221+66.75 = SR 4202 CONSTRUCTION @ STA 221+66.75, 3.23' RT
- SR 1006**  
SR 1006 SURVEY & CONSTR. @ STA 25+76.61 BK = SR 1006 SURVEY & CONSTR. @ STA 26+00.00 AHD  
FERRY ROAD CONSTRUCTION @ STA STA 810+00.00 = PARK & FERRY SURVEY & R/W @ STA 79+60.46, 4.73' RT

**STOP WORK**  
STA 31+13.00  
SEG 0070 OFF 3090  
SR 1006 SEC 002

**START WORK**  
STA 100+00.00  
BRIDGEWATER COURT  
CHALFONT BOROUGH

**STOP WORK**  
STA 101+80.36  
BRIDGEWATER COURT  
CHALFONT BOROUGH

**LIMIT OF WORK & START WORK**  
STA 101+80.36  
SR 2025 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

**START WORK**  
STA 23+25.00  
SEG 0070 OFF 2325  
SR 1006 SEC 002

**START WORK**  
STA 199+00.00  
SEG 0050 OFF 1200  
SR 4202 SEC 002

**STOP WORK**  
STA 06+50.00  
SEG 0070 OFF 0650  
SR 1006 SEC 002

**START WORK**  
STA 00+12.00  
SEG 0070 OFF 0012  
SR 1006 SEC 002

**LIMIT OF WORK**  
STA 00+00.00  
SEG 0070 OFF 0000  
SR 1006 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

**LIMIT OF WORK**  
STA 222+50.00  
SEG 0060 OFF 1224  
SR 4202 SEC 002  
NEW BRITAIN BOROUGH  
BUCKS COUNTY

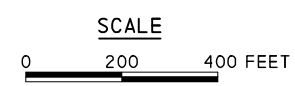
**STOP WORK**  
STA 221+66.75  
SEG 0060 OFF 1140  
SR 4202 SEC 002

**LIMIT OF WORK**  
STA 130+50.00  
SEG 0012 OFF 0835  
SR 2025 SEC 002  
NEW BRITAIN BOROUGH  
CHALFONT BOROUGH  
BUCKS COUNTY

**STOP WORK**  
STA 130+00.00  
SEG 0012 OFF 0785  
SR 2025 SEC 002

**LIMIT OF WORK**  
STA 198+50.00  
SEG 0050 OFF 1150  
SR 4202 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

**INDEX MAP**  
SHEET 1 OF 3



**SHEET INDEX BLOCK**

DESCRIPTION	SHEET
TITLE SHEET	1
INDEX MAP	2 TO 4
LOCATION MAP	5 TO 6
GEOMETRY PLAN	7 TO 9
TYPICAL SECTIONS	10 TO 21
DETAIL SHEETS	22
SUMMARY SHEETS	XX TO XX
TABULATION SHEETS	XX TO XX
PLAN SHEETS	23 TO 38
PROFILE SHEETS	39 TO 54

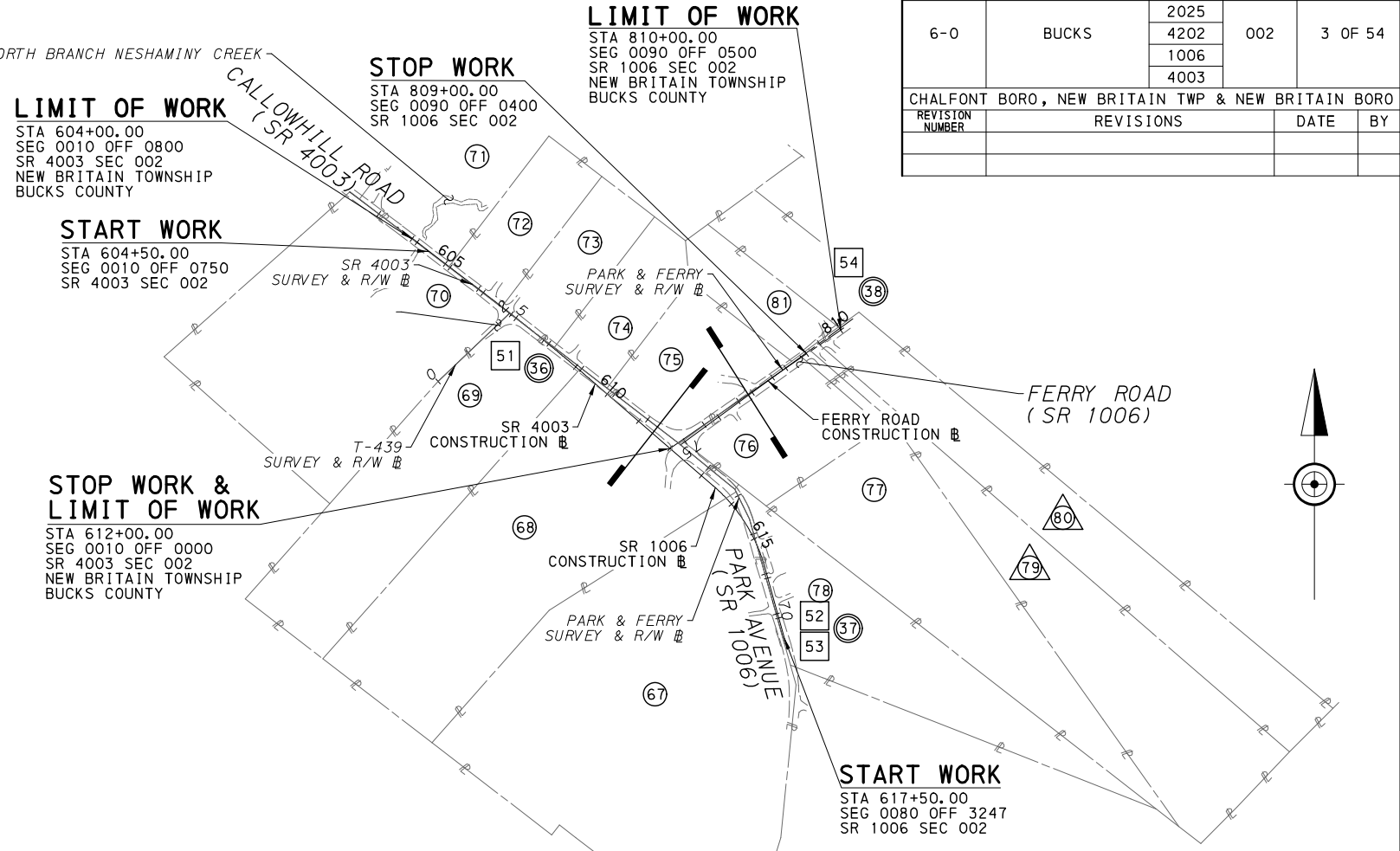
c:\pwworking\p\proj\ect\w\se\1\ol\sem\0279191\index01.dgn  
 6/5/2024 12:15:10 PM jolison

**RECORD OF EXISTING TYPES OF ROADWAY PAVEMENT**

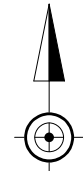
NOTE: THE DEPTHS OF MATERIAL SHOWN ARE FOR DESIGN PURPOSES ONLY. ANY RISK OF UNANTICIPATED COSTS ASSOCIATED WITH DIFFERENCES BETWEEN THE LISTED DEPTHS AND THE ACTUAL DEPTHS SHALL BE ACCEPTED BY THE CONTRACTOR.

SEG 0012 OFF 0000 TO SEG 0012 OFF 0835	1.50" SPAV, HMA WRG, 64-22, 9.5MM, H 2.50" BITUMINOUS WEARING COURSE FB-1 2" BITUMINOUS WEARING COURSE AT-1 6" UNKNOWN BASE COURSE
SEG 0050 OFF 1300 TO SEG 0050 OFF 2232	1.50" SPAV, HMA WEARING COURSE 62-22, 12.5MM, H 1.50" RECYCLED BIT WEARING COURSE 10" BITUM CONCRETE BASE COURSE
SEG 0050 OFF 2232 TO SEG 0050 OFF 2331	6" BITUM CONCRETE BASE COURSE 9" PLAIN CEMENT CONCRETE PVMT
SEG 0060 OFF 0000 TO SEG 0060 OFF 1224	1.5" SPAV, HMA WEARING COURSE 64-22, 9.5MM, H 1.5" RECYCLED BIT WEARING COURSE 5" BITUM CONCRETE BASE COURSE 9" PLAIN CEMENT CONCRETE PVMT
SEG 0070 OFF 0000 TO SEG 0070 OFF 0700	24 FT OF 1.5" SWWH3 SPAV, WMA WRG, 64-22, 9.5MM, H 20 FT OF 1.5" FB1MO BITUMINOUS WEARING CRSE FB-1 14 FT OF 2" CP200 BITUM WEARING COURSE CP-2 14 FT OF 10" NSBOO NATIVE STONE (GRANULAR) SUBB
SEG 0070 OFF 2315 TO SEG 0070 OFF 3103	1.50" SPAV, WMA WRG, 64-22, 9.5MM, H 3" BITUM WEARING COURSE 6" NATIVE STONE (GRANULAR) SUBBASE
SEG 0080 OFF 3247 TO SEG 0080 OFF 3797	22 FT OF 1.5" SWWH3 SPAV, WMA WRG, 64-22, 9.5MM, H 20 FT OF 1.5" FB1MO BITUMINOUS WEARING CRSE FB-1 20 FT OF 2.5" ID2UO BITUMINOUS WEARING CRSE ID-2 14 FT OF 2" UBWCO UNKNOWN BITUM WEARING COURSE 14 FT OF 6" NSBOO NATIVE STONE (GRANULAR) SUBB
SEG 0090 OFF 0000 TO SEG 0090 OFF 0490	22 FT OF 1.5" SWWH3 SPAV, WMA WRG, 64-22, 9.5MM, H 20 FT OF 2.25" FB1MO BITUMINOUS WEARING CRSE FB-1 20 FT OF 2.5" ID2UO BITUMINOUS WEARING CRSE ID-2 14 FT OF 2" UBWCO UNKNOWN BITUM WEARING COURSE 14 FT OF 6" NSBOO NATIVE STONE (GRANULAR) SUBB
SR 4003 SEG 0090 OFF 0000 TO SEG 0090 OFF 0490	20 FT OF 1.5" SPWH3 SPAV, HMA WRG, 64-22, 9.5MM, H 20 FT OF 3" FB1UO BITUMINOUS WEARING CRSE FB-1 20 FT OF 3" SUB20 2A SUBBASE

UNNAMED TRIBUTARY NORTH BRANCH NESHAMINY CREEK



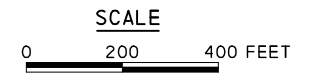
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	3 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



**LEGEND**

- PLAN    □ PROFILE
- PARCEL IDENTIFICATION NUMBER (SEE SHEET 4 FOR PROPERTY OWNER NAMES)
- △ PARCEL IDENTIFICATION NUMBER - NO TAKE (SEE SHEET 4 FOR PROPERTY OWNER NAMES)

**INDEX MAP**  
SHEET 2 OF 3



**LIST OF PROPERTY OWNERS**

- ① JOHN MEENAN
- ② THE BOARD OF SUPERVISORS OF THE TOWNSHIP OF NEW BRITAIN, COUNTY OF BUCKS, COMMONWEALTH OF PENNSYLVANIA
- ③ MICHAEL KLAWITER AND KATHLEEN M. KLAWITER
- ④ EDWARD M. HAUSLE AND SUSAN M. HAUSLE
- ⑤ MARIA L. BURKHOLDER
- ⑥ DENNIS J. SHORTALL AND VICKI L. SHORTALL, H/W
- ⑦ EDWARD QUINLAN AND DOREEN P. QUINLAN
- ⑧ CECILIA M. PARENT
- ⑨ MARY T. MCCLURE
- ⑩ CHRISTINA J. GIRARD AND KEVIN D. JAKEMAN
- ⑪ JAMES L. LAUGHLIN AND KIMBERLY S. LAUGHLIN, H/W
- ⑫ KEITH SIMONSEN & ANDREA SIMONSEN
- ⑬ CHRISTOPHER P. SWIGGARD AND MARGARET SWIGGARD, H/W
- ⑭ WILLIAM S. BATTY AND JENNIFER P. BATTY
- ⑮ GREGORY H. LEE
- ⑯ RONALD H. STEINHAUER III AND JENNIFER M. STEINHAUER
- ⑰ LAURA BALTRA
- ⑱ STEVEN J. STEIN AND NADINE H. STEIN
- ⑲ NEW BRITAIN TOWNSHIP
- ⑳ YEW SERVICE CORPORATION (A PENNSYLVANIA CORP.), GRANOR BRITAIN CORP. (A PENNSYLVANIA CORP.) AND PRICE HOMES CORP. (A PENNSYLVANIA CORP.)
- ㉑ FOREST PARK OWNER'S ASSOCIATION
- ㉒ PHILADELPHIA SUBURBAN WATER COMPANY
- ㉓ ALFRED S. HALAS AND EDWARD R. DIPAUL
- ㉔ FOUR (401) BUTLER LLC
- ㉕ AFN ABSPROPO01 LLC
- ㉖ PHILADELPHIA ELECTRIC COMPANY
- ㉗ JABEMAC, LP
- ㉘ NSG REALTY
- ㉙ SEPTA
- ㉚ GEORGEIA RUTH LEVY AND JENNIFER B. LEVY
- ㉛ 84SLB1, LLC
- ㉜ BILL MITCHELL'S AUTO SERVICE, INC.
- ㉝ MARGARET T. SCHWIND
- ㉞ DAVID KEMMERER
- ㉟ SUSAN L. SUPPER
- ㊱ STANLEY F. CLEMENS AND MARY JANE CLEMENS
- ㊲ TIMOTHY J. SHANAHAN AND MARY C. SHANAHAN, HIS WIFE

- ③⑧ BARRY G. WARREN AND SARA M. WARREN, HUSBAND AND WIFE
- ③⑨ 32 BRISTOL ROAD, LP, A PENNSYLVANIA LP
- ④⑩ RAP CHALFONT, LP
- ④① ND2198, LLC
- ④② JVW PROPERTIES
- ④③ THOMAS B. CARR AND DEBRA K. CARR, AS TRUSTEES UNDER THE THOMAS B. CARR TRUST
- ④④ PARAMOUNT L.P.
- ④⑤ SHAYA NEW BRITAIN, LP
- ④⑥ SUBURBAN COMMUNITY BANK (A PA CORP.)
- ④⑦ ANNA MARIA TOLOMELLO
- ④⑧ NORTH PENN WATER AUTHORITY
- ④⑨ NORTH WALES WATER AUTHORITY, A MUNICIPAL AUTHORITY
- ⑤⑩ PAUL BRAZIL AND KELLY BRAZIL
- ⑤⑪ PAUL DARREFF AND CRISTINA DEMATTEO
- ⑤⑫ JERC PARTNERS XXV, LP
- ⑤⑬ JERC PARTNERS XXV, LP
- ⑤⑭ PINE CREEK ASSOCIATES
- ⑤⑮ DAWN MACO
- ⑤⑯ WILLIAM W. WRIGHT AND ALYSSA C. WRIGHT
- ⑤⑰ JEFFREY J. REEDER AND DALISSA M. REEDER
- ⑤⑱ JEFFREY J. REEDER AND DALISSA M. REEDER
- ⑤⑲ LADD S. MCCOY AND MARIA G. MCCOY TRUSTEES OF MCCOY LIVING TRUST
- ⑥⑩ GAIL M. SANTINI AND STEVEN H. SANTINI
- ⑥⑪ JERC PARTNERS XXV, LLC
- ⑥⑫ JOSEPH D. WITT
- ⑥⑬ PENNS PROPERTY GROUP
- ⑥⑭ 136 MAIN ST., LLC
- ⑥⑮ SETH M. CORNELL AND HANNAH CORNELL
- ⑥⑯ CHRISTIAN W. MATTHESON AND MARGARET CORNILLON MATTHESON
- ⑥⑰ SARAH A. GERDING & ANN GERDING HURLEY
- ⑥⑱ ERIC D. SOROKER
- ⑦⑰ RAYMOND P. KIRSCH & CATHERINE J. BIRKENSTOCK KIRSCH H/W
- ⑦⑰ ANN C. DURANTE & JUDITH A. ROGERS
- ⑦⑰ ANTHONY E. LEPORE
- ⑦⑰ LISA A. STREETSMITH
- ⑦⑰ JEFFREY R. RAYSBROOK & LAURA E. RAYSBROOK
- ⑦⑰ MARK BARR & MARGE NOCTON-BARR
- ⑦⑰ BARBARA B. KEENA
- ⑦⑰ KATHARINE ELIZABETH BROWN & RICHARD C. KARASZKIEWICZ, III
- ⑦⑰ MICHAEL F. CARTER

- ⑦⑱ HENRY R. SCHMIDT & MARIE SCHMIDT
- ⑦⑲ BERNARD J. PICKETT, JR.
- ⑧⑰ JOHN J. CAHILL & PATRICIA A. CAHILL
- ⑧⑱ JEFFREY A. HARRIS & GAIL L. HARRIS

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	4 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



**PUBLIC UTILITIES**

SYMBOL	OWNER	ADDRESS	REPRESENTATIVE	TELEPHONE	EMAIL
—W1—	AQUA PENNSYLVANIA, INC.	762 W LANCASTER AVENUE, BRYN MAWR, PA 19010	BRENNAN KELLY	(610) 645-4297	BTKELLY@AQUAAMERICA.COM
—G2—	TEXAS EASTERN TRANSMISSION, LP (ENBRIDGE)	560 POTTSTOWN PIKE, CHESTER SPRINGS, PA 19425	DAVE LEIB	(610) 458-1712	DAVE.LEIB@ENBRIDGE.COM
—SEP—	SOUTHEASTERN PA TRANSPORTATION AUTHORITY (SEPTA)	1234 MARKET STREET, 12TH FLOOR, PHILADELPHIA, PA 19107	TYLER LADD	(215) 580-7800	TLADD@SEPTA.ORG
—S2—	CHALFONT-NEW BRITAIN TWP. JOINT SEWAGE AUTHORITY	1645 UPPER STATE ROAD, DOYLESTOWN, PA 18901	JOHN SCHMIDT	(215) 345-1225	JSCHMIDT@CNBSA.ORG
—C—	COMCAST CABLE COMMUNICATIONS	190 SHOEMAKER ROAD, POTTSTOWN, PA 19464	MIKE KIMBERLY	(610) 327-6395	MIKE.KIMBERLY@CABLE.COMCAST.COM
—W—	NORTH PENN WATER AUTHORITY	300 FORTY FOOT ROAD, LANSDALE, PA 19446	DAN PRESTON	(215) 855-3617	DPRESTON@NORTH PENN WATER.ORG
—W2—	NORTH WALES WATER AUTHORITY	200 WEST WALNUT STREET, NORTH WALES, PA 19454	CHASE HOWES	(215) 699-4836	CHOWES@NWWATER.COM
—G1—	PECO ENERGY COMPANY (GAS)	1050 WEST SWEDES FORD ROAD, BERWYN, PA 19312	MICHAEL GABRIEL	(215) 485-0047	MIKE.GABRIEL@PECO-ENERGY.COM
—E1—	PECO ENERGY COMPANY (ELECTRIC)	1050 WEST SWEDES FORD ROAD, BERWYN, PA 19312	CHARLES DETTLING	(610) 725-7129	CHARLES.DETTLING@EXELONCORP.COM
—TU—	VERIZON PENNSYLVANIA, LLC	1050 VIRGINIA DRIVE, FORT WASHINGTON, PA 19034	CHRIS ATKINSON	(215) 478-5218	CHRIS.J.ATKINSON@VERIZON.COM
—FOU—	CROWN CASTLE	3200 HORIZON DR, STE 150, KING OF PRUSSIA, PA 19406	LAUREN LEVITT	(610) 635-3234	LAUREN.LEVITT@CROWNCastle.COM
—S—	CHALFONT BOROUGH	40 NORTH MAIN STREET, CHALFONT, PA 18914	SHAWN CURRAN	(215) 822-7295	SCURRAN@CHALFONTBOROUGH.COM
—S1—	BUCKS COUNTY WATER & SEWER AUTHORITY	1275 ALMHOUSE ROAD, WARRINGTON, PA 18976	JAMES NAPOLEON	(215) 343-2538	N.JIM@BCWSA.NET
PENNSYLVANIA ONE CALL SYSTEM, INC. (800) 242-1776		PA ONE CALL DESIGN SERIAL NOS: 20200063010, 20200063011, 20200063065, 20200063066, 20200063067, 20200063103, 20200063104, 20200063138, 20200063139, 20200063183, 20200063184, 20200063218, 20200063242, 20200063243, 20200063286, 20200063287			

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	5 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	

**EARTHWORK SUMMARY ENTIRE PROJECT**

THE INFORMATION ON ESTIMATED AMOUNTS OF EARTHWORK HAS BEEN USED IN THE PRELIMINARY ESTIMATE. DO NOT USE AS A WAIVER OF ANY PROVISIONS OF THE SPECIFICATIONS AND CONTRACTS.

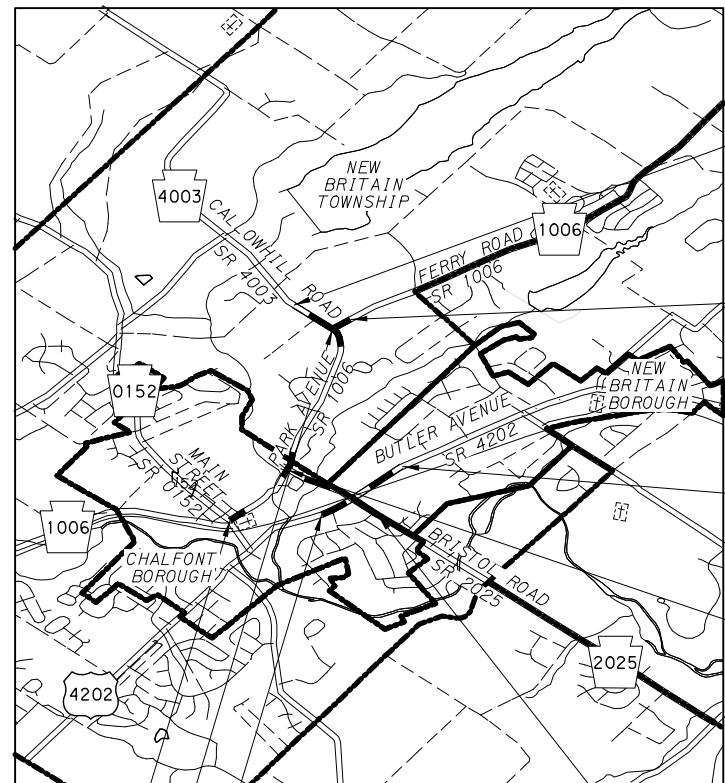
CUBIC YARDS OF EXCAVATION						CUBIC YDS OF COMPLETED EMBANKMENT*	CUBIC YDS OF BORROW EXCAVATION	CUBIC YDS OF SELECT BORROW	CUBIC YDS OF WASTE
CLASS 1	CLASS 1A	CLASS 1B	CLASS 2	CLASS 3**	CLASS 4				

**TABULATION OF OVERALL LENGTH**

STATE ROUTE	STATION	FT	MI
SR 2025	101+80.36 TO 130+50.00	2869.64	0.543
SR 4202	198+50.00 TO 222+50.00	2400.00	0.455
SR 1006	0+00.00 TO 810+00.00	8014.00	1.518
SR 4003	604+00.00 TO 612+00.00	800.00	0.152
<b>TOTAL LENGTH</b>		<b>14083.64</b>	<b>2.667</b>

**TABULATION OF CONSTRUCTION LENGTH**

STATE ROUTE	STATION	FT	MI
BRIDGEWATER CT.	100+00.00 TO 101+80.36	180.36	0.034
SR 2025	101+80.36 TO 130+00.00	2819.64	0.534
SR 4202	199+00.00 TO 221+66.75	2266.75	0.429
SR 1006	23+25.00 TO 31+13.00	788.00	0.149
SR 1006	00+12.00 TO 6+50.00	638.00	0.121
SR 1006	617+50.00 TO 612+00.00	550.00	0.104
SR 1006	805+00.00 TO 809+00.00	400.00	0.076
SR 4003	604+50.00 TO 612+00.00	750.00	0.142
<b>TOTAL LENGTH</b>		<b>8392.75</b>	<b>1.590</b>



**LIMIT OF WORK**

STA 604+00.00  
SR 4003 SEC 002  
SEG 0010 OFF 0800  
NEW BRITAIN TOWNSHIP  
BUCKS COUNTY

**LIMIT OF WORK**

STA 810+00.00  
SEG 0090 OFF 0500  
SR 1006 SEC 002  
NEW BRITAIN TOWNSHIP  
BUCKS COUNTY

**LIMIT OF WORK**

STA 222+50.00  
SEG 0060 OFF 1224  
SR 4202 SEC 002  
NEW BRITAIN BOROUGH  
BUCKS COUNTY

**LIMIT OF WORK**

STA 101+80.36  
SR 2025 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

**LIMIT OF WORK**

STA 00+00.00  
SEG 0070 OFF 0000  
SR 1006 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

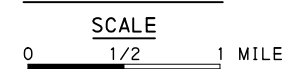
**LIMIT OF WORK**

STA 612+00.00  
SEG 0010 OFF 0000  
SR 4003 SEC 002  
NEW BRITAIN TOWNSHIP  
BUCKS COUNTY

**LIMIT OF WORK**

STA 198+50.00  
SEG 0050 OFF 1150  
SR 4202 SEC 002  
CHALFONT BOROUGH  
BUCKS COUNTY

**LOCATION MAP**



**LEGEND**

- PROJECT
- STATE HIGHWAY
- - - TOWNSHIP ROAD
- + + RAILROAD
- · · · STREAM
- MUNICIPALITY BOUNDARY

GENERAL NOTES AND LOCATION MAP

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**GENERAL NOTES**

THE LEGAL RIGHT-OF-WAY FOR SR 4202 FROM STATION 97+30 TO 110+00 IS VARIABLE WIDTH FROM FORTY-EIGHT FEET TO FIFTY-THREE FEET AS ACQUIRED BY SECTION 511 OF ACT 428 OF JUNE 1, 1945.

THE LEGAL RIGHT-OF-WAY FOR SR 4202 FROM STATION 110+00 TO 154+11 IS FIFTY FEET, TWENTY-FIVE FEET EACH SIDE OF CENTERLINE BASED ON A PLAN FOR CONSTRUCTION OF ROUTE NO 178, SECTION 3, APPROVED AND SIGNED BY THE GOVERNOR ON JUNE 6, 1924, RECORDED IN HIGHWAY BOOK 2, PAGE 3 OF THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 4202 FROM STATION 127+37 TO 136+17, LEFT, IS VARIABLE WIDTH FOR TWENTY-FIVE FEET TO FORTY-FOUR FEET LEFT OF CENTERLINE AS SHOWN AND OFFERED FOR DEDICATION TO NEW BRITAIN BOROUGH ON A PLAN FOR FOREST PARK RECORDED ON JANUARY 1, 1988 IN PLAN BOOK 243, PAGE 70 AT THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL ROW FOR MONROE ROAD, BOROUGH ROAD FROM STATION 0+00 TO 0+50 IS VARIABLE WIDTH FROM THIRTY-THREE FEET TO EIGHTY-FOUR FEET BASED ON A PLAN FOR CONSTRUCTION AND CONDEMNATION OF RIGHT-OF-WAY FOR ROUTE NO 178, SECTION 11, APPROVED AND SIGNED BY THE GOVERNOR ON FEBRUARY 9, 1950, RECORDED IN HIGHWAY BOOK 2, PAGE 19 OF THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 2025 FROM STATION 645+00 TO 654+65 IS THIRTY-THREE FEET, SIXTEEN AND ONE HALF FEET EACH SIDE OF THE CENTERLINE BASE ON ROAD DOCKET A-15, DATED MARCH 1736, RECORDED IN ROAD BOOK 6, PAGE 27 ON REEL 1 AT THE CLERK OF THE CIRCUIT COURT FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 2025 FROM STATION 654+65 TO 655+65 IS VARIABLE WIDTH FROM FORTY FEET TO NINETY FEET BASED ON A PLAN FOR CONSTRUCTION AND CONDEMNATION OF RIGHT-OF-WAY FOR ROUTE NO 178, SECTION 11, APPROVED AND SIGNED BY THE GOVERNOR ON FEBRUARY 9, 1950, RECORDED IN HIGHWAY BOOK 2, PAGE 19 OF THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 0152 IS FORTY-THREE FEET AS ESTABLISHED BY CHALFONT BOROUGH ORDINANCE NO 5, OF SEPTEMBER 30, 1903.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 0-06.22 TO 12+60 IS THIRTY-SIX FEET, EIGHTEEN FEET EACH SIDE OF CENTERLINE AS ESTABLISHED BY CHALFONT BOROUGH ORDINANCE NO 9, SECTION 3, DATED JUNE 13, 1909.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 7+53 TO 15+00, LEFT, IS VARIABLE WIDTH FROM THIRTY-THREE FEET TO THIRTY-FIVE FEET FROM CENTERLINE AS SHOWN ON A PLAN FOR MAINS -ABARBANEL PARTNERSHIP RECORDED ON MARCH 3, 1993 IN PLAN BOOK 269, PAGE 90 AND DEDICATED TO CHALFONT BOROUGH BY DEED DATED DECEMBER 8, 1995 AND RECORDED IN DEED BOOK 1176, PAGE 1061 AT THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 12+60 TO 28+00 IS VARIABLE WIDTH FROM THIRTY-THREE FEET TO ONE HUNDRED SIXTY-FIVE FEET AS SHOWN ON DRAWINGS AUTHORIZING CONDEMNATION OF RIGHT-OF-WAY OF LEG. ROUTE 9076, SECTION 2R/W, APPROVED AND SIGNED BY THE GOVERNOR ON AUGUST 13, 1969, RECORDED IN HIGHWAY BOOK 45, PAGE 6 OF THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 28+00 TO 35+89, LEFT IS SIXTEEN AND ONE HALF FEET LEFT OF CENTERLINE BASE ON ROAD DOCKET A-19, DATED MARCH 1738, RECORDED IN ROAD BOOK 6, PAGE 30 ON REEL 1 AT THE CLERK OF THE CIRCUIT COURT FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 26+55 TO 40+57, RIGHT, IS FORTY FEET RIGHT OF CENTERLINE AS SHOWN ON A PLAN FOR FOREST PARK WEST RECORDED ON JUNE 1, 1994 IN PLAN BOOK 275, PAGE 6 AND DEDICATED TO NEW BRITAIN TOWNSHIP BY DEED DATED NOVEMBER 28, 1995 AND RECORDED IN DEED BOOK 1175, PAGE 223 AT THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 35+89 TO 43+88, LEFT, IS VARIABLE WIDTH FROM THIRTY-FIVE TO FORTY FEET LEFT OF CENTERLINE AS SHOWN ON A PLAN OF FAIRWOOD RECORDED ON SEPTEMBER 28, 1977 IN PLAN BOOK 160, PAGE 35 AT THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR 1006 FROM STATION 40+57 TO 43+88, RIGHT, IS VARIABLE WIDTH FROM FORTY TO FORTY-THREE FEET LEFT OF CENTERLINE AS SHOWN ON A PLAN OF FAIRWOOD RECORDED ON SEPTEMBER 28, 1977 IN PLAN BOOK 160, PAGE 35 AT THE RECORDER OF DEEDS FOR BUCKS COUNTY.

THE LEGAL RIGHT-OF-WAY FOR SR-1006, (PARK AVE. & FERRY RD.), FROM STATION 69+24.84 TO STATION 80+00, IS 33 FEET, PER RIGHT-OF-WAY RECORD, (FORM 989), FOR LR-09076.

THE LEGAL RIGHT-OF-WAY FOR SR-4003, (CALLOWHILL RD.) FROM STATION 0+00 TO STATION 8+60, IS 33 FEET, PER FINAL PLAN OF LOTS OF WILLOW BROOK, RECORDED 06-08-1967 IN BOOK 45 PAGE 18.

THE LEGAL RIGHT-OF-WAY FOR T-403, (PEACE VALLEY RD.) FROM STATION 0+00 TO STATION 2+37.69, IS 33 FEET, PER FINAL PLAN OF LOTS OF WILLOW BROOK, RECORDED 06-08-1967 IN BOOK 45 PAGE 18.

 THE DEPARTMENT RESERVES THE RIGHT TO ELIMINATE ANY OR ALL OF THIS WORK. DO NOT PERFORM WORK EXCEPT THAT WHICH IS WITHIN THE HIGHWAY RIGHT-OF-WAY UNTIL SO ORDERED IN WRITING BY THE ENGINEER.

BUILDINGS AND STRUCTURES MARKED  HAVE BEEN OR ARE TO BE REMOVED OR ALTERED BY THE DEPARTMENT OR OTHER AUTHORITY RESPONSIBLE FOR THE PAYMENT OF PROPERTY DAMAGES AND SUCH WORK IS NOT PART OF THIS CONTRACT.

BUILDINGS AND STRUCTURES MARKED  ARE ENCROACHMENTS WHICH ARE THE RESPONSIBILITY OF THE PROPERTY OWNER TO REMOVE. IN CASE ANY ENCROACHMENTS SO MARKED ARE NOT REMOVED BY THE PROPERTY OWNER, DO NOT INTERFERE WITH OR REMOVE SAME UNTIL AUTHORIZED IN WRITING BY THE ENGINEER.

DEMOLISH AND REMOVE BUILDINGS AND STRUCTURES MARKED  AS LUMP SUM ITEMS AT THE LOCATIONS INDICATED.

REMOVE BUILDINGS AND STRUCTURES MARKED  UNDER THE ITEM FOR CLEARING AND GRUBBING.

**GENERAL NOTES CONTINUED**

DETAILS OTHER THAN THOSE INDICATED ARE ON THE FOLLOWING STANDARD DRAWINGS:

- |                          |                           |
|--------------------------|---------------------------|
| RC-20M NOVEMBER 1, 2022  | BC-753M JANUARY 31, 2019  |
| RC-21M JUNE 1, 2010      | BC-755M JANUARY 31, 2019  |
| RC-22M FEBRUARY 8, 2019  | BC-766M NOVEMBER 23, 2022 |
| RC-23M FEBRUARY 8, 2019  | BC-767M NOVEMBER 23, 2022 |
| RC-24M FEBRUARY 19, 2021 | BC-783M JANUARY 31, 2019  |
| RC-26M NOVEMBER 30, 2021 | TC-8600 JUNE 13, 2013     |
| RC-27M NOVEMBER 30, 2021 | TC-8602 JUNE 13, 2013     |
| RC-28M SEPTEMBER 1, 2023 | TC-8604 AUGUST 17, 2021   |
| RC-45M FEBRUARY 19, 2021 | TC-8702B JUNE 13, 2013    |
| RC-50M FEBRUARY 19, 2021 | TC-8716 JUNE 13, 2013     |
| RC-51M SEPTEMBER 1, 2023 |                           |
| RC-53M NOVEMBER 30, 2021 |                           |

HORIZONTAL CONTROL IS TIED TO THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NORTH AMERICAN DATUM 1983 (NAD 83).

VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM (NAVD 1988).

AVERAGE COMBINED FACTOR IS 0.0XXXXXXX. NEED FOR BRE AREA AND PFC

THREE TO TEN WORKING DAYS PRIOR TO EXCAVATION BASED ON THE COMPLEXITY OF THE PROJECT, THE CONTRACTOR MUST CONTACT THE PA ONE CALL SYSTEM, INC., PHONE 1-800-242-1776, SERIAL NO. \_\_\_\_\_, AND \_\_\_\_\_ FOR CHALFONT BOROUGH, NEW BRITAIN BOROUGH, AND NEW BRITAIN TOWNSHIP. ADDITIONAL INFORMATION IS AVAILABLE AT [HTTPS://WWW.PA1CALL.ORG/PA811/PUBLIC/](https://www.pa1call.org/pa811/public/).

THE CONTRACTOR IS REQUIRED TO NOTIFY THE DEPARTMENT AND SUBMIT AN ALLEGED VIOLATION REPORT (AVR) TO THE PA PUBLIC UTILITY COMMISSION THROUGH THE PA ONE CALL SYSTEM, WWW.PA1CALL.ORG, WITHIN TEN (10) BUSINESS DAYS AFTER A UTILITY LINE IS STRUCK, DAMAGED, OR PREVIOUS DAMAGE IS DISCOVERED AS REQUIRED BY PENNSYLVANIA'S UNDERGROUND UTILITY LINE PROTECTION LAW ACT 50 (P.L.852, NO. 287 AMENDED OCT. 30, 2017). SEE SPECIAL PROVISION UTILITIES-FOR USE ON PROJECTS WITH MINIMUM OR NO EXCAVATION FOR UTILITY CONTACT INFORMATION.

DO NOT INTERFERE WITH THE OPERATION OF ANY FIRE HYDRANT, FIRE CALL BOX, OR POLICE CALL BOX.

TEMPORARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OF THE PROJECT. THE EASEMENT IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE DEPARTMENT.

DRAINAGE EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF HIGHWAY DRAINAGE FACILITIES. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY FLOW OF WATER. HOWEVER, NO STRUCTURE OF ANY KIND MAY BE ERRECTED IN THE AREA, NOR MAY ANY PIPE OR DITCH BE CONNECTED TO THE DEPARTMENT'S PIPE OR DITCH WITHOUT ADVANCED WRITTEN APPROVAL BY THE DEPARTMENT OF TRANSPORTATION.

SIDEWALK EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF A SIDEWALK. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO ITS USE FOR SIDEWALK PURPOSES.

TRAFFIC SIGNAL EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, OPERATION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF A TRAFFIC SIGNAL AND APPURTENANCE THERETO. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO ITS USE FOR TRAFFIC SIGNAL PURPOSES.

REQUIRED PRIVATE ACCESS. LAND ACQUIRED IN THE INTEREST DESIGNATED (EITHER FEE SIMPLE OR EASEMENT FOR DRIVEWAY PURPOSES) FOR THE BENEFIT AND USE OF THE PROPERTY OR PROPERTIES DESIGNATED, ALONG WITH A TEMPORARY CONSTRUCTION EASEMENT FOR THE BENEFIT AND USE OF THE COMMONWEALTH, IF NECESSARY. TITLE SHALL VEST IN THE OWNERS OF THE DESIGNATED PROPERTIES UPON ACQUISITION AND MAINTENANCE RESPONSIBILITY SHALL TRANSFER UPON THE COMPLETION OF CONSTRUCTION, IF ANY

STATE ROUTE 4202, PREVIOUSLY KNOWN SR 0202 AND LR 00178.

STATE ROUTE 1006, PREVIOUSLY KNOWN 09076 & 09074

STATE ROUTE 4003, PREVIOUSLY KNOWN 09078

STATE ROUTE 0152, PREVIOUSLY KNOWN 658

STATE ROUTE 2025, PREVIOUSLY KNOWN 09034

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	6 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

SUMMARY OF CURVE DATA

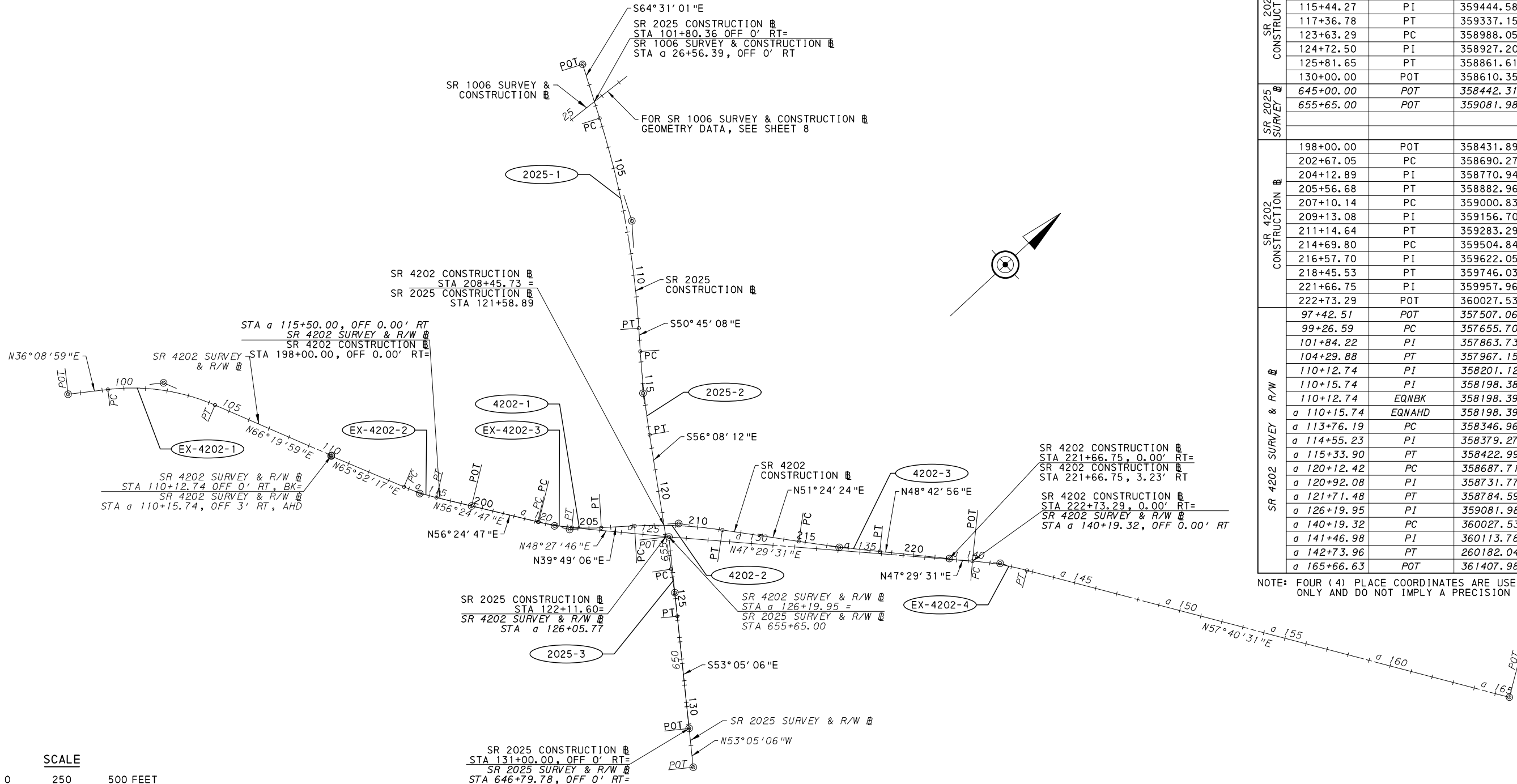
	2025-1	2025-2	2025-3	4202-1	4202-2	4202-3	EX-4202-1	EX-4202-2	EX-4202-3	EX-4202-4
PI STA	107+54.88	115+44.27	124+72.50	204+12.89	209+13.08	216+57.70	101+84.22	a 114+55.23	a 120+92.08	a 141+46.98
DELTA	13°45'54" RT	5°23'04" LT	3°03'06" RT	16°35'40" LT	11°35'17" RT	2°41'28" LT	30°11'00" RT	9°27'30" LT	7°57'01" LT	10°11'00" RT
T	494.88	192.80	109.21	145.84	202.94	187.90	257.63	79.04	79.66	127.65
L	985.00	385.31	218.36	289.63	404.50	375.73	503.29	157.71	159.06	254.64
R	4100.00	4100.00	4100.00	1000.00	2000.00	8000.00	955.37	955.37	1146.28	1432.69
E	29.76	4.53	1.45	10.58	10.27	2.21	34.13	3.26	2.76	5.68
SE	NC	NC	NC	NC	NC	NC	EX SLOPE	EX SLOPE	EX SLOPE	EX SLOPE
PCC STA										
PC STA	102+59.99	113+51.47	123+63.29	202+67.05	207+10.14	214+69.80	99+26.59	a 113+76.19	a 120+12.42	a 140+19.32
PT STA	112+44.99	117+36.78	125+81.65	205+56.68	211+14.64	218+45.53	104+29.88	a 115+33.90	a 121+71.48	a 142+73.96
PCC STA										

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	7 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

SUMMARY OF PROJECT COORDINATES  
 BASED ON THE PENNSYLVANIA STATE PLANE  
 COORDINATE SYSTEM (SOUTH ZONE), NAD DATUM 1983

RTE	STATION	POINT	COORDINATES		BEARINGS
			NORTH	EAST	
SR 2025 CONSTRUCTION	100+00.00	POT	360271.8122	2678386.6220	
	102+59.99	PC	360159.9521	2678621.3215	S64°31'01" E
	107+54.88	PI	359947.0327	2679068.0586	
	112+44.99	PT	359633.9320	2679451.3036	S50°45'08" E
	113+51.47	PC	359566.5638	2679533.7643	
	115+44.27	PI	359444.5866	2679683.0681	
	117+36.78	PT	359337.1582	2679843.1593	S58°08'12" E
	123+63.29	PC	358988.0586	2680363.3920	
	124+72.50	PI	358927.2067	2680454.0744	
	125+81.65	PT	358861.6136	2680541.3887	S53°05'06" E
SR 2025 SURVEY	130+00.00	POT	358610.3528	2680875.8552	
	645+00.00	POT	358442.3111	2681099.5445	N53°05'06" W
SR 4202 CONSTRUCTION	655+65.00	POT	359081.9804	2680248.0469	
	198+00.00	POT	358431.8994	2679401.1556	
	202+67.05	PC	358690.2724	2679790.2312	N56°24'47" E
	204+12.89	PI	358770.9489	2679911.7193	
	205+56.68	PT	358882.9619	2680005.1064	N39°49'07" E
	207+10.14	PC	359000.8305	2680103.3755	
	209+13.08	PI	359156.7049	2680233.3306	
	211+14.64	PT	359283.2977	2680391.9479	N51°24'24" E
	214+69.80	PC	359504.8449	2680669.5404	
	216+57.70	PI	359622.0552	2680816.4016	
	218+45.53	PT	359746.0312	2680957.5979	N48°42'56" E
	221+66.75	PC	359957.9690	2681198.9740	
	222+73.29	POT	360027.5318	2681279.6403	N47°29'31" E
SR 4202 SURVEY & R/W	97+42.51	POT	357507.0662	2677886.4881	
	99+26.59	PC	357655.7069	2677995.0762	N36°08'59" E
	101+84.22	PI	357863.7373	2678147.0510	
	104+29.88	PT	357967.1551	2678383.0123	N66°19'59" E
	110+12.74	PC	358201.1269	2678916.8502	S24°07'43" E
	110+15.74	PI	358198.3890	2678918.0766	
	110+12.74	EQNBK	358198.3901	2678918.0789	
	a 110+15.74	EQNAHD	358198.3901	2678918.0789	
	a 113+76.19	PC	358346.9628	2679249.7743	N65°52'17" E
	a 114+55.23	PI	358379.2714	2679321.9048	
	a 115+33.90	PT	358422.9941	2679387.7454	N56°24'47" E
	a 120+12.42	PC	358687.7115	2679786.3748	
	a 120+92.08	PI	358731.7775	2679852.7324	
	a 121+71.48	PT	358784.5982	2679912.3572	N48°27'46" E
	a 126+19.95	PI	359081.9804	2680248.0469	
a 140+19.32	PC	360027.5318	2681279.6403	N47°29'31" E	
a 141+46.98	PI	360113.7871	2681373.7446		
a 142+73.96	PT	260182.0461	2681481.6162	N57°40'31" E	
a 165+66.63	POT	361407.9801	2683418.9933		

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.



GEOMETRY PLAN

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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	8 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

**SUMMARY OF CURVE DATA**

	1006-1	1006-2	1006-3	1006-4	0152-1	0152-2
PI STA	9+79.98	21+84.88	a 30+29.55	a 35+74.00	11+25.16	14+14.24
DELTA	11°32'47" LT	31°34'00" LT	9°57'04" RT	8°44'51" RT	17°08'10" LT	1°41'43" LT
T	36.28	323.90	182.82	151.82	50.91	64.11
L	72.31	631.34	364.73	303.06	101.06	128.21
R	358.82	1145.92	2100.00	1985.00	337.89	4333.00
E	1.83	44.90	7.94	5.80	3.81	0.47
SE	NC	5.20%	3.80%	EX SLOPE	EX SLOPE	EX SLOPE
PCC STA						
PC STA	9+43.70	18+60.98	a 28+46.73	a 34+22.18	10+74.25	13+50.13
PT STA	10+16.01	24+92.31	a 32+11.46	a 37+25.24	11+75.31	14+78.34
PCC STA						

**SUMMARY OF PROJECT COORDINATES**  
BASED ON THE PENNSYLVANIA STATE PLANE  
COORDINATE SYSTEM (SOUTH ZONE), NAD DATUM 1983

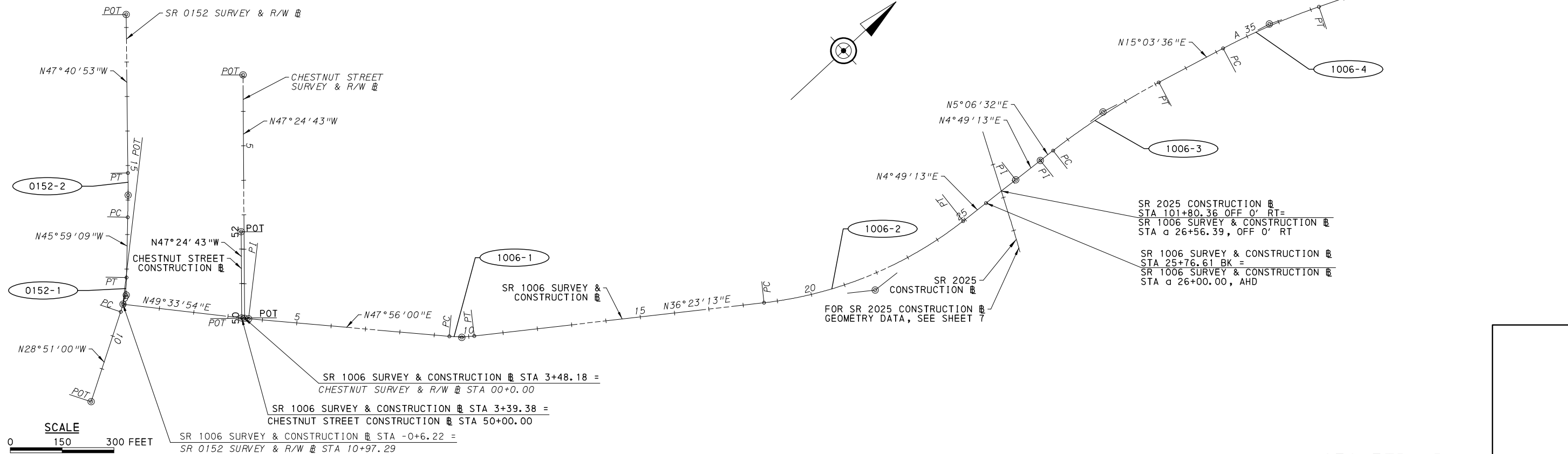
RTE	STATION	POINT	COORDINATES		BEARINGS
			NORTH	EAST	
SR 0152 SURVEY & R/W	8+00.00	POT	357848.1950	2677200.5863	
	10+74.25	PC	358088.4094	2677068.2549	N28°51'00"W
	11+25.16	PI	358132.9992	2677043.6909	
	11+75.31	PT	358168.3719	2677007.0794	N45°59'09"W
	13+50.13	PC	358289.8417	2676881.3557	
	14+14.24	PI	358334.3887	2676835.2487	
	14+78.34	PT	358377.5521	2676787.8438	N47°40'53"W
19+37.82	POT	358686.8994	2676448.0986		
CHESTNUT STREET SURVEY & R/W	50+00.00	POT	358332.7290	2677319.9657	N47°24'43"W
	52+50.00	POT	358501.9099	2677135.9064	
CHESTNUT STREET SURVEY & R/W	0+00.00	POT	358338.0479	2677326.2076	N47°24'43"W
	7+05.67	POT	358815.5896	2676806.6692	

**SUMMARY OF PROJECT COORDINATES**  
BASED ON THE PENNSYLVANIA STATE PLANE  
COORDINATE SYSTEM (SOUTH ZONE), NAD DATUM 1983

RTE	STATION	POINT	COORDINATES		BEARINGS
			NORTH	EAST	
SR 1006 SURVEY & CONSTRUCTION	-0+06.22	POT	358108.1911	2677056.4613	N49°33'54"E
	3+60.29	PI	358345.9070	2677335.4306	
	9+43.70	PC	358736.7851	2677768.5322	N47°56'00"E
	9+79.98	PI	358761.0913	2677795.4639	
	10+61.01	PT	358790.2963	2677816.9855	N36°23'13"E
	18+60.98	PC	359470.5167	2678318.2493	
	21+84.88	PI	359731.2673	2678510.4000	
	24+92.31	PT	360054.0240	2678537.6178	N4°49'13"E
	25+76.61	EQNBK	360138.0250	2678544.7016	
	a 26+00.00	EQNAH	360138.0250	2678544.7016	
	a 27+09.32	PI	360246.9542	2678553.8876	
	a 27+99.99	PI	360337.3085	2678561.5072	
	a 28+46.73	PC	360383.8606	2678565.6690	N5°06'32"E
	a 30+29.55	PI	360565.9577	2678581.9488	
	a 32+11.46	PT	360742.5021	2678629.4515	N15°03'36"E
	a 34+22.18	PC	360945.9849	2678684.2025	
	a 35+74.00	PI	361092.5954	2678723.6509	
	a 37+25.24	PT	361231.5012	2678784.9373	N23°48'27"E
	a 44+47.81	POT	361892.5907	2679076.6158	

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.



**GEOMETRY PLAN**

# SUMMARY OF PROJECT COORDINATES

BASED ON THE PENNSYLVANIA STATE PLANE  
COORDINATE SYSTEM (SOUTH ZONE), NAD DATUM 1983

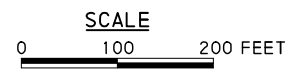
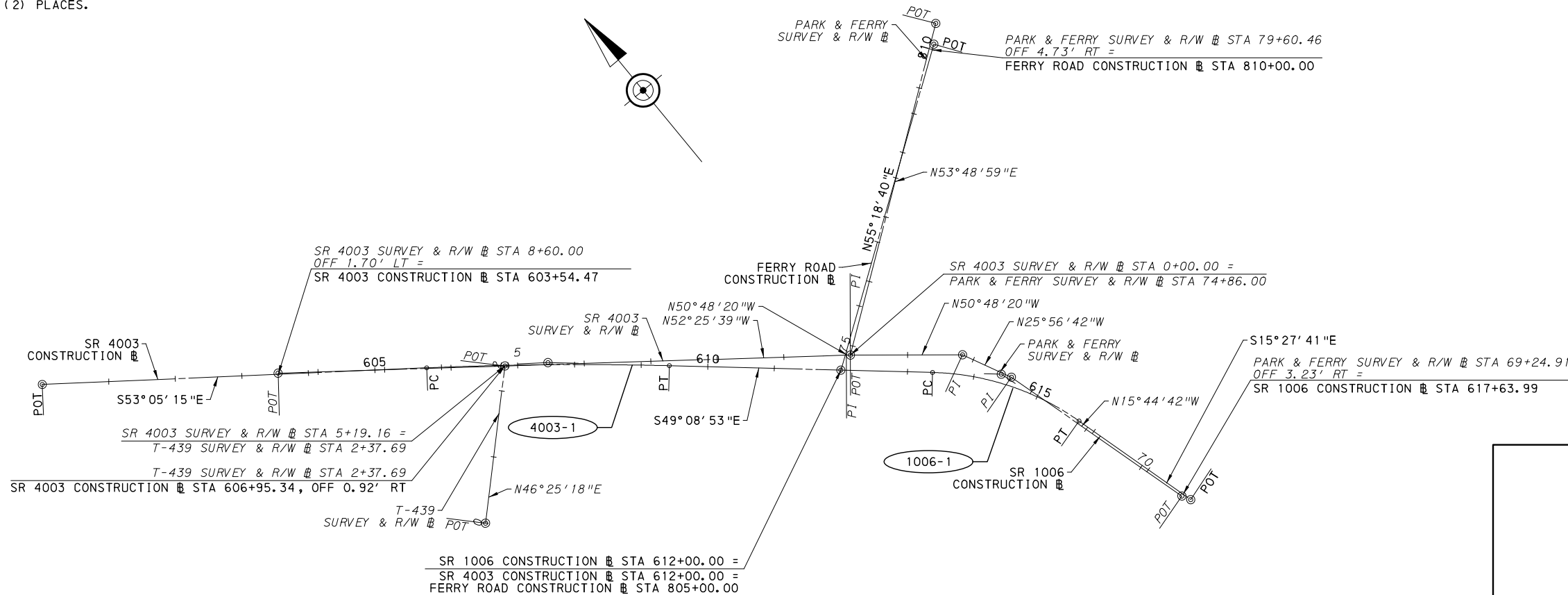
RTE	STATION	POINT	COORDINATES		BEARINGS
			NORTH	EAST	
SR 4003 SURVEY & R/W	0+00.00	POT	364756.6827	2679426.2916	N50°48'20"W
	0+06.17	PI	364760.5847	2679421.5063	
	8+60.00	POT	365281.2167	2678744.7780	N52°25'39"W
PARK & FERRY SURVEY & R/W	69+24.84	POT	364277.3325	2679676.7763	N15°44'42"W
	72+36.90	PI	364577.6823	2679592.0980	N25°56'42"W
	73+17.64	PI	364650.2850	2679556.7737	N50°48'20"W
	74+86.00	PI	364756.6827	2679426.2916	N50°48'20"W
	80+00.00	POT	365060.1353	2679841.1560	N53°48'59"E
SR 1006 / SR 4003 CONSTRUCTION	600+00.00	POT	365492.7605	2678460.3218	S53°05'15"E
	605+77.82	PC	365145.7261	2678922.3183	
	607+60.10	PI	365036.2500	2679068.0607	
	609+42.23	PT	364917.0195	2679205.9372	S49°08'53"E
	613+37.69	PC	364658.3507	2679505.0585	
	614+58.78	PI	364579.1400	2679596.6568	
	615+72.86	PT	364462.4250	2679628.9398	S15°27'41"E
FERRY ROAD CONSTRUCTION	805+00.00	POT	364748.4126	2679400.9122	N55°18'40"E
	810+08.68	POT	365037.9132	2679819.1765	
T-439 SURVEY & R/W	0+00.00	POT	364909.5328	2678842.7321	N46°25'18"E
	2+37.69	POT	365073.3837	2679014.9237	

# SUMMARY OF CURVE DATA

	4003-1	1006-1
PI STA	607+60.10	614+58.78
DELTA	3°56'22" RT	33°41'12" RT
T	182.28	121.10
L	364.42	235.18
R	5300.00	400.00
E	3.13	17.93
SE	NC	6.00%
PCC STA		
PC STA	605+77.82	613+37.69
PT STA	609+42.23	615+72.86
PCC STA		

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	9 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.

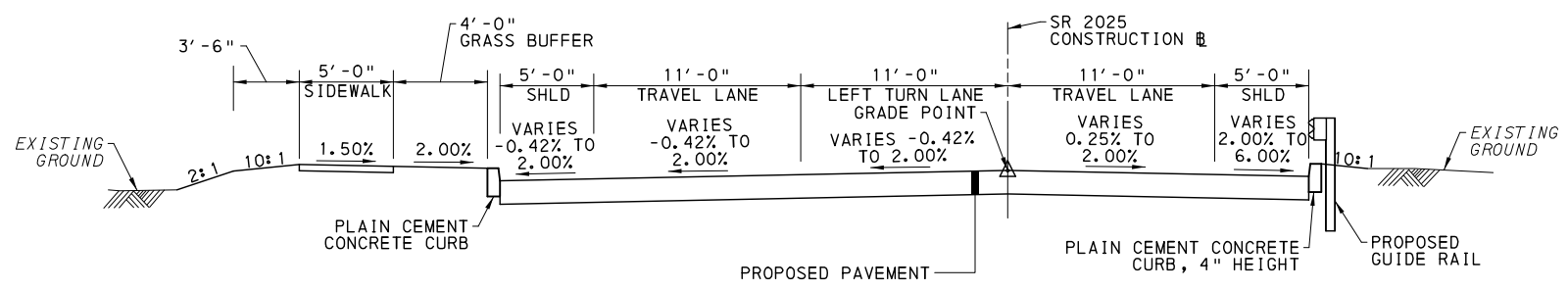


GEOMETRY PLAN

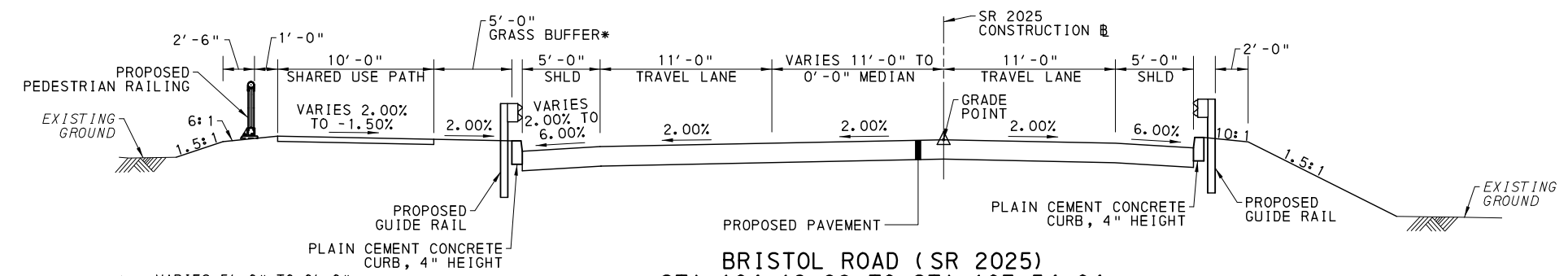
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	10 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

NOTE: PROPOSED PAVEMENT SECTION TO BE DETERMINED

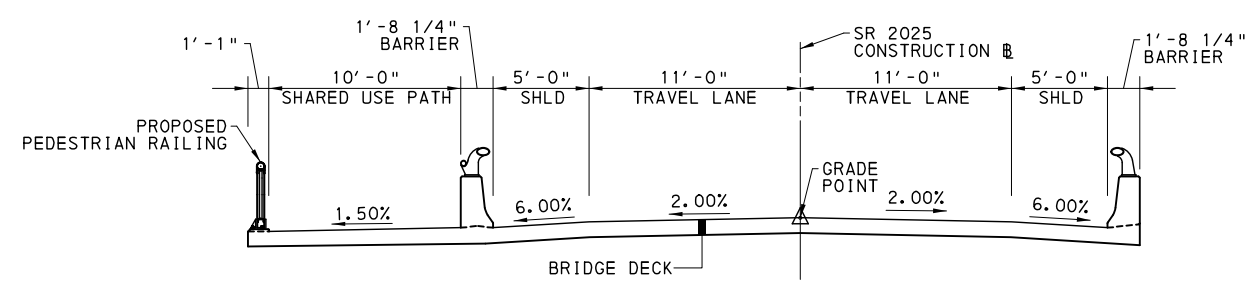


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STA 101+99.60 TO STA 104+12.98  
TYPICAL SECTION  
NOT TO SCALE

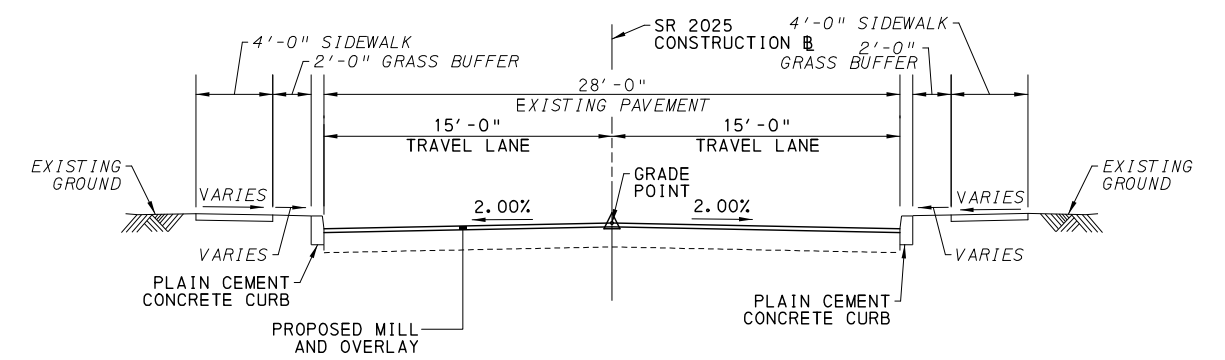


BRISTOL ROAD (SR 2025)  
STA 104+12.98 TO STA 107+54.04  
TYPICAL SECTION  
NOT TO SCALE

\* - VARIES 5'-0" TO 0'-0"  
FROM STA 106+64.15 TO 107+20.84



BRISTOL ROAD (SR 2025)  
STA 107+54.04 TO STA 112+56.00  
TYPICAL BRIDGE SECTION  
NOT TO SCALE

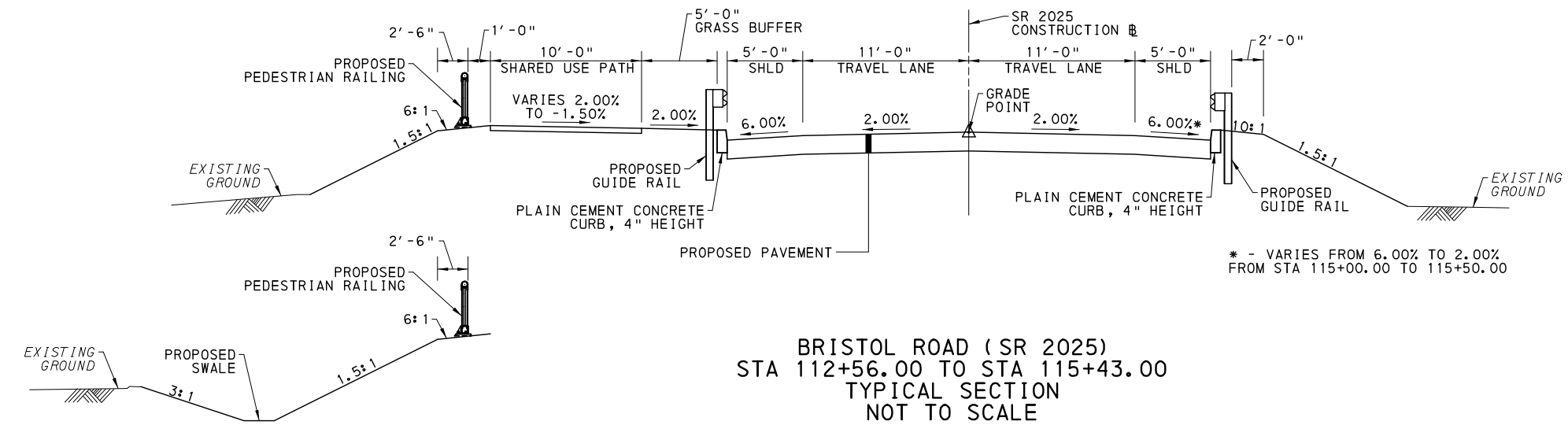


BRIDGEWATER COURT  
STA 100+00.00 TO STA 101+57.65  
TYPICAL SECTION  
NOT TO SCALE

TYPICAL SECTIONS

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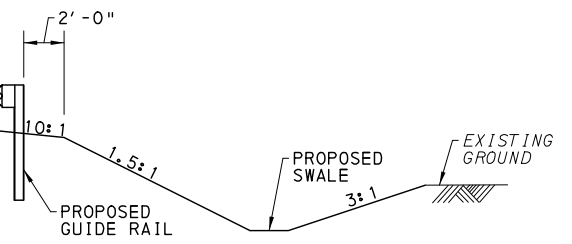
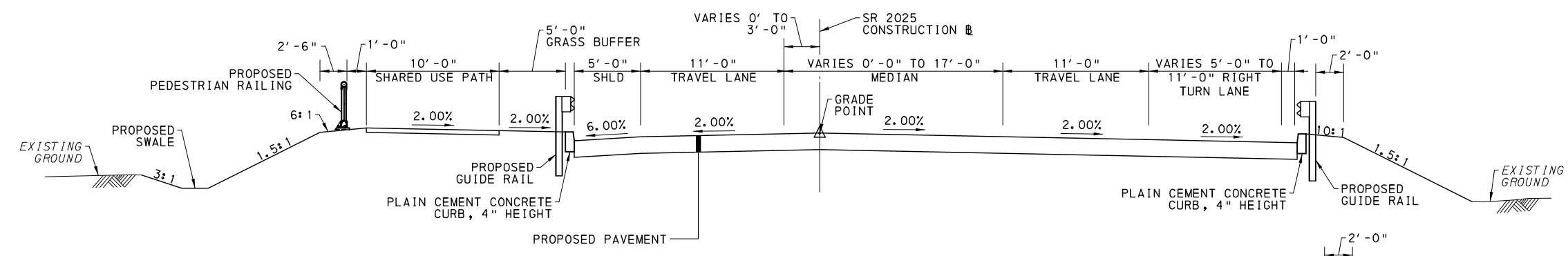
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	11 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED

SWALE LEFT  
PARTIAL SECTION  
STA XX+XX.00 TO XX+XX.00  
NOT TO SCALE

BRISTOL ROAD (SR 2025)  
STA 112+56.00 TO STA 115+43.00  
TYPICAL SECTION  
NOT TO SCALE

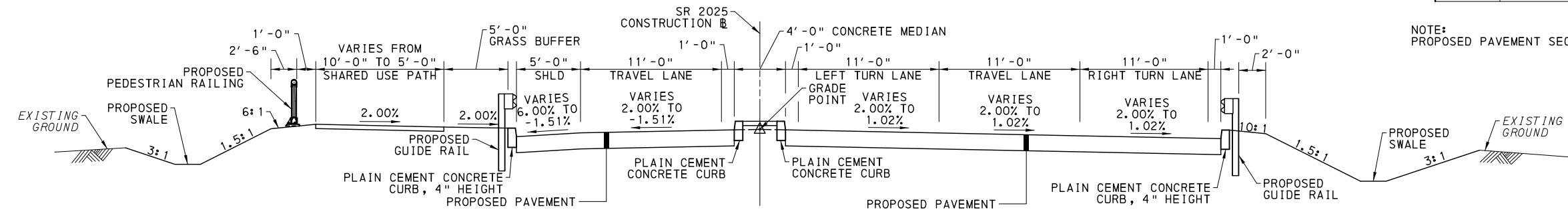


SWALE RIGHT  
PARTIAL SECTION  
STA XX+XX.00 TO XX+XX.00  
NOT TO SCALE

BRISTOL ROAD (SR 2025)  
STA 115+43.00 TO STA 118+76.87  
TYPICAL SECTION  
NOT TO SCALE

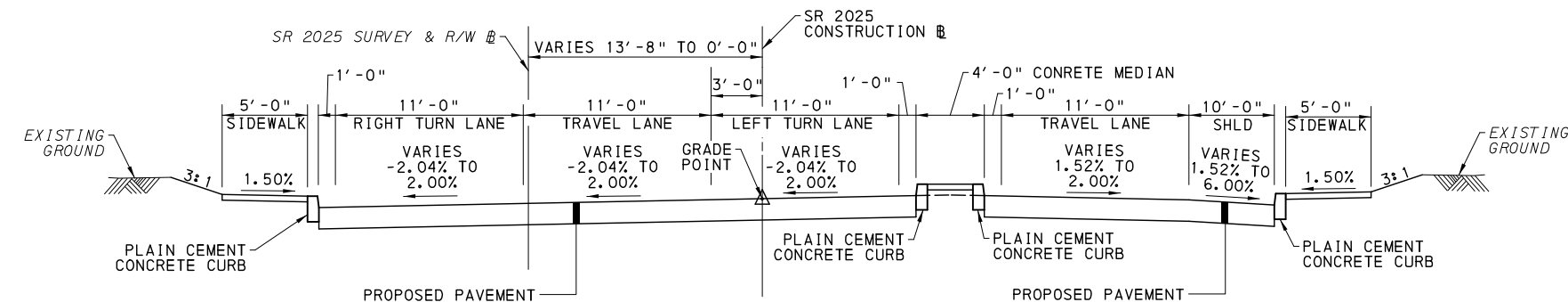
TYPICAL SECTIONS

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	12 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED

BRISTOL ROAD (SR 2025)  
STA 118+76.87 TO STA 121+58.98  
TYPICAL SECTION  
NOT TO SCALE



BRISTOL ROAD (SR 2025)  
STA 121+58.98 TO STA 124+53.07  
TYPICAL SECTION  
NOT TO SCALE

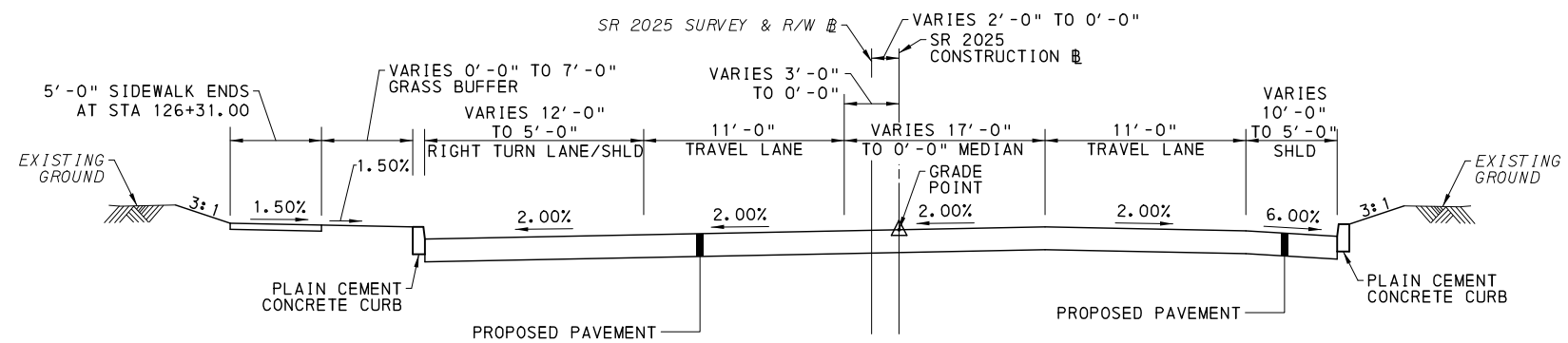
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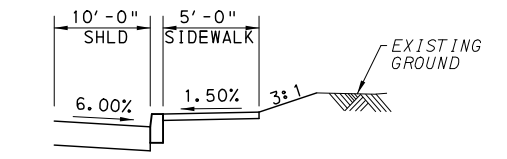


DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	13 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

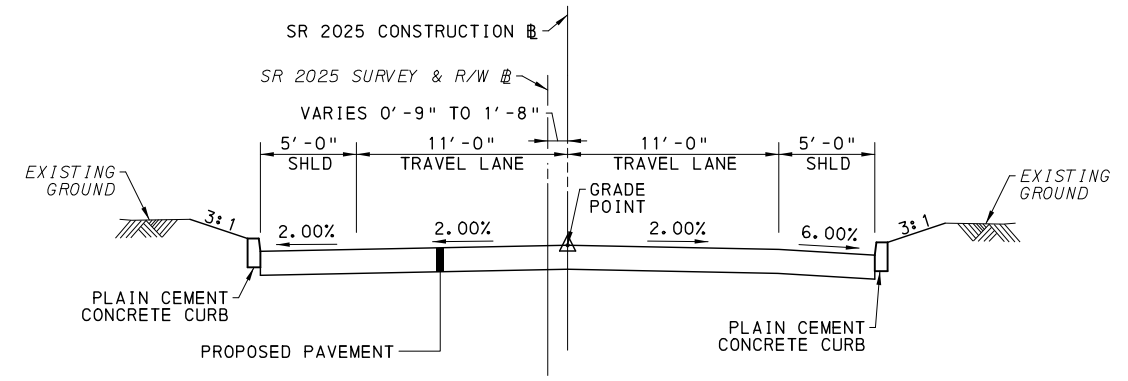
NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED



BRISTOL ROAD (SR 225)  
STA 124+53.07 TO STA 127+38.02  
TYPICAL SECTION  
NOT TO SCALE



SIDEWALK RIGHT  
PARTIAL SECTION  
STA 124+53.07 TO STA 124+90.32  
NOT TO SCALE



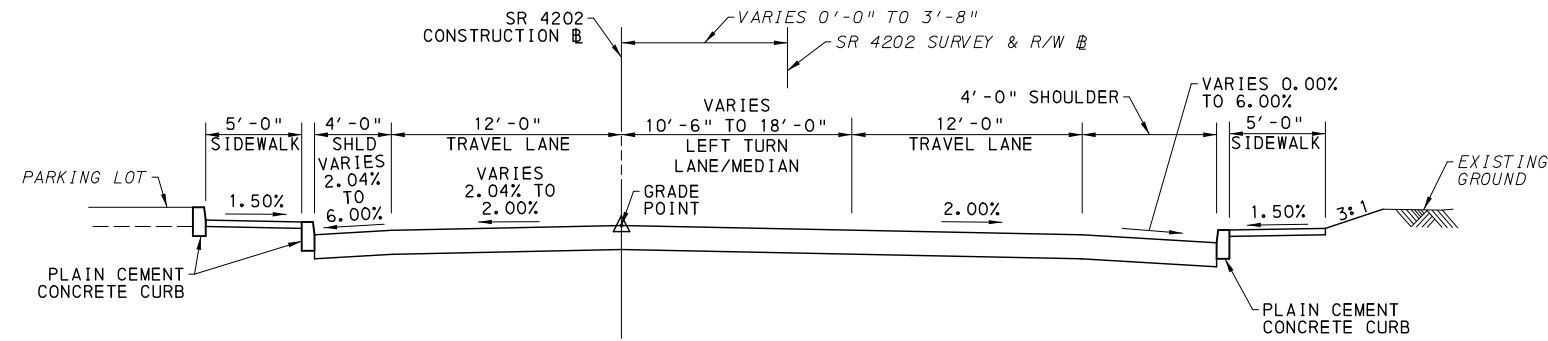
BRISTOL ROAD (SR 225)  
STA 127+38.02 TO STA 129+50.00  
TYPICAL SECTION  
NOT TO SCALE

TYPICAL SECTIONS

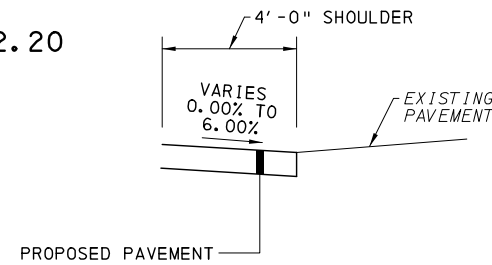
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	14 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	

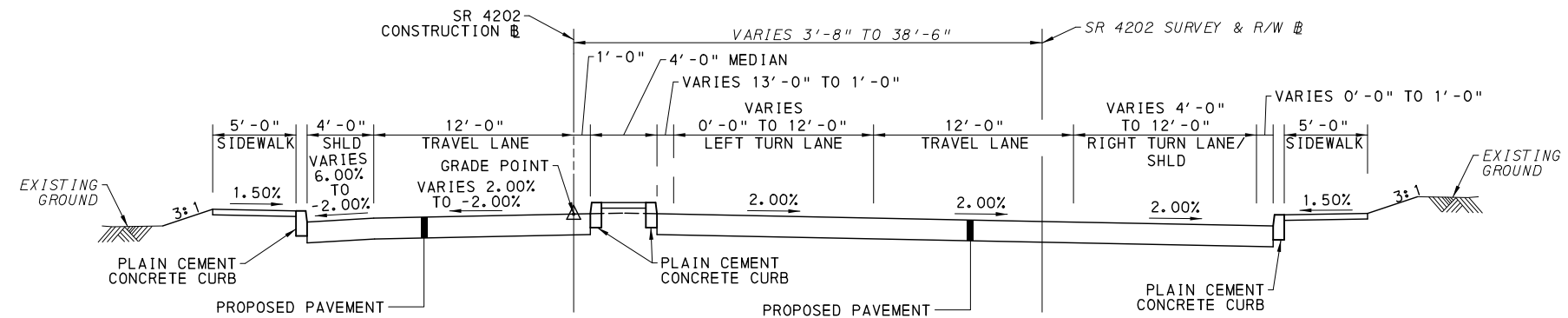
NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED



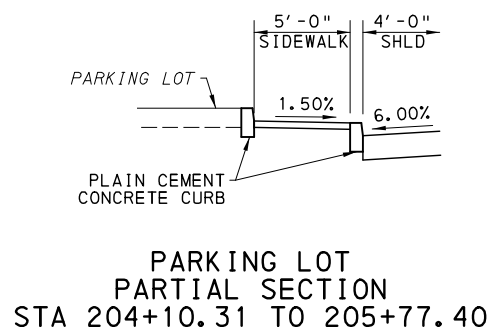
BUTLER AVE (SR 4202)  
STA 202+75.00 TO STA 204+82.20  
TYPICAL SECTION  
NOT TO SCALE



END CONDITION  
PARTIAL SECTION  
STA 202+75.00 TO STA 203+47.94



BUTLER AVE (SR 4202)  
STA 204+82.20 TO STA 207+30.51  
TYPICAL SECTION  
NOT TO SCALE

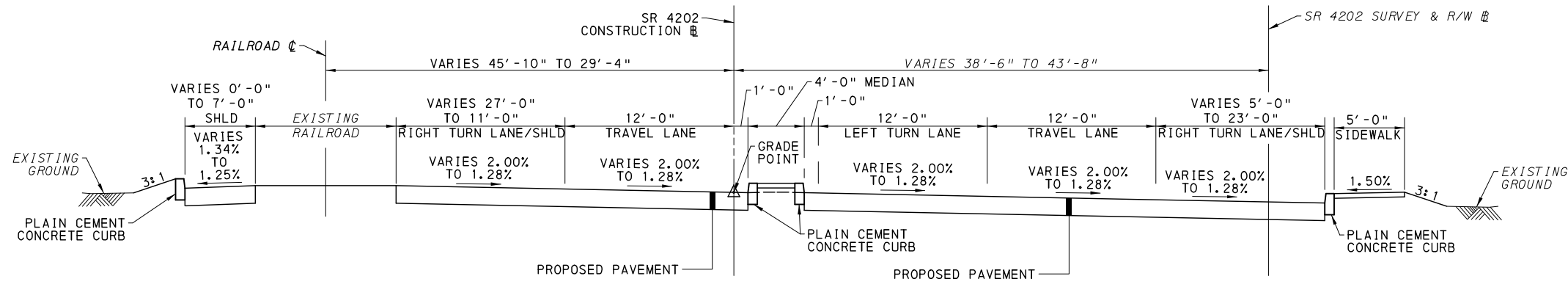


PARKING LOT  
PARTIAL SECTION  
STA 204+10.31 TO STA 205+77.40

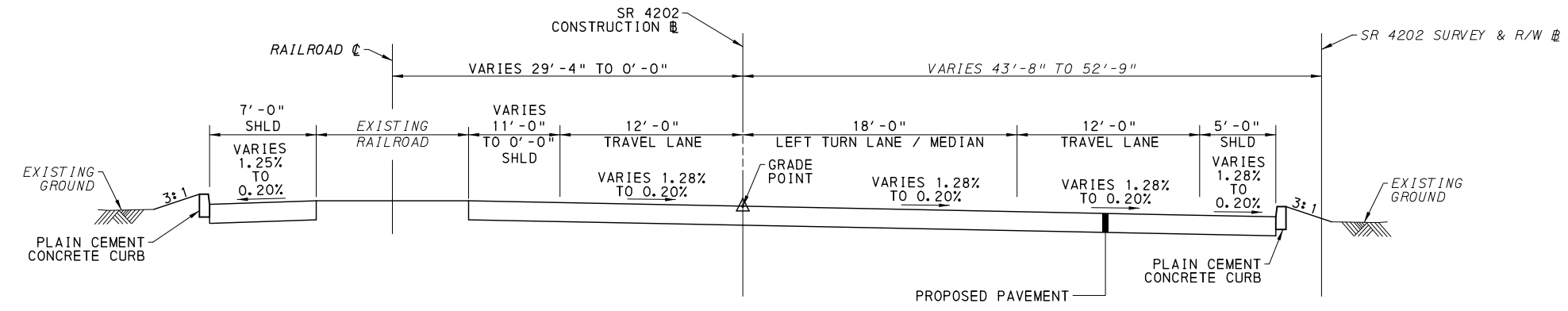
TYPICAL SECTIONS

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	15 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

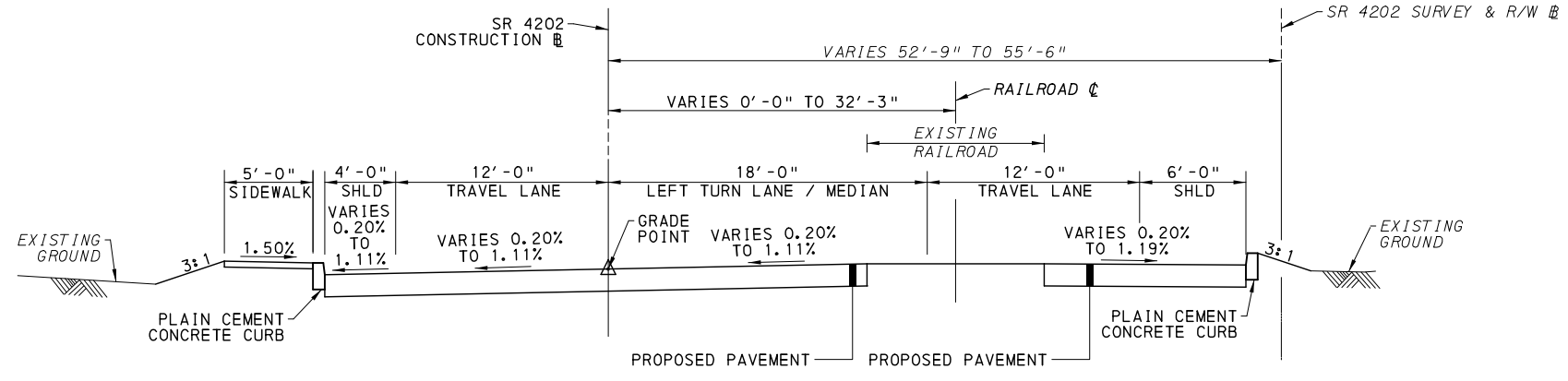
NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED



BUTLER AVE (SR 4202)  
STA 207+30.51 TO STA 207+70.18  
TYPICAL SECTION  
NOT TO SCALE



BUTLER AVE (SR 4202)  
STA 207+70.18 TO STA 208+45.73  
TYPICAL SECTION  
NOT TO SCALE



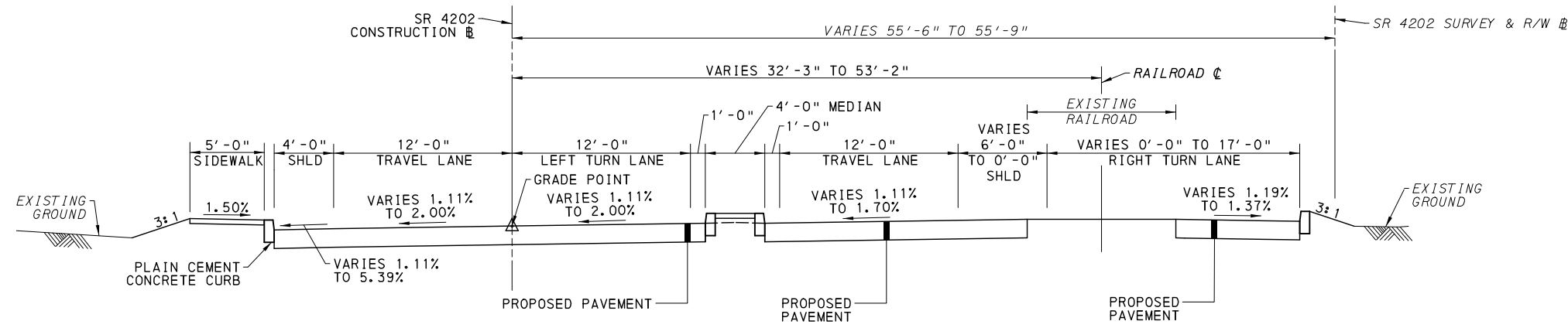
BUTLER AVE (SR 4202)  
STA 208+45.73 TO STA 209+42.98  
TYPICAL SECTION  
NOT TO SCALE

TYPICAL SECTIONS

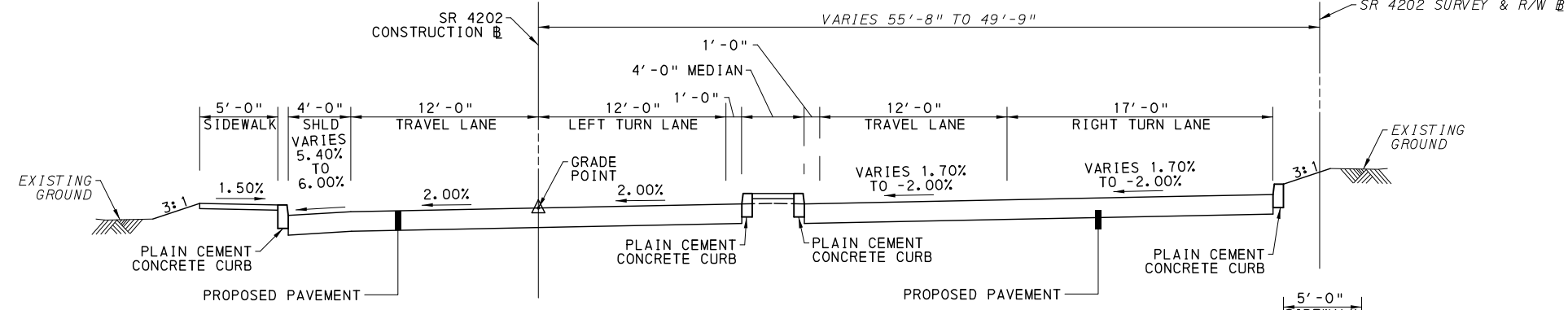
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	16 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	

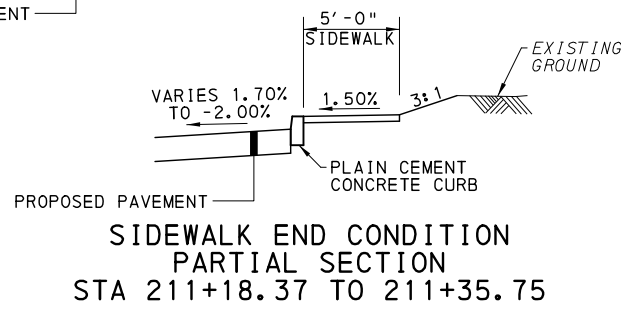
NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED



BUTLER AVE (SR 4202)  
STA 209+42.98 TO STA 210+17.41  
TYPICAL SECTION  
NOT TO SCALE



BUTLER AVE (SR 4202)  
STA 210+17.41 TO STA 211+35.75  
TYPICAL SECTION  
NOT TO SCALE

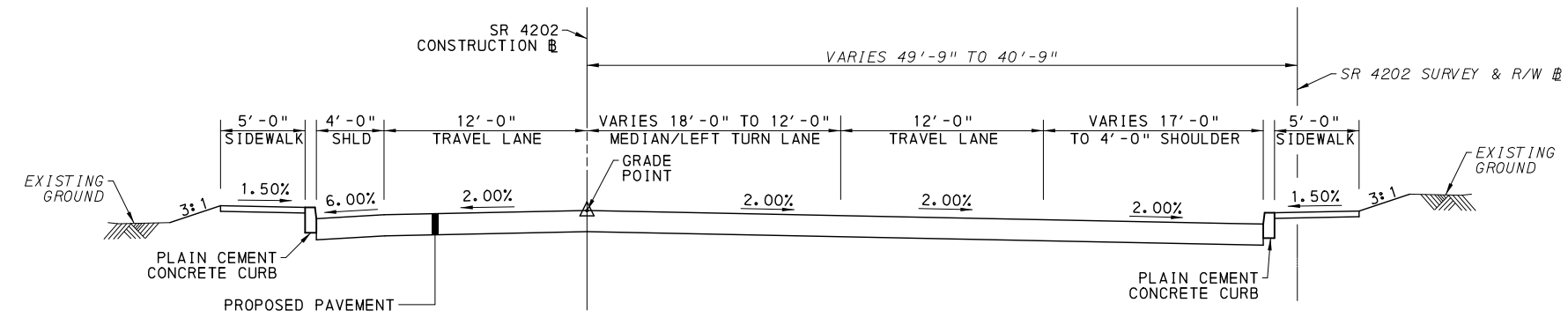


SIDEWALK END CONDITION  
PARTIAL SECTION  
STA 211+18.37 TO 211+35.75

TYPICAL SECTIONS

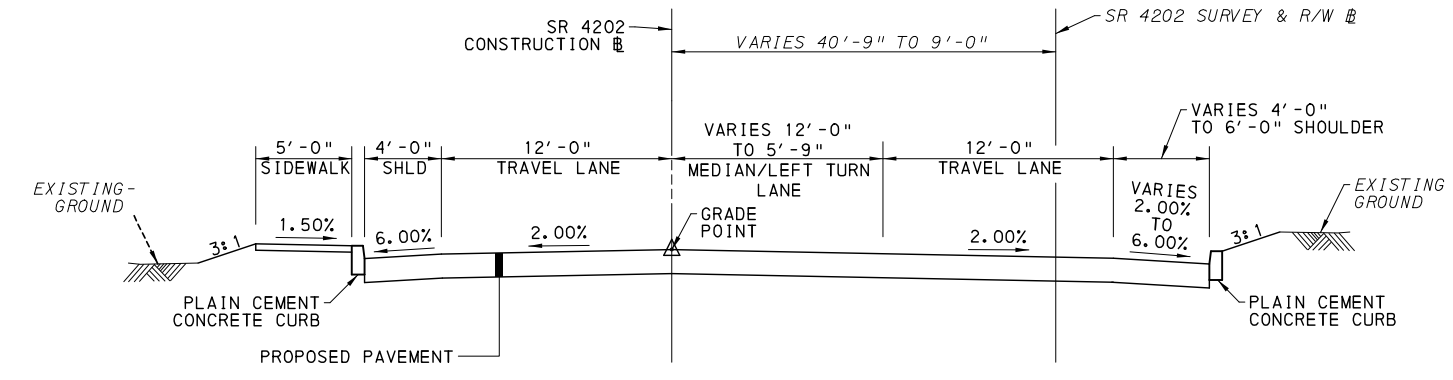
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	17 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

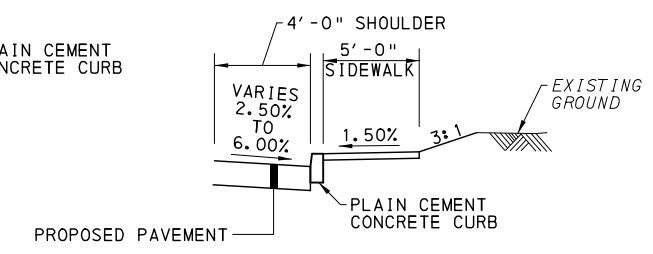


BUTLER AVE (SR 4202)  
 STA 211+35.75 TO STA 212+69.04  
 TYPICAL SECTION  
 NOT TO SCALE

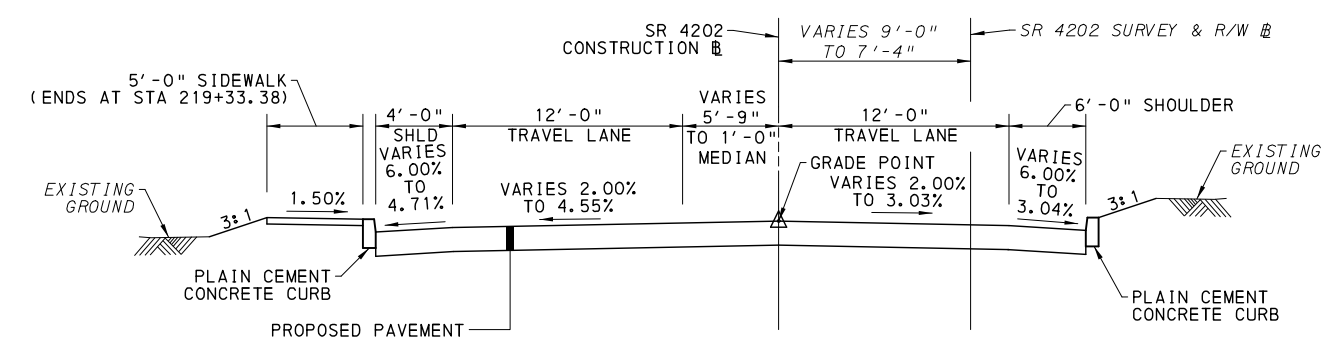
NOTE:  
 PROPOSED PAVEMENT SECTION TO BE DETERMINED



BUTLER AVE (SR 4202)  
 STA 212+69.04 TO STA 218+92.91  
 TYPICAL SECTION  
 NOT TO SCALE



SIDEWALK END CONDITION  
 PARTIAL SECTION  
 STA 212+69.04 TO 213+13.04



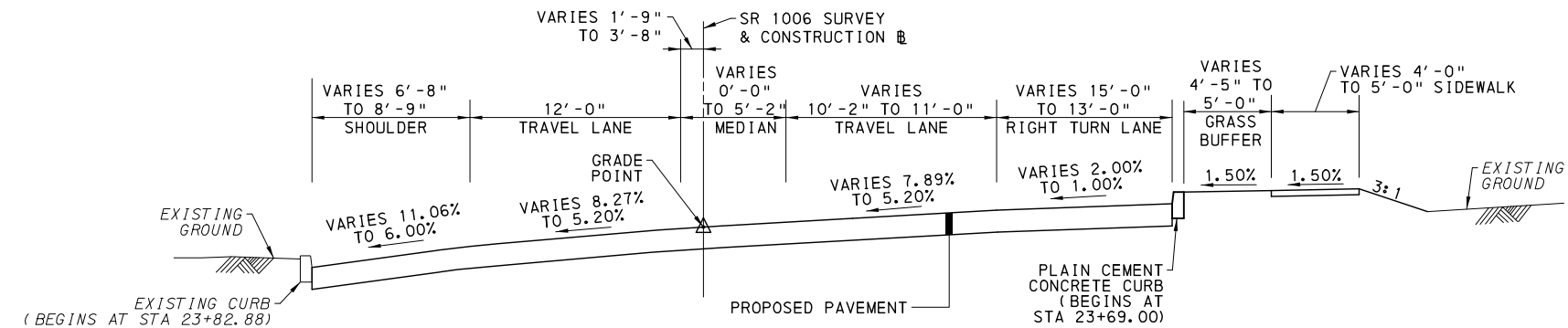
BUTLER AVE (SR 4202)  
 STA 218+92.91 TO STA 219+75.00  
 TYPICAL SECTION  
 NOT TO SCALE

TYPICAL SECTIONS

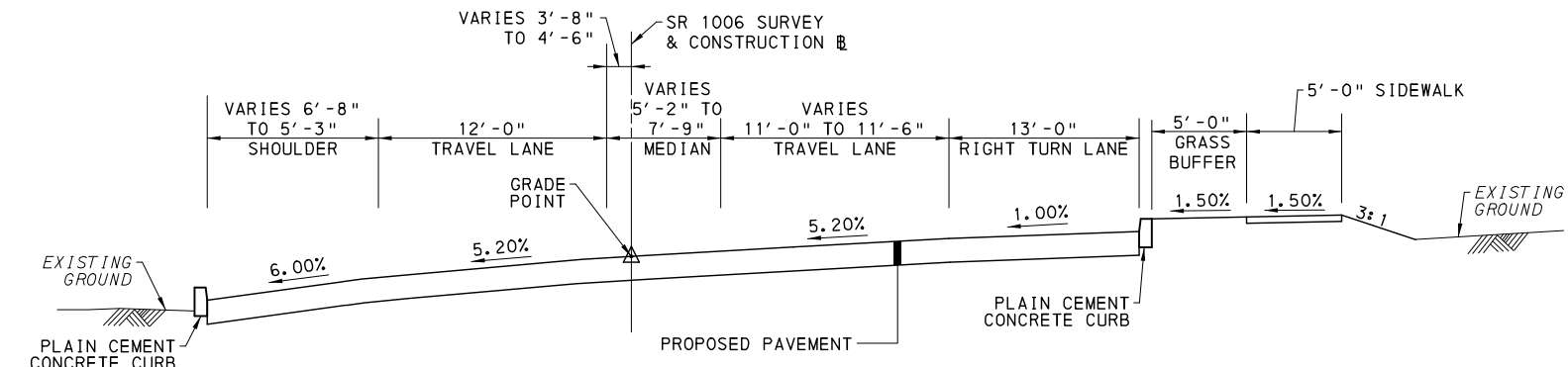
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	18 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

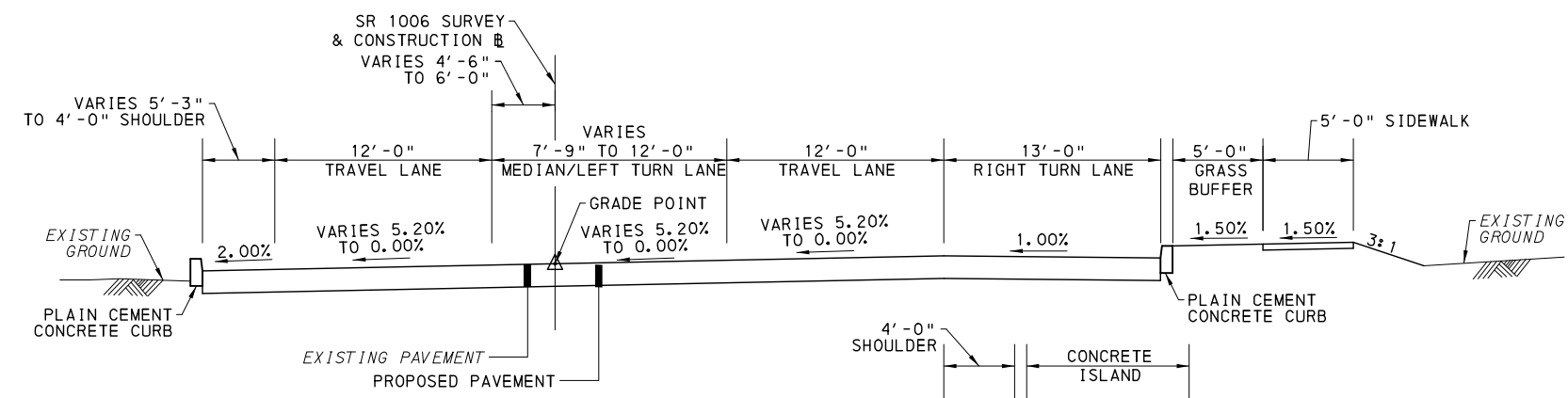
NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED



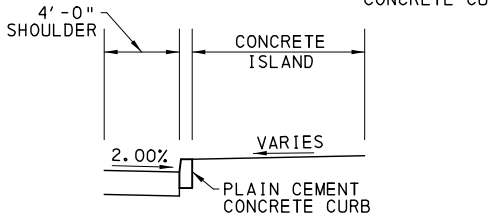
PARK AVENUE (SR 1006)  
STA 23+25.00 TO STA 24+00.00  
TYPICAL SECTION  
NOT TO SCALE



PARK AVENUE (SR 1006)  
STA 24+00.00 TO STA 24+39.31  
TYPICAL SECTION  
NOT TO SCALE



PARK AVENUE (SR 1006)  
STA 24+39.31 TO STA 27+05.00  
TYPICAL SECTION  
NOT TO SCALE

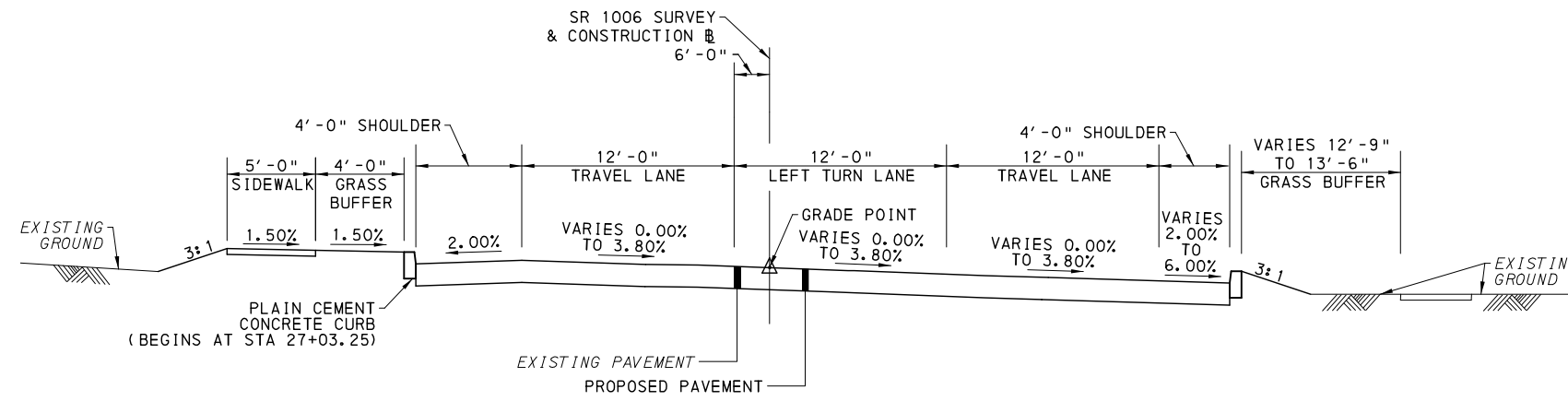


ISLAND RIGHT  
PARTIAL SECTION  
STA 25+44.22 TO 27+05.00  
NOT TO SCALE

TYPICAL SECTIONS

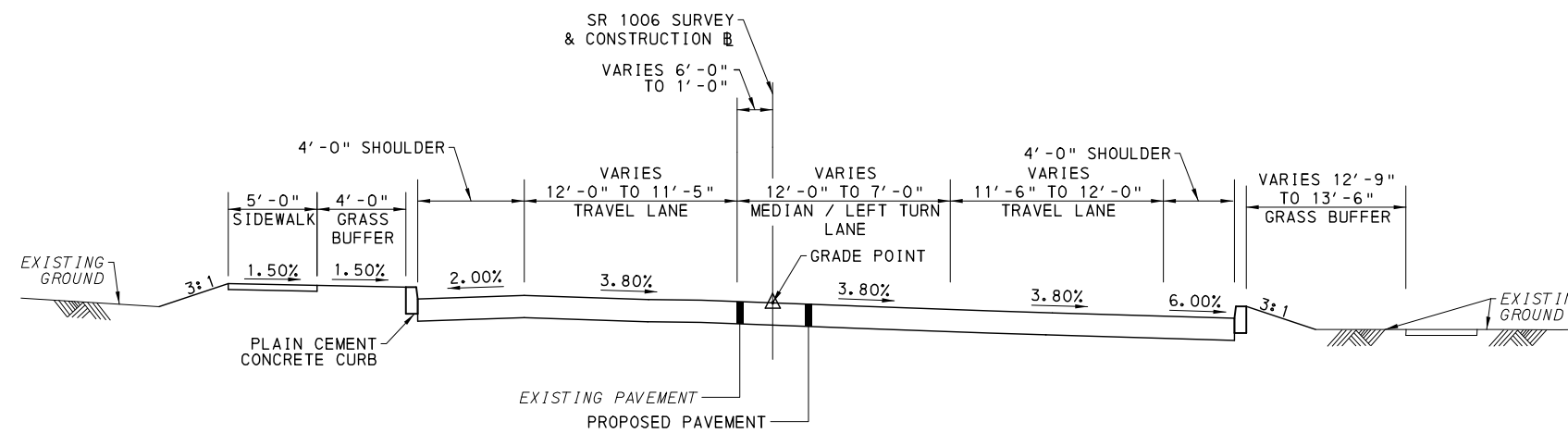
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	19 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

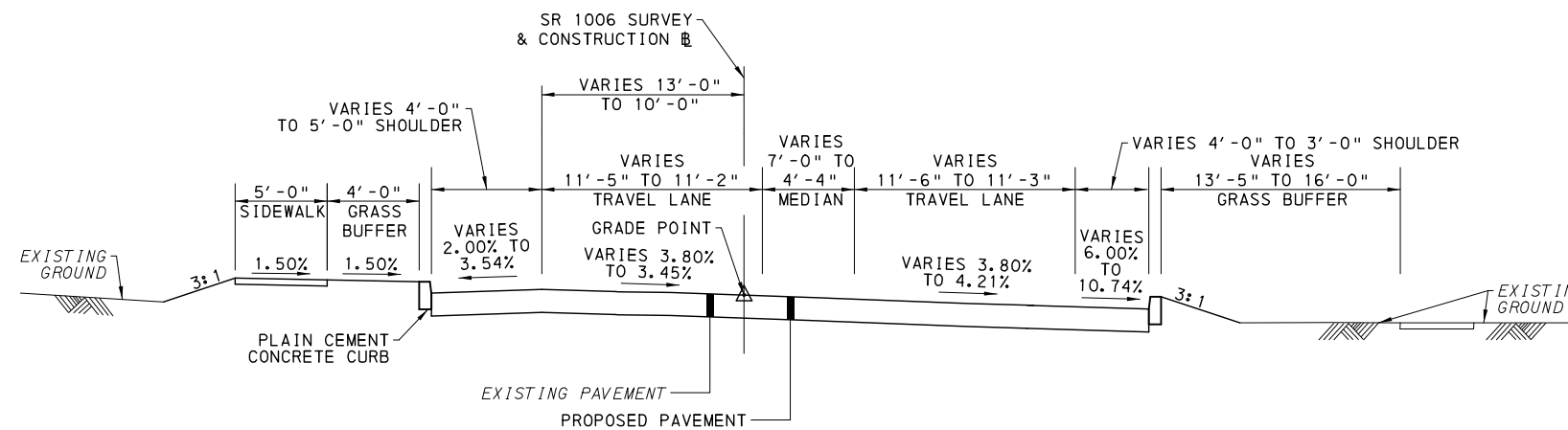


PARK AVENUE (SR 1006)  
 STA 27+05.00 TO STA 28+84.73  
 TYPICAL SECTION  
 NOT TO SCALE

NOTE:  
 PROPOSED PAVEMENT SECTION TO BE DETERMINED



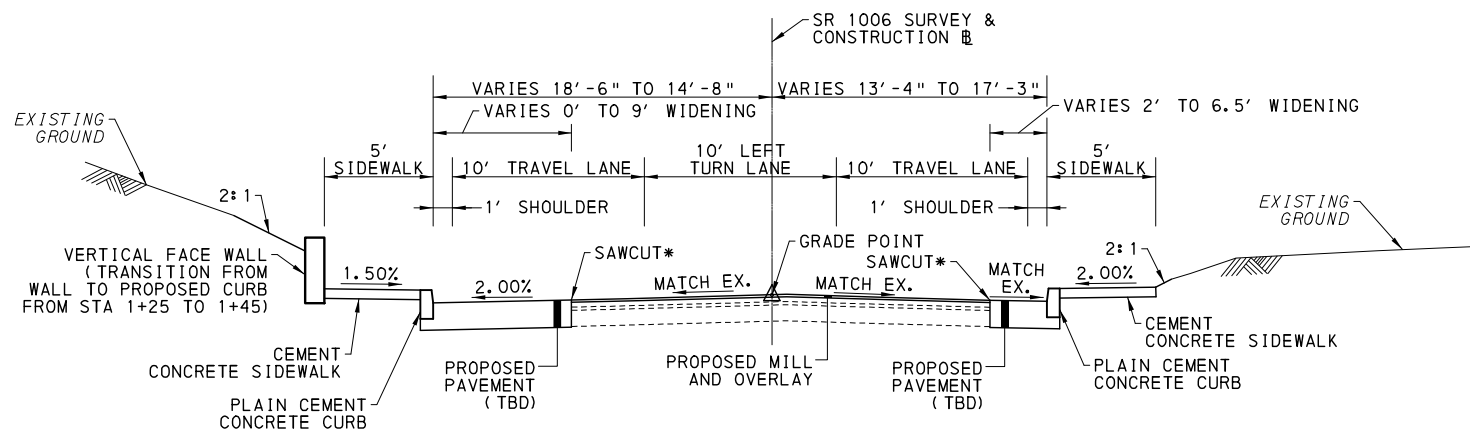
PARK AVENUE (SR 1006)  
 STA 28+84.73 TO STA 30+63.00  
 TYPICAL SECTION  
 NOT TO SCALE



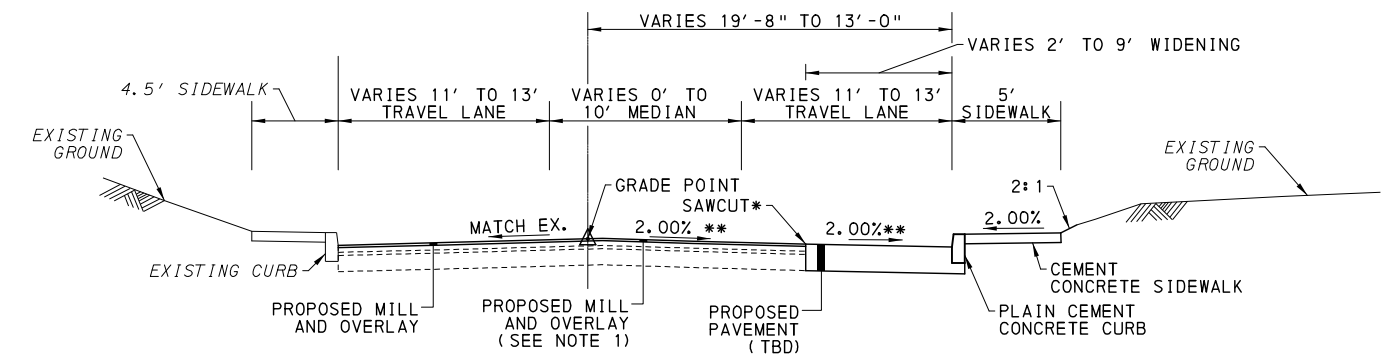
PARK AVENUE (SR 1006)  
 STA 30+63.00 TO STA 31+13.00  
 TYPICAL SECTION  
 NOT TO SCALE

TYPICAL SECTIONS

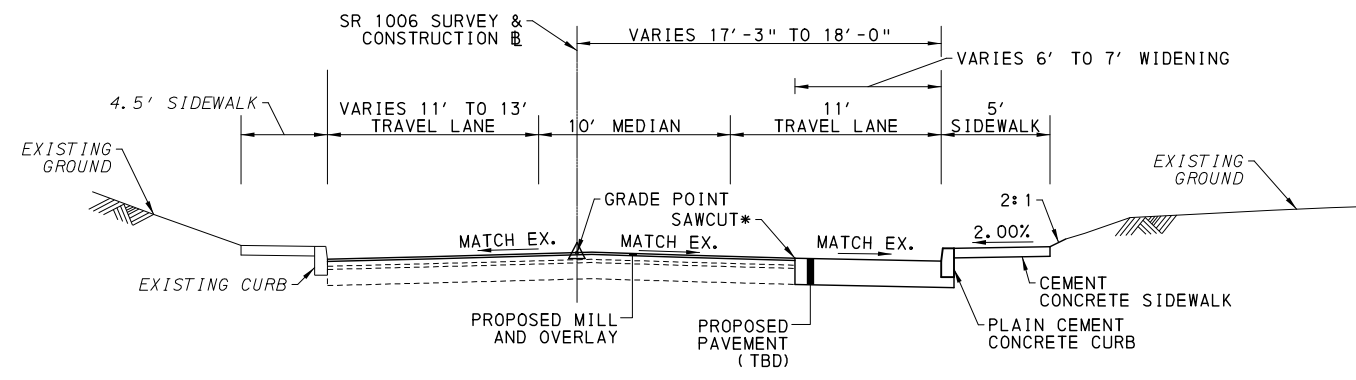
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	20 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



PARK AVENUE (SR 1006)  
STA 00+12.00 TO STA 3+26.26  
TYPICAL SECTION  
NOT TO SCALE



PARK AVENUE (SR 1006)  
STA 3+50.74 TO STA 6+00.00  
TYPICAL SECTION  
NOT TO SCALE



PARK AVENUE (SR 1006)  
STA 3+26.26 TO STA 3+50.74  
TYPICAL SECTION  
NOT TO SCALE

NOTES:  
 \* SAWCUT EXISTING PAVEMENT 2'-0" FROM THE FACE OF EXISTING CURB.  
 1. CORRECT CROSS SLOPE USING MILLING OF ASPHALT PAVEMENT SURFACE, VARIABLE DEPTH, MILLED MATERIAL RETAINED BY CONTRACTOR BETWEEN STATION 3+50.74 RT TO STA 5+50.00 RT.  
 PROPOSED PAVEMENT SECTION TO BE DETERMINED

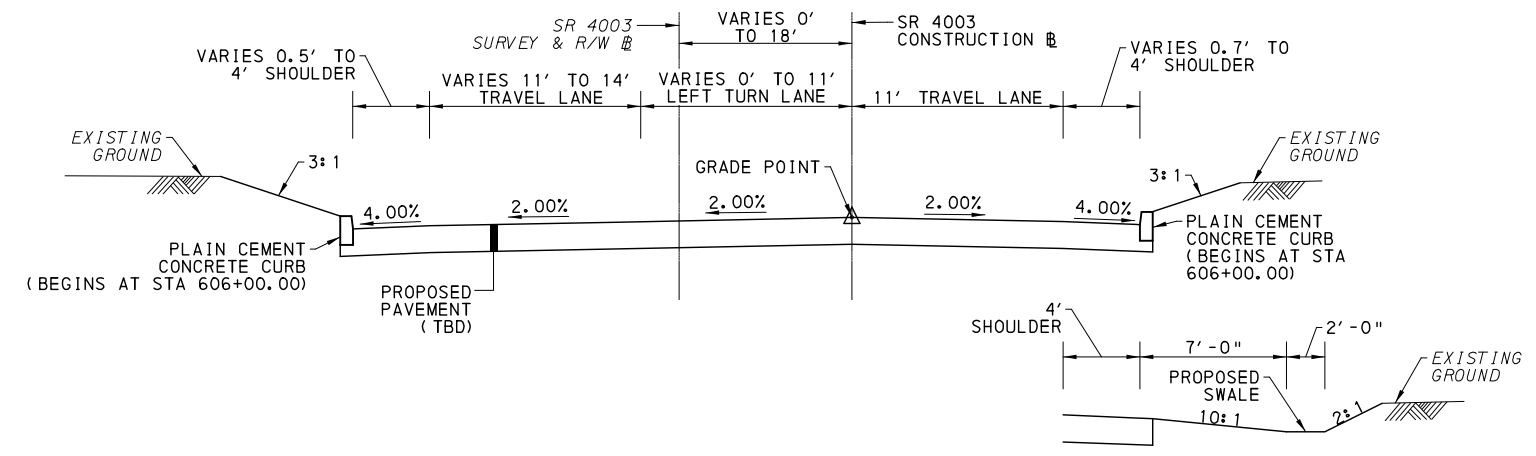
\*\* - VARIES 0.20% TO 2.00% FROM STA 3+50.74 TO STA 4+00.74.  
 \*\* - 2.00% FROM STA 4+00.74 TO STA 5+25.00  
 \*\* - VARIES 2.00% TO 2.98% FROM STA 5+25.00 TO STA 5+50.00.  
 \*\* MATCH EXISTING FROM STA 5+50.00 TO STA 6+00.00

TYPICAL SECTIONS



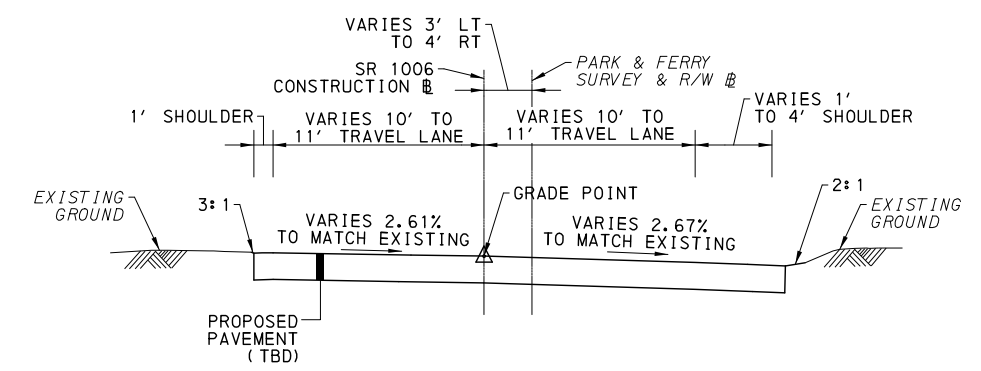
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	21 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

NOTE:  
PROPOSED PAVEMENT SECTION TO BE DETERMINED

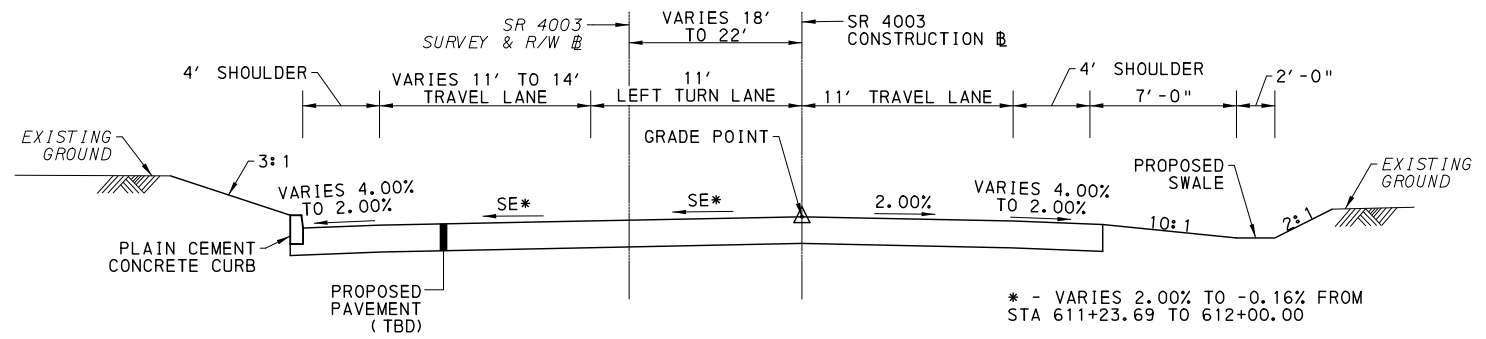


SWALE RIGHT  
PARTIAL SECTION  
STA 607+46.00 TO 611+23.69  
NOT TO SCALE

ALLOWHILL ROAD (SR 4003)  
STA 605+00.00 TO STA 611+23.69  
TYPICAL SECTION  
NOT TO SCALE

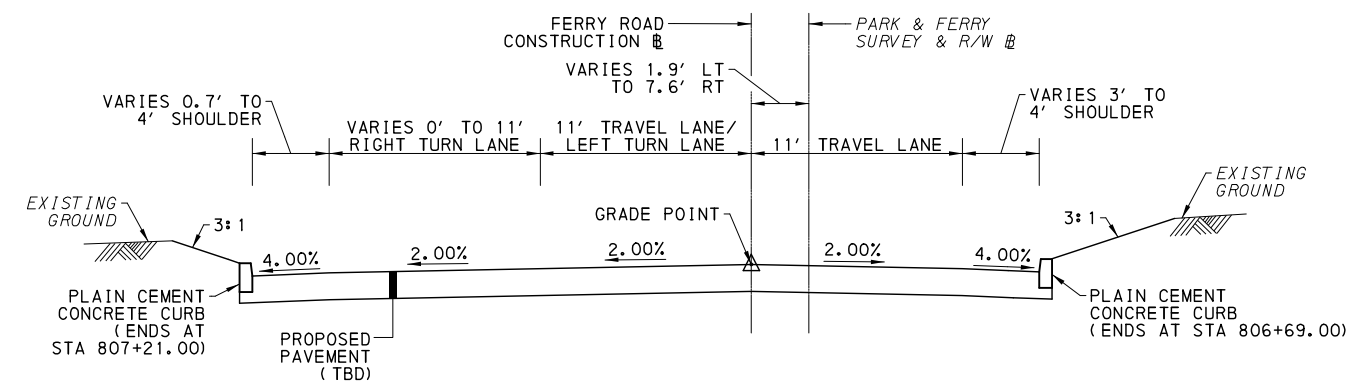


PARK AVENUE (SR 1006)  
STA 616+20.89 TO STA 617+50.00  
TYPICAL SECTION  
NOT TO SCALE

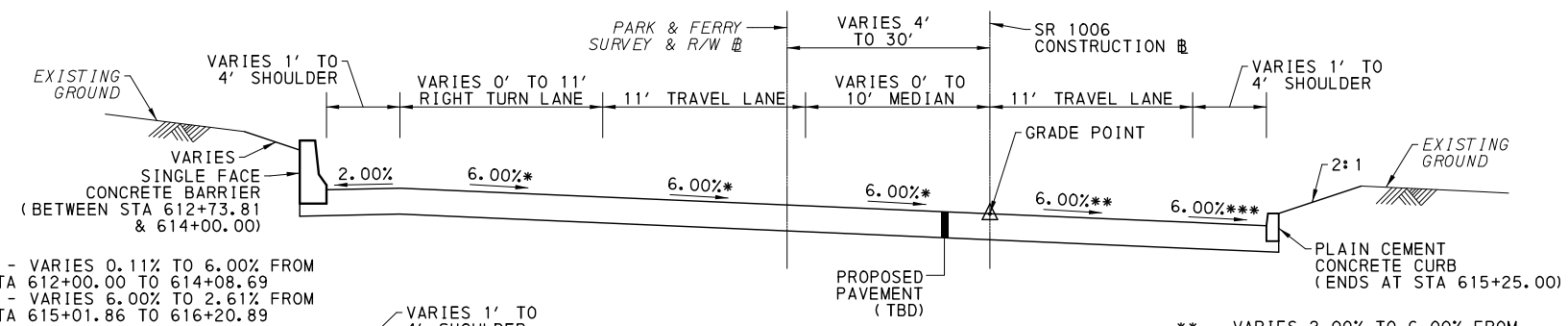


\* - VARIES 2.00% TO -0.16% FROM STA 611+23.69 TO 612+00.00

ALLOWHILL ROAD (SR 4003)  
STA 611+23.69 TO STA 612+00.00  
SUPERELEVATED SECTION  
NOT TO SCALE

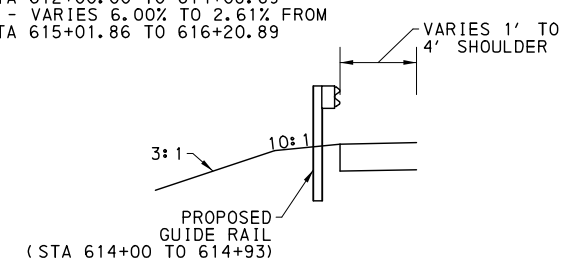


FERRY ROAD (SR 1006)  
STA 805+00.00 TO STA 808+90.00  
TYPICAL SECTION  
NOT TO SCALE



\*\* - VARIES 2.00% TO 6.00% FROM STA 612+65.69 TO 614+08.69  
\*\*\* - VARIES 6.00% TO 2.67% FROM STA 615+01.86 TO 616+20.89  
\*\*\* - VARIES 2.00% TO 6.00% FROM STA 612+65.69 TO 614+08.69  
\*\*\* - VARIES 6.00% TO 4.00% FROM STA 615+01.86 TO 616+20.89

PARK AVENUE (SR 1006)  
STA 612+00.00 TO STA 616+20.89  
SUPERELEVATED SECTION  
NOT TO SCALE

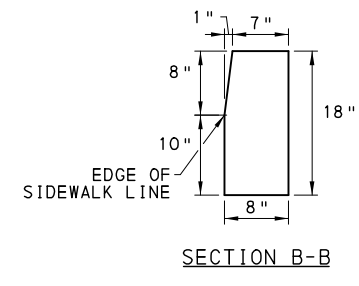
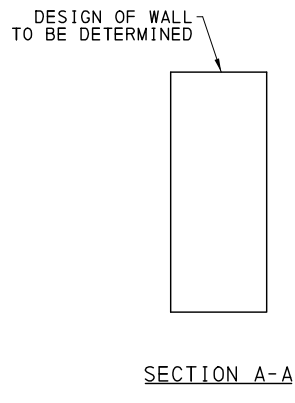
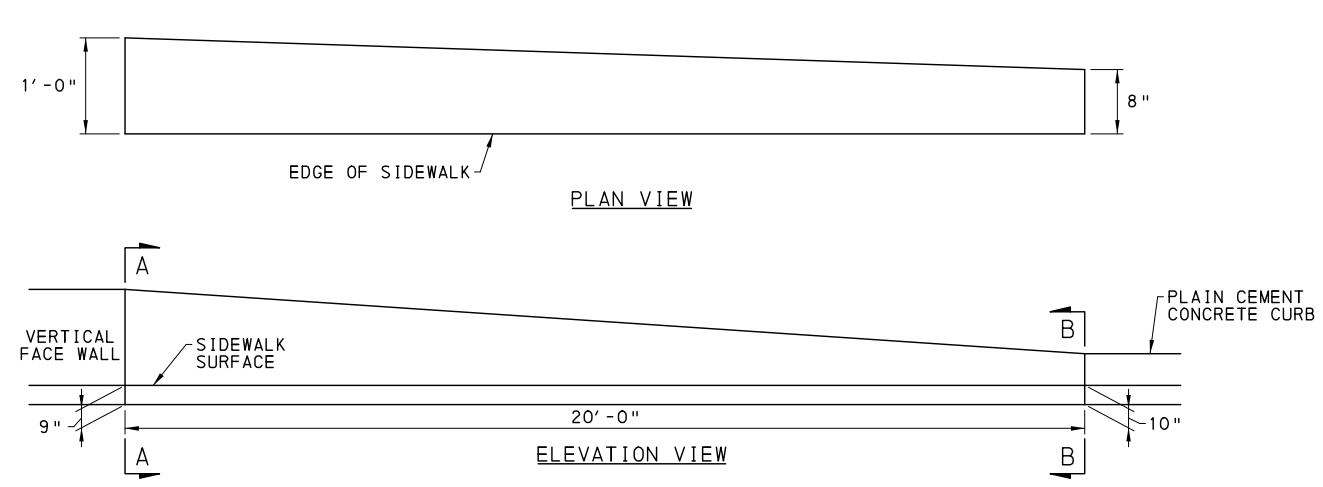


\* - VARIES 0.11% TO 6.00% FROM STA 612+00.00 TO 614+08.69  
\* - VARIES 6.00% TO 2.61% FROM STA 615+01.86 TO 616+20.89

TYPICAL SECTIONS

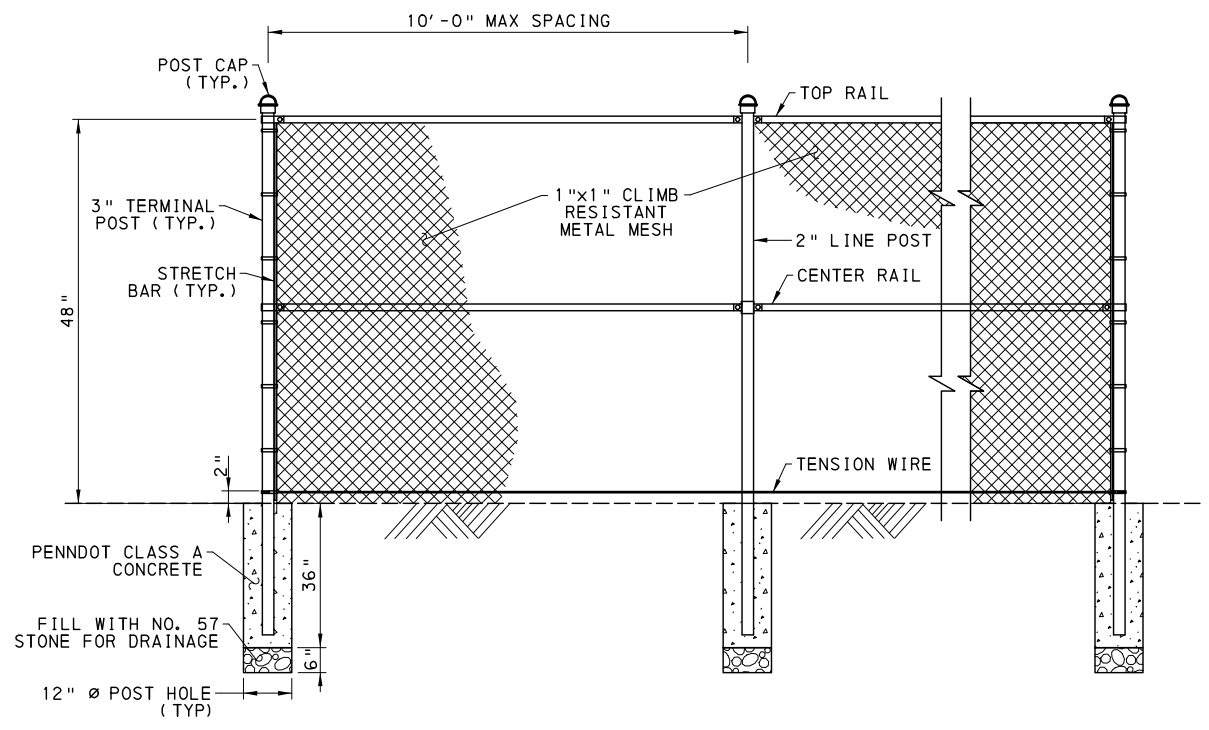
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	22 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



- NOTES:**
1. FOR SLOTTED PLATE CONNECTION DETAILS, SEE STANDARD DRAWING RC-58M.
  2. FOR DRAINAGE TREATMENT, SEE RC-58M.

**VERTICAL WALL TRANSITION TO PLAIN CEMENT CONCRETE CURB (ITEM 9000-XXXX)**  
(NOT TO SCALE)



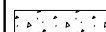








**PEDESTRIAN RAILING (ITEM 9000-XXXX)**  
(NOT TO SCALE)

DETAILS

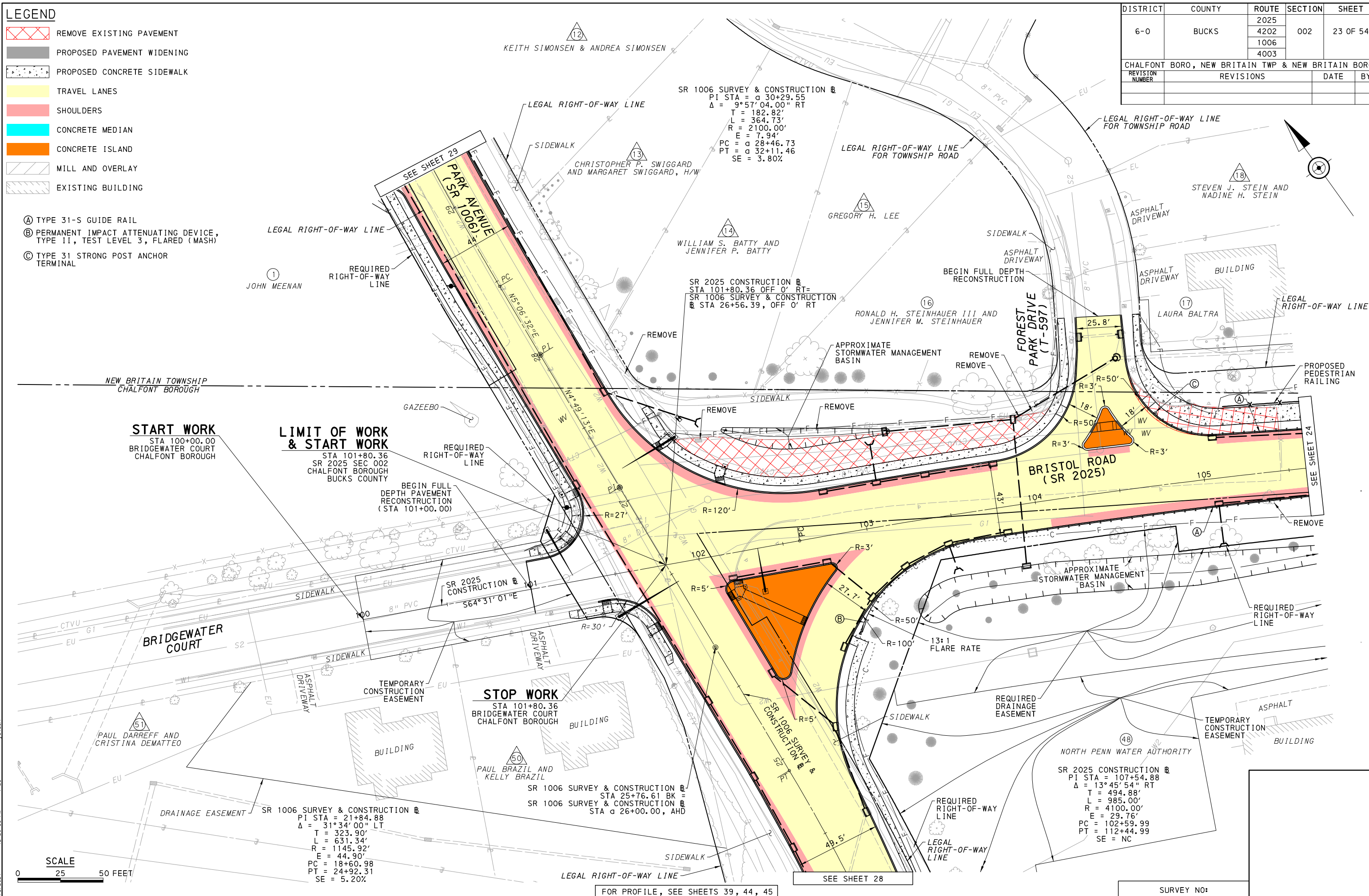
c:\pwork\mg\proj\ectwise\olison\dd279191\delra111901.dgn  
 6/5/2024 12:17:53 PM 1:1

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

- (A) TYPE 31-S GUIDE RAIL
- (B) PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3, FLARED (MASH)
- (C) TYPE 31 STRONG POST ANCHOR TERMINAL

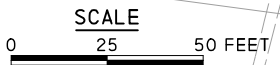
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	23 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	



**START WORK**  
 STA 100+00.00  
 BRIDGEWATER COURT  
 CHALFONT BORO

**LIMIT OF WORK & START WORK**  
 STA 101+80.36  
 SR 2025 SEC 002  
 CHALFONT BORO  
 BUCKS COUNTY

**STOP WORK**  
 STA 101+80.36  
 BRIDGEWATER COURT  
 CHALFONT BORO



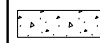
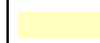







FOR PROFILE, SEE SHEETS 39, 44, 45

SURVEY NO:

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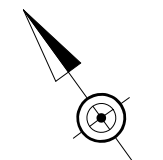
**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	24 OF 54
		4202		
		1006		
		4003		

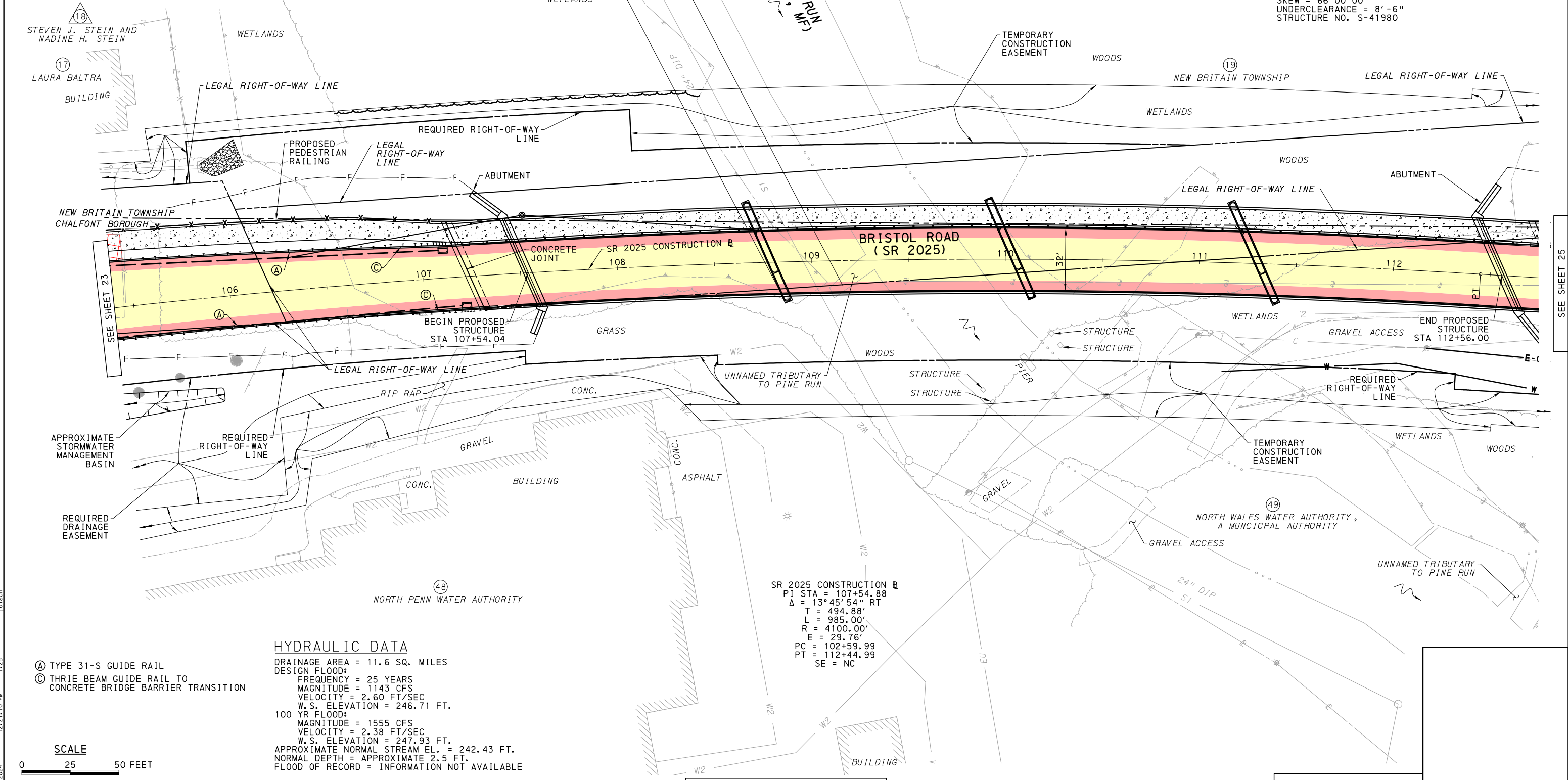
  

CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	



**PROPOSED STRUCTURE**

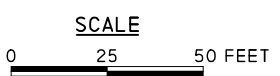
STA 107+54.04, SR 2025 CONSTRUCTION #  
 4-SPAN CONTINUOUS COMPOSITE PRESTRESSED  
 CONCRETE BULB-TEE  
 CLEAR SPAN = 500'-6" C/C BRGS.  
 LOW CHORD EL. = 250.92  
 CLEAR RDWY WIDTH = 46'-5 1/2" OUT-TO-OUT  
 SKEW = 66°00'00"  
 UNDERCLEARANCE = 8'-6"  
 STRUCTURE NO. S-41980



SR 2025 CONSTRUCTION #  
 PI STA = 107+54.88  
 $\Delta = 13^\circ 45' 54''$  RT  
 T = 494.88'  
 L = 985.00'  
 R = 4100.00'  
 E = 29.76'  
 PC = 102+59.99  
 PT = 112+44.99  
 SE = NC

**HYDRAULIC DATA**  
 DRAINAGE AREA = 11.6 SQ. MILES  
 DESIGN FLOOD:  
 FREQUENCY = 25 YEARS  
 MAGNITUDE = 1143 CFS  
 VELOCITY = 2.60 FT/SEC  
 W.S. ELEVATION = 246.71 FT.  
 100 YR FLOOD:  
 MAGNITUDE = 1555 CFS  
 VELOCITY = 2.38 FT/SEC  
 W.S. ELEVATION = 247.93 FT.  
 APPROXIMATE NORMAL STREAM EL. = 242.43 FT.  
 NORMAL DEPTH = APPROXIMATE 2.5 FT.  
 FLOOD OF RECORD = INFORMATION NOT AVAILABLE

- (A) TYPE 31-S GUIDE RAIL
- (C) THREE BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION





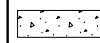
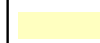





FOR PROFILE, SEE SHEET 39

SURVEY NO:

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 6/5/2024 12:21:10 PM jolson

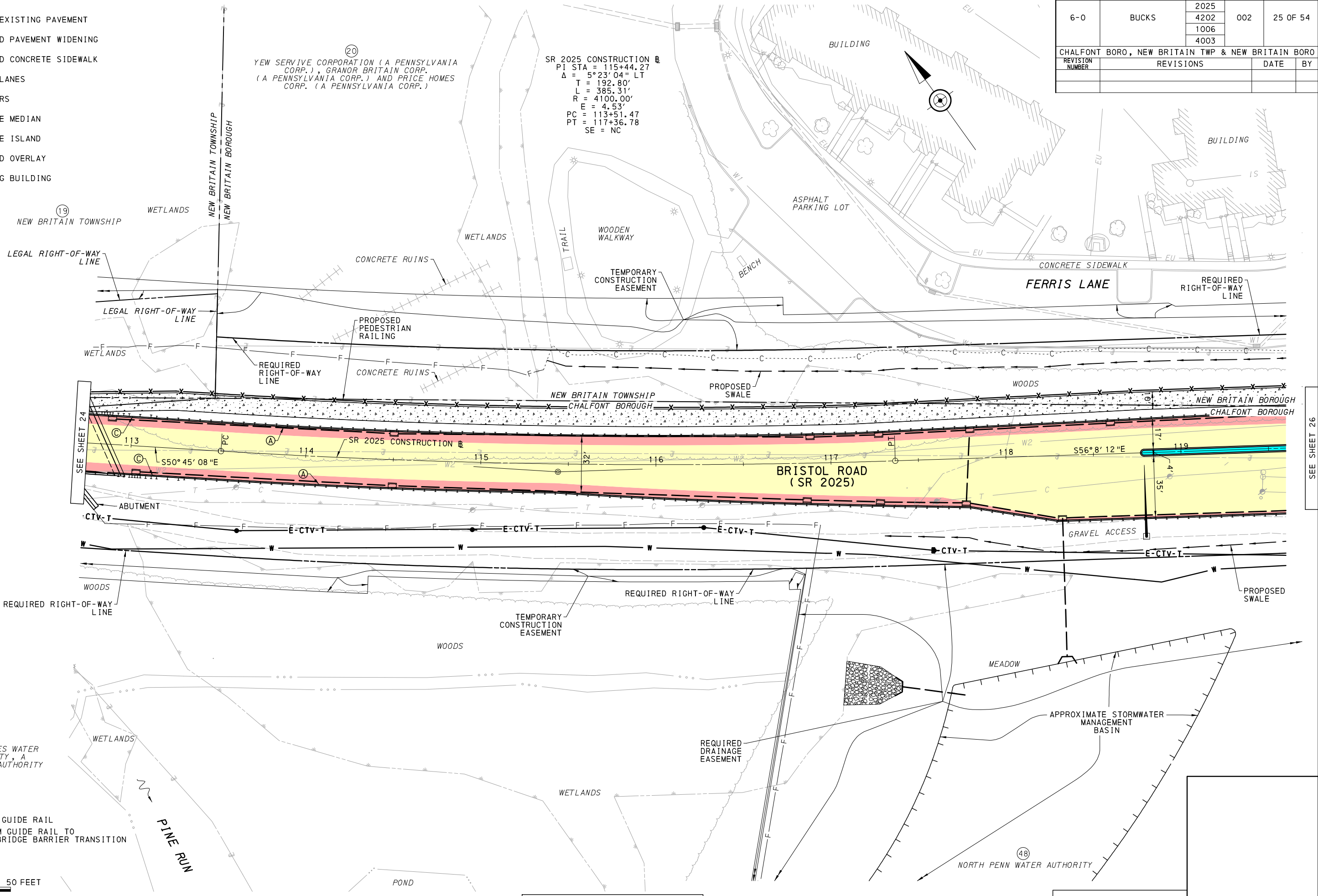
SEE SHEET 25

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	25 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

SR 2025 CONSTRUCTION B  
 PI STA = 115+44.27  
 $\Delta = 5^{\circ}23'04''$  LT  
 T = 192.80'  
 L = 385.31'  
 R = 4100.00'  
 E = 4.53'  
 PC = 113+51.47  
 PT = 117+36.78  
 SE = NC



NEW BRITAIN TOWNSHIP

NEW BRITAIN TOWNSHIP  
NEW BRITAIN BOROUGH

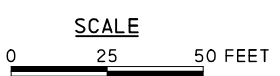
NEW BRITAIN TOWNSHIP  
CHALFONT BOROUGH

NEW BRITAIN BOROUGH  
CHALFONT BOROUGH

BRISTOL ROAD  
(SR 2025)

49  
NORTH WALES WATER  
AUTHORITY, A  
MUNICIPAL AUTHORITY

48  
NORTH PENN WATER AUTHORITY



FOR PROFILE, SEE SHEET 42










SURVEY NO:

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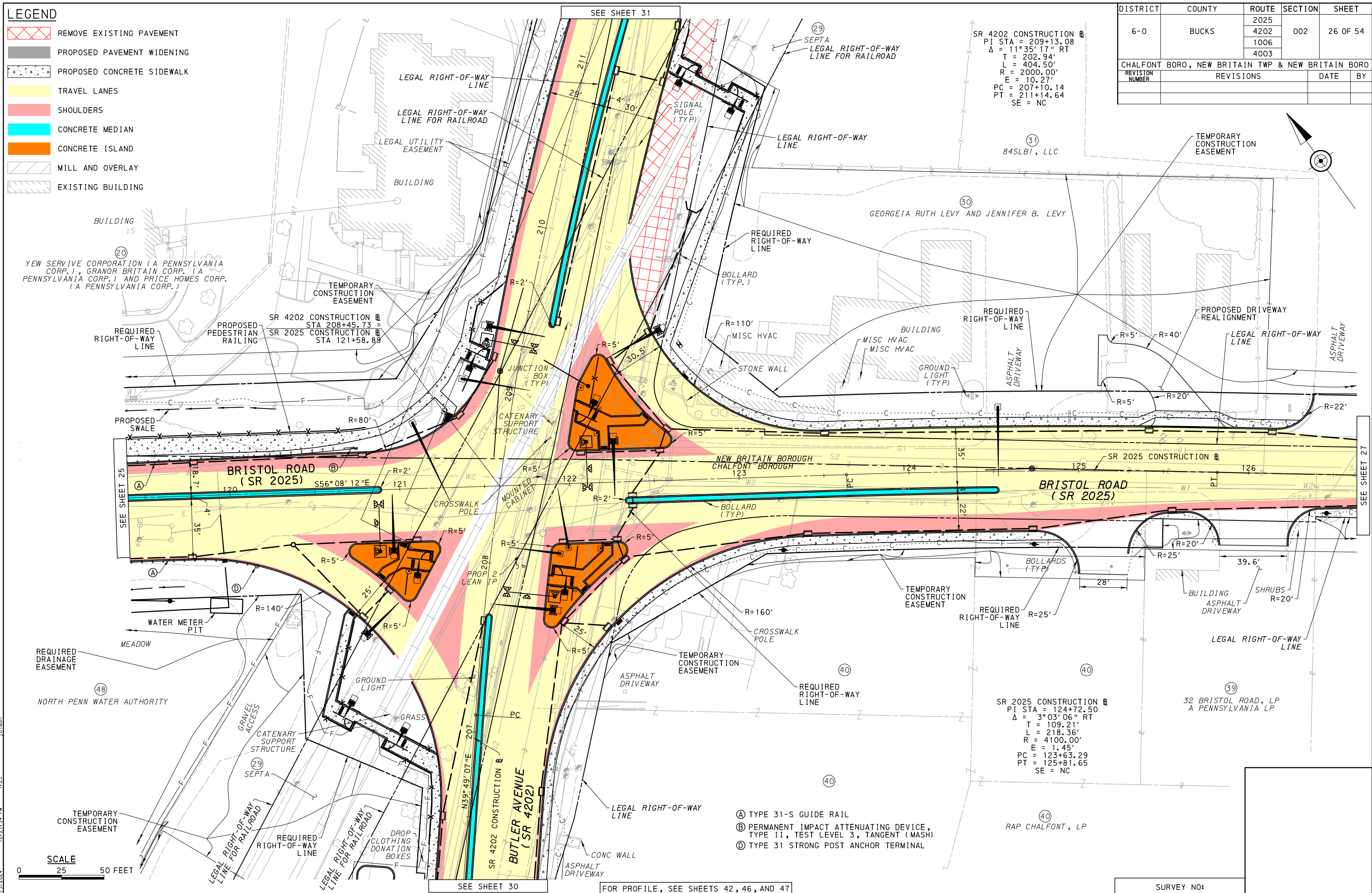
SEE SHEET 26

SEE SHEET 24

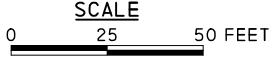
**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	26 OF 54
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	



- Ⓐ TYPE 31-S GUIDE RAIL
- Ⓑ PERMANENT IMPACT ATTENUATING DEVICE, TYPE 11, TEST LEVEL 3, TANGENT (MASH)
- Ⓒ TYPE 31 STRONG POST ANCHOR TERMINAL



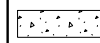
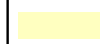







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 6/5/2024 12:21:34 PM 1:25

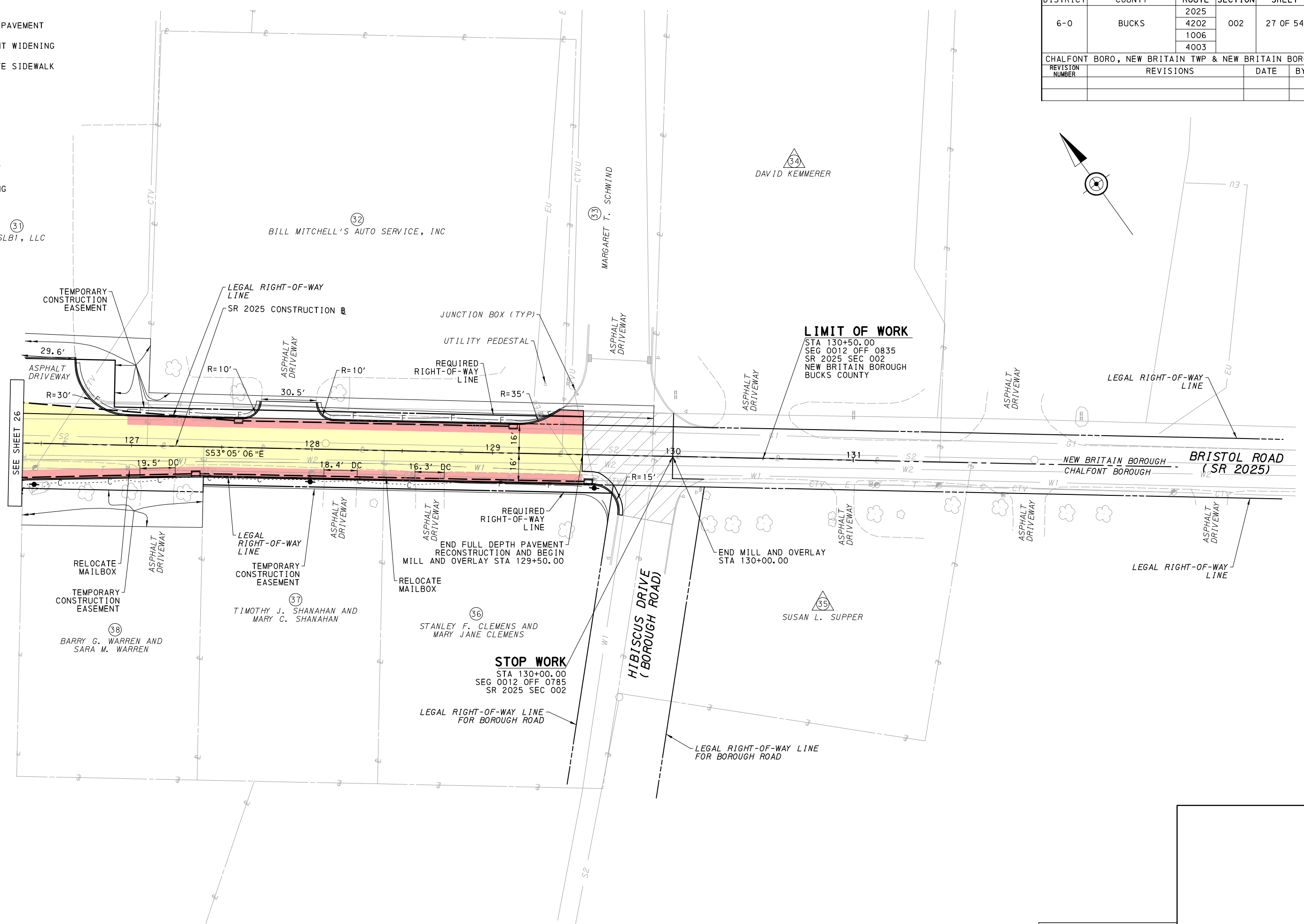
FOR PROFILE, SEE SHEETS 42, 46, AND 47

SURVEY NO:

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	27 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY





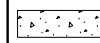
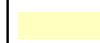





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 6/5/2024 12:21:45 PM 1:25

SCALE  
0 25 50 FEET

FOR PROFILE, SEE SHEET 43

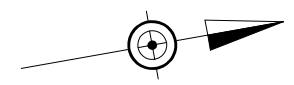
SURVEY NO:

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

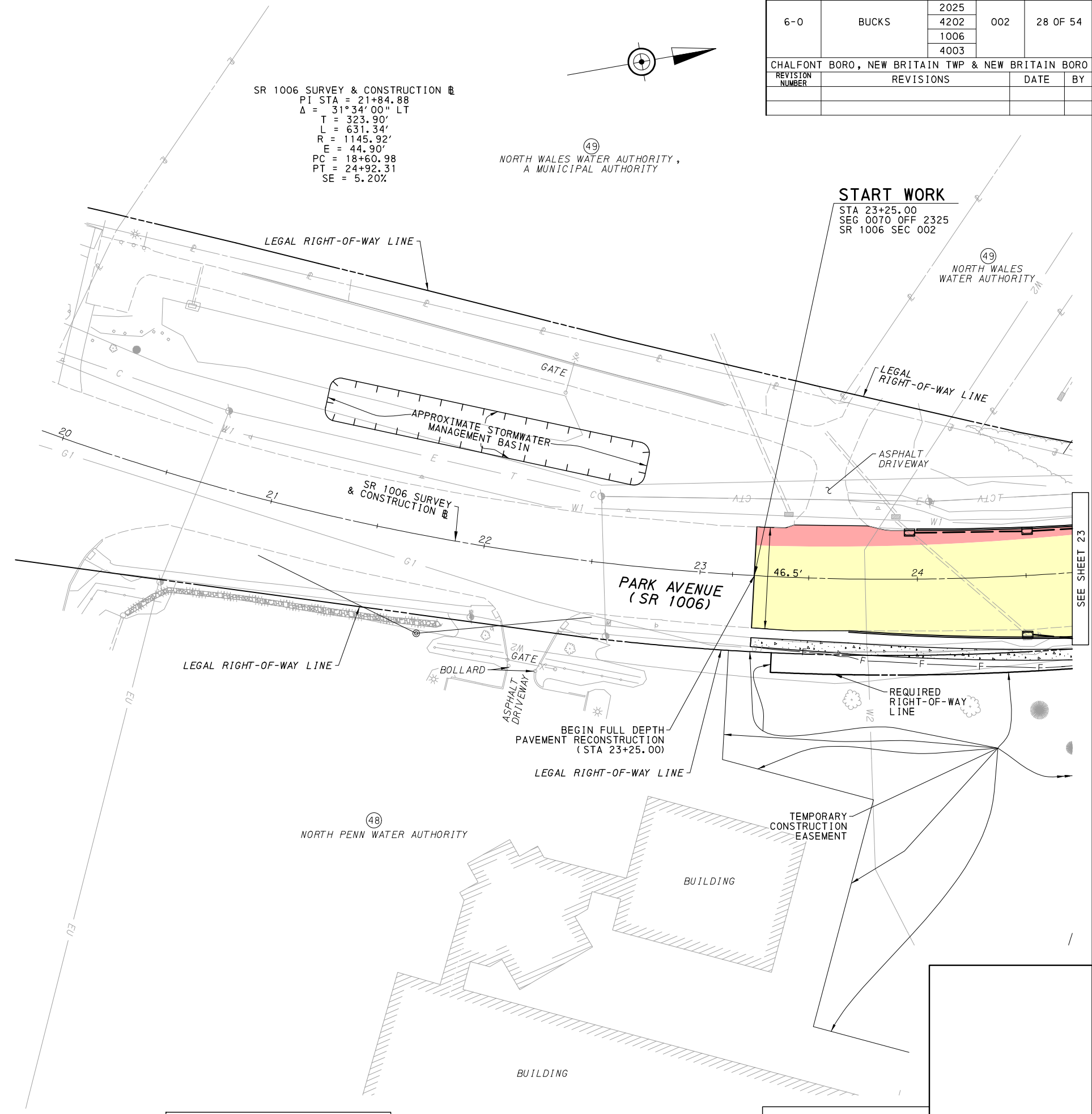
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	28 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

SR 1006 SURVEY & CONSTRUCTION  
 PI STA = 21+84.88  
 $\Delta = 31^{\circ}34'00''$  LT  
 T = 323.90'  
 L = 631.34'  
 R = 1145.92'  
 E = 44.90'  
 PC = 18+60.98  
 PT = 24+92.31  
 SE = 5.20%



(49)  
 NORTH WALES WATER AUTHORITY,  
 A MUNICIPAL AUTHORITY

**START WORK**  
 STA 23+25.00  
 SEG 0070 OFF 2325  
 SR 1006 SEC 002



SEE SHEET 23

**SCALE**












FOR PROFILE, SEE SHEET 44

SURVEY NO:

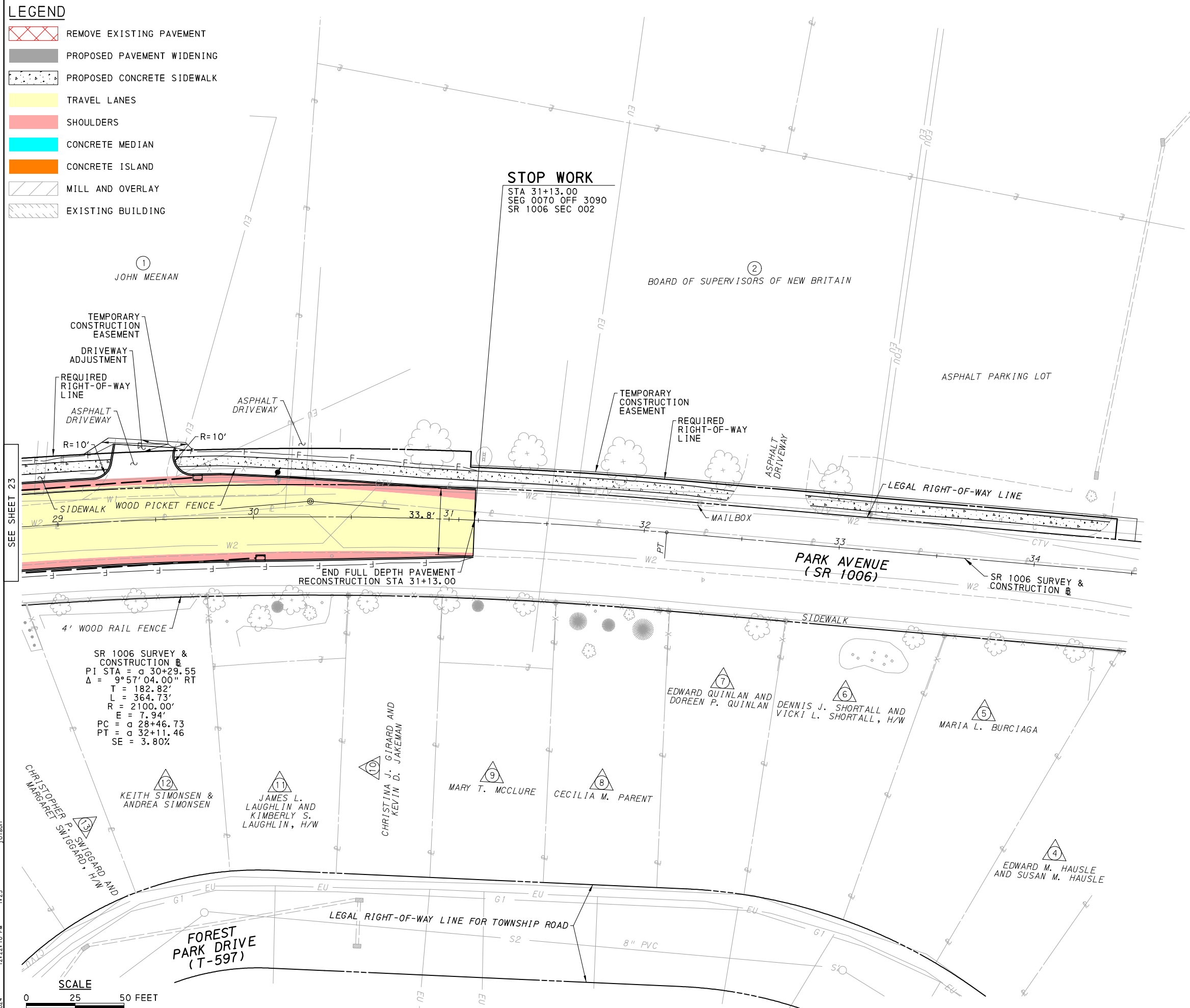
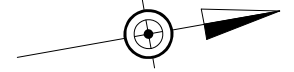
c:\pwork\proj\actwise\ol\son\dp279191\p\on-bre-06.dgn  
 6/5/2024 12:21:57 PM 1:25



**LEGEND**

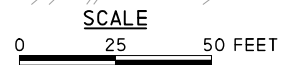
-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	29 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SEE SHEET 23

SR 1006 SURVEY & CONSTRUCTION B  
 PI STA =  $\alpha$  30+29.55  
 $\Delta = 9^\circ 57' 04.00''$  RT  
 T = 182.82'  
 L = 364.73'  
 R = 2100.00'  
 E = 7.94'  
 PC =  $\alpha$  28+46.73  
 PT =  $\alpha$  32+11.46  
 SE = 3.80%












FOR PROFILE, SEE SHEET 45

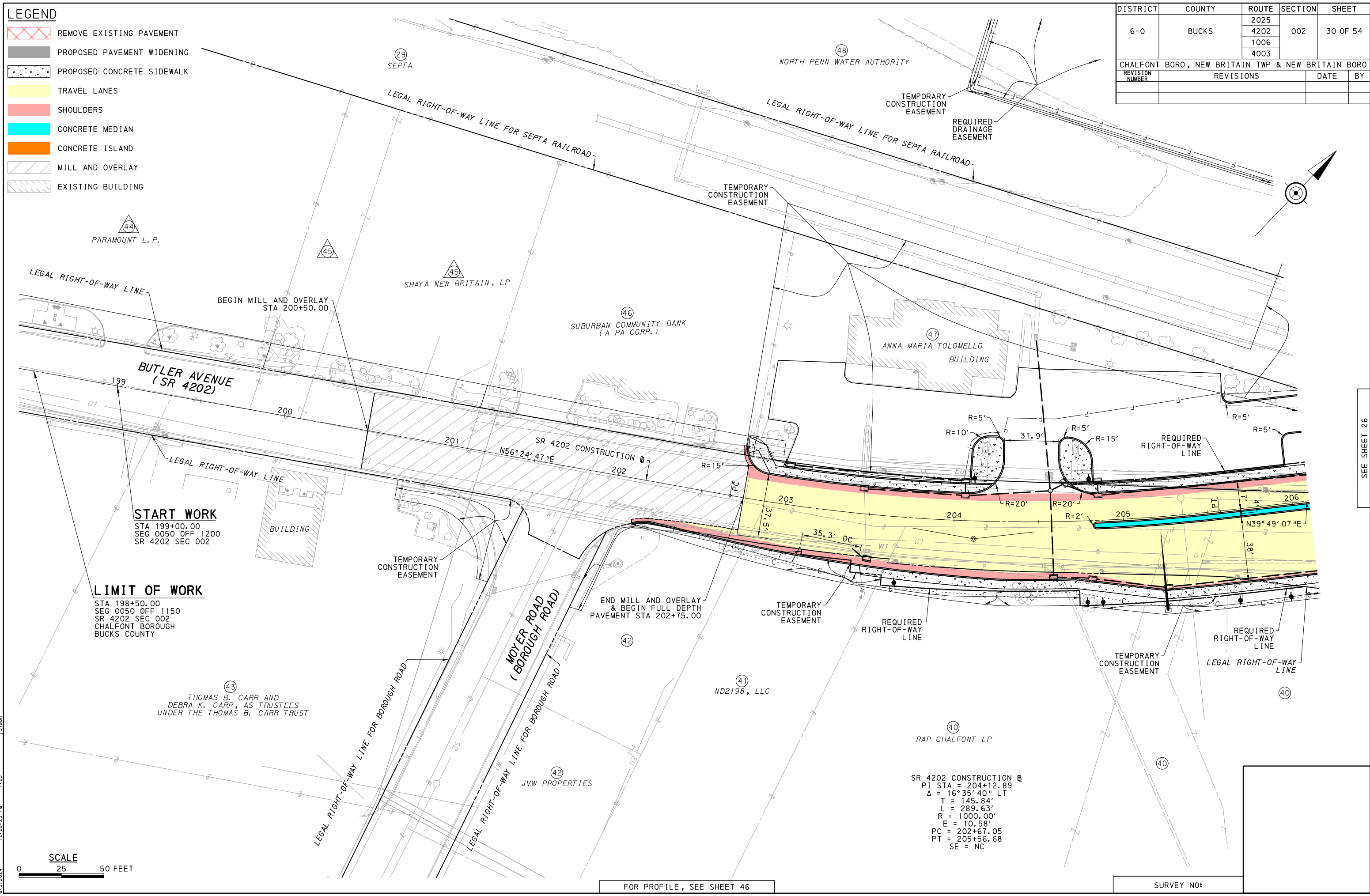
SURVEY NO:

c:\pwworking\proj\ect\156\1\156\156.dwg 6/5/2024 12:22:10 PM 1:25

**LEGEND**

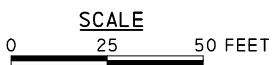
-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	30 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SR 4202 CONSTRUCTION B

PI STA =	204+12.89
Δ =	16° 35' 40" LT
T =	145.84'
L =	289.63'
R =	1000.00'
E =	10.58'
PC =	202+67.05
PT =	205+56.68
SE =	NC





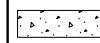
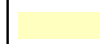





FOR PROFILE, SEE SHEET 46

SURVEY NO:

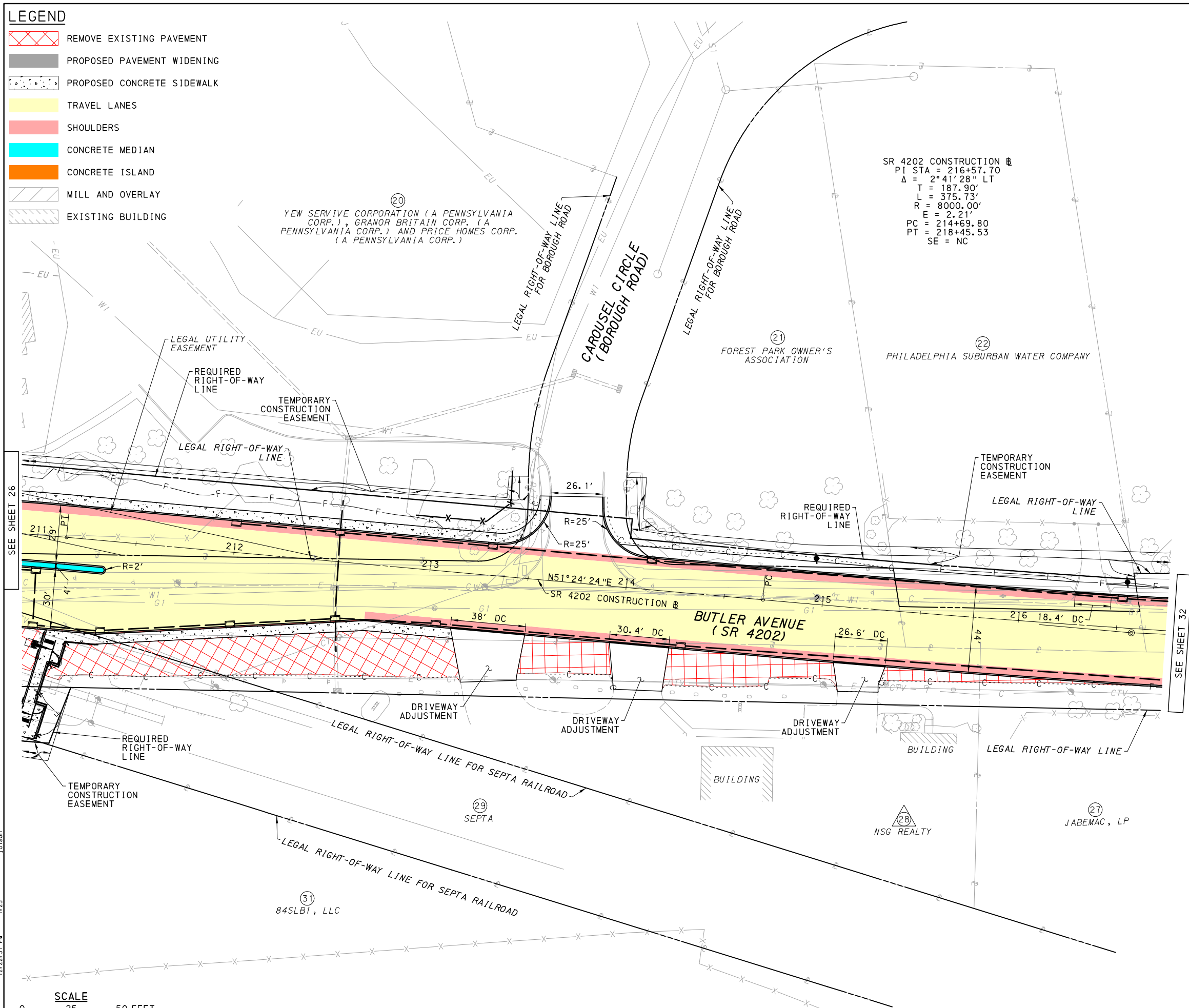
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 6/5/2024 12:22:23 PM 1:25

SEE SHEET 26

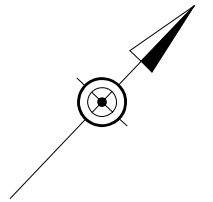
**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	31 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

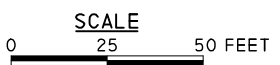


SR 4202 CONSTRUCTION B  
 PI STA = 216+57.70  
 $\Delta = 2^\circ 41' 28''$  LT  
 T = 187.90'  
 L = 375.73'  
 R = 8000.00'  
 E = 2.21'  
 PC = 214+69.80  
 PT = 218+45.53  
 SE = NC



SEE SHEET 26

SEE SHEET 32



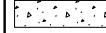








FOR PROFILE, SEE SHEETS 47 AND 48

SURVEY NO:

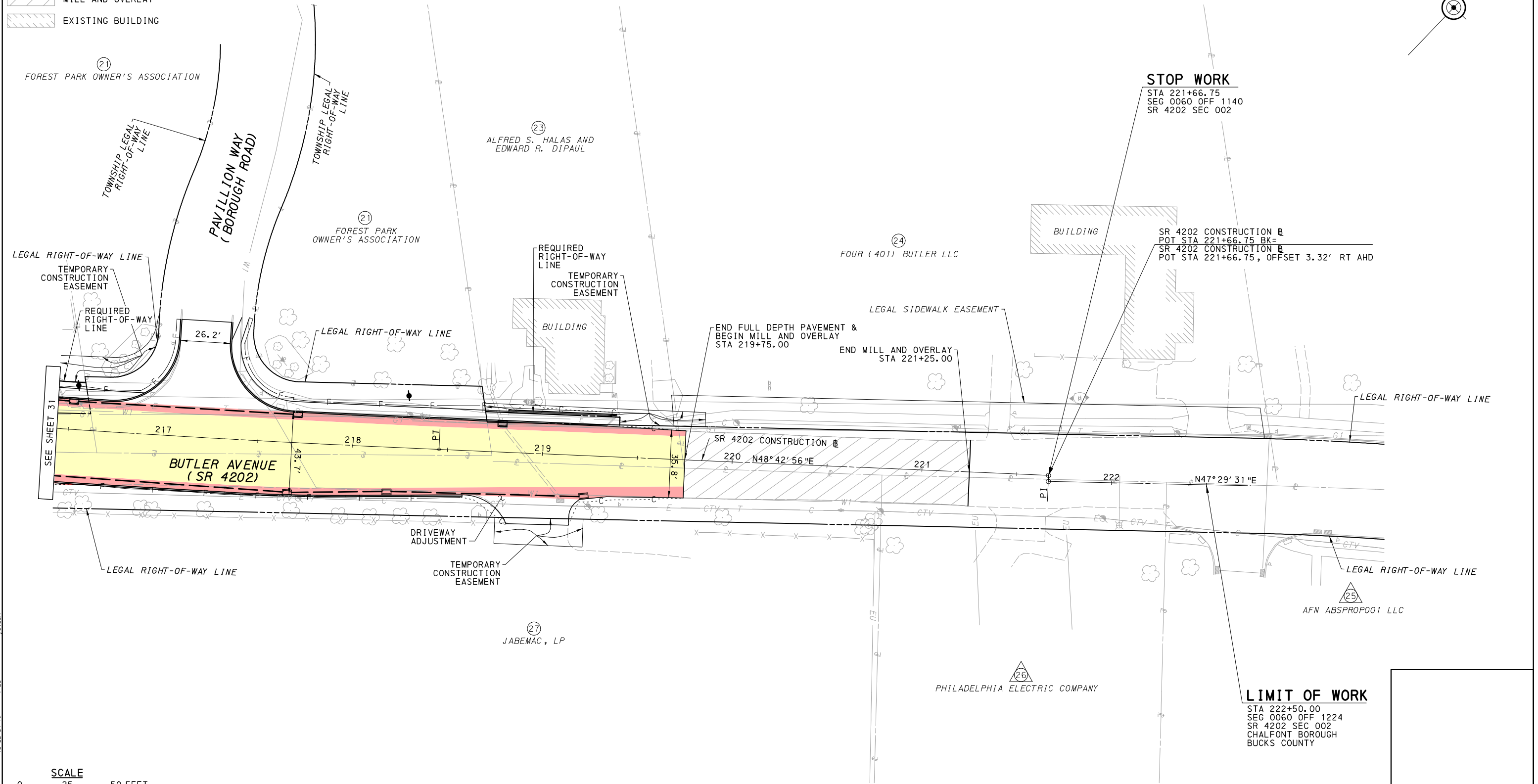
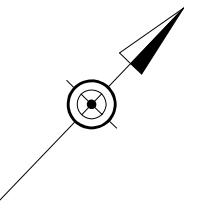
c:\pwork\proj\proj\ect\w\se\1\0\1\se\dd279191\p\on-bre-09.dgn 6/5/2024 12:22:37 PM 1:25

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

SR 4202 CONSTRUCTION #  
 PI STA = 216+57.70  
 $\Delta = 2^\circ 41' 28" \text{ LT}$   
 $T = 187.90'$   
 $L = 375.73'$   
 $R = 8000.00'$   
 $E = 2.21'$   
 $PC = 214+69.80$   
 $PT = 218+45.53$   
 $SE = \text{NC}$

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	32 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	



**STOP WORK**

STA 221+66.75  
 SEG 0060 OFF 1140  
 SR 4202 SEC 002

**SR 4202 CONSTRUCTION #**

POT STA 221+66.75 BK=  
 SR 4202 CONSTRUCTION #  
 POT STA 221+66.75, OFFSET 3.32' RT AHD

**LIMIT OF WORK**

STA 222+50.00  
 SEG 0060 OFF 1224  
 SR 4202 SEC 002  
 CHALFONT BOROUGH  
 BUCKS COUNTY

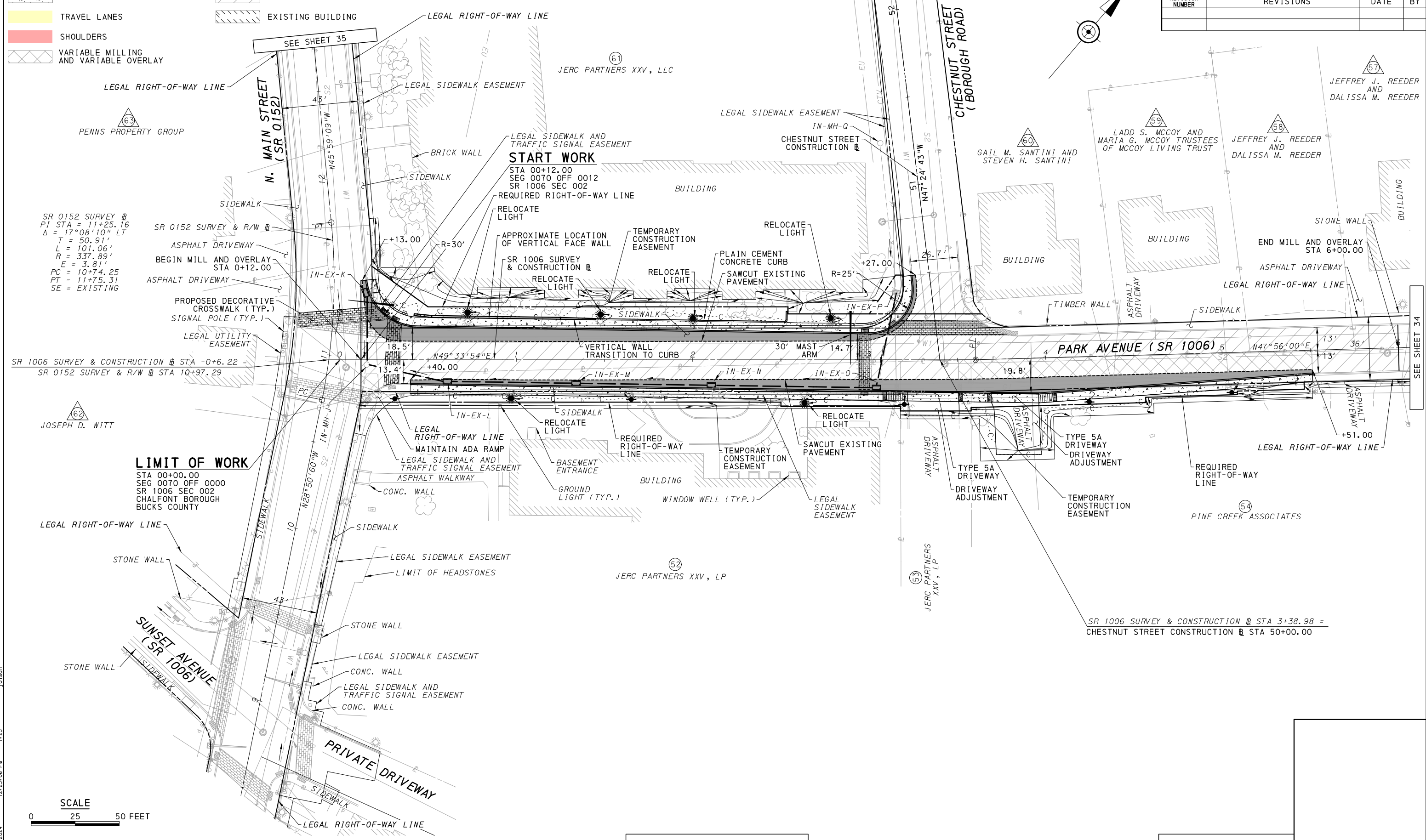
FOR PROFILE, SEE SHEETS 48 AND 49

SURVEY NO: \_\_\_\_\_

LEGEND

- REMOVE EXISTING PAVEMENT
- PROPOSED PAVEMENT WIDENING
- PROPOSED CONCRETE SIDEWALK
- TRAVEL LANES
- SHOULDER
- VARIABLE MILLING AND VARIABLE OVERLAY
- CONCRETE MEDIAN
- CONCRETE ISLAND
- MILL AND OVERLAY
- EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	33 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	



SR 0152 SURVEY  
 PI STA = 11+25.16  
 $\Delta = 17^{\circ}08'10''$  LT  
 T = 50.91'  
 L = 101.06'  
 R = 337.89'  
 E = 3.81'  
 PC = 10+74.25  
 PT = 11+75.31  
 SE = EXISTING

**LIMIT OF WORK**  
 STA 00+00.00  
 SEG 0070 OFF 0000  
 SR 1006 SEC 002  
 CHALFONT BOROUGH  
 BUCKS COUNTY



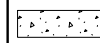
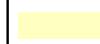





SR 1006 SURVEY & CONSTRUCTION @ STA 3+38.98 =  
 CHESTNUT STREET CONSTRUCTION @ STA 50+00.00



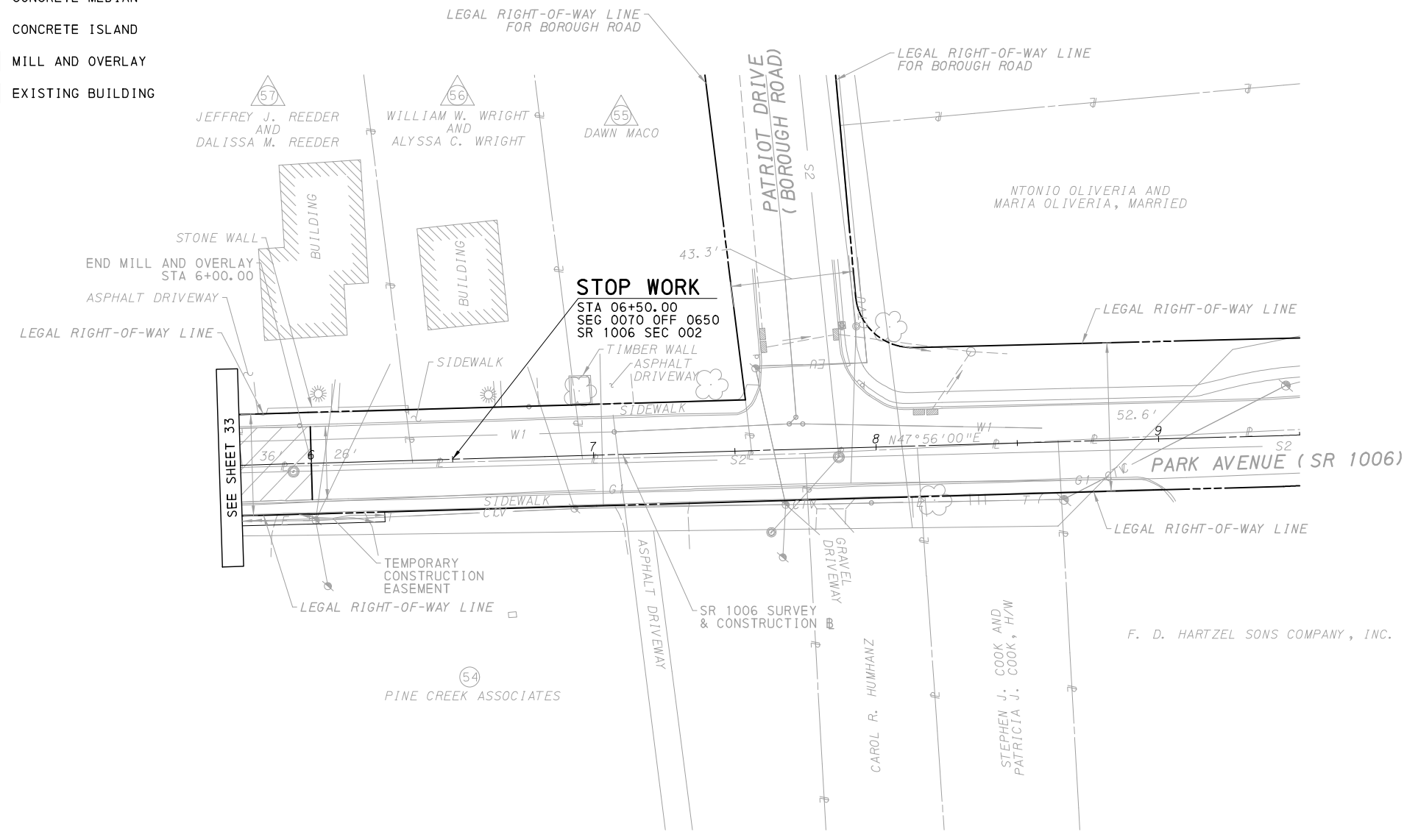
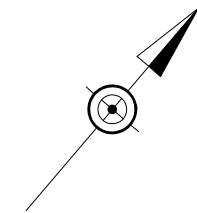
FOR SR 1006 PROFILE, SEE SHEET 50

SURVEY NO:

**LEGEND**

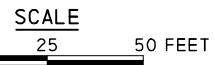
-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	34 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SEE SHEET 33

**STOP WORK**  
 STA 06+50.00  
 SEG 0070 OFF 0650  
 SR 1006 SEC 002












FOR PROFILE, SEE SHEET 50

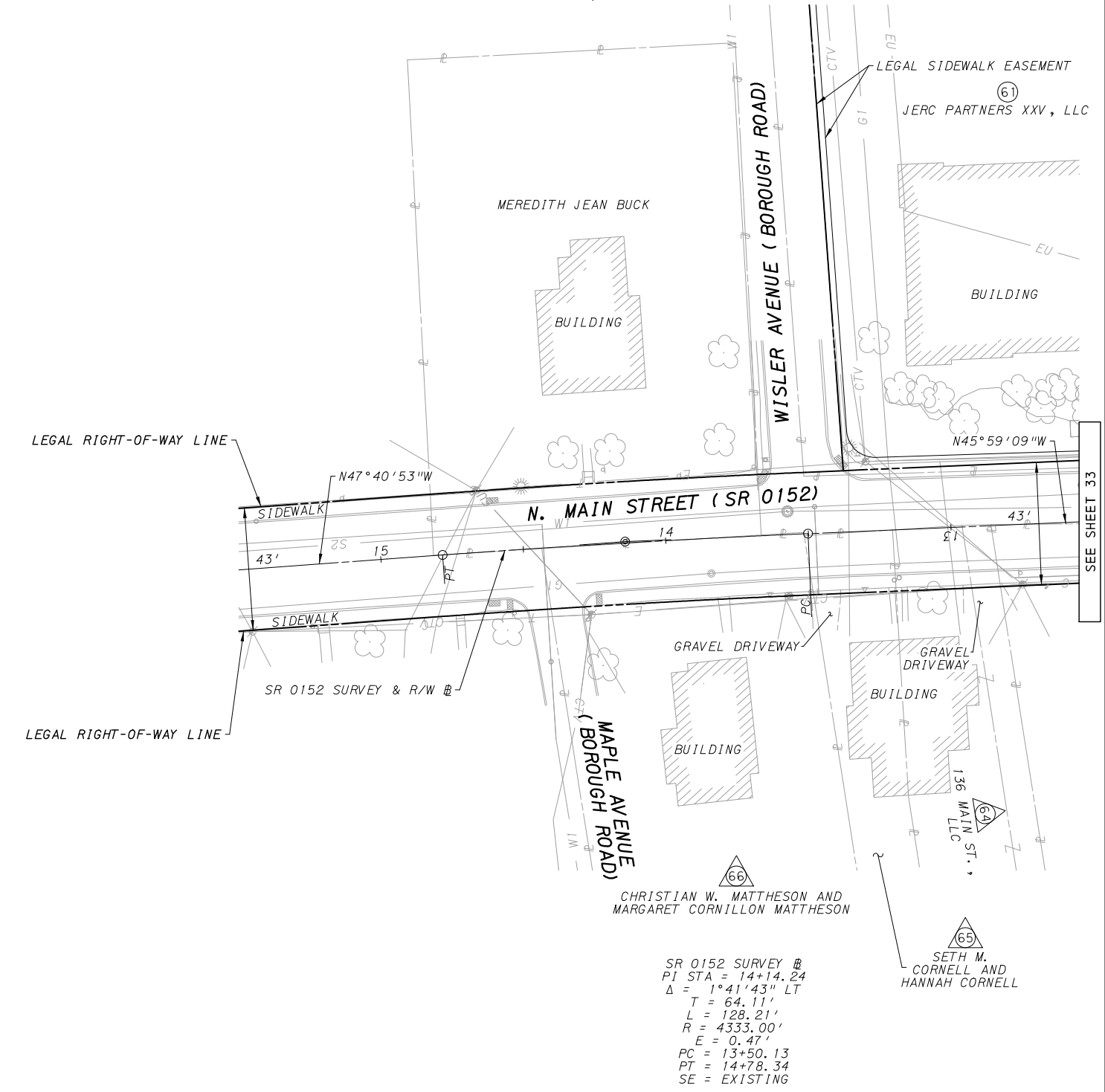
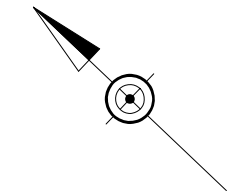
SURVEY NO:

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**LEGEND**

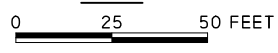
-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	35 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SR 0152 SURVEY #  
 PI STA = 14+14.24  
 $\Delta = 1^{\circ}41'43''$  LT  
 T = 64.11'  
 L = 128.21'  
 R = 4333.00'  
 E = 0.47'  
 PC = 13+50.13  
 PT = 14+78.34  
 SE = EXISTING

**SCALE**












SURVEY NO:

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 6/5/2024 12:23:31 PM olison

SEE SHEET 33

**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

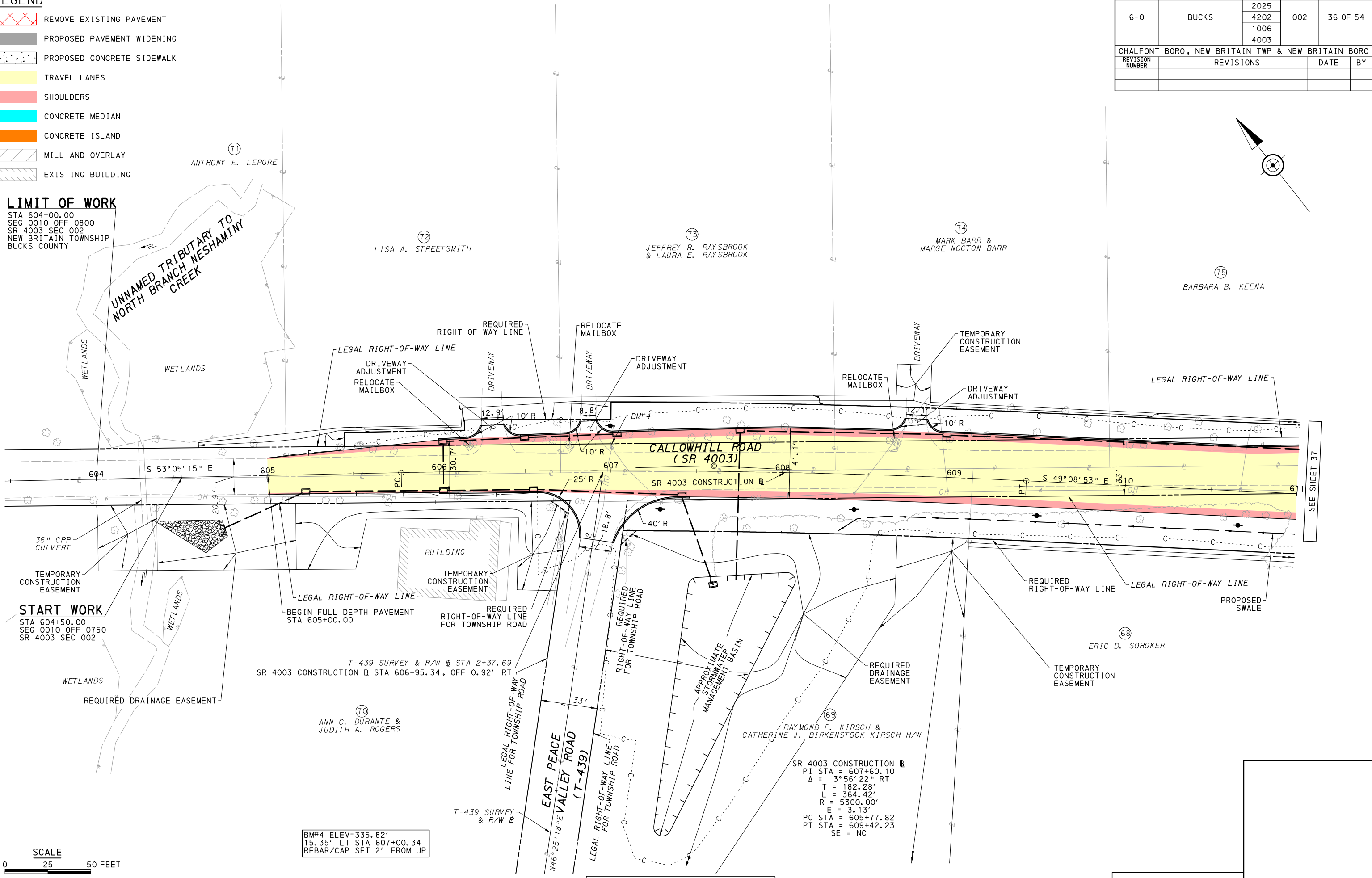
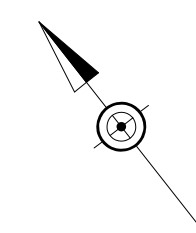
**LIMIT OF WORK**

STA 604+00.00  
 SEG 0010 OFF 0800  
 SR 4003 SEC 002  
 NEW BRITAIN TOWNSHIP  
 BUCKS COUNTY

**START WORK**

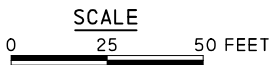
STA 604+50.00  
 SEG 0010 OFF 0750  
 SR 4003 SEC 002

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	36 OF 54	
		4202			
		1006 4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



BM#4 ELEV=335.82'  
 15.35' LT STA 607+00.34  
 REBAR/CAP SET 2' FROM UP

SR 4003 CONSTRUCTION @  
 PI STA = 607+60.10  
 $\Delta = 3^{\circ}56'22''$  RT  
 T = 182.28'  
 L = 364.42'  
 R = 5300.00'  
 E = 3.13'  
 PC STA = 605+77.82  
 PT STA = 609+42.23  
 SE = NC



FOR PROFILE, SEE SHEET 51



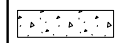
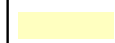





SURVEY NO:

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 6/5/2024 12:23:45 PM 1:25

SEE SHEET 37

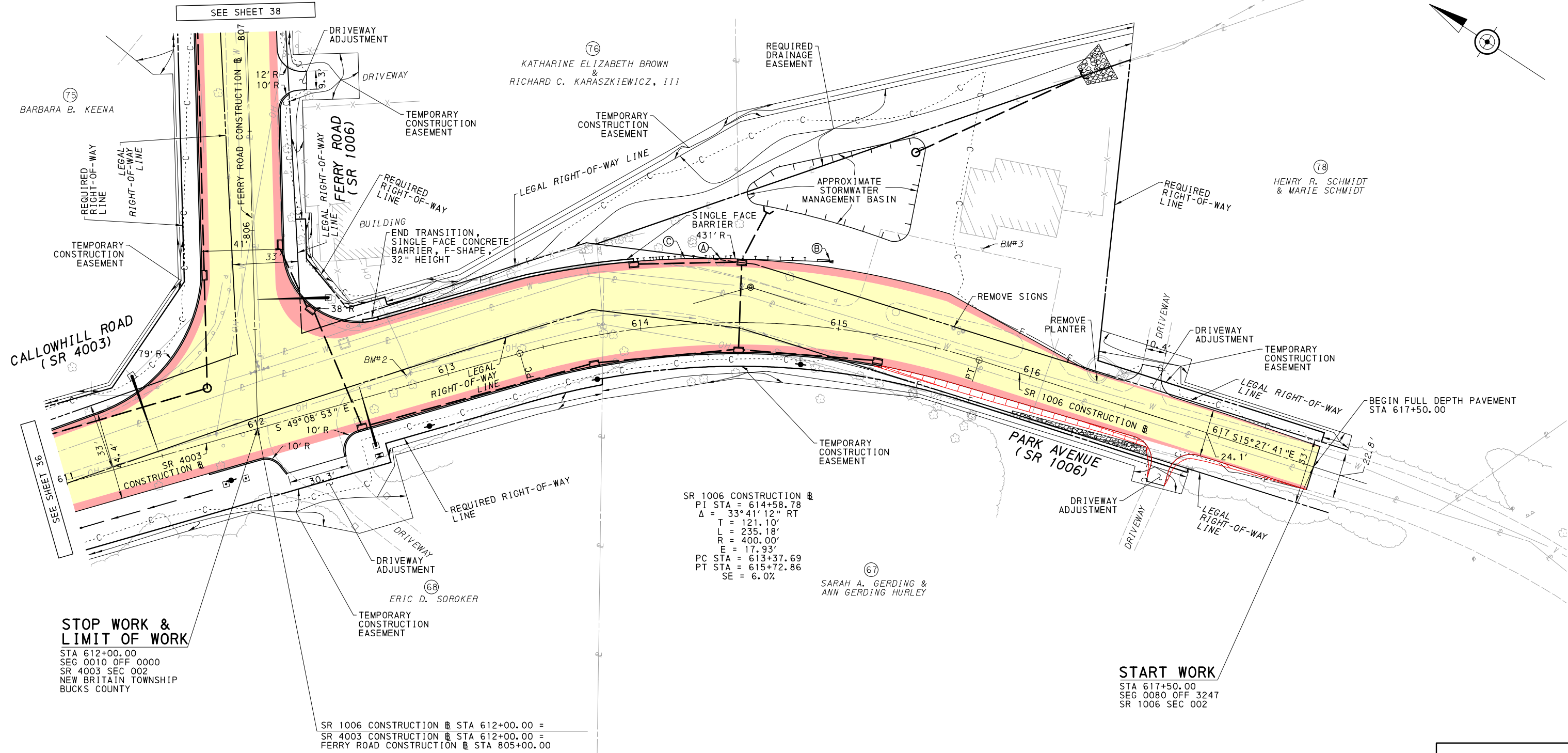
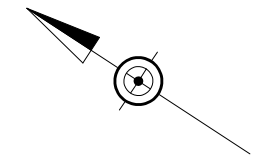


**LEGEND**

-  REMOVE EXISTING PAVEMENT
-  PROPOSED PAVEMENT WIDENING
-  PROPOSED CONCRETE SIDEWALK
-  TRAVEL LANES
-  SHOULDERS
-  CONCRETE MEDIAN
-  CONCRETE ISLAND
-  MILL AND OVERLAY
-  EXISTING BUILDING

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	37 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS	DATE	BY	

77  
MICHAEL F. CARTER



SR 1006 CONSTRUCTION #  
 PI STA = 614+58.78  
 $\Delta = 33^\circ 41' 12''$  RT  
 T = 121.10'  
 L = 235.18'  
 R = 400.00'  
 E = 17.93'  
 PC STA = 613+37.69  
 PT STA = 615+72.86  
 SE = 6.0%

SR 1006 CONSTRUCTION # STA 612+00.00 =  
 SR 4003 CONSTRUCTION # STA 612+00.00 =  
 FERRY ROAD CONSTRUCTION # STA 805+00.00

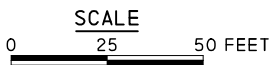
BM#2 ELEV=357.094'  
 4.96' LT STA 612+79.59  
 REBAR/CAP SET 2' FROM UP #90642

BM#3 ELEV=343.422'  
 60.44' LT STA 615+58.11  
 TOP OF STEP ON SE CORNER

**STOP WORK & LIMIT OF WORK**  
 STA 612+00.00  
 SEG 0010 OFF 0000  
 SR 4003 SEC 002  
 NEW BRITAIN TOWNSHIP  
 BUCKS COUNTY

**START WORK**  
 STA 617+50.00  
 SEG 0080 OFF 3247  
 SR 1006 SEC 002

- (A) TYPE 31-S GUIDE RAIL
- (B) PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3, TANGENT (MASH)
- (C) THREE BEAM GUIDE RAIL TO CONCRETE BRIDGE BARRIER TRANSITION



FOR PROFILE, SEE SHEETS 52 AND 53

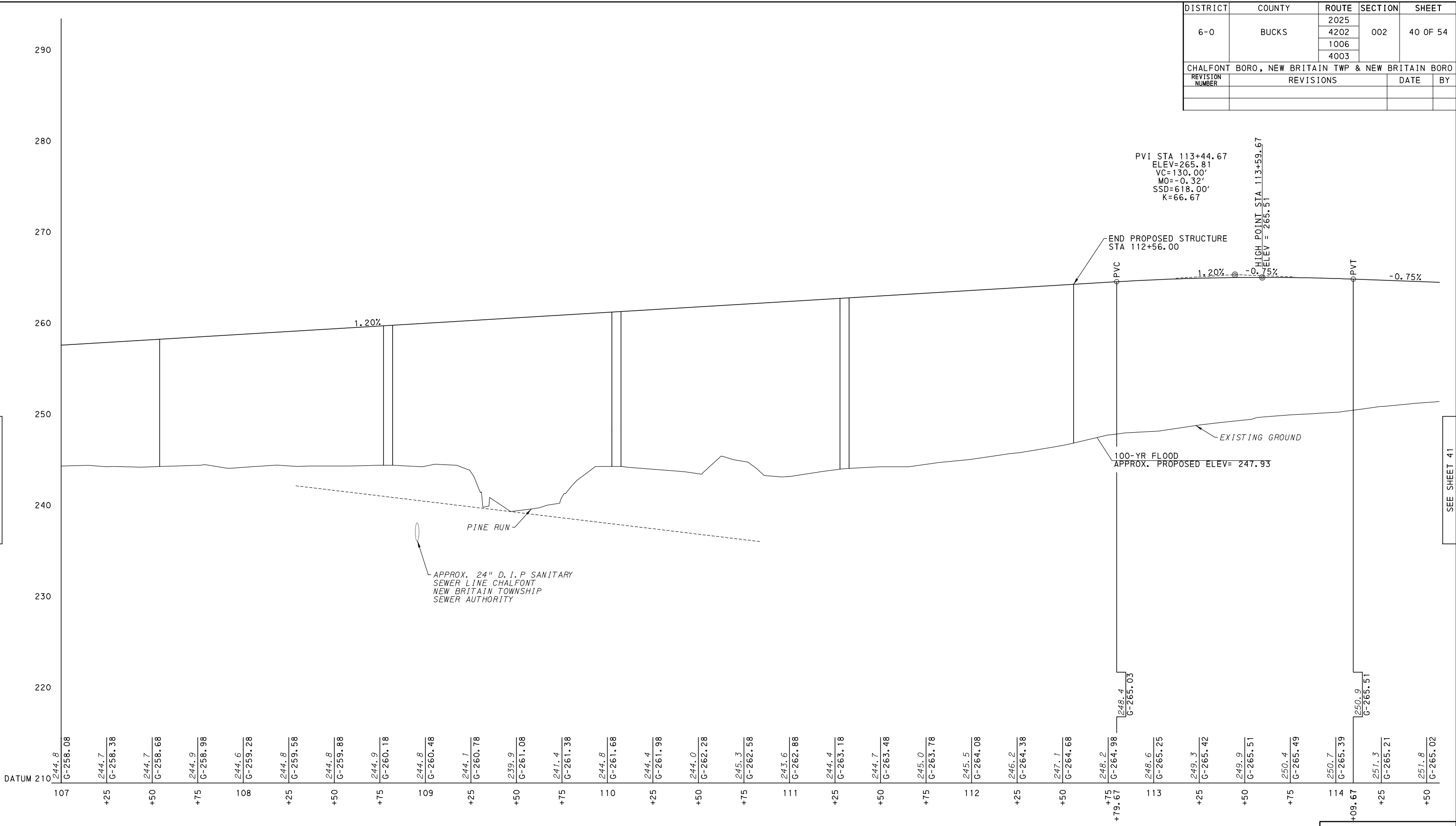
SURVEY NO:

c:\pwworking\prow\project\661101\661101.dwg 12/24/24 11:25 6/5/2024





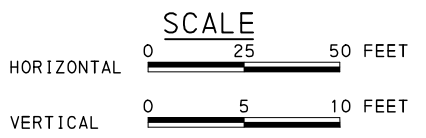
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	40 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



SEE SHEET 39

SEE SHEET 41

BRISTOL ROAD (SR 2025)



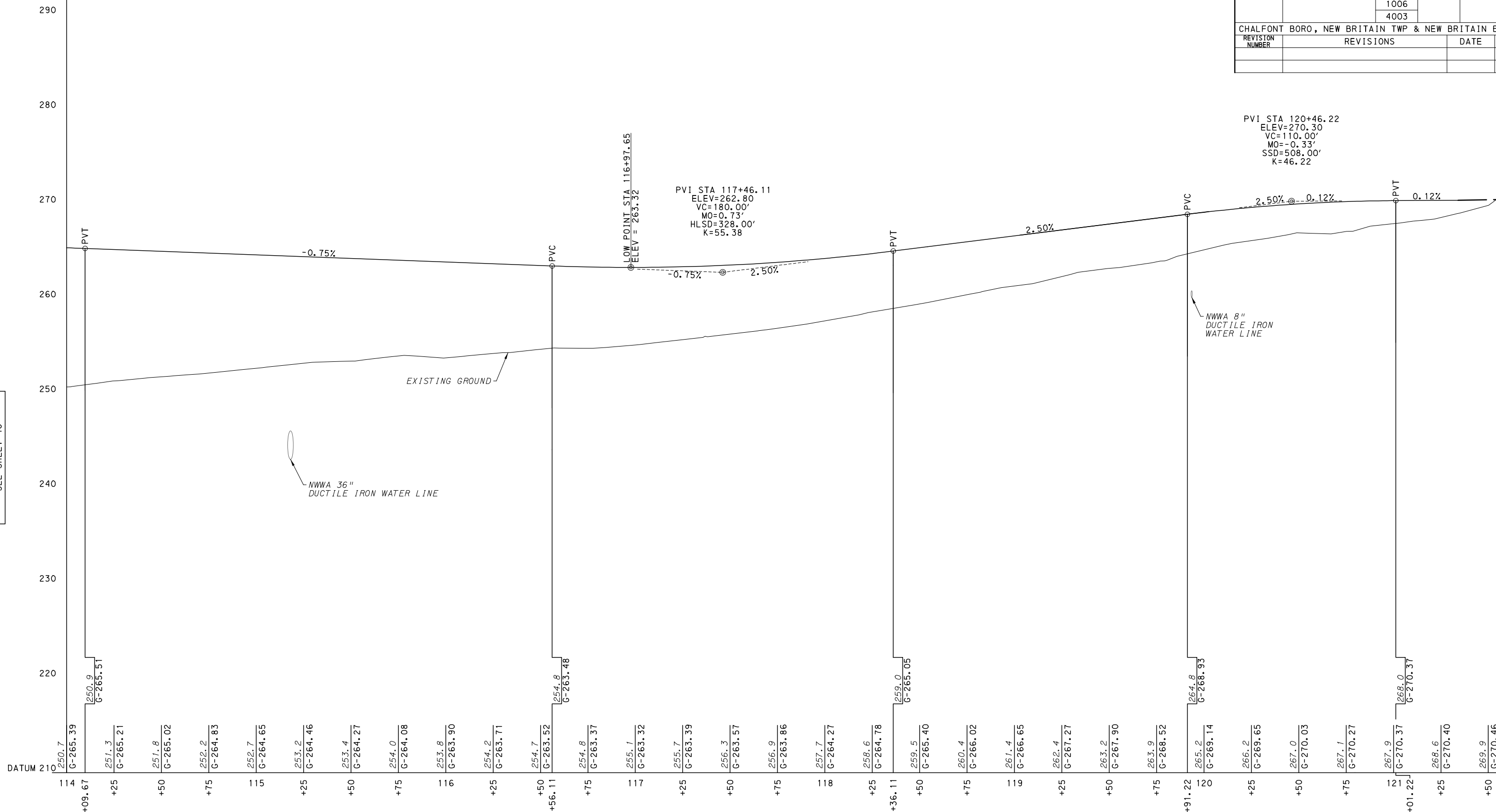
FOR PLAN, SEE SHEET 24

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	41 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

PVI STA 120+46.22  
 ELEV=270.30  
 VC=110.00'  
 MO=-0.33'  
 SSD=508.00'  
 K=46.22

PVI STA 117+46.11  
 ELEV=262.80  
 VC=180.00'  
 MO=0.73'  
 HLSD=328.00'  
 K=55.38

LOW POINT STA 116+97.65  
 ELEV = 263.32



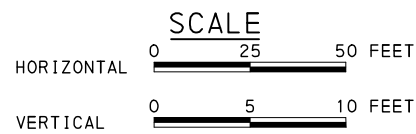
SEE SHEET 40

SEE SHEET 42

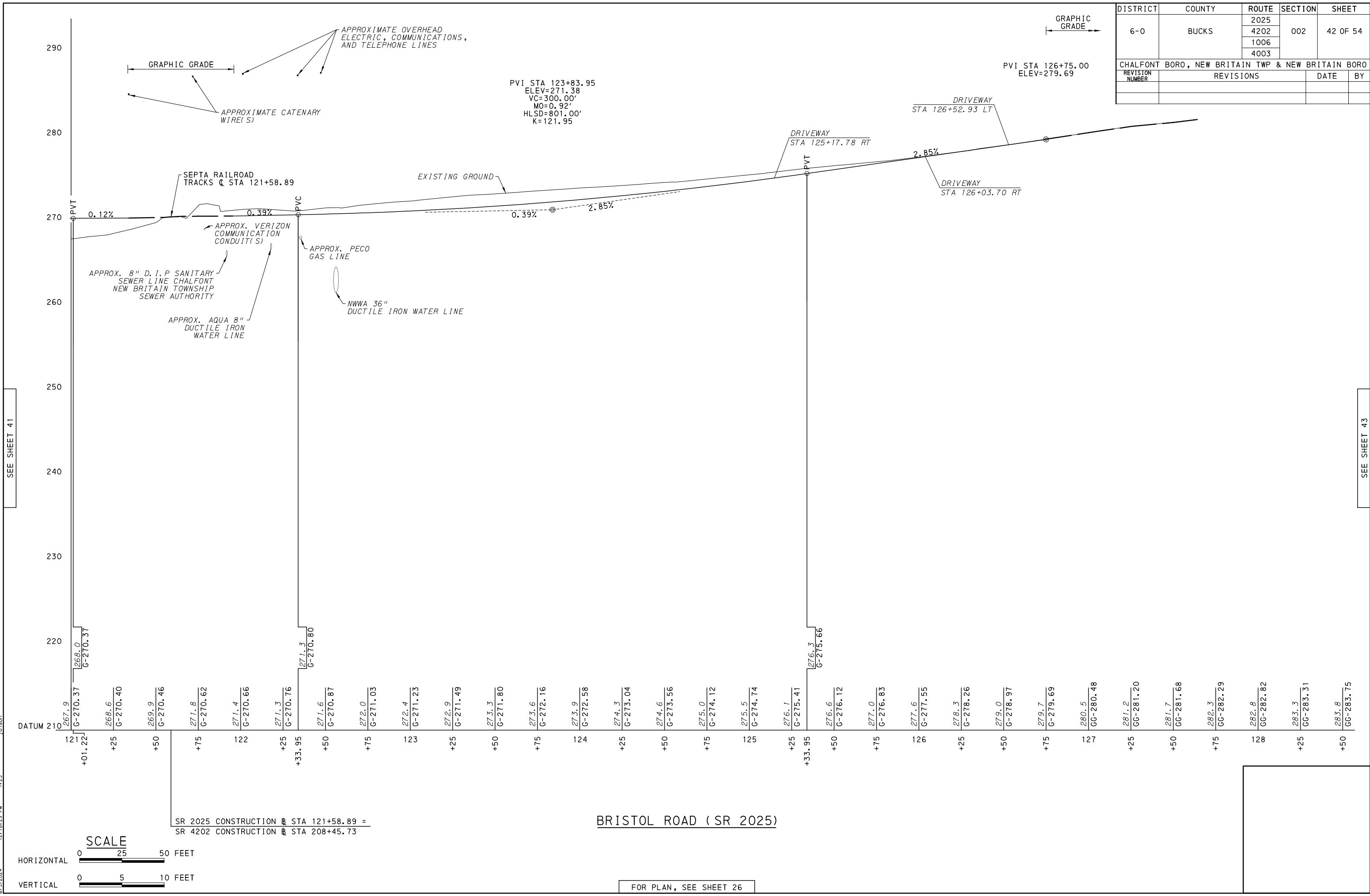
BRISTOL ROAD (SR 2025)

FOR PLAN, SEE SHEET 25

ct:\pwork\p\proj\ectw\se\l\o\lson\dp02791\p\prof1\le-BRE-Bri\sr10\Road-03.dgn 6/5/2024 12:18:24 PM 1:25



DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	42 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



SEE SHEET 41

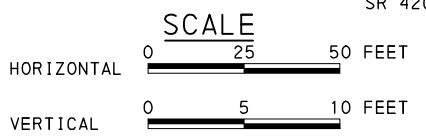
SEE SHEET 43

c:\pwork\proj\proj\sect\sect\12118123\12118123.dwg 6/5/2024 1:25

DATUM 210	267.9	268.0	268.6	269.9	271.8	271.4	271.3	271.6	272.0	272.4	272.9	273.3	273.6	273.9	274.3	274.6	275.0	275.5	276.1	276.6	277.0	277.6	278.3	279.0	279.7	280.5	281.2	281.7	282.3	282.8	283.3	283.8	
	G-270.37	G-270.37	G-270.40	G-270.46	G-270.62	G-270.66	G-270.76	G-270.80	G-271.03	G-271.23	G-271.49	G-271.80	G-272.16	G-272.58	G-273.04	G-273.56	G-274.12	G-274.74	G-275.41	G-276.12	G-276.83	G-277.55	G-278.26	G-278.97	G-279.69	GG-280.48	GG-281.20	GG-281.68	GG-282.29	GG-282.82	GG-283.31	GG-283.75	
	+01.22	+25	+50	+75	122	+25	+33.95	+50	+75	123	+25	+50	+75	124	+25	+50	+75	125	+25	+33.95	+50	+75	126	+25	+50	+75	127	+25	+50	+75	128	+25	+50

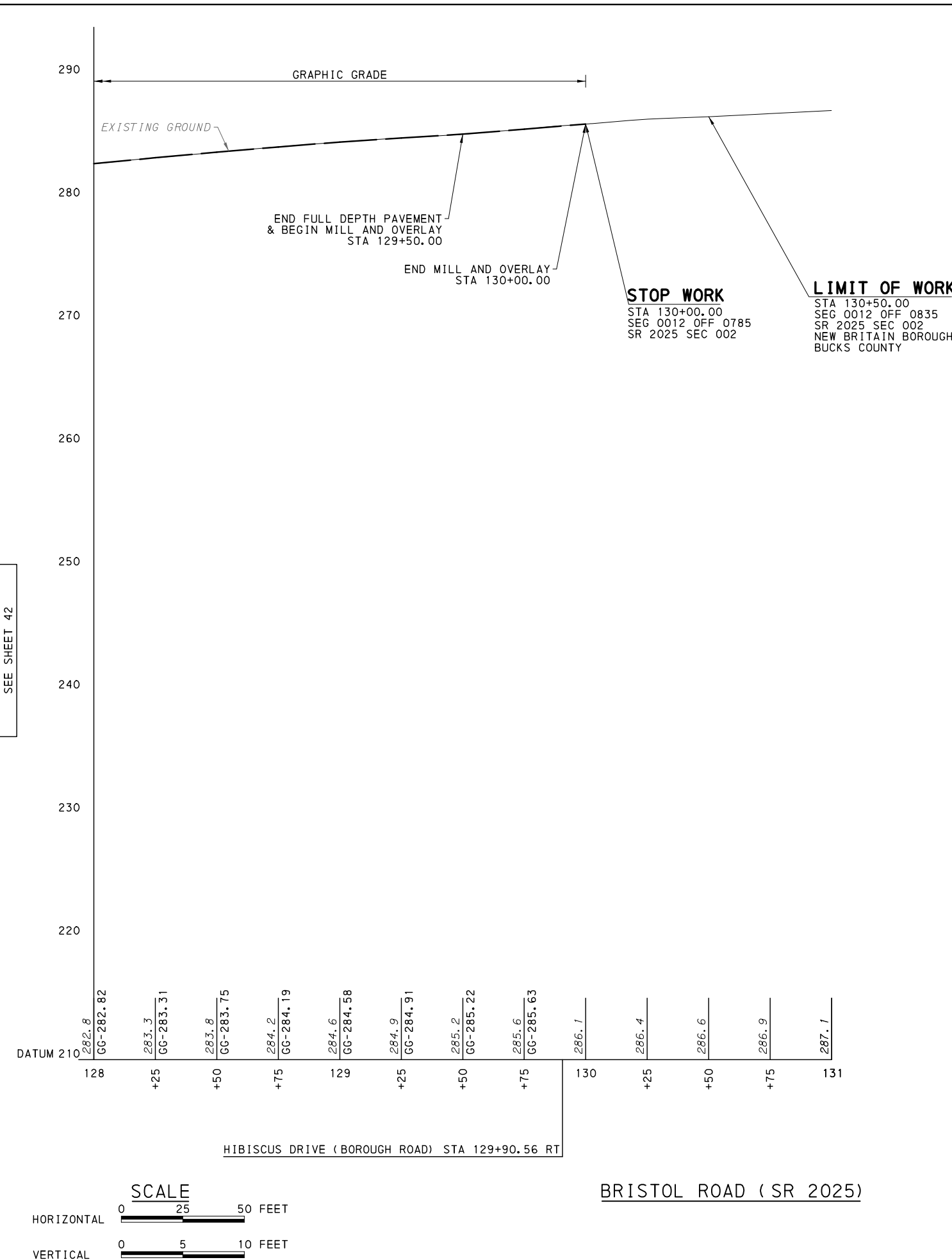
SR 2025 CONSTRUCTION # STA 121+58.89 =  
SR 4202 CONSTRUCTION # STA 208+45.73

**BRISTOL ROAD (SR 2025)**

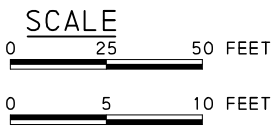


FOR PLAN, SEE SHEET 26

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	43 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



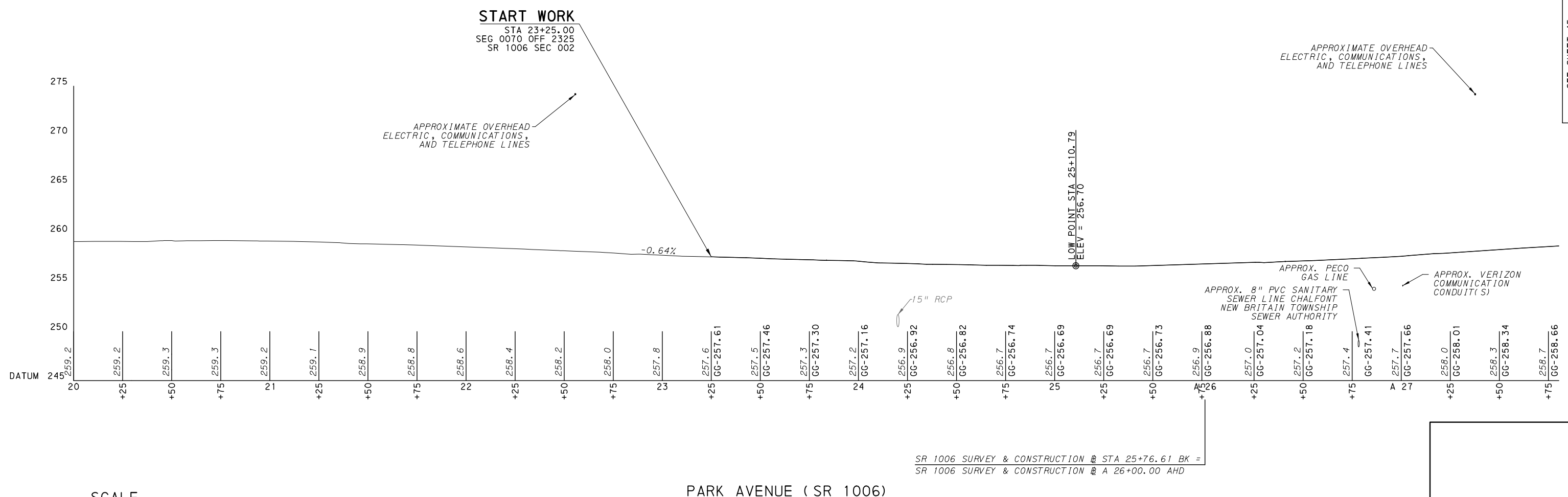
SEE SHEET 42



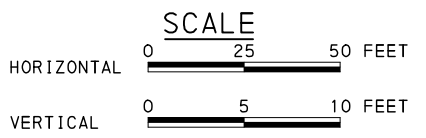
FOR PLAN, SEE SHEET 27

c:\pwworking\proj\ect\isec\l\ol\son\dd0279191\p\prof11e-BRE-Br\to\Road-05.dgn  
 6/5/2024 12:18:33 PM 1:25

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	44 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



**PARK AVENUE (SR 1006)**



SR 1006 SURVEY & CONSTRUCTION @ STA 25+76.61 BK =  
 SR 1006 SURVEY & CONSTRUCTION @ A 26+00.00 AHD

FOR PLAN, SEE SHEET 28

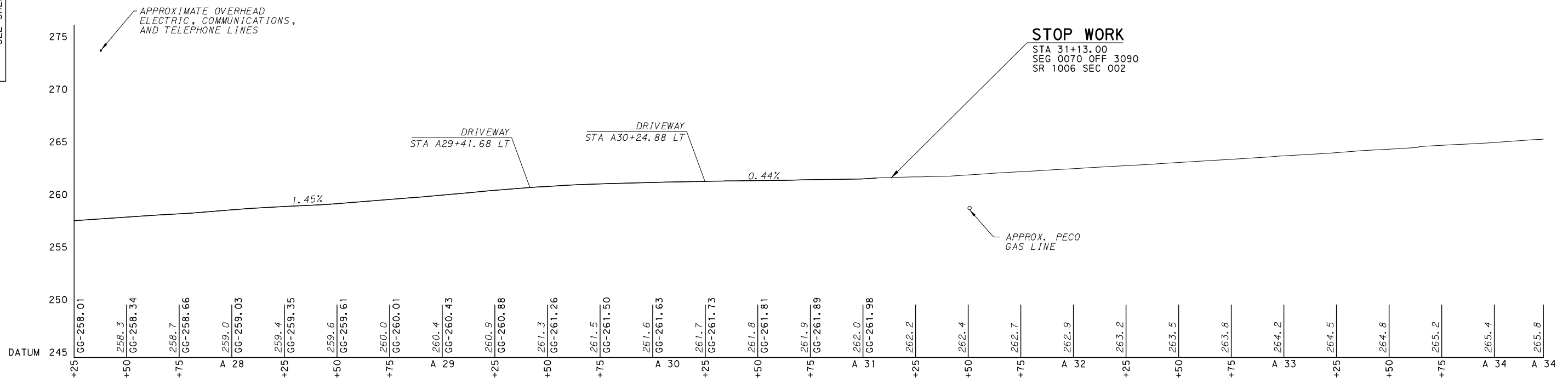
SEE SHEET 45

ct:\pwork\proj\elect\se\ol\lson\dd02791\p\prof11e-BRE-PerkAvenue-01.dgn  
 6/5/2024 12:18:40 PM 1:25



DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	45 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

SEE SHEET 44



PARK AVENUE (SR 1006)

SCALE

HORIZONTAL 0 25 50 FEET

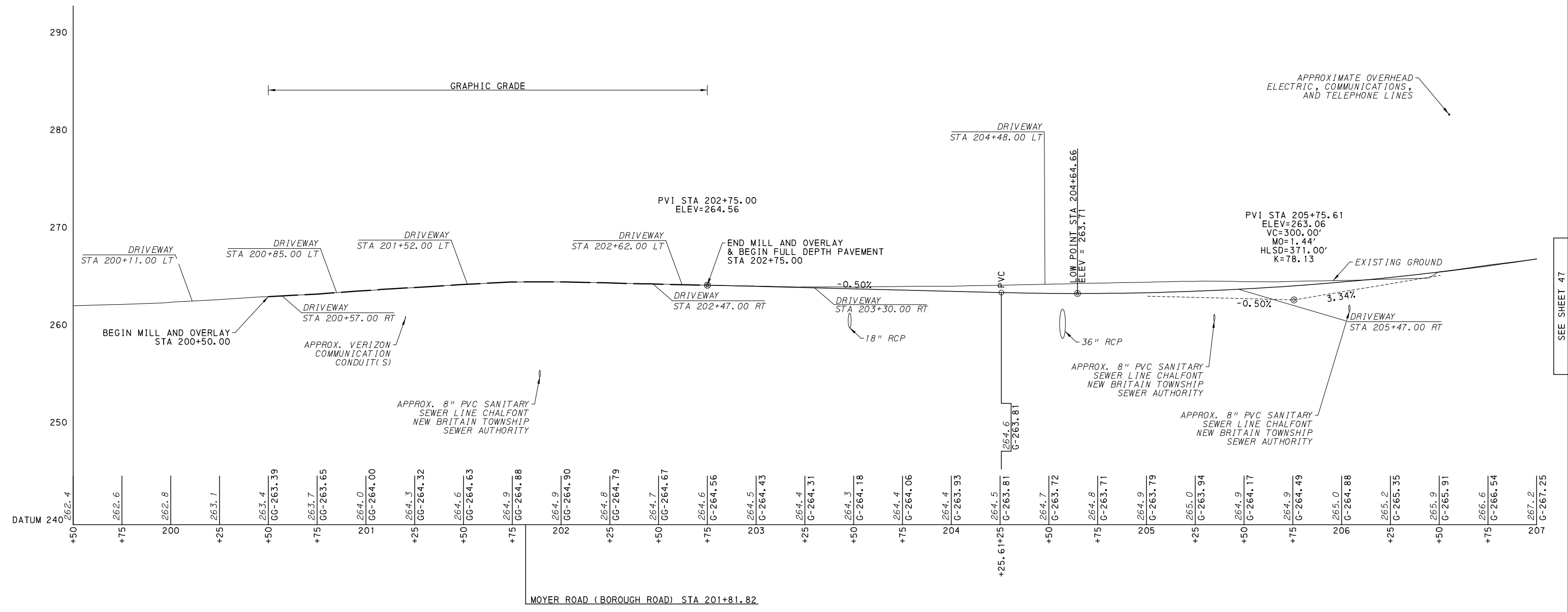
VERTICAL 0 5 10 FEET

FOR PLAN, SEE SHEET 29

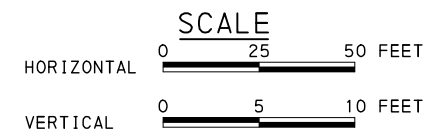
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	46 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY

**← LIMIT OF WORK**  
 STA 198+50.00  
 SEG 0050 OFF 1150  
 SR 4202 SEC 002  
 CHALFONT BOROUGH  
 BUCKS COUNTY

**← START WORK**  
 STA 199+00.00  
 SEG 0050 OFF 1200  
 SR 4202 SEC 002



STATION	ELEVATION	REMARKS
198+50	262.4	DATUM
198+75	262.6	
199+00	262.8	
199+25	263.1	
199+50	263.4	GG-263.39
199+75	263.7	GG-263.65
200+00	264.0	GG-264.00
200+25	264.3	GG-264.32
200+50	264.6	GG-264.63
200+75	264.9	GG-264.88
201+00	264.9	GG-264.90
201+25	264.8	GG-264.79
201+50	264.7	GG-264.67
201+75	264.6	G-264.56
202+00	264.5	G-264.43
202+25	264.4	G-264.31
202+50	264.3	G-264.18
202+75	264.4	G-264.06
203+00	264.4	G-263.93
203+25	264.5	G-263.81
203+50	264.7	G-263.72
203+75	264.8	G-263.71
204+00	264.9	G-263.79
204+25	265.0	G-263.94
204+50	264.9	G-264.17
204+75	264.9	G-264.49
205+00	265.0	G-264.88
205+25	265.2	G-265.35
205+50	265.9	G-265.91
205+75	266.6	G-266.54
206+00	267.2	G-267.25



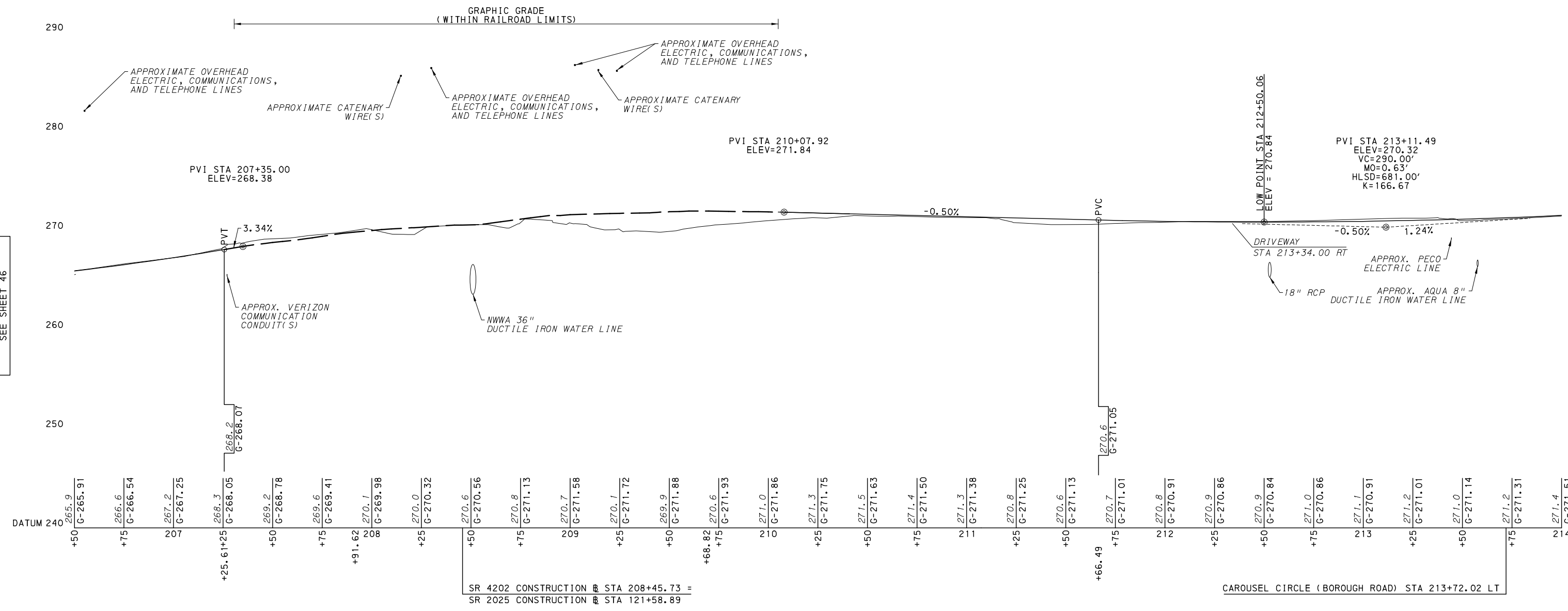
**BUTLER AVENUE (SR 4202)**

FOR PLAN, SEE SHEET 30

SEE SHEET 47

c:\pwork\proj\sect\se\j\ol\se\dd279191\p\prof\1le-BRE-ButlerAvenue-01.dgn  
 6/5/2024 12:19:01 PM jolson 1:25

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	47 OF 54	
		4202			
		1006 4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



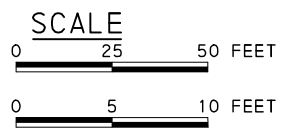
SEE SHEET 46

SEE SHEET 48

SR 4202 CONSTRUCTION @ STA 208+45.73 =  
SR 2025 CONSTRUCTION @ STA 121+58.89

CAROUSEL CIRCLE (BOROUGH ROAD) STA 213+72.02 LT

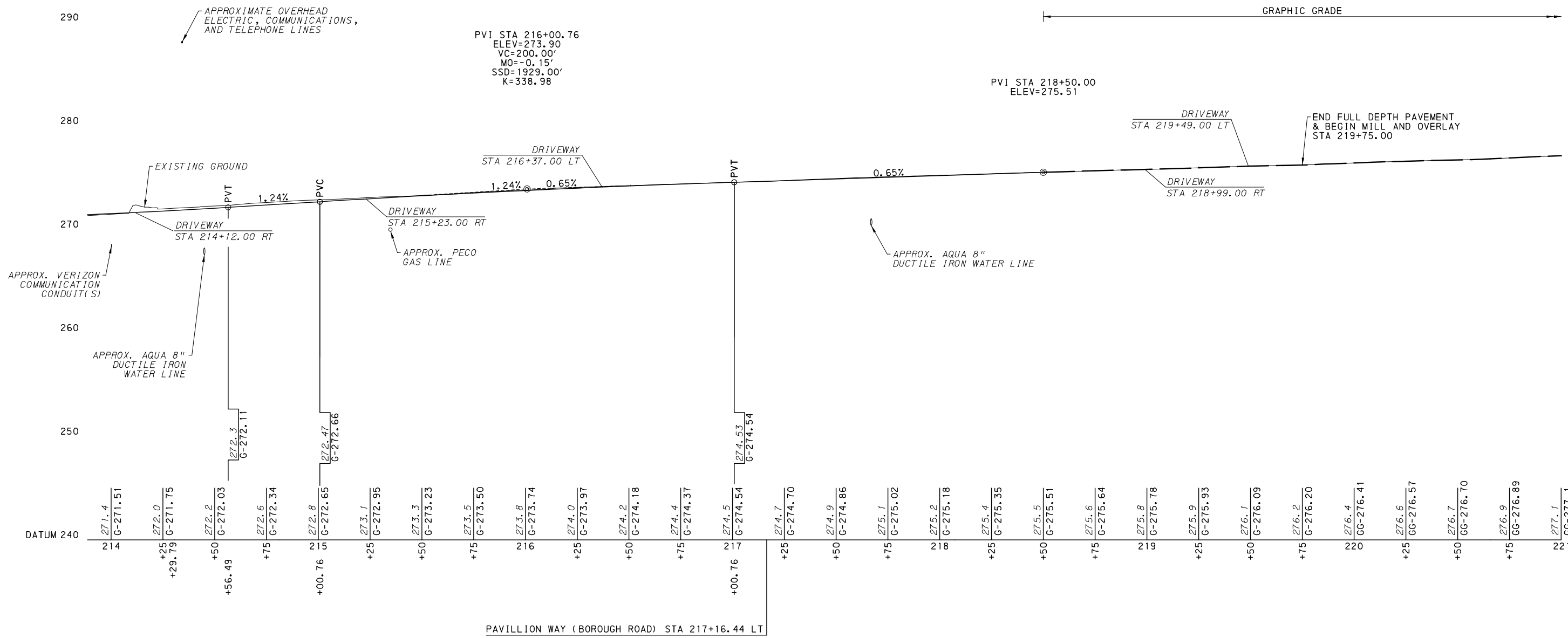
**BUTLER AVENUE (SR 4202)**



FOR PLAN, SEE SHEET 31

c:\pwork\k\proj\elect\se\j\olson\dd279191\p\prof11e-BRE-ButlerAvenue-02.dgn 6/5/2024 12:19:08 PM j\_olson

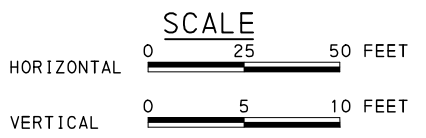
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	48 OF 54
		4202		
		1006		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SEE SHEET 47

SEE SHEET 49

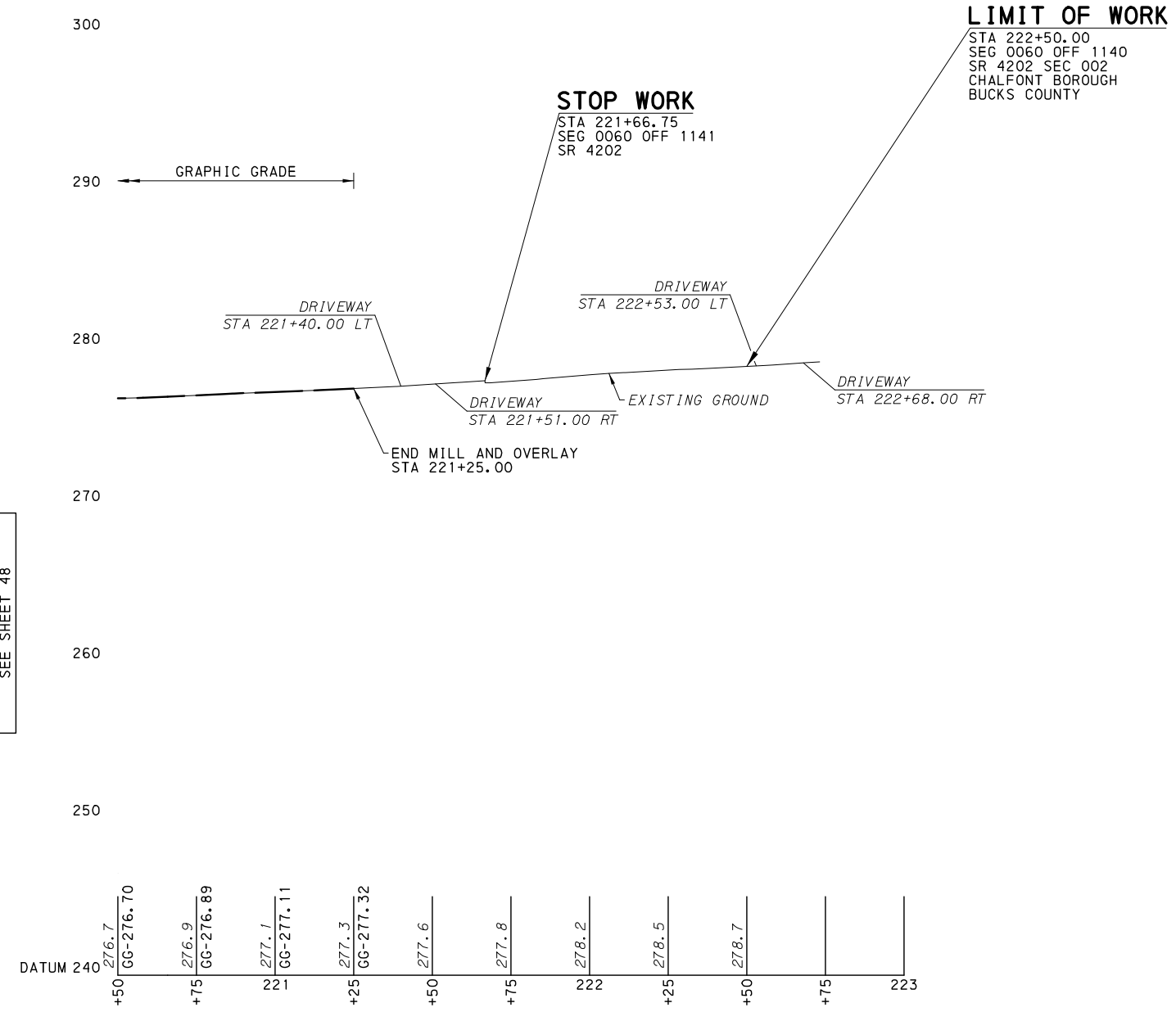
BUTLER AVENUE (SR 4202)



FOR PLAN, SEE SHEET 32

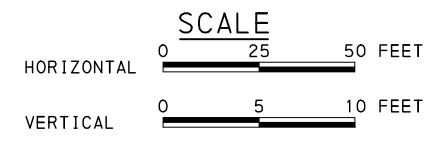
c:\pwork\proj\elect\isa\j\olson\022791\p\prof11e-BRE-ButlerAvenue-03.dgn  
 6/5/2024 12:19:22 PM jolson

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	2025	002	49 OF 54
		4202		
		1006		
		4003		
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO				
REVISION NUMBER	REVISIONS		DATE	BY



SEE SHEET 48

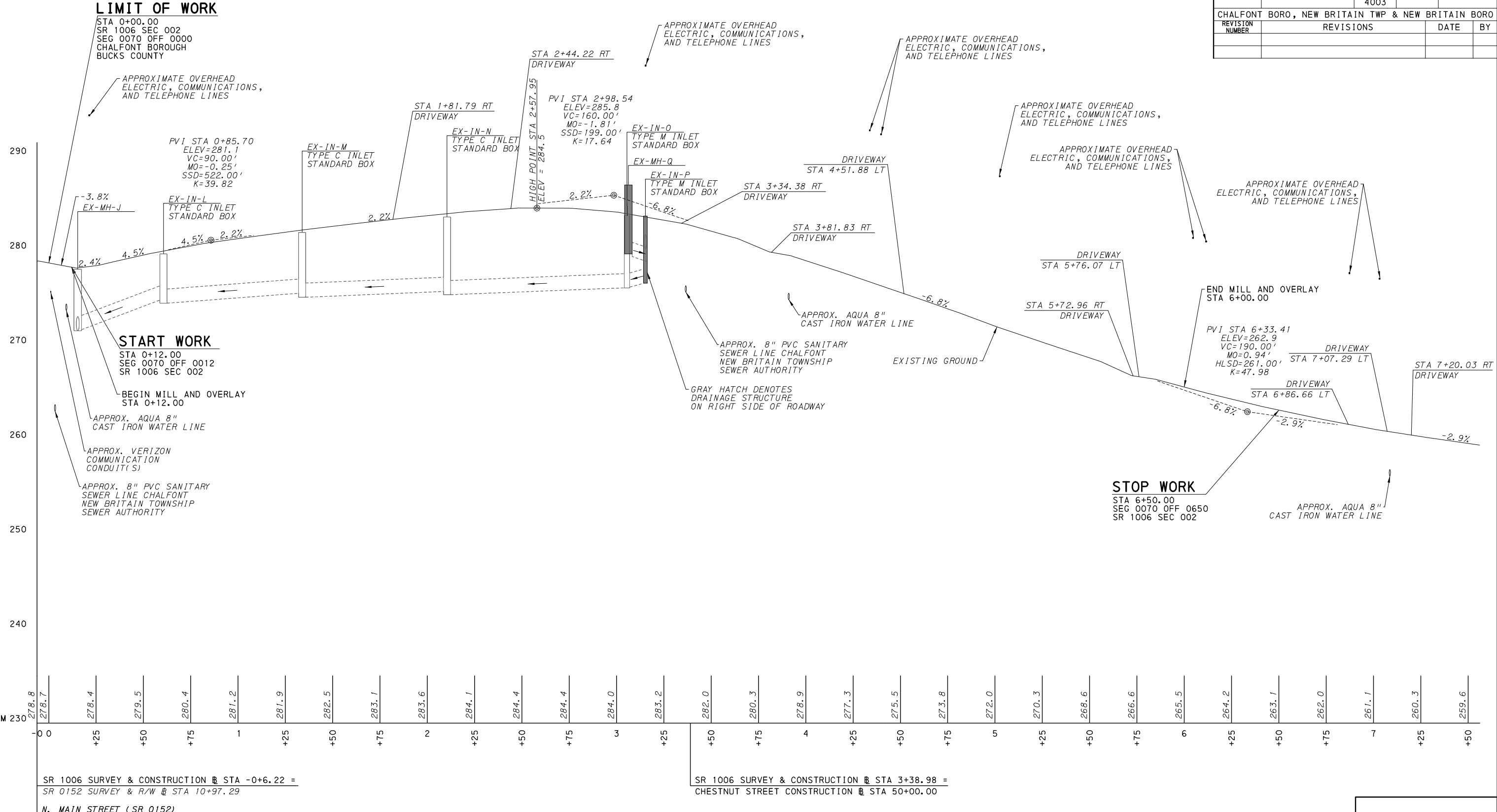
BUTLER AVENUE (SR 4202)



FOR PLAN, SEE SHEET 32

c:\pwork\proj\proj\ectw\se\olson\olson\dd279191\prof11e-BRE-ButlerAvenue-04.dgn 6/5/2024 12:19:36 PM jolson

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	50 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



**LIMIT OF WORK**

STA 0+00.00  
SR 1006 SEC 002  
SEG 0070 OFF 0000  
CHALFONT BORO  
BUCKS COUNTY

**START WORK**

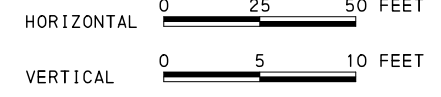
STA 0+12.00  
SEG 0070 OFF 0012  
SR 1006 SEC 002

**STOP WORK**

STA 6+50.00  
SEG 0070 OFF 0650  
SR 1006 SEC 002

**PARK AVENUE (SR 1006)**

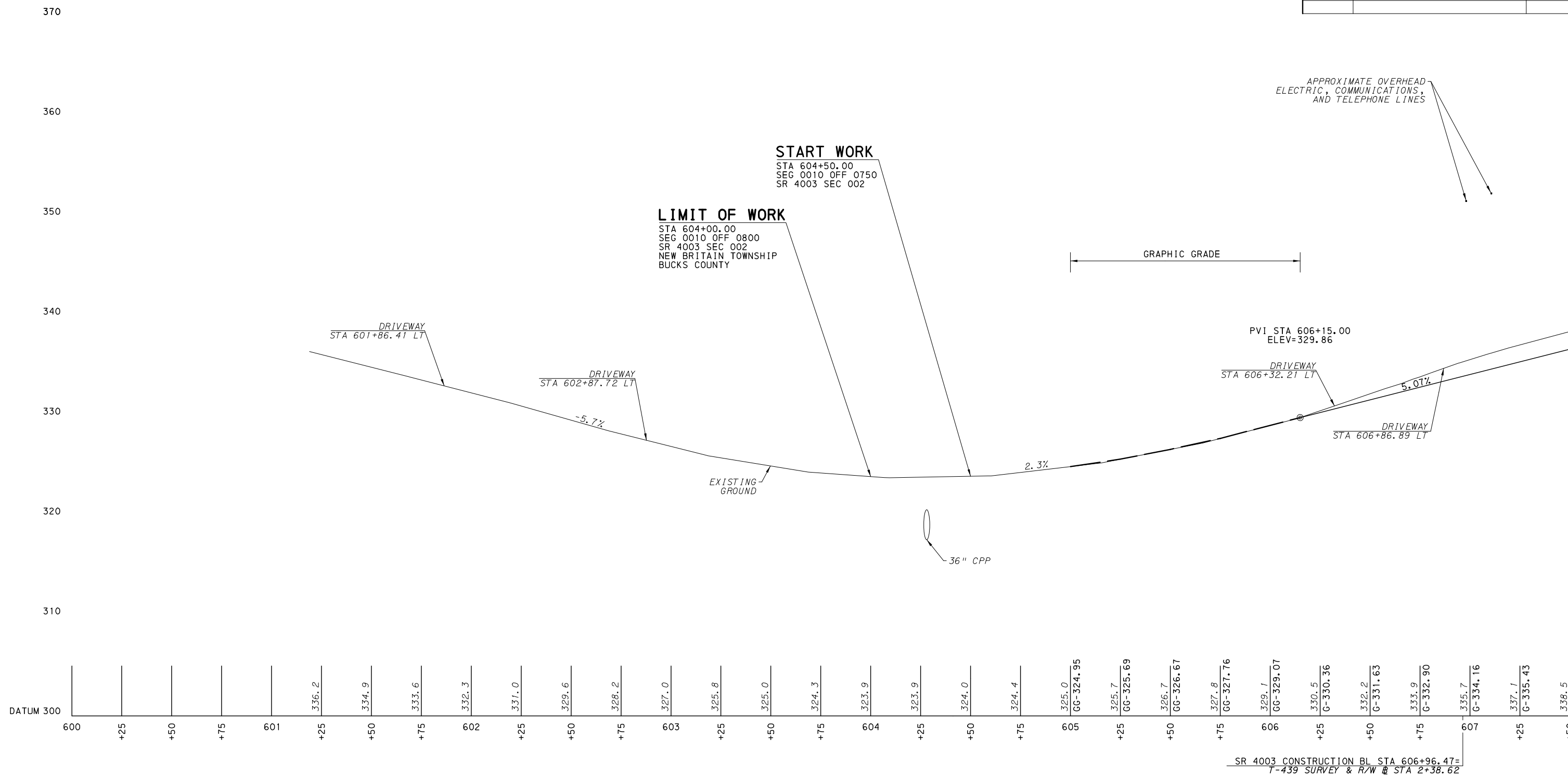
**SCALE**



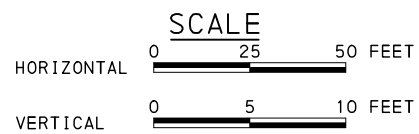
FOR PLAN, SEE SHEET 33

c:\pwworking\proj\ect\is\p\ol\son\0279191\prof\11e-FM-ParkAvenue-01.dgn  
 6/5/2024 12:19:40 PM jolson

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	51 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



**ALLOWHILL ROAD (SR 4003)**



FOR PLAN, SEE SHEET 36

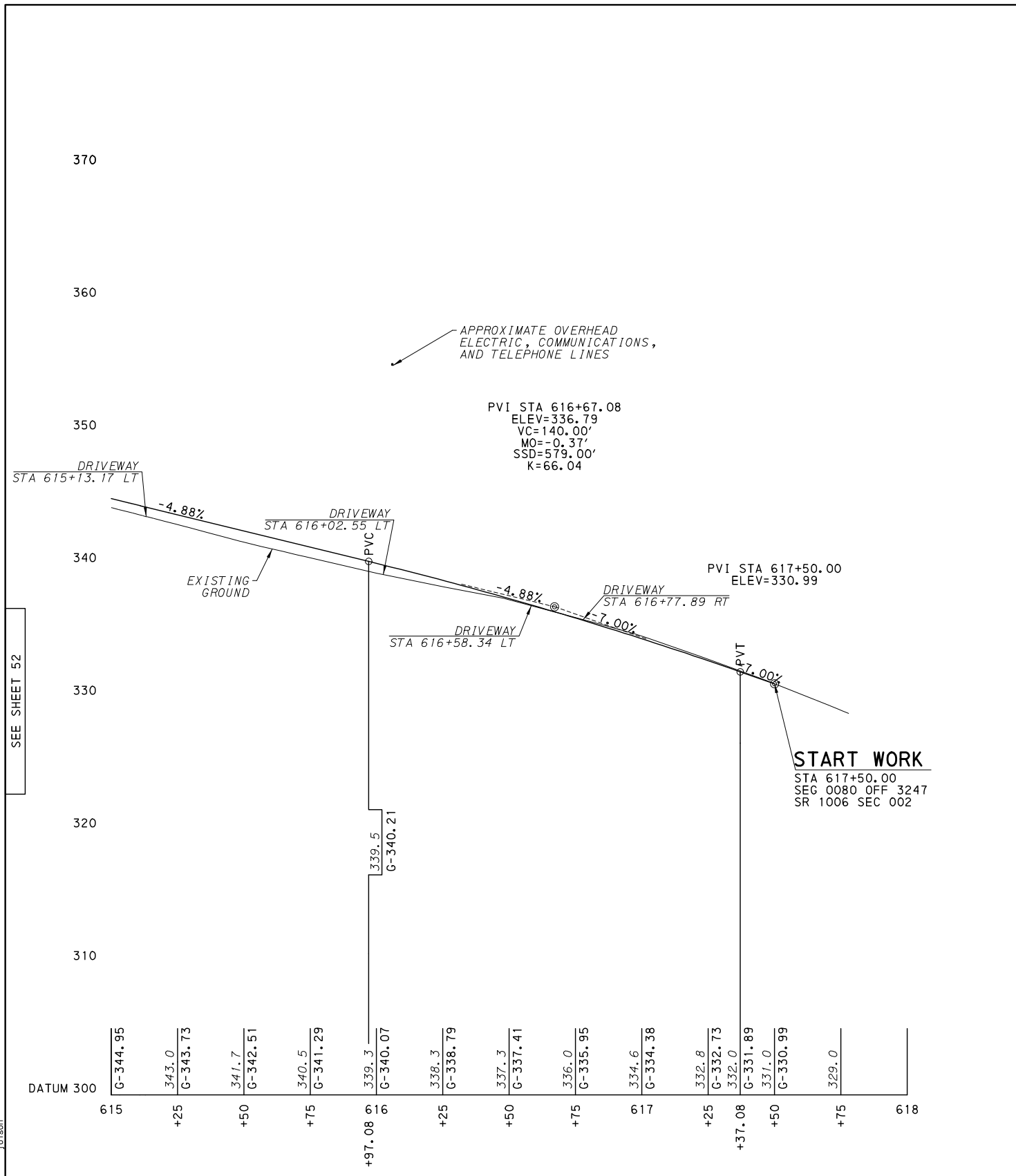
SEE SHEET 52

ct:\pwork\ing\proj\ect\w\se\l\ol\se\nd0279191\p\rof\l\le-PFC-01.dgn  
6/5/2024 12:19:47 PM 1:25

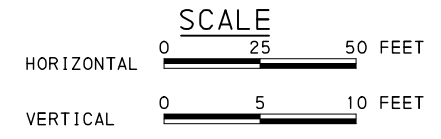




DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	53 OF 54	
		4202			
		1006			
		4003			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY



SEE SHEET 52



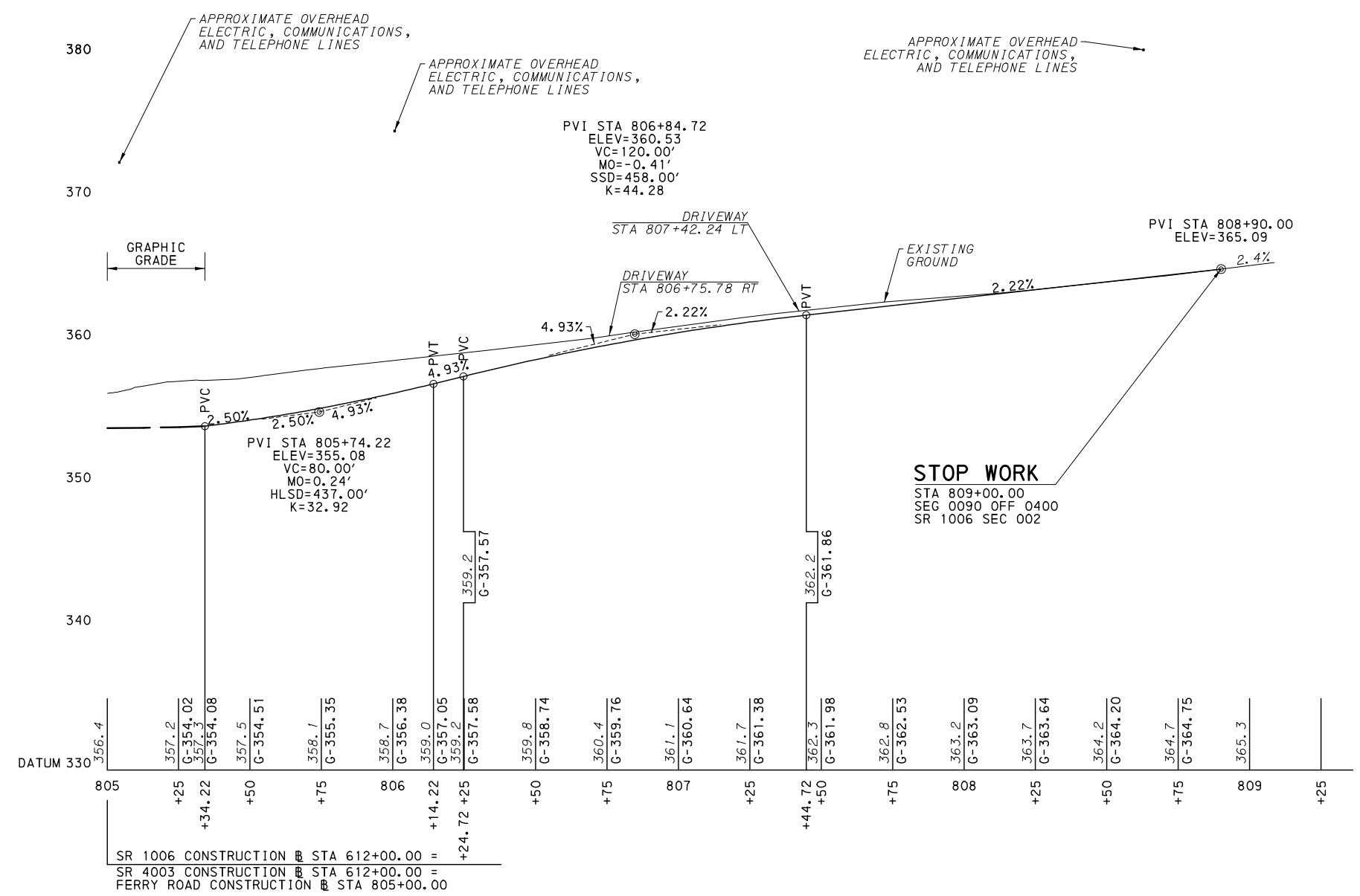
PARK AVENUE (SR 1006)

FOR PLAN, SEE SHEET 37

ct:\pwork\ing\proj\ect\w\se\l\ol\se\dd0279191\p\rof\1\le-PFC-03.dgn  
6/5/2024 12:20:00 PM 1:25

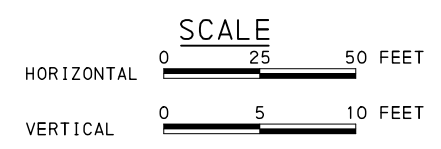
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
6-0	BUCKS	2025	002	54 OF 54	
		4202			
		1006			
CHALFONT BORO, NEW BRITAIN TWP & NEW BRITAIN BORO					
REVISION NUMBER	REVISIONS			DATE	BY

**LIMIT OF WORK**  
 STA 810+00.00  
 SEG 0090 OFF 0500  
 SR 1006 SEC 002  
 NEW BRITAIN TOWNSHIP  
 BUCKS COUNTY



**STOP WORK**  
 STA 809+00.00  
 SEG 0090 OFF 0400  
 SR 1006 SEC 002

SR 1006 CONSTRUCTION # STA 612+00.00 =  
 SR 4003 CONSTRUCTION # STA 612+00.00 =  
 FERRY ROAD CONSTRUCTION # STA 805+00.00



FERRY ROAD (SR 1006)

FOR PLAN, SEE SHEET 38

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 6/5/2024 12:20:07 PM 1:25